



DATE: September 7, 2021

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RE: INDOT Lead Des. No. 1702255, Additional Des. Nos. 1702260, 1702254, 1592187, 1702257, 170225 8, 1702259, 1701215, & 1900579 Kentucky Transportation Cabinet (KYTC) Item ID 5-64

Additional Information to the Final Categorical Exclusion (CE) Level 4 Document, approved October 7, 2020, for the proposed Bridge, Associated Approaches, and Road Improvements located on West 5th Street, West Spring Street, and West Elm Street near the I-64 ramps, I-64 and US 50 Sherman Minton Bridge crossing the Ohio River in New Albany, Floyd County, Indiana and in Louisville, Jefferson County, Kentucky, extending from the I-62 / I-264 interchange in Louisville Kentucky, 3.5 miles to the northwest, to the I-64 / I-265 interchange in New Albany, Indiana.

ADDITIONAL INFORMATION

This memorandum provides additional information to the approved CE document, for the bridge and roadway improvements on Sherman Minton Bridge and side streets in Floyd County, Indiana and Jefferson County, Kentucky. Unless specifically discussed in this document, the impacts as identified in the approved CE document, approved October 7, 2020, remain unchanged. The approved CE document, without attachments, is located in Appendix G, pages G-1 to G-54.

Purpose and Need

The need for the project is due to the deteriorating structural condition of the existing Sherman Minton Bridge over the Ohio River, the deteriorating associated Indiana and Kentucky approaches, and deteriorating pavement of select associated side streets.

The purpose of the project is to address the deterioration of structural elements of the Sherman Minton Bridge, the associated Indiana and Kentucky approaches, and select associated side streets with the goal of extending the service life of the I-64 Interstate crossing over the Ohio River up to 30 years.

Please refer to Appendix G, pages G-5 to G-7 for the Purpose and Need, in its entirety.

Project and Environmental Document History

The approved CE document identifies this project as the Sherman Minton Renewal Project (SMRP); however, prior to request for proposals and selection of the Design-Build Team, the official name of the project was changed to Sherman Minton Corridor Project (SMCP). The project name was changed due to adding asphalt overlay and preventative maintenance work on West 5th Street, West Spring Street, and West Elm Street near the I-64 ramps in New Albany, Indiana, which are included in the approved CE document. SMCP shall be the reference to the project throughout this document.

The SMCP is the rehabilitation of the Sherman Minton Bridge, related approaches, and selected side streets in Indiana and Kentucky. The goal of SMCP is to address the deteriorating structural condition of the existing bridges to extend the service life of the bridges up to 30 years. Project elements include bridge deck replacements, bridge deck overlays, structural repairs, replacement lighting, bridge painting, local streets Hot Mix Asphalt (HMA) overlay and Americans with Disabilities Act (ADA) ramp reconstruction, and interstate ramp frictionalization. SMCP is a joint effort between INDOT and KYTC. INDOT is leading SMCP in close collaboration with key staff from KYTC. The entire description of the preferred alternative is located in Appendix G, pages G-9 to G-11.

Since the CE was approved on October 7, 2020, additional work has been added to this project as discussed below:

Additional work in Kentucky

A temporary access road will be installed on the levee, southwest of the bridge in Louisville, Kentucky, in order to provide access beneath the Kentucky approach bridges to perform structural repairs. A Location Map, Topographic Map, 2019 Aerial Photo Location Map, and Site Photographs are located in Appendix A, pages A-1 to A-12. The access road shall extend from levee Stations 329+00 to 342+00 and adjacent to I-64 Station 65+00 (Appendix A, page A-20).

The temporary access road will be approximately 12 feet (ft.) wide, 1 ft. thick, and 1,440 ft. long and comprised of temporary aggregate over geotextile fabric. The levee access road will then traverse down the slope into the project right-of-way (ROW). Project access areas beneath the bridge extend 40 ft. beyond the drip line along both sides of the Kentucky approach bridges up to Station 315+00 where the work area is restricted to 25 ft. beyond the drip line along both sides of the structure (Appendix A, page A-14). This restriction extends all the way to the culmination of the work zone adjacent to the Ohio River.

Construction of the access road and aggregate pads under the approach spans will require approximately 20 loads of stone per day for four days, one Cat D6 bulldozer to strip topsoil to a depth of six to eight inches and spread stone for four days, one 72-inch smooth drum roller to compact stone for four days, and an occasional pickup truck in and out of the site during the levee road access construction process. The stripped topsoil will be stored and stabilized for the duration of the project.

After the access road is constructed, normal day-to-day operations in the project area will include water trucks making passes as needed for dust control, occasional pickup trucks entering and exiting the site, one to two concrete trucks with light loads (two to three cubic yards) utilizing the access road for pier cap patching, and an occasional low-boy trailer will be needed to bring equipment in and out (anticipated to be an infrequent occurrence after the initial mobilization).

During hydro-demolition activities one additional water truck will utilize the access road emptying frac tanks from hydro-demo operation site to be hauled offsite, and occasional pickup trucks will enter and leave the site.

During removal of the access road, aggregate pads, and subsequent stabilization one track-hoe for loading dump trucks will be required for four days, approximately 20 dump truck loads will occur per day for four days, one Cat D6 bulldozer will be used for two days redressing the levee, one hydro seeder will be needed for one day to stabilize and plant grass seed on the levee once the work is complete, and occasional pickup trucks will be entering and leaving the site.

The full area of the levee surveyed consists of approximately 6.77 acres and includes the levee within KYTC ROW and beyond. The area of disturbance associated with the temporary access road is approximately 2.11 acres. Upon completion of construction activities, the aggregate and geotextile fabric will be removed, and the topsoil that had been placed in storage will be replaced. The topsoil will be graded and seeded to restore it to preconstruction conditions. This will include application of seed and mulch to facilitate final stabilization of the replaced topsoil. No permanent alterations or impacts to the levee are anticipated.

Two separate staging and lay down areas are proposed to be utilized within the I-64 and I-264 interchange infield areas (Appendix A, page A-3 and A-14).

There will be no work below the ordinary high water mark (OHWM) of the Ohio River; however, there will be temporary impacts to an emergent wetland (0.16 acre) and a forested wetland (0.32 acre) beneath the Kentucky approach bridges for access beneath the structures to allow staging, bridge deck replacement, structural steel painting, and concrete pier rehabilitation. Please refer to Waters Determination / Wetlands Delineation under the Supplemental Resource Review section below for additional information.

The Kentucky side of the project is bound on both sides of the ROW by the Shawnee Park Golf Course (SPGC). The SPGC is a U.S. Department of Transportation Section 4(f) resource as a recreational facility. The SPGC is also a Land and Water Conservation Fund (LWCF) Section 6(f) resource due to use of a LWCF grant for improvements to the SPGC maintenance facility and the park pavilion. Additionally, the Louisville Riverwalk, also known as the Louisville Loop, a Section 4(f) resource, is located along the edge of the SPGC proper between the SPGC and the Ohio River. Because the Louisville Loop is on property considered part of the SPGC, it is also considered a Section 6(f) resource.

The I-64 roadway is elevated above the Section 4(f) and Section 6(f) resources at this location, with several bridge approach spans on piers crossing the SPGC. Two golf cart paths within SPGC converge within the ROW and pass under the I-64 approach spans between holes 1 and 2, and between holes 7 and 8. The Louisville Loop also passes under an approach span within the highway ROW. The Louisville Loop is not being maintained at this location and has been closed to public use since approximately 2009/2010, with barricades across the trail. The area of the trail within the ROW is approximately 640 square ft. (Appendix A, page A-16).

During active project construction, from July 2021 to June 2023, the cart paths will merge before entering the work zone, and a single path will pass under the approach spans under a protective canopy. Bridge deck repairs and painting will occur above the cart path, hence the canopy to shield the golfers. Impact will occur to approximately 5,500 square ft. (0.13 acre) of cart path. There will be repair and rehabilitation of the piers and associated structures during the construction period, within the highway ROW. There will be no change in the recreational use of the SPGC, and access will be maintained during all times the SPGC is open to the public. The cart paths will be returned to their pre-construction condition once construction is completed. There will be no permanent or temporary ROW needed from the facility (Appendix A, page A-16. Please refer to Section 4(f) and Section 6(f) under the Supplemental Resource Review section below for additional information.

All work will be completed in previously disturbed soils in existing ROW. Although suitable habitat is present, no trees are currently planned to be trimmed or cleared during construction activities. In the event it becomes necessary to trim or clear trees during construction, coordination shall occur with all appropriate parties prior to initiation of work.

Additional work in Indiana

Temporary access beneath the Indiana approach bridges will be required for staging and access for structural repairs. Access beneath the Indiana approach bridges will be attained via local streets. Temporary aggregate fill, approximately 12 inches in depth, over geotextile fabric, will be installed beneath the approach bridges, within the ROW, adjacent to the Ohio River. The temporary impacted area will be restored using Seed Mix, Type Floodplain, per INDOT Standard Specifications. No permanent impacts shall occur in association with this work (Appendix A, page A-18).

The temporary access beneath the Indiana approach bridges will temporarily result in minor impacts to two local trails, the Levee Trail and the Ohio River Greenway Trail, and require Section 4(f) coordination and approval. Both the Levee Trail near Jaycee Park and the Ohio River Greenway Trail terminate within the ROW beneath the Indiana approaches and will be closed for nearly the full duration of construction, approximately July 2021 to June 2023 (Appendix A, page A-18). Please refer to Section 4(f) and Section 6 (f) under the Supplemental Resource Review section below for additional information.

An Intelligent Transportation Systems (ITS) is an advanced application which aims to provide innovative services relating to different modes of transport and traffic management. The ITS will provide real-time sensor data that feeds INDOT's traffic wise mobile and web map. Data is collected from roadway cameras, digital message sign text for each sign in the State, portable digital message signs current major roadway incidents recorded in Hoosier Helper dispatch system, color road segments of highway speeds, and travel time sign values.

The ITS will be installed on the southwest side of I-64 between the railroad tracks and W. Main Street. The type and actual placement of the ITS is not known at this time (Appendix A, page A-18). The Ohio Valley Truck and Tractor (Agency ID No. 107467) is located at 303 to 305 West Main Street, approximately 90 ft. west of this ITS location. Based on the INDOT Red Flag Investigation (RFI), approved by INDOT SAM on April 3, 2019, in February 2014, the Ohio Valley Truck and Tractor facility was issued a Brownfields Petroleum Eligibility Letter, stating the landowners and the New Albany Redevelopment Commission may perform site assessment activities at the property. No further records are available on the VFC. No impact is expected. The RFI can be viewed in the approved October 7th CE appendices (E-1 to E-15) which can be found in the originally approved document currently located on the <https://shermanmintonrenewal.com> website or can be requested by contacting INDOT environmental services.

The ITS will also be installed at the northeast and southwest sides of I-64 between W. Main Street and W. Market Street, between the I-64 westbound on and off ramps, the northeast and southwest sides of I-64 between W. Market Street and W. Spring Street, and between W. Spring Street and the I-64 eastbound on ramp and westbound off ramp. The type and actual placement of the ITS is not known at this time (Appendix A, pages A-18 to A-19). Based on the INDOT RFI approved by INDOT SAM on April 3, 2019, and the INDOT Limited RFI, approved by INDOT SAM on October 11, 2019, there are no hazardous materials within or near the ITS project areas. Therefore, no impact is expected. The RFI and Limited RFI can be viewed in the approved October 7th CE appendices (E-1 to E-22) which can be found in the originally approved document currently located on the <https://shermanmintonrenewal.com> website or can be requested by contacting INDOT environmental services.

Additionally, based on the Waters of the U.S. Determination Report, approved by INDOT Ecology Waterway and Permitting Office (EWPO) on September 20, 2019, there are no water resources within any of the ITS installation locations. Therefore, no impacts are expected. The Waters of the U.S. Determination Report can be viewed in the approved October 7th CE appendices (F-1 to F-54) which can be found in the originally approved document currently located on the <https://shermanmintonrenewal.com> website or can be requested by contacting INDOT environmental services.

There will be a staging area within the Spring Street-I-64 Eastbound (EB) on-ramp (Appendix A, page A-19).

All work will be completed in previously disturbed soils in existing ROW. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10 inches dbh or greater (5:1 mitigation based on the number of large trees).

Justification for Additional Information

Due to the additional work described above, additional services to include Maintenance of Traffic (MOT), Waters Determination / Wetland Delineation, Threatened or Endangered Species, Section 106 Cultural Resources, Section 4(f) / Section 6(f) resources, and Permits were required. No other additions or changes to the project are anticipated.

SUPPLEMENTAL RESOURCE REVIEW:

Public Involvement - Since approval of the original CE on October 7, 2020,

A combined effort of DLZ and C2 Strategic Communications, the following public involvement activities have been conducted, on behalf of the Indiana Finance Authority (IFA).

- On March 16, 2021, the start of bridge inspection was advertised.
- On April 19, 2021, the start of ramp improvements and description of work was advertised.
- On April 22, 2021, a major update was made to the project website.
- On June 9, 2021, a presentation of the project was given in southern Indiana by INDOT.
- On July 12, 2021, an elected officials briefing for Indiana and Kentucky were held virtually.
- On July 21, 2021, an update for start of painting and brief summary of work was advertised.
- On July 30, 2021, upcoming weekend closures was advertised.
- August 13, 2021, the beginning of Phase 1 construction was advertised.

All of the above public involvement advertisements were distributed to multiple print and broadcast media including the following in Louisville, Kentucky: Courier-Journal, Courier-Journal Online Louisville, LouisvilleKY.com, Business First, WFPL-FM, Louisville Defender, WHAS-TV, WHAS-TV Online, WHAS-AM, Total Traffic, Transport Topics, WDRB-TV, The Lane Report, WLKY-TV, Louisville Public Radio, WGTK/WRVI Radio, Kentucky Today, Spectrum News, Spectrum News 1, al dia en America, WAVE-TV, Louisville Business First, LEO Weekly, Oldham Era, as well as The Indianapolis Star, Extol Magazine in Southern Indiana, WRTV Indianapolis Indiana, News and Tribune Associated Press in New Albany and Louisville, and Gov Delivery (C2 Strategic Communications e-newsletter and text updating system).

Additionally, a weekly notice is sent to the SPGC representative of planned construction activities and media updates are provided that will or do identify anticipated construction traffic in neighborhoods on either side of the river. Additional public outreach may be conducted in the future with the neighborhood adjacent to the SPGC.

Maintenance of Traffic (MOT) During Construction

As discussed in the approved CE document, October 7, 2020, six MOT options were developed and evaluated during design engineering, traffic modeling, Community Impact Assessment (CIA) and Environmental Justice (EJ) Analysis. As design progressed, the contractor has proposed to utilize MOT option 1 throughout construction. MOT option 1 consists of the following:

The MOT for the project will require one EB and one westbound (WB) lane will be closed throughout construction. Open travel lanes will shift location on the Sherman Minton Bridge during construction. Two EB and two WB travel lanes will remain open for cross-river traffic and existing access ramps will remain open except for the following allowances:

180 nights per construction year during which two EB and two WB lanes and associated access ramps will be closed each night approximately from 9 pm to 4 am and 10 pm to 5 am, respectively. Cross-river traffic will be maintained with one EB and one WB travel lane open and a temporary crossover lane in Kentucky for I-64 WB to merge with I-264. See Permitted Mainline Interstate Off-Peak Movement Closures below for off peak closures and times.

Table 12-1: Permitted Mainline Interstate Off-Peak Movement Closures

| Roadway | From | To | Minimum Number of Lanes Maintained on the Bridges | Number of Off-Peak Periods | Restrictions |
|---------|-------|-------|---|--|---|
| I-64 EB | I-265 | I-264 | 1 lane | [360] Prior to Substantial Completion | Between 9:00 p.m. and 5:00 a.m. each night - Sunday night through Thursday night |
| | | | | [60] After Substantial Completion for approved Punch List work only. | Between 10:00 p.m. and 6:00 a.m. Friday night Between 10:00 p.m. and 9:00 a.m. Saturday night |
| I-64 WB | I-264 | I-265 | 1 lane | [360] Prior to Substantial Completion | Between 10:00 p.m. and 6:00 a.m. each night - Sunday night through Thursday night |
| | | | | [60] after Substantial Completion for approved Punch List work only. | Between 10:00 p.m. and 9:00 a.m. Friday night Between 10:00 p.m. and 10:00 a.m. Saturday night |

Short-term closure of the Sherman Minton Bridge will be allowed for one (1) nine (9) consecutive day period and up to three (3) weekend closures during each construction year; excluding holidays and community events detailed below. During the short-term bridge closure, all I-64 (US 150) cross-river traffic will be diverted to detour routes.

| Table 10-1-1 | |
|---|---|
| Allowable Mainline Interstate Movement Closures | |
| Segment | Maximum Movement Closures |
| I-64 EB (from approximate MM 121.4 to approximate 123.8 (Kentucky State Line) and from MM 0.0 (Kentucky State Line) to approximate MM 1.4) | <p>One (1) nine (9) consecutive day closure per calendar year period;</p> <p>Up to three (3) weekend periods per calendar year (Work may commence no earlier than Friday at 10 p.m. and conclude no later than Monday at 5 a.m.); and</p> <p>Subject to the approved IHCP Exception Request obtained in accordance with <u>Section 12.3.11.1.4</u> of the Technical Provisions, one (1) lane for one (1) fifteen (15) consecutive day closure for As-Built Bridge Reference Document Verification inspection.</p> |
| I-64 WB (from approximate MM 121.4 to approximate 123.8 (Kentucky State Line) and from MM 0.0 (Kentucky State Line) to approximate MM 1.4) | <p>One (1) nine (9) consecutive day closure per calendar year period;</p> <p>Up to three (3) weekend periods per calendar year (Work may commence no earlier than Friday at 10 p.m. and conclude no later than Monday at 6 a.m.); and</p> <p>Subject to the approved IHCP Exception Request obtained in accordance with <u>Section 12.3.11.1.4</u> of the Technical Provisions, one (1) lane for one (1) fifteen (15) consecutive day closure for As-Built Bridge Reference Document Verification inspection.</p> |

I-64 EB & WB MOT – Shoulder rehabilitation for travel lane reconfiguration and Indiana Crossover; no pavement footprint widening.

I-64 / I-265 Interchange MOT – Mill and fill shoulder rehabilitation to existing pavement footprint on I-265 WB to I-64 WB and I-64 EB to I-265 EB; existing one-lane ramps will be restriped and converted to 2-lane ramps. The ramps will be returned to their current configurations when construction is complete.

I-65 / I-265 Interchange MOT – Mill and fill shoulder rehabilitation to existing pavement footprint on EB to SB ramp; existing one-lane ramp will be restriped and converted to a 2-lane ramp. The ramps will be returned to their current configurations when construction is complete.

Provisions are included for local traffic access and through-traffic dependent businesses by retaining existing access ramps in Indiana and Kentucky; through-traffic dependent businesses by maintaining cross-river travel lanes in both directions; public notification, signage according to MOT, and posting requirements during construction; and detour routes that remain within the interstate system to alternate local river crossings. The MOT plans are located in Appendix A, pages A-30 to A-53.

Indiana (Alternate Routes)

- I-64 through traffic may use I-265 and the I-65 paired Kennedy/Lincoln bridges.
- Local access to New Albany could follow the same alternate route or remain on the local roadway network; State Street will be the alternate route during Spring Street access ramp closures.

Kentucky (Alternate Route)

- Both I-264 and I-64 through traffic may use I-65 paired Kennedy/Lincoln bridges.
- Local access to west Louisville could follow the same alternate route or remain on the local arterial network.

Minimization measures included additional accommodation for local special events and festivals with the exclusion of bridge closure work during the following:

New Year's Day - If New Year's Day falls on a Sunday, work shall be suspended from noon December 31 until sunrise January 3. or if New Year's Day falls on a Monday through Saturday, work shall be suspended from noon December 31 until sunrise January 2.

Good Friday - Work shall be suspended from noon on Good Friday until sunrise Monday.

Memorial Day - Work shall be suspended from noon the Friday before Memorial Day until sunrise Tuesday, the day after Memorial Day.

Independence Day - If Independence Day falls on a:

Sunday - work shall be suspended from noon Friday, July 2, until sunrise Tuesday, July 6. Monday - work shall be suspended from noon Friday, July 1, until sunrise Tuesday, July 5. Tuesday - work shall be suspended from noon Friday, June 30, until sunrise Wednesday, July 5.

Wednesday - work shall be suspended from sunset on Tuesday, July 3, until sunrise Thursday, July 5. Thursday - work shall be suspended from noon Wednesday, July 3, until sunrise Monday, July 8.

Friday - work shall be suspended from noon Thursday, July 3, until sunrise Monday, July 7. Saturday - work shall be suspended from noon Thursday, July 2, until sunrise Monday, July 6.

Labor Day - Work shall be suspended from noon the Friday before Labor Day until sunrise Tuesday, the day after Labor Day.

Thanksgiving Day - Work shall be suspended from noon the Wednesday before Thanksgiving Day until sunrise the Monday after Thanksgiving Day.

Christmas Day - Work shall be suspended from noon December 24 until sunrise December 27.

Thunder Over Louisville - Work suspended from Midnight Friday till 6:00 a.m. Sunday.

Kentucky Derby -Work suspended from Thursday at midnight until Monday at 6:00 am.

Harvest Homecoming Festival - First Saturday in October to second Saturday in October.

Construction Traffic

The area for construction access traffic was identified in the approved CE-4 on appendix page I-21 as an area where minority and low income (EJ) populations reside.

To access the project beneath the Kentucky approach structures, construction traffic will be routed off of I-264 via the Bank Street Exit. Construction traffic will then utilize N. 38th Street to Northwestern Parkway to the Levee access point, adjacent to the SPGC. Upon leaving the construction project traffic will be routed via Bank Street to I-264.

The temporary impacts to the EJ populations include additional traffic (light to heavy construction vehicles) along N. 38th Street and Northwestern Parkway, which could cause increased vehicular air pollution and noise. The impacts will be temporary in nature, as described in the four phases below:

1. Construction of the access road and aggregate pads under the approach spans
 - Approximately 20 loads of stone / day for 4 days
 - 1 - Cat D6 Dozer stripping topsoil / spreading stone for 4 days
 - 1 - 72" smooth drum roller compacting stone for 4 days
 - Occasional pickup trucks in and out of the site
2. Normal day to day operations throughout the project
 - Water Truck making passes as needed for dust control
 - Occasional Pickup trucks in and out of the site
 - 1 to 2 concrete trucks with light loads (2 CY to 3 CY) for pier cap patching
 - Occasional low boy trailer bringing equipment as needed in an out for KCC and North Star (this will be infrequent after the initial mobilization)
3. Hydro demolition activities
 - 1 Additional water truck emptying frac tanks from hydro-demo operation and hauling offsite
 - Occasional pickup trucks in and out of the site
4. Removal of the access road and aggregate pads and subsequent stabilization
 - 1 - Track-hoe loading dump trucks for 4 days
 - Approximately 20 dump truck loads / day for 4 days
 - 1 - D6 Dozer for 2 days redressing the levee
 - Occasional pickup trucks in and out of the site
 - 1 - Hydro seeder for 1 day to stabilize the levee once we are complete

Most of the construction period, a small number of vehicles will be traveling along N. 38th Street, Northwestern Parkway, and Bank Street each day. Therefore, this would not cause disproportionately high or adverse effects to EJ populations.

Waters Determination / Wetlands Delineation

As discussed in the approved CE document, October 7, 2020, A *Waters of the U.S. Determination / Wetland Delineation Report* was completed by Kaskaskia Engineering for the project area, including both Indiana and Kentucky. The INDOT Ecology and Waterway Permitting Office approved the report on September 20, 2019.

The *Waters of the U.S. Determination / Wetland Delineation Report* identified two wetlands (wetland 10 and wetland 11) beneath the Kentucky approach bridges. At the time the CE document was prepared and approved, no impacts to any wetlands were anticipated. As design progressed, it was determined that access beneath the Kentucky approach bridges will be necessary and will result in temporary placement of fill within wetland 10 and wetland 11 in order to provide stable access for construction equipment. Overall, there will be a total of 975 cubic yards over 0.48 acre of temporary impact to the two wetlands. Upon completion of construction activities all temporary fill materials shall be removed. However, due to the wetlands existing within maintained highway right of way (ROW), USACE requested mitigation associated with these impacts be provided since the restored wetlands cannot be guaranteed to be maintained in perpetuity. The wetlands will be restored using early successional seed mix and will be mitigated with the purchase of wetland bank credits at a 2:1 ratio. (Appendix A, pages A-15 to A-16).

Wetland 10: As depicted in Appendix A, page A-16, wetland 10 is an approximately 0.56 acre palustrine forested wetland located adjacent to the Kentucky east bank of the Ohio River, within the Eastern Mountains and Piedmont Region. A portion of Wetland 10 is classified as a National Wetland Inventory (NWI) wetland, Palustrine Forested Broad-Leaved Deciduous Temporary Flooded Diked/Impounded (PFO1Ah). It is within the floodplain of the Ohio River. Wetland 10 will be temporarily impacted during construction due to the placement of a 1 ft. thick temporary coarse aggregate pad over geotextile fabric resulting in approximately 516 cubic yards of temporary aggregate fill over 0.32 acre.

Wetland 11: As depicted in Appendix A, page A-15, wetland 11 is an approximately 0.60 acre palustrine emergent wetland under the WB lanes of the Sherman Minton Bridge and extending along the north side EB I-64 within the Eastern Mountains and Piedmont region. Wetland 11 is not classified as an NWI wetland. It is within the floodplain of the Ohio River. Wetland 11 will be temporarily impacted during construction due to the placement of a 1 ft. thick temporary coarse aggregate pad over geotextile fabric. In addition, an approximate 12-inch diameter by 252 ft. long temporary steel pipe shall be placed within wetland 11 to maintain drainage through the project area. These temporary impacts will result in approximately 459 cubic yards of fill material over 0.16 acre. Upon project completion, the temporary impacts will be removed.

In conformity with the requirements of Section 401 of the Clean Water Act, a Kentucky General Certification of the Nationwide Permit 14 for linear transportation projects and a USACE 404 Nationwide permit are in the process of being obtained for this work. A Kentucky Department of Water (KY DOW) exemption for the floodplain permit was applied and no permit was deemed necessary for the temporary impacts within the floodway on the Kentucky side of the project.

On March 19, 2021, Metric conducted a field reconnaissance to determine the absence or presence of Waters of the U.S., Waters of the State, and potential wetlands within the levee area to be used for temporary access to the Kentucky approach bridges.

A *Waters of the U.S. Determination / Wetland Delineation Report* was prepared by Metric Environmental, dated March 31, 2021. It was determined no wetlands and no streams were identified within the project study limits. On June 4, 2021, KYTC concurred with Metric's findings in the *Waters Determination Report*. (Appendix D, pages D-1 to D-20).

Terrestrial Habitat

Tree clearing or trimming may become necessary during construction, In the event tree clearing or trimming becomes necessary, coordination shall occur with all appropriate agencies prior to initiating any activities. This firm commitment is included in the Additional Firm Environmental Commitments section of this AI.

Threatened or Endangered Species

Re-coordination was not required with the Indiana Section 7 determination because the impacts are on the Kentucky side. None of the determination key questions change on the Indiana side; therefore, the project is still found to have “no effect” to the Indiana bat and/or the Northern Long-eared bat (Appendix B, pages B-1 to B-3).

On May 19, 2021, Metric Environmental coordinated with KYTC to determine if additional coordination needs to occur for USFWS/IPaC documentation as it relates to the use of the levee access road, wetland impacts or potential temporary lighting on the Kentucky side of the project. On May 26, 2021, KYTC responded the new access along the levee did not change any determination they made in regard to Section 7. Coordination is located in Appendix B, pages B-4 to B-6.

A new species list was generated on May 27, 2021, which identifies the entire project area, including the levee access road, which has been added to this project. The bird, Least tern (*Sterna antillarum*) and clam, Purple cat’s paw (*Epioblasma obliquata obliquata*) are no longer included on the species list. The clam, Pink mucket (*Lampsilis abrupta*) has been added to the species list since originally generated on August 27, 2019. As no work or impacts are proposed within the river, there is no potential for impact to this species, thus no coordination with USFWS has occurred. No critical habitat has been designated for this species in the project area. The updated list of threatened and endangered species list is located in Appendix B, pages B-7 to B-15.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Section 106 Cultural Resources

Due to the subsequent design change in Kentucky, the original Area of Potential Effect (APE) has been widened as part of the “reasonable and good faith effort” that is required by the Section 106 process to identify historic properties (36 CFR Part § 800.4(b)(1)). The APE includes most of the SPGC, an USACE earthen levee, a portion of the Shawnee neighborhood along the Northwestern Parkway corridor, a portion of the Portland neighborhood on the north side of Rudd Avenue, and a large portion of river frontage that contains the McAlpine Locks and Dam Visitors Area, the Portland Wharf Park, and the Sherman Minton approach bridges. Northwestern Parkway and the Portland Historic District are both listed on the NRHP. And, as stated previously, the SPGC and the earthen levee have been previously determined NRHP eligible. Though the McAlpine Locks and Dam Visitors Center is within the APE, the engineering system itself lies outside of the APE, and thus was not evaluated for this project. Since the Sherman Minton Bridge is part of the Interstate system, it has previously been determined not eligible for the NRHP under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005. The APE is located in Appendix C, pages C-32 to C-33.

Archaeological Investigation Summary:

A Qualified Professional archaeologist from Metric Environmental, who meets the Secretary of the Interior’s Professional Qualifications Standards for Section 106 work per 36 CFR Part § 61, reviewed the proposed newly added project area by conducting a literature review. The expanded APE required for the temporary access road encompasses the full limits of the levee including 6.77 acres located immediately adjacent to and southwest of the area examined during the original archaeological investigation for the project in 2020. Based on review of the documentation Metric received regarding the previous investigation, there are no previously recorded archaeological resources within the additional acreage nor any sites eligible for listing in the National Register of Historic Places (NRHP) within its vicinity.

The Qualified Professional conducted a literature review utilizing online information from the United States Department of Agriculture, Natural Resource Conservation Service Web Soil Survey, as well as historical county soil surveys. Mapped soils within the additional acreage consist entirely of Urban land-Udorthents complex, smoothed, 0 to 50 percent slopes; udorthents comprise a deep and very deep mixture of geologic and artificial materials that have been graded and smoothed. The 6.77 acres that encompass the levee have been heavily disturbed from the levee's construction, and the levee itself is composed of artificial fill. For these reasons, Metric Environmental determined no archaeological investigations were warranted as the area within the expanded APE was not considered to have the potential to contain archaeological resources.

An archaeological summary report was submitted to the KYTC in early May 2021, and on May 13, 2021, the KYTC agreed with the findings and recommended that a "No Historic Properties Affected" determination was still appropriate for the undertaking (Appendix C, pages C-85 to C-90). On May 17, 2021, the Kentucky Heritage Council (KHC) also gave their concurrence with this determination (Appendix C, page C-86).

Above Ground Investigation Summary:

A Qualified Professional historian who meets the Secretary of the Interior's Professional Qualifications Standards for Section 106 work per 36 CFR Part § 61, conducted a literature review of the project area and established a revised APE to include the addition of the levee access road. The revised APE was submitted to KYTC on May 24, 2021, for review and submittal to the KHC for their review and comment. In a letter dated June 3, 2021, KHC acknowledged receipt of the revised APE from the KYTC and expressed their office's concurrence with the APE's adequacy for this undertaking (Appendix C, page C-91). The KHC "strongly suggest(s) avoidance/minimization to avoid the NRHP-eligible or listed resources within the proposed APE" to avoid adverse effects to those resources.

On June 29, 2021, an on-site meeting was held to discuss potential impacts to historic properties within the APE. The meeting summary is located in Appendix F, pages F-1 to F-3.

The Qualified Professional recommended the following resources within the APE as NRHP eligible: the Shawnee Golf Course, as a contributing resource to the previously-listed Olmsted Park System of Louisville Historic District (NPS File No. 82002715) under Criteria A and C; the earthen levee west of I-64 as a contributing resource for the proposed Louisville Reach of the Louisville Metro Flood Protection System HD, under Criteria A; the Shawnee Residential Historic District under Criteria A and C; and the continuing NRHP eligibility of the Portland Historic District (NPS File No. 80001615). Please see Appendix C, pages C-16 to C-24.

There are no anticipated physical changes occurring to the Shawnee Golf Course and Northwestern Parkway, Shawnee Residential Historic District, and the Portland Historic District as part of this undertaking, and no temporary or permanent right-of-way will be acquired. Construction traffic will utilize Northwestern Parkway at certain times during construction. Kokosing will document existing roadway conditions via video camera before construction begins and will compare and make patches as needed after the project is completed.

Impacts to the earthen levee include temporary physical changes due to the construction of a temporary access road. The proposed access road to be built on top of the levee would be constructed of temporary gravel aggregate over geotextile fabric, and it would measure 12 ft. wide, one ft. thick, and 1,440 ft. long. The stripped topsoil will be stored and stabilized for the duration of the project. Total temporary right-of-way needed for the access road is approximately 6.77 acres. Upon completion of construction activities, the aggregate and geotextile fabric would be removed, and the topsoil that had been placed in storage would be replaced. The topsoil would be graded and seeded to restore it to pre-construction conditions. This will

include seeding the topsoil. No permanent alterations or impacts to the levee are anticipated. A water truck will be on site to manage dust control and will be used on an as-needed basis.

Based upon the project's temporary and minor impacts to the NRHP-eligibility resources within the APE, the recommended effect finding for the project was No Adverse Effect. Please see Appendix C, pages C-24 to C-29.

The additional project information, including NRHP Evaluation and Effects Determination Recommendations was provided to the KHC for a 30-day review and comment period on July 16, 2021. In a letter dated July 27, 2021, the KHC provided concurrence with a revised conditional finding of No Adverse Effect, which includes the following:

- A revised Cultural Historic that addresses the issues outlined via email on July 27th, 2021, is submitted and accepted by KHC office no later than six months from the date of this letter. KHC office is withholding comment on eligibility discussions included within the report until the revised copy is received. This will include a paper copy of the revised report, if requested by KHC office.
- Survey forms, likely to be submitted by a different consultant, are submitted and accepted by KHC office for all resources within the APE no later than one year from the date of this letter. This should be coordinated with KHC office, and follow the guidelines discussed via email on July 27th, 2021. This will include a paper copy of the survey forms, if requested by KHC office.
- Use of the NRHP-eligible SPGC, Shawnee Residential Historic District and Northwestern Parkway are temporary, with any damages to the roadway caused by construction vehicles to be repaired by the end of the project.
- Use of the NRHP-eligible Louisville Floodwall levee for construction vehicle access shall protect the resource from adverse effect, with no more than one foot of topsoil removed from the levee, before the aggregate and geotextile road is set up. All area of removed matter shall be returned to the state in which it was found at the end of the project duration, conforming to pre-existing dimensions including height, width, and slope of the levee, as this will be temporarily altered by the construction of a road for contractor access. This shall be confirmed with SHPO at the end of this undertaking.

The KHC letter response is located in Appendix C, pages C-1 to C-2.

Section 4(f) de minimis impacts to historical resources (SPGC, Levee, Northwestern Parkway, Shawnee Residential and Portland Historic Districts) did not require notification due to an existing Memorandum of Understanding (MOU) between KHC and KYTC (Appendix C, pages C-128 to C-129).

Section 4(f)

Kentucky: There are two Section 4(f) resources located within the project area which include the SPGC and Louisville Loop (Appendix A, page A-3 and A-16).

In a letter, dated August 2, 2021, the KY FHWA proposed the Section 4(f) impact to the SPGC qualifies for a Temporary Occupancy Exemption, because the impact is minor, the cart path will be returned to its pre-construction condition, and the impact lasts less than the entire duration of the project, which is from July 2021 to August 2023. It is proposed the impact to the Louisville Loop will also qualify as a Temporary Occupancy Exemption since the trail is not open to the public at this time, no permanent changes will occur, and that status will not change. This effectively means there is 'no use' of either resource. On August 2, 2021, Louisville Parks and Recreation Department, the official with jurisdiction (OWJ) over the SPGC

including the Louisville Loop for Section 4(f), concurred with the Section 4(f) determination of temporary occupancy ‘no use’ (Appendix F, pages F-4 to F-5).

Indiana: There are two Section 4(f) resources located within the project area which include the Levee Trail and Ohio Greenway Trail (Appendix A, page A-3 and A-18).

In a letter dated August 10, 2021, the City of New Albany, official with jurisdiction (OWJ) over the Levee Trail and Ohio River Greenway Trail, understands that the *de minimis* use of the Section 4(f) resource is necessary and is in the opinion that such work will not permanently adversely affect the activities, features, or attributes of the recreational resource. The OWJ concurs with the FHWA Section 4(f) *de minimis* finding for this project (Appendix F, pages F-6 to F-7).

A public notice was advertised in the News and Tribune newspaper on July 13, 2021, describing the temporary effects of the Levee Trail and the Ohio River Greenway Trail, within the Indiana side of the SMCP. The Proof of Publication and Public Notice are located in Appendix F, pages F-8 to F-9. No comments were received by the comment deadline date of July 26, 2021.

Section 6(f)

As discussed in the approved CE document, dated October 7, 2020, the Shawnee Park Pavilion and the SPGC Maintenance Building within the SPGC are Section 6(f) resources. The Louisville Loop is attached to the SPGC and therefore, should be considered part of the Section 6(f) resource. These properties are owned by the City of Louisville and operated by the Louisville Parks and Recreation.

On July 12, 2021, FHWA proposed the Section 6(f) finding for both the SPGC and Louisville Loop resources will be ‘no conversion’ because there will be no constructive changes to the current use of either facility. There will be no conversion of recreational land to a non-recreational use, and no change in ownership. The trail is closed to the public and will remain closed.

On July 20, 2021, LWCF “Recreational Trails Program, Federal Program Specialist, Kentucky Department for Local Governments concurred with the Section 6(f) ‘no conversion’ for the SPGC including the Louisville Loop (Appendix F, page F-11).

Addendum to Red Flag Investigation (RFI)

Indiana:

As the original RFI for this project was approved by INDOT Site Assessment and Management (SAM) on April 1, 2019, more than two years ago, Metric Environmental conducted a review of the findings to evaluate any changes.

On June 4, 2021, Metric Environmental coordinated with INDOT SAM regarding the evaluation. There was no new infrastructure, water resources, mining and mineral resources, and hazardous material concerns within or near the project area. On June 16, 2021, INDOT SAM responded that it does not appear an RFI Addendum is warranted. Coordination is located in Appendix E, page E-1 to E-2.

Kentucky:

Construction of the access road along the levee will require excavation of approximately 1 ft.; however, the levee is approximately 30 ft. above ground level. Therefore, no coordination with Kentucky Energy and Environment Cabinet was conducted. No impacts of hazardous materials are anticipated.

Permits

A US Army Corps of Engineers (USACE) Section 408 levee permit and Louisville MSD Flood Protection System Modification permit are in the process of being obtained due to performing work on the levee on the Kentucky side of the project. The Louisville MSD Flood Protection System Modification permit will provide temporary permission to access and temporarily alter the levee for construction activities.

Although impacts will occur within the floodplain on the Kentucky side of the project, correspondence was received from KY DOW indicating that the proposed construction activities are exempt from the KY DOW floodplain permit. This email correspondence is included in Appendix B, pages B-16 to B-17.

A KY DOW Section 401 general permit has been obtained and a USACE Section 404 Water Quality Certification is in the process of being obtained for the temporary impacts to regulated wetlands on the Kentucky side of the project.

There will be earth disturbance, greater than 1 acre, associated with the MOT and staging within the I-264 and I-64 interchange ramps. A National Pollutant Discharge Elimination System (NPDES) General Permit was approved via the Kentucky KYR10 permit process. Permit coverage extends from July 23, 2021 through July 23, 2023. An extension of permit coverage will be requested prior to expiration due to construction activities planned to extend through September 2023. In addition, a Louisville MSD local site disturbance permit is in the process of being obtained for the disturbed areas on the levee that extend beyond the KYTC ROW.

An Indiana Department of Natural Resources (IDNR) Construction in a Floodway (CIF) permit is in the process of being obtained for temporary impacts beneath the Indiana approach bridges.

A USACE Section 408 levee permit is in the process of being obtained due to performing work within 50 ft. of the levee on the Indiana side of the project.

Earth disturbance for the project will result in more than one (1) acre of disturbance; therefore, an Indiana Department of Environmental Management (IDEM) Rule 5 Notice of Intent is in the process of being obtained for the project.

Additional Firm Environmental Commitments:

1. In the event tree clearing or trimming becomes necessary, coordination shall occur with all appropriate agencies prior to initiating any activities. (KYTC and INDOT).
2. A revised Cultural Historic Report that addresses the issues outlined by KY-SHPO in email communication to KYTC on July 27, 2021 shall be provided to KY-SHPO no later than January 27, 2022. The revised report will be drafted by Metric Environmental on behalf of Kokosing Construction Company and submitted by KYTC to KY-SHPO.
3. Survey forms for all historical resources within the revised APE shall be submitted and accepted by the KY-SHPO no later than July 27, 2022. Submittal of the survey forms shall be coordinated with KY-SHPO prior to submittal and shall follow the guidelines discussed in email communication to KYTC on July 27, 2021. This will be the responsibility of KYTC.
4. Any damages incurred by construction vehicles to the NRHP-eligible Northwestern Parkway shall be repaired prior to project completion and shall be the responsibility of Kokosing Construction Company.
5. The NRHP-eligible Louisville Floodwall levee is to be used for temporary vehicle access throughout construction. No more than one foot of topsoil is to be removed from the levee to facilitate construction operations. All area of removed matter shall be returned to the state in which it was found prior to project completion, conforming to pre-existing dimensions including height,

width, and slope of the levee. This shall be the responsibility of Kokosing Construction Company and shall be confirmed by KY-SHPO prior to project completion.

CONCLUSIONS:

Adding the temporary access road in Kentucky and the staging areas and ITS in Indiana to this project has no effect. Adding the staging area beneath the Kentucky approach bridges will temporarily impact approximately 0.32 acre of wetland 10 and 0.16 acre of wetland 11. No additional impacts are expected during construction of this project.

All other proposed improvements identified in the approved CE document, dated October 7, 2020, remain unchanged. Therefore, supplemental review of environmental resources has been completed.

Name and organization of AI Preparer: Susan Castle, Metric Environmental, LLC

| | | |
|-----------------|--|-----------------|
| Approval | Brandon Miller <small>Digitally signed by Brandon Miller Date: 2021.09.07 13:51:34 -04'00'</small> | <u>9/7/2021</u> |
| | INDOT ESD Signature | Date |
| | Erica Tait <small>Digitally signed by Erica Tait Date: 2021.09.08 13:55:27 -04'00'</small> | <u>9/8/2021</u> |
| | INDIANA FHWA Signature | Date |
| | <i>David Harmon</i> | <u>9/7/2021</u> |
| | KYTC DEA Signature | Date |

APPENDICES

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- Waters Delineation ReportD-1

APPENDIX E: Red Flag and Hazardous Materials

- E-mail regarding RFI Addendum (Indiana)E-1

APPENDIX F: Section 4(f) and 6(f)

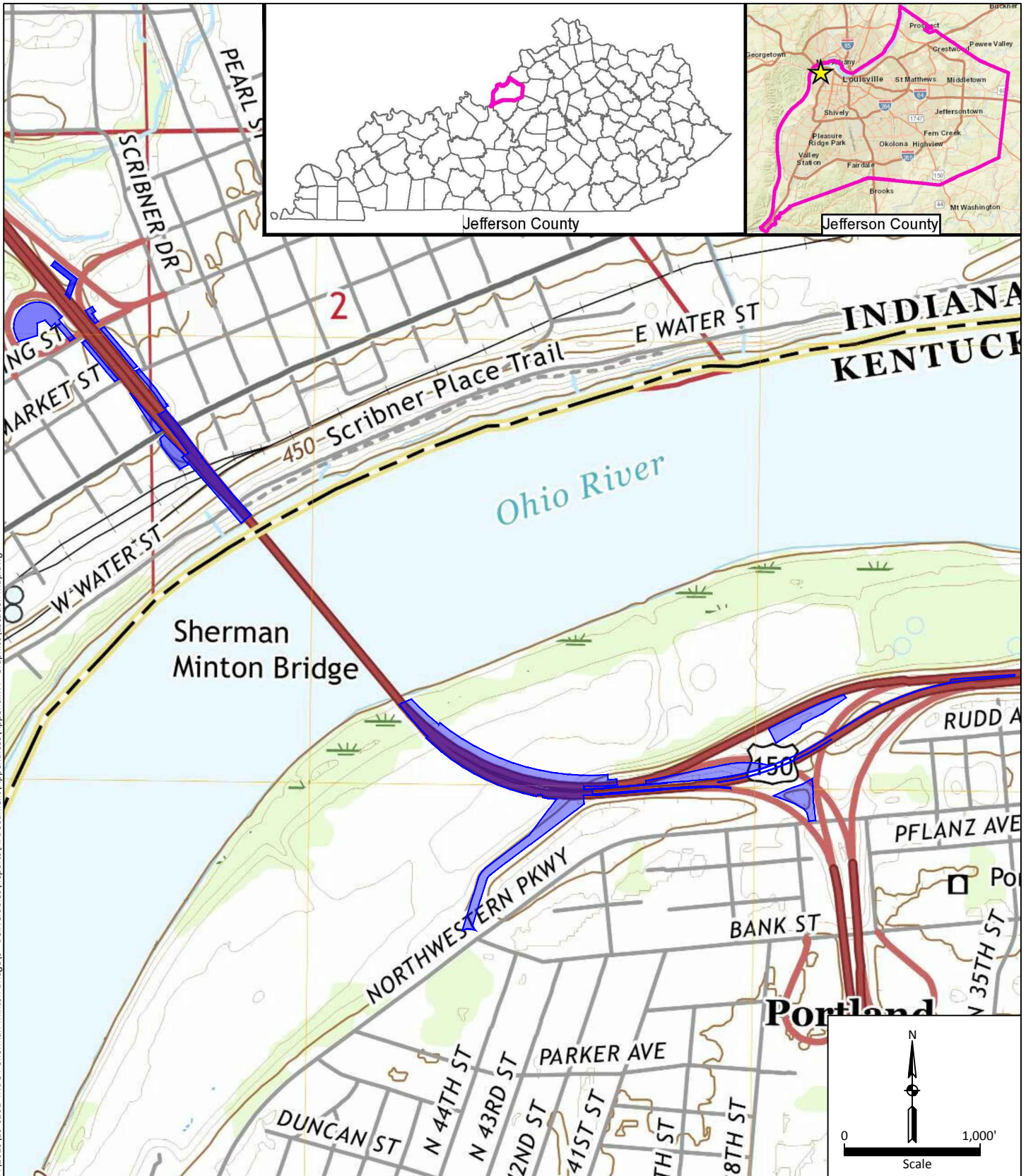
- Section 4(f) and 6(f) Meeting Summary.....F-1
- SPGC and Louisville Loop Section 4(f) no use letterF-4
- Levee Trail and Ohio River Greenway Trail Section 4(f) de minimis letterF-6
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APPENDIX G: Approved CE Document

- Approved CE Document without appendicesG-1


APPENDIX A:

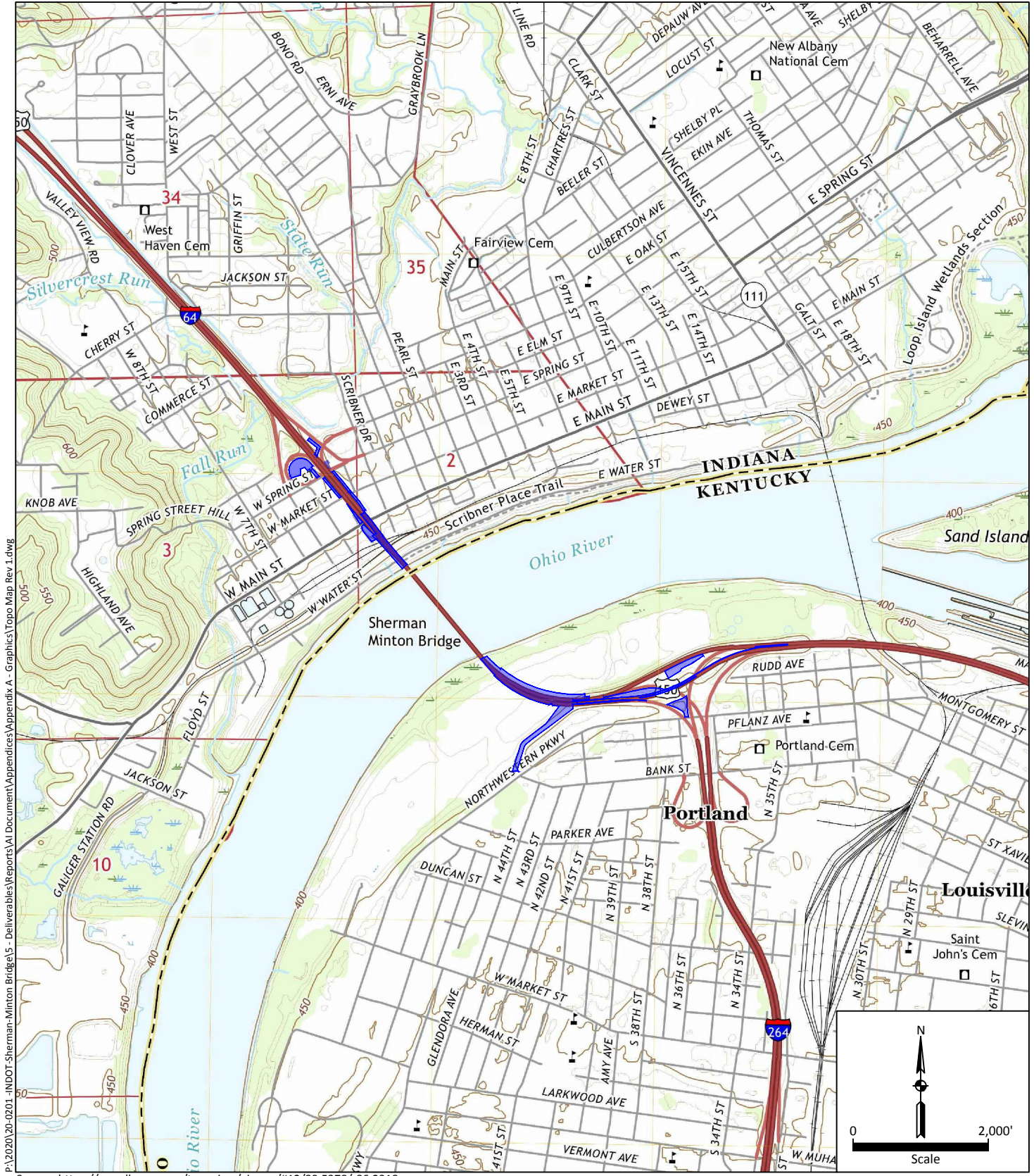
Graphics



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Source: <https://ngmdb.usgs.gov/topoview/viewer/#12/39.5378/-86.2918>

| | | |
|--|---|--|
| <p>Location Map Sherman Minton Corridor Project Bridge Rehabilitation City of Louisville, Jefferson County, Kentucky INDOT Lead Des. No., 1702255</p> | <p>All locations approximate</p> <p> Project Area</p> |  <p>Drawn by: <u>ILJ</u> Checked by: <u>SC</u> Approved by: <u>JRP</u> Date: <u>July, 2021</u></p> |
|--|---|--|



P:\2020\20-0201-INDOT-Sherman-Minton Bridges\5 - Deliverables\Reports\AI Document\Appendices\Appendix A - Graphics\Topo Map Rev 1.dwg

Source: <https://ngmdb.usgs.gov/topoview/viewer/#12/39.5378/-86.2918>

USGS Topographic Map
 Sherman Minton Corridor Project
 Bridge Rehabilitation
 City of Louisville, Jefferson County, Kentucky
 INDOT Lead Des. No., 1702255

All locations approximate
 Base map:
 2019 New Albany, IN
 7.5 Minute Quadrangle

Project Area

School

Drawn by: ILJ

Checked by: SC

Approved by: JRP

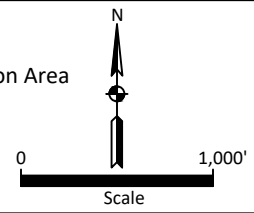
Date: July, 2021



Source: Google Earth Pro

2020 Aerial Photograph
 Sherman Minton Corridor Project
 Bridge Rehabilitation
 City of Louisville, Jefferson County, Kentucky
 INDOT Lead Des. No., 1702255

- Note: All locations are approximate
- Temporary Construction Laydown Area
 - Temporary Construction Access Area
 - Temporary MOT Construction Area
 - Temporary Aggregate Placed As Needed
 - Utility Construction Area



METRIC ENVIRONMENTAL

Drawn by: ILJ
 Checked by: SC
 Approved by: JRP
 Date: July, 2021



 Project Limits for Temporary Access Area

 Culvert (CV)

 Roadside Ditch (RSD)

Photo Location Map
 Sherman Minton Corridor Project
 City of Louisville, Jefferson County, Kentucky
 Des. No. 1702255
 Metric Project No. 20-0201

All locations approximate
 Source: Louisville/Jefferson County
 Information Consortium (LOJIC)(2019)



0 100 200 400
 Feet





1. View from southern project study limits (PSL), looking northwest.



2. View from southern PSL, looking northwest.



3. View of levee from southern PSL, looking northeast.



4. View of Northwestern Pkwy right-of-way (ROW), looking northeast.

SITE PHOTOGRAPHS—3/19/21

Sherman Minton Corridor Project

City of Louisville, Jefferson County, Kentucky

INDOT Des. No. 1702255





5. View of western side of levee, looking northeast.



6. View of southern terminal point of Roadside Ditch (RSD) 1, looking southwest.



7. View of RSD 1 from its southern terminal point, looking northeast.



8. View of RSD 1 from Culvert (CV) 1, looking southwest.

SITE PHOTOGRAPHS—3/19/21

Sherman Minton Corridor Project

City of Louisville, Jefferson County, Kentucky

INDOT Des. No. 1702255





9. View of CV-1 inlet, looking northeast.



10. View of eastern side of levee, CV 1, and RSD 1, looking southwest.



11. View of eastern side of levee, looking southeast.



12. View of eastern side of levee, looking northeast.

SITE PHOTOGRAPHS—3/19/21

Sherman Minton Corridor Project

City of Louisville, Jefferson County, Kentucky

INDOT Des. No. 1702255





13. View of levee, looking southwest.



14. View of levee, looking northeast.



15. View of western side of levee, looking southwest.



16. View of Shawnee Golf Course from western side of levee, looking northwest.

SITE PHOTOGRAPHS—3/19/21

Sherman Minton Corridor Project

City of Louisville, Jefferson County, Kentucky

INDOT Des. No. 1702255





17. View of western side of levee, looking northeast.



18. View of levee, looking southwest.



19. View of levee, looking northeast.



20. View of eastern side of levee, looking southwest.

SITE PHOTOGRAPHS—3/19/21

Sherman Minton Corridor Project

City of Louisville, Jefferson County, Kentucky

INDOT Des. No. 1702255





21. View of eastern side of levee, looking east.



22. View of western side of levee, looking southwest.



23. View of western side of levee, looking northeast.



24. View of CV-2 and western side of levee, looking northeast.

SITE PHOTOGRAPHS—3/19/21

Sherman Minton Corridor Project

City of Louisville, Jefferson County, Kentucky

INDOT Des. No. 1702255





25. View of CV-2 and western side of levee, looking southwest.



26. View of eastern side of levee from northern PSL, looking south.



27. View of levee from northern PSL, looking southwest.



28. View of western side of levee near northern PSL, looking southwest.

SITE PHOTOGRAPHS—3/19/21

Sherman Minton Corridor Project

City of Louisville, Jefferson County, Kentucky

INDOT Des. No. 1702255





29. View of western side of levee near northern PSL, looking northwest.



30. View of forested area near northeastern PSL, looking northeast.



31. View of eastern side of levee from northwestern PSL, looking southwest.

SITE PHOTOGRAPHS—3/19/21

Sherman Minton Corridor Project

City of Louisville, Jefferson County, Kentucky


INDOT Des. No. 1702255



Sherman Minton Corridor Project

Permit Exhibits

UTILITIES



**811 Know what's below.
Call before you dig.**

INDIANA UNDERGROUND
1-800-382-5544 OR CALL 811
24 HOURS A DAY 7 DAYS A WEEK

BEFORE YOU DIG

↓

The contractor is instructed to call 1-800-752-6007 to reach KY 811, the one-call system for information on the location of existing underground utilities. The call is to be placed a minimum of two (2) and no more than ten (10) business days prior to excavation. The contractor should be aware that owners of underground facilities are not required to be members of the KY 811 one-call Before-U-Dig (BUD) service. The contractor must coordinate excavation with the utility owners, including those whom do not subscribe to KY 811. It may be necessary for the contractor to contact the County Court Clerk to determine what utility companies have facilities in the area.

LEVEE ACCESS GENERAL NOTES

- 1) The Contactor will provide digital pictures of before, during, and after conditions of the levee. These photos are to be submitted to both the Levee Sponsor and the USACE Levee Safety Point of Contact, Brandon Adair at brandon.l.adair@usace.army.mil within 30 days of completion of the approved project.
RESPONSE:
USACE:
- 2) The Contractor shall complete as-built drawings of all final features within 50 feet of the levee to where construction activities will occur and provide them to both the Levee Sponsor and the Louisville District USACE at brandon.l.adair@usace.army.mil within 30 days of completion of the approved project.
RESPONSE:
USACE:
- 3) The Contractor shall be responsible for any damage to the existing levee during construction. Damages shall be repaired by the Contractor at their expense. If damages beyond the approved scope of work occur, the Contractor shall immediately contact the Levee Sponsor and the USACE Levee Safety Point of Contact, Brandon Adair at 502-315-6237.
RESPONSE:
USACE:
- 4) Please provide notice to USACE at least one week prior to construction near the levee area to allow a USACE representative the option to be on site. POC is Brandon Adair at 502-315-6237.
RESPONSE:
USACE:
- 5) Any modification to the approved submittals deemed necessary during the construction phase of the project may require additional technical review and approval by the USACE prior to continuing construction. Consult the USACE Levee Safety Point of Contact, Brandon Adair, to determine if this review is necessary.
RESPONSE:
USACE:

INDEX

| SHEET NO. | DRAWINGS INDEX |
|-----------|----------------------------------|
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| 2-8 | ENVIRONMENTAL IMPACTS |
| 9-17 | EROSION CONTROL PLAN |
| 18-28 | LEVEE ACCESS ROAD CROSS SECTIONS |

COORDINATE DATUM

Horizontal Datum:
The horizontal datum used for this project is the Floyd County Zone of the Indiana Geospatial Coordinate System (INGCS), NAD83(2011). All coordinates and distances are grid values and units are US Survey Feet, unless otherwise noted.

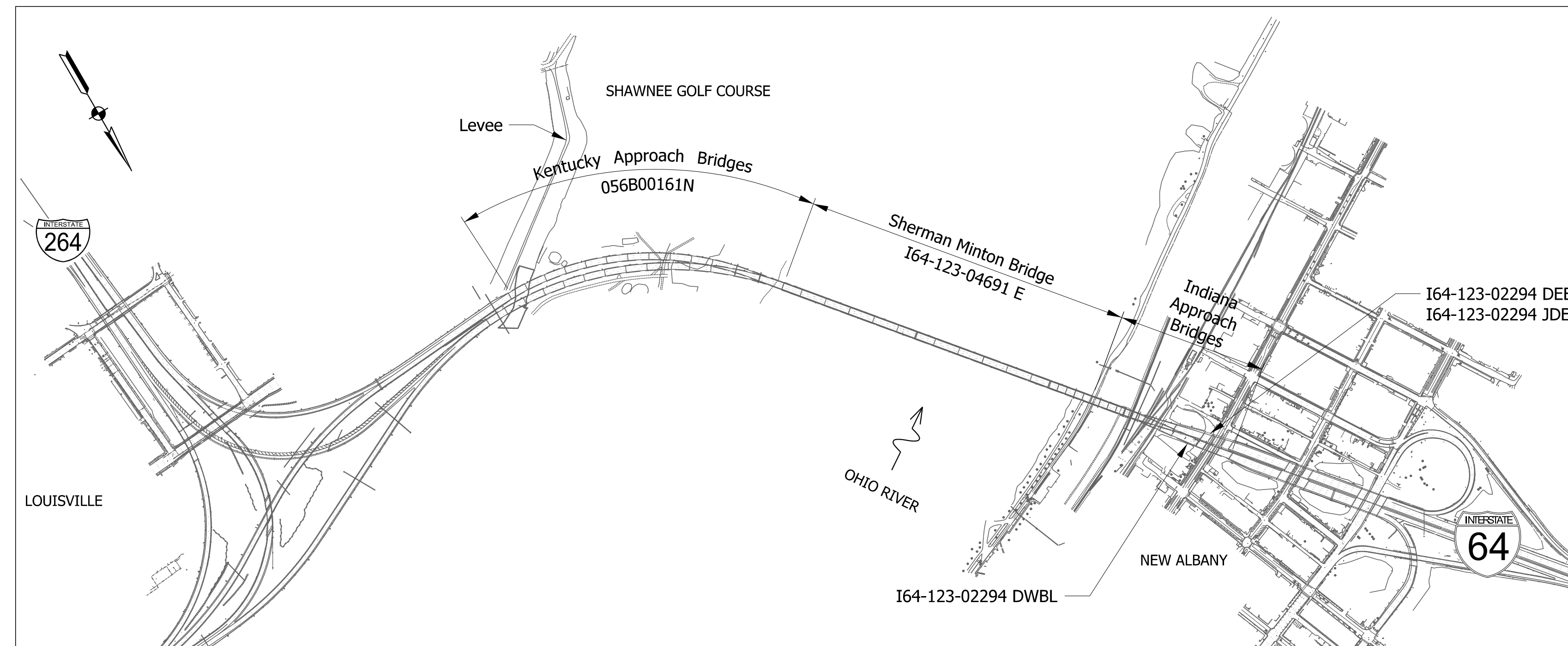
Vertical Datum:
The vertical datum for this project is the North American Vertical Datum of 1988 (NAVD88).

GENERAL NOTES

- 1) This set of exhibits is for permitting purposes to show design intent only. They should not be used for construction.

TABLE OF APPLICABLE EXHIBITS

| PERMIT | APPLICABLE EXHIBITS |
|--------------------|----------------------|
| IDNR CIF | 1, 5-7, 12-14, 16-17 |
| KY 401 | 1-5, 8-12, 15-28 |
| KY 404 | 1-5, 8-12, 15-28 |
| IN 408 | 1, 5-7, 12-14, 16-17 |
| KY 408 | 1-5, 8-12, 15-28 |
| Louisville MSD 408 | 1-5, 8-12, 15-28 |
| IDEM Rule 5 | 1, 13-14, 16-17 |
| KYR10 | 1, 9-11, 15-17 |



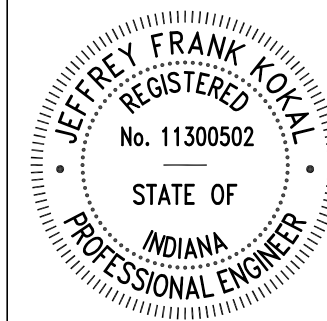
PROJECT DESCRIPTION


The project is located on Interstate 64 (I-64) over the Ohio River connecting New Albany, Floyd County, Indiana and Louisville, Jefferson County, Kentucky. The purpose of this project (INDOT Des. No. 1702255) is to address the deterioration of structural elements on the following Bridges:

- Sherman Minton (Bridge No. I64-123-04691 E)
- Indiana Westbound Approach (I64-123-02294 DWBL)
- Indiana Eastbound Approach (I64-123-02294 DEBL)
- Indiana Eastbound Approach (I64-123-02294 JDEB)
- Kentucky Approach (056B00161N)
- Market Street (I64-123-04690 BEBL)

The goal of the structural rehabilitation is to extend the service life of the I-64 crossing over the Ohio River up to 30 years. Other work includes pavement rehabilitation of local streets in New Albany (Elm St., Spring St., and 5th St.), new lighting on I-64 and new ITS systems on I-64.

NOT FOR CONSTRUCTION

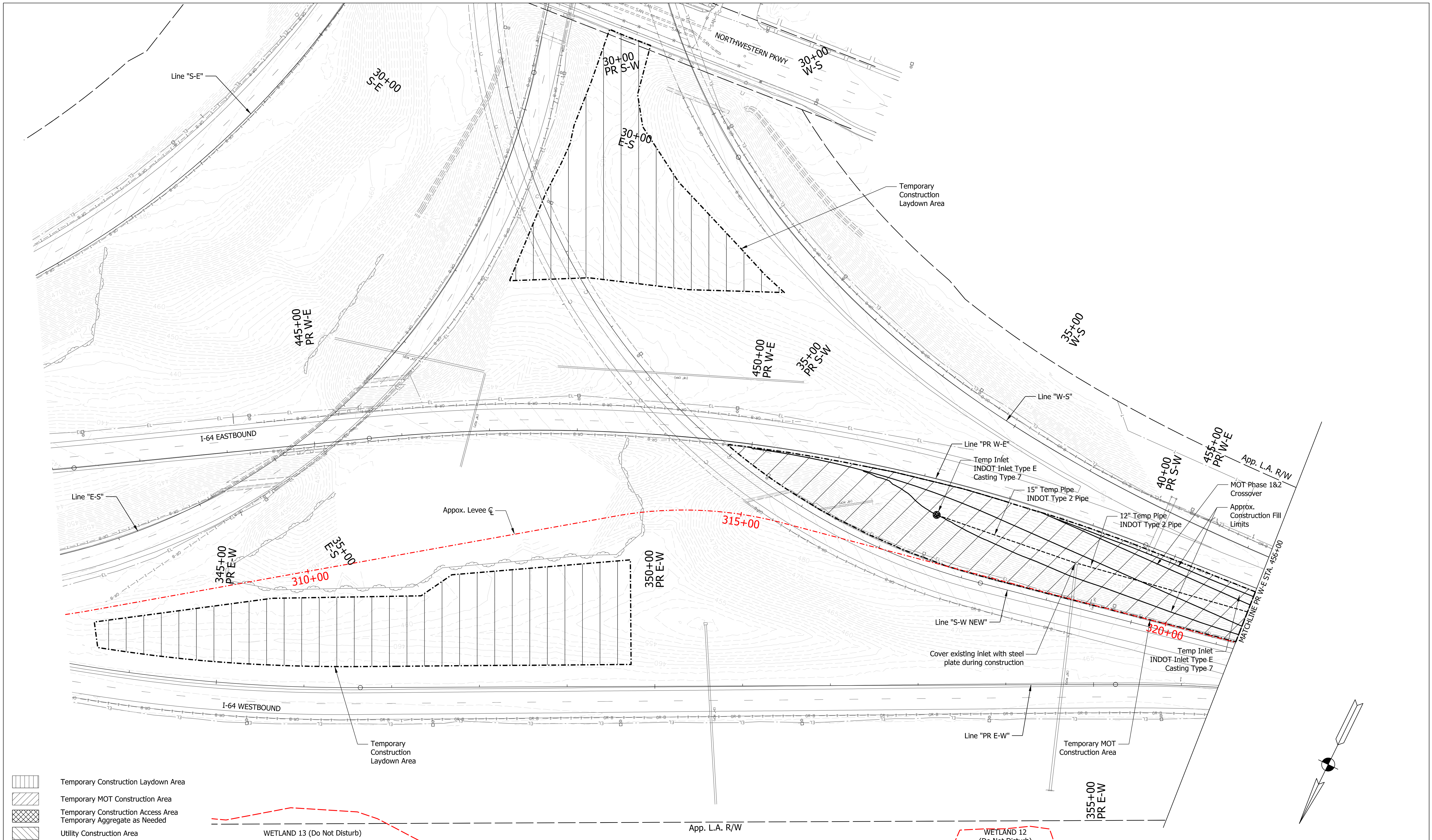


| | | | |
|--------------------------|---|--------------|--------|
| RECOMMENDED FOR APPROVAL |  | 7-9-21 | |
| | DESIGN ENGINEER | DATE | |
| DESIGNED: DPS | 4/1/21 | DRAWN: DPS | 4/1/21 |
| CHECKED: JFK | 4/5/21 | CHECKED: JFK | 4/5/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION

INDEX AND
GENERAL NOTES

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=500' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| | 1 of 28 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |



- Temporary Construction Laydown Area
- Temporary MOT Construction Area
- Temporary Construction Access Area
- Temporary Aggregate as Needed
- Utility Construction Area

NOT FOR CONSTRUCTION



| | | | | |
|--------------------------|-------------------|--------------|-----------------|------|
| RECOMMENDED FOR APPROVAL | <i>AKL</i> 7-9-21 | | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 4/1/21 | DRAWN: DPS | 4/1/21 | |
| CHECKED: JFK | 4/5/21 | CHECKED: JFK | 4/5/21 | |

INDIANA
DEPARTMENT OF TRANSPORTATION

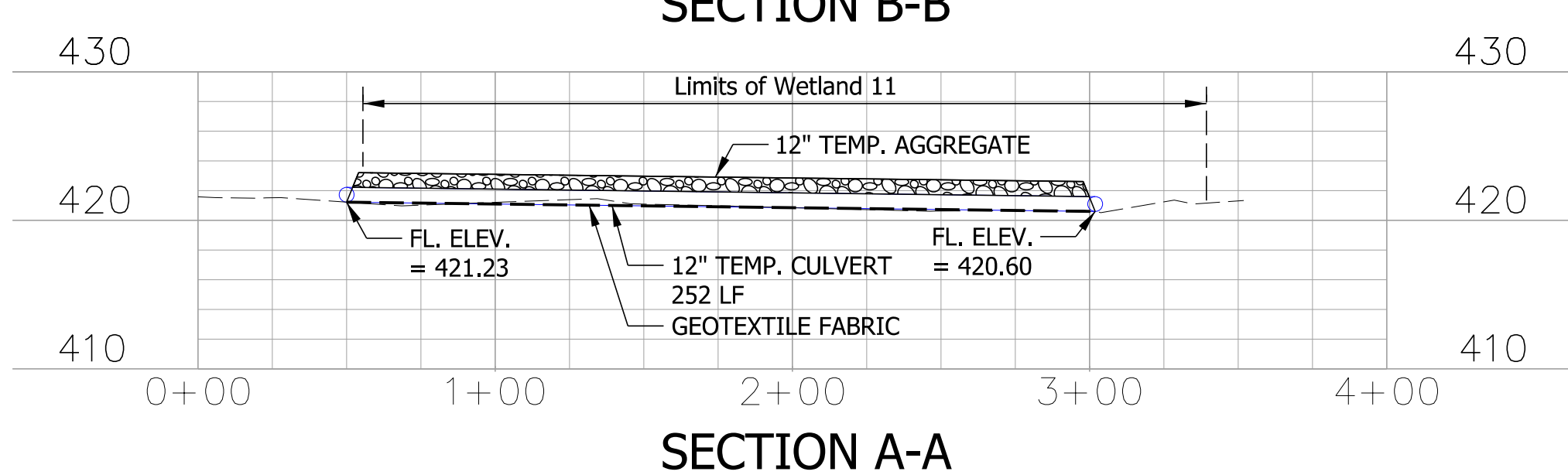
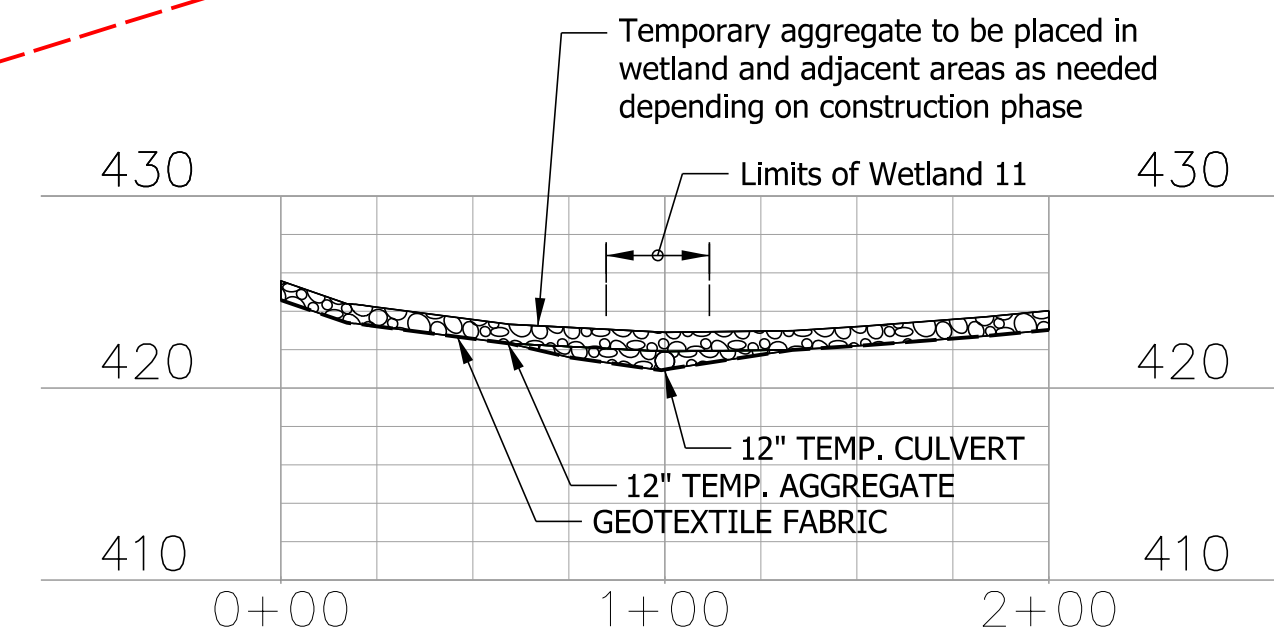
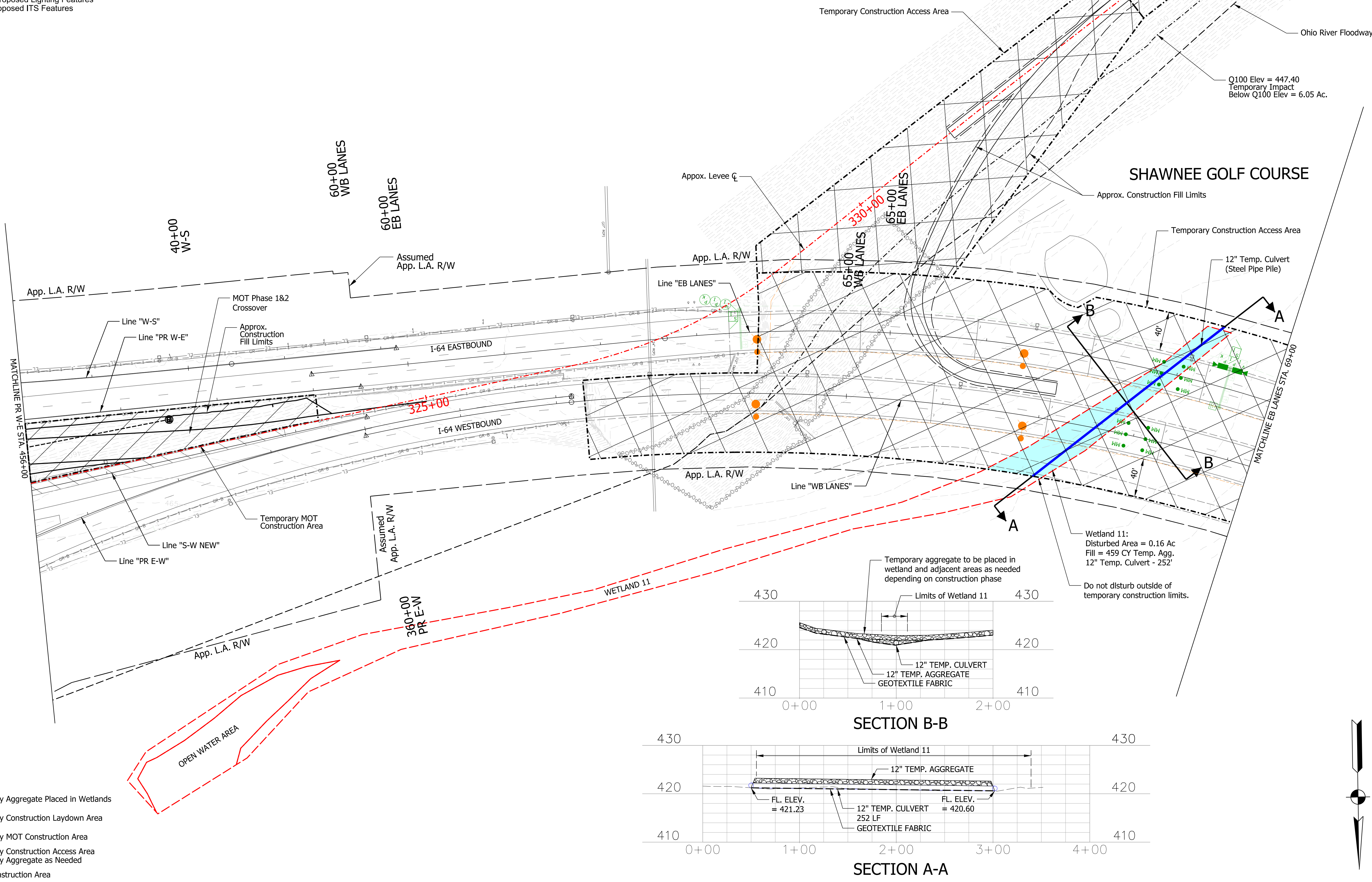
ENVIRONMENTAL IMPACTS
STA. 442+00 TO STA. 456+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| | 2 of 28 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 09-JUL-2021 07:56 AM

Notes:

- Proposed utilities are preliminary and are shown for reference only.
 Orange - Proposed Lighting Features
 Green - Proposed ITS Features



NOT FOR CONSTRUCTION



| | | | |
|--------------------------|-------------------|-----------------|--------|
| RECOMMENDED FOR APPROVAL | <i>MAK 7-9-21</i> | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 4/1/21 | DRAWN: DPS | 4/1/21 |
| CHECKED: JFK | 4/5/21 | CHECKED: JFK | 4/5/21 |

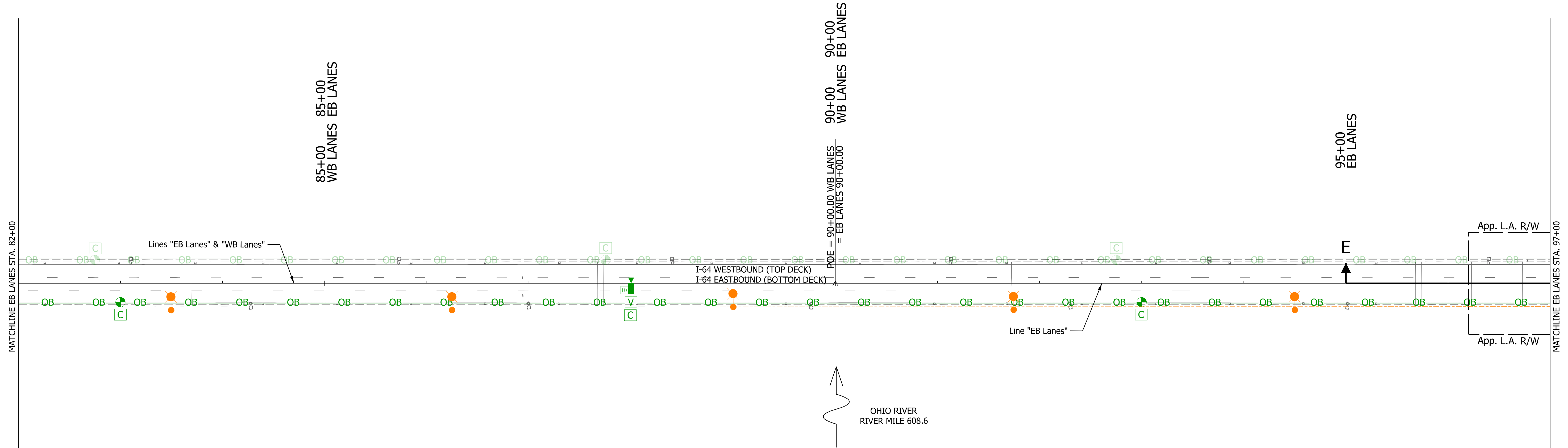
INDIANA DEPARTMENT OF TRANSPORTATION
ENVIRONMENTAL IMPACTS
 STA. 456+00 TO STA. 69+00


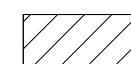

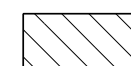
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| VERTICAL SCALE | DESIGNATION |
| 1"=10' | 1702255 |
| BUILDABLE UNIT | SHEETS |
| | 3 of 28 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 09-JUL-2021 07:56 AM

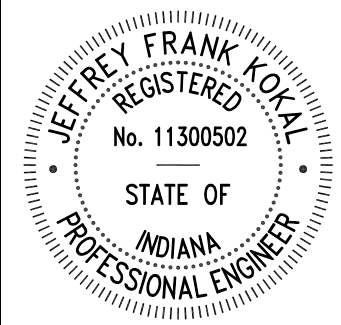
Notes:


- Proposed utilities are preliminary and are shown for reference only.
 Orange - Proposed Lighting Features
 Green - Proposed ITS Features



-  Temporary Construction Laydown Area
-  Temporary MOT Construction Area
-  Temporary Construction Access Area
Temporary Aggregate as Needed
-  Utility Construction Area

NOT FOR CONSTRUCTION

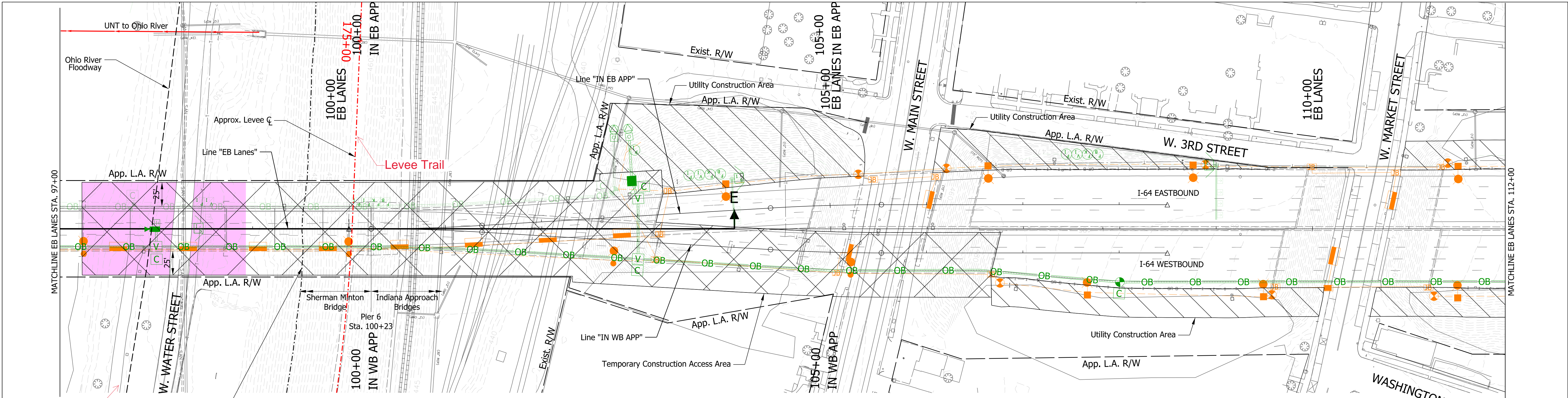


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| DESIGNED: DPS | 4/1/21 | DRAWN: DPS | 4/1/21 | |
| CHECKED: JFK | 4/5/21 | CHECKED: JFK | 4/5/21 | |

INDIANA
DEPARTMENT OF TRANSPORTATION

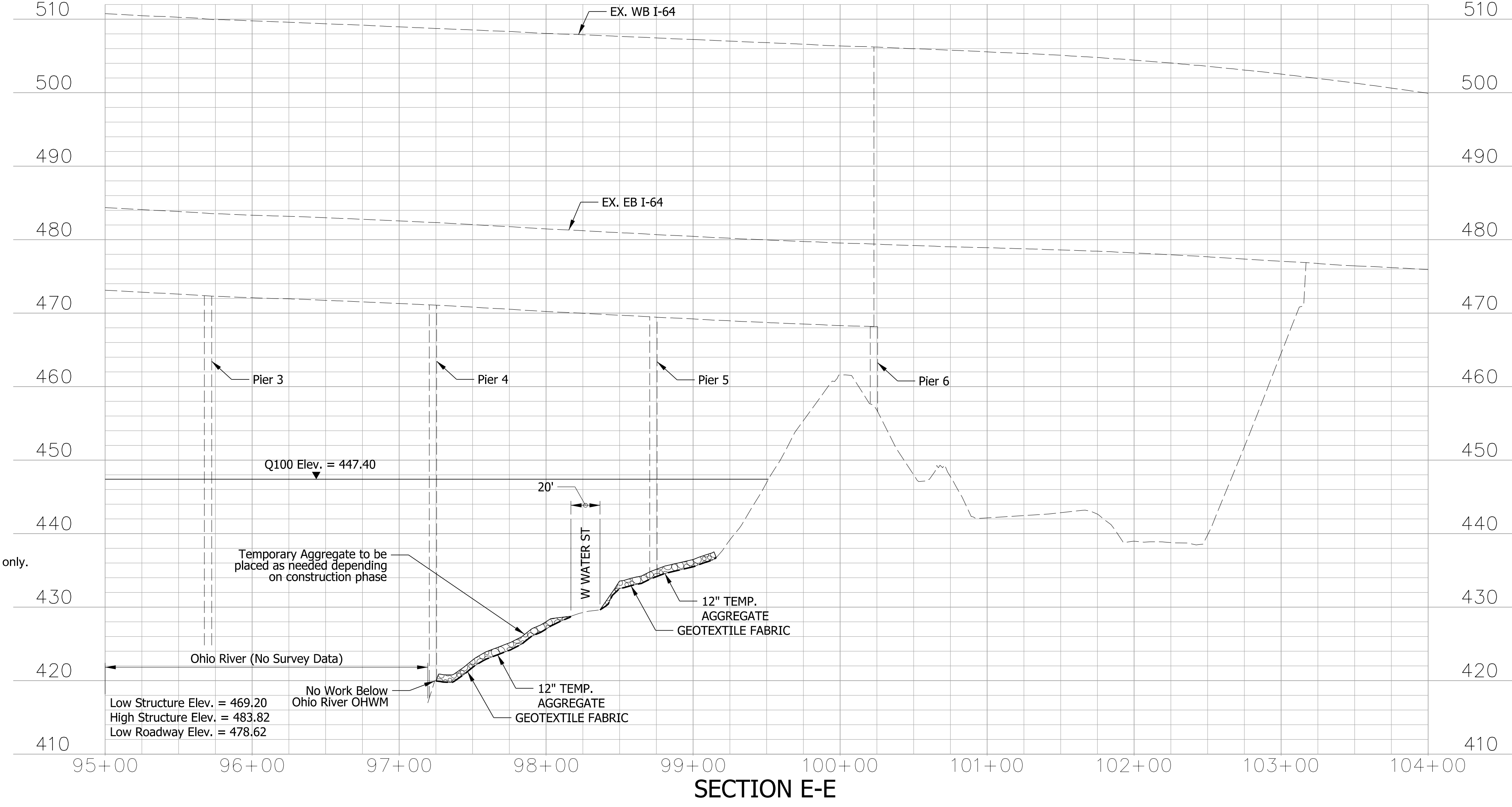
ENVIRONMENTAL IMPACTS
STA. 82+00 TO STA. 97+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| | 5 of 28 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |



Ohio River Greenway Trail

Q100 Elev = 447.40
 Temporary Impact Below Q100 Elev = 0.55 Ac.
 Exist. Vegetation = Early Successional
 Temp Agg. Fill = 536 CY



- Notes:
- Proposed utilities are preliminary and are shown for reference only.
 Orange - Proposed Lighting Features
 Green - Proposed ITS Features

- Temporary Aggregate Placed as Needed
- Temporary Construction Laydown Area
- Temporary MOT Construction Area
- Temporary Construction Access Area
- Temporary Aggregate as Needed
- Utility Construction Area

NOT FOR CONSTRUCTION



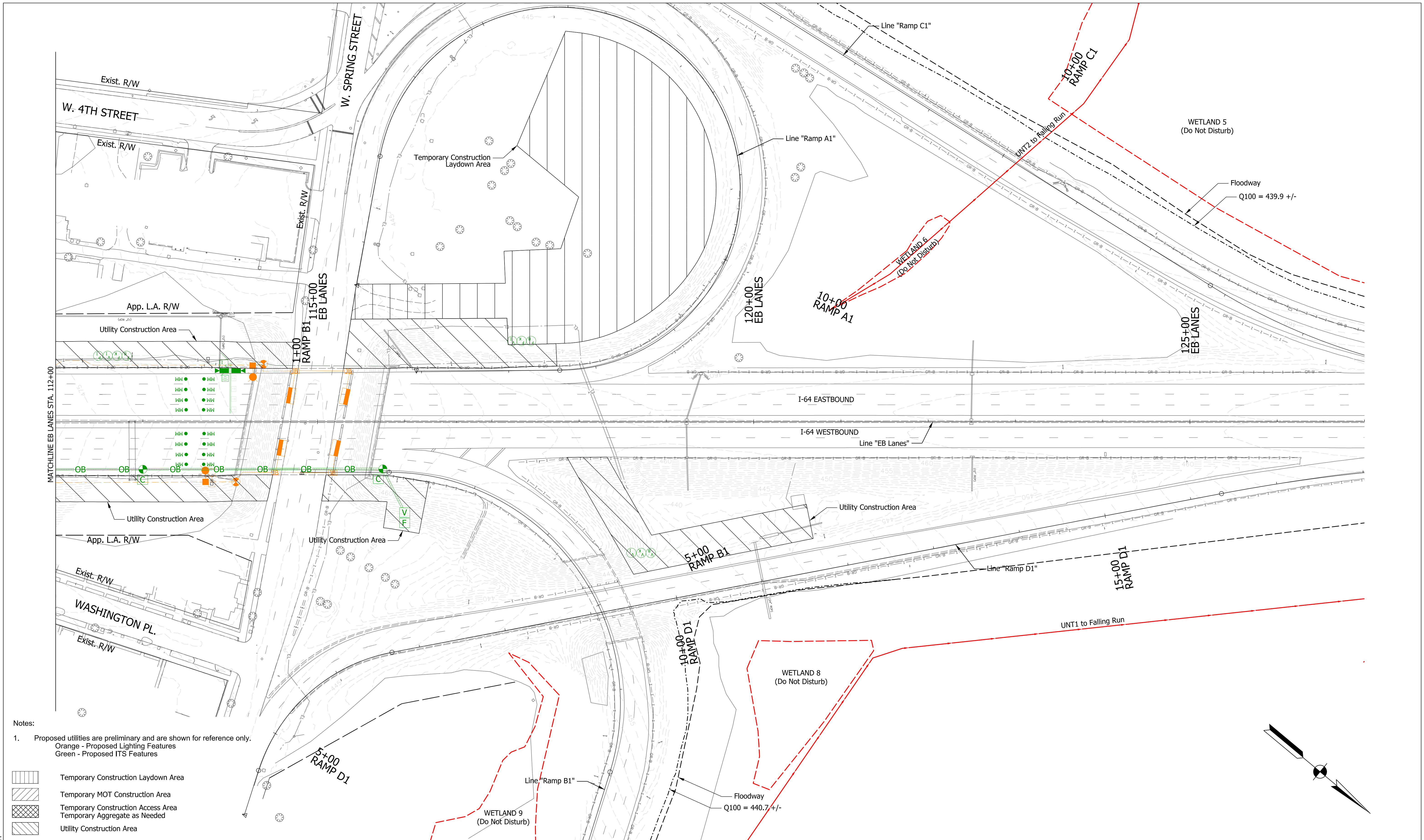
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| CHECKED: JFK | 4/5/21 | CHECKED: JFK | 4/5/21 |

INDIANA DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL IMPACTS
 STA. 97+00 TO STA. 112+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| 1"=10' | 1702255 |
| BUILDABLE UNIT | SHEETS |
| | 6 of 28 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 09-2021-07:57 AM



- Notes:
- Proposed utilities are preliminary and are shown for reference only.
 Orange - Proposed Lighting Features
 Green - Proposed ITS Features

- Temporary Construction Laydown Area
- Temporary MOT Construction Area
- Temporary Construction Access Area
- Temporary Aggregate as Needed
- Utility Construction Area

NOT FOR CONSTRUCTION



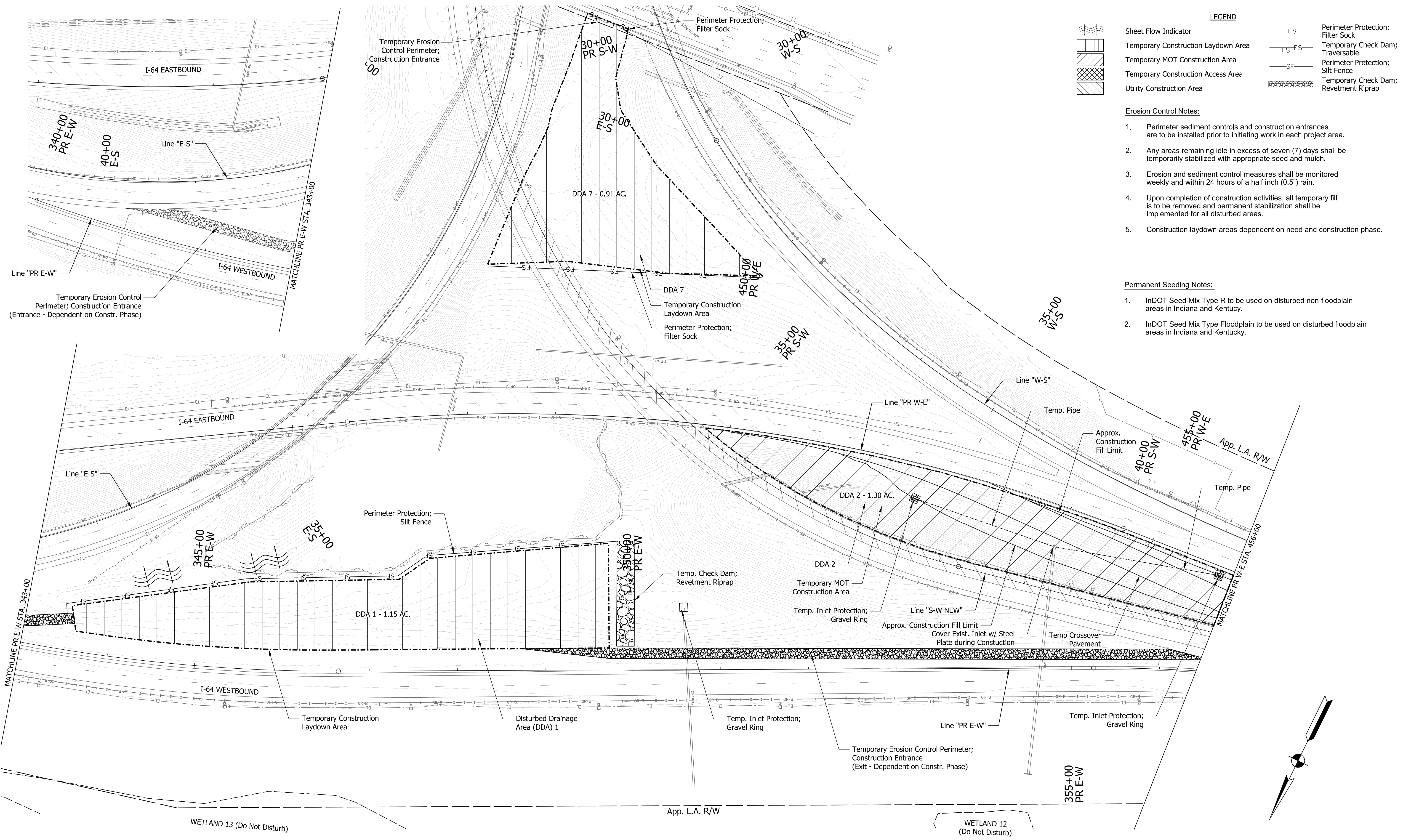
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| DESIGNED: DPS | 4/1/21 | DRAWN: DPS | 4/1/21 |
| CHECKED: JFK | 4/5/21 | CHECKED: JFK | 4/5/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL IMPACTS
STA. 112+00 TO STA. 127+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| | 7 of 28 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 09-JUL-2021 07:58 AM



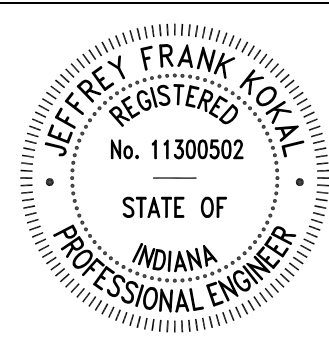
LEGEND

| | | | |
|--|-------------------------------------|--|---------------------------------------|
| | Sheet Flow Indicator | | Perimeter Protection; Filter Sock |
| | Temporary Construction Laydown Area | | Temporary Check Dam; Traversable |
| | Temporary MOT Construction Area | | Perimeter Protection; Silt Fence |
| | Temporary Construction Access Area | | Temporary Check Dam; Retention Riprap |
| | Utility Construction Area | | |

- Erosion Control Notes:**
1. Perimeter sediment controls and construction entrances are to be installed prior to initiating work in each project area.
 2. Any areas remaining idle in excess of seven (7) days shall be temporarily stabilized with appropriate seed and mulch.
 3. Erosion and sediment control measures shall be monitored weekly and within 24 hours of a half inch (0.5") rain.
 4. Upon completion of construction activities, all temporary fill is to be removed and permanent stabilization shall be implemented for all disturbed areas.
 5. Construction laydown areas dependent on need and construction phase.

- Permanent Seeding Notes:**
1. InDOT Seed Mix Type R to be used on disturbed non-floodplain areas in Indiana and Kentucky.
 2. InDOT Seed Mix Type Floodplain to be used on disturbed floodplain areas in Indiana and Kentucky.

NOT FOR CONSTRUCTION

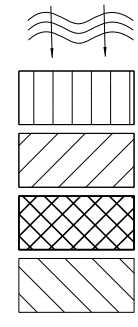


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| RECOMMENDED FOR APPROVAL | | 5-13-21 | DATE |
| DESIGNED: SKW | 4/1/21 | DRAWN: KLO | 4/1/21 |
| CHECKED: JFK | 4/6/21 | CHECKED: JFK | 4/6/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION
EROSION CONTROL PLAN
STA. 442+00 TO STA. 456+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| | 9 of 28 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 12-MAY-2021 10:08 AM

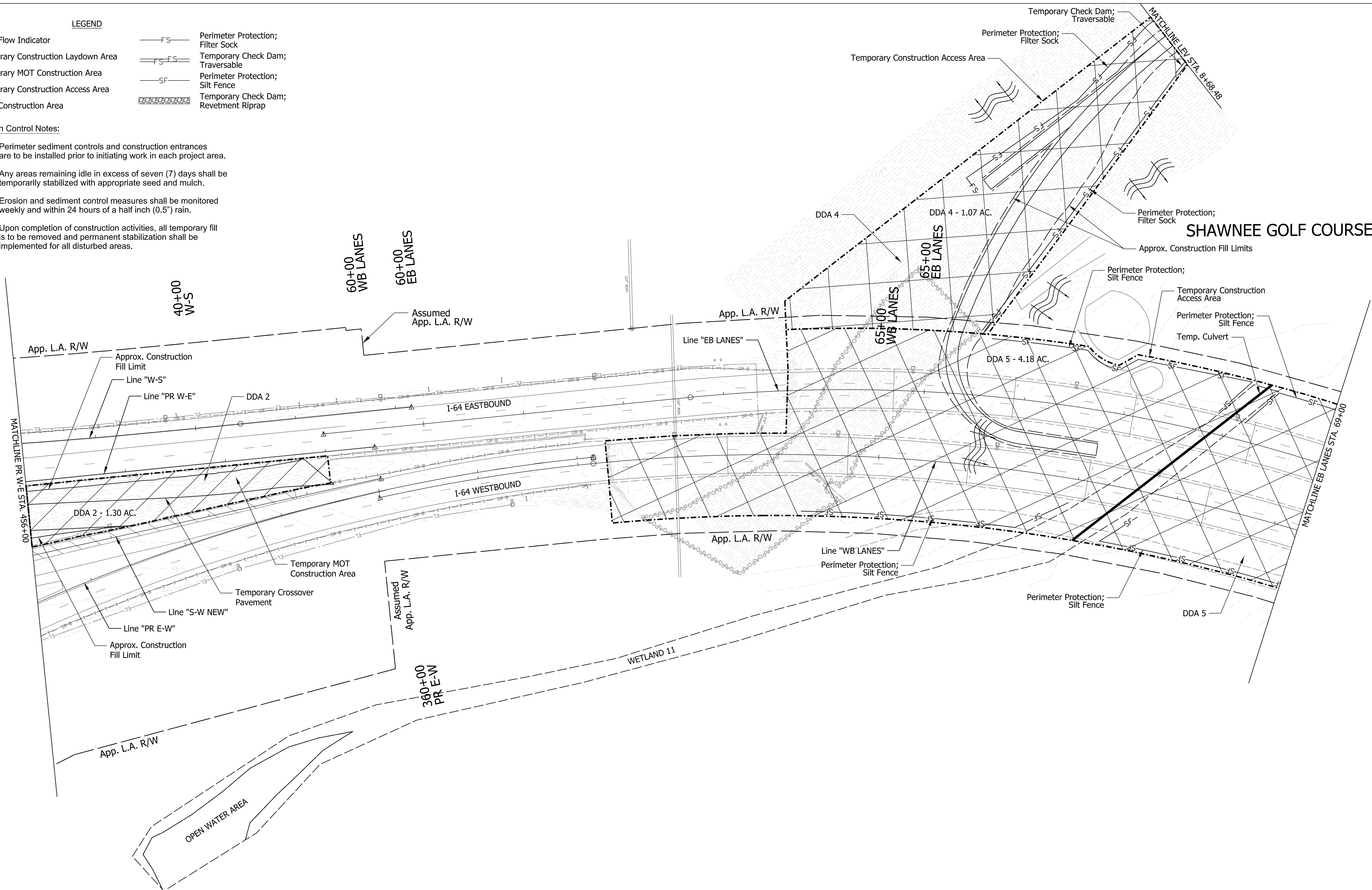


LEGEND

- Sheet Flow Indicator
- Temporary Construction Laydown Area
- Temporary MOT Construction Area
- Temporary Construction Access Area
- Utility Construction Area
- FS Perimeter Protection; Filter Sock
- FS-FS Temporary Check Dam; Traversable
- SF Perimeter Protection; Silt Fence
- FS-FS-SF Temporary Check Dam; Retention Riprap

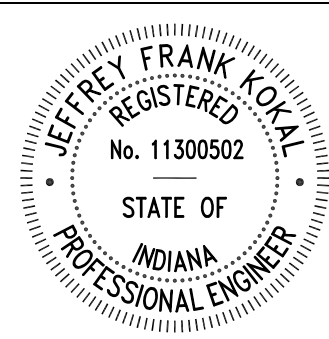
Erosion Control Notes:

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4. Upon completion of construction activities, all temporary fill is to be removed and permanent stabilization shall be implemented for all disturbed areas.



SHAWNEE GOLF COURSE

NOT FOR CONSTRUCTION



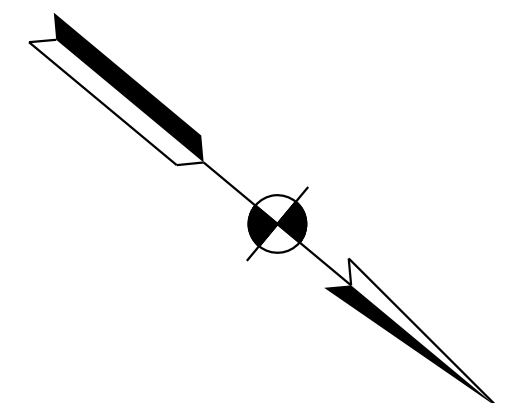
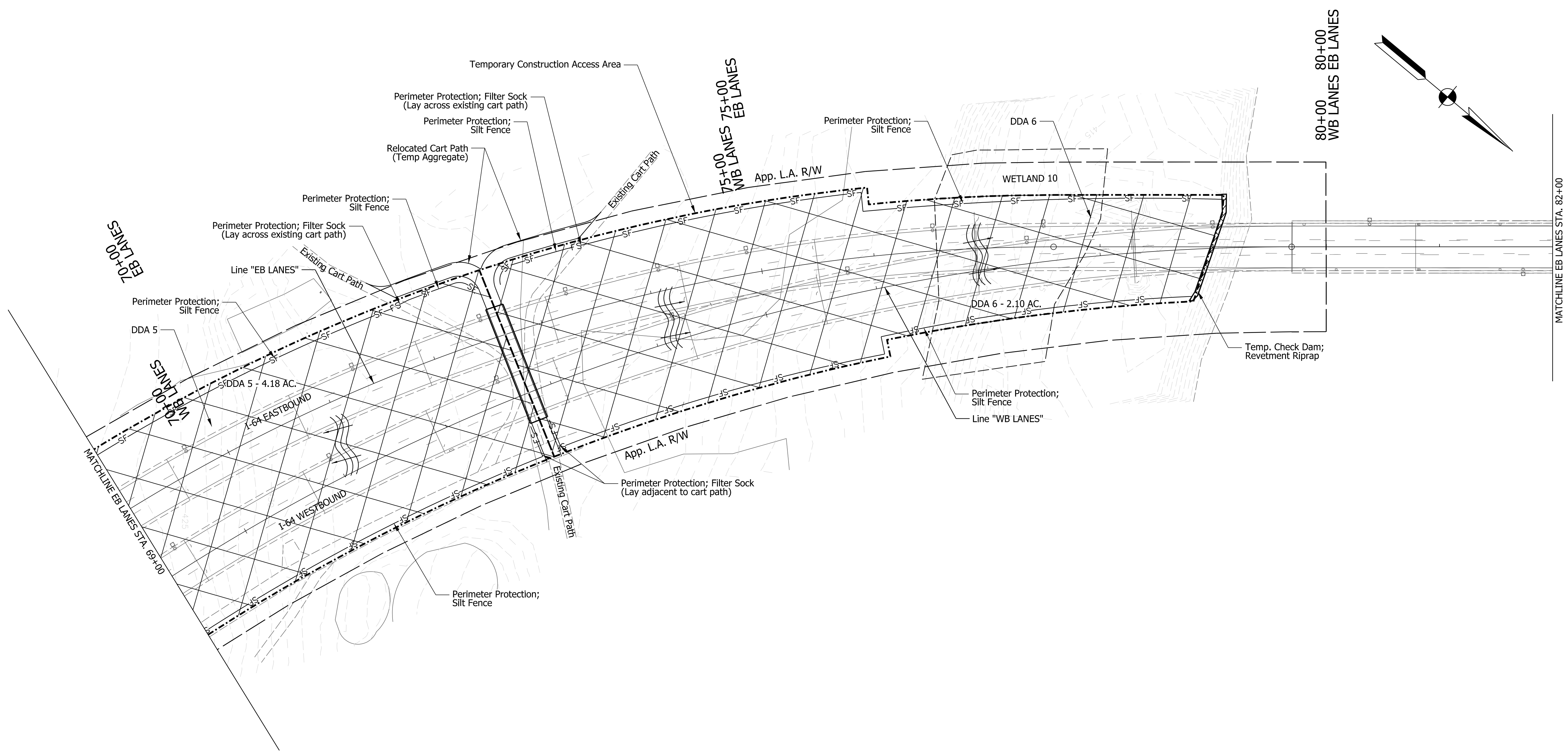
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| RECOMMENDED FOR APPROVAL | <i>[Signature]</i> 5-13-21 | DESIGN ENGINEER | DATE |
| DESIGNED: SKW | 4/1/21 | DRAWN: KLO | 4/1/21 |
| CHECKED: JFK | 4/6/21 | CHECKED: JFK | 4/6/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL PLAN
STA. 456+00 TO STA. 69+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| | 10 of 28 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

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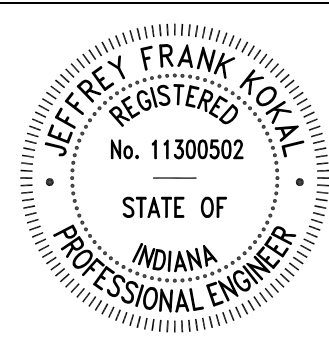


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LEGEND

| | | | |
|--|-------------------------------------|--|---------------------------------------|
| | Sheet Flow Indicator | | Perimeter Protection; Filter Sock |
| | Temporary Construction Laydown Area | | Temporary Check Dam; Traversable |
| | Temporary MOT Construction Area | | Perimeter Protection; Silt Fence |
| | Temporary Construction Access Area | | Temporary Check Dam; Revetment Riprap |
| | Utility Construction Area | | |

NOT FOR CONSTRUCTION



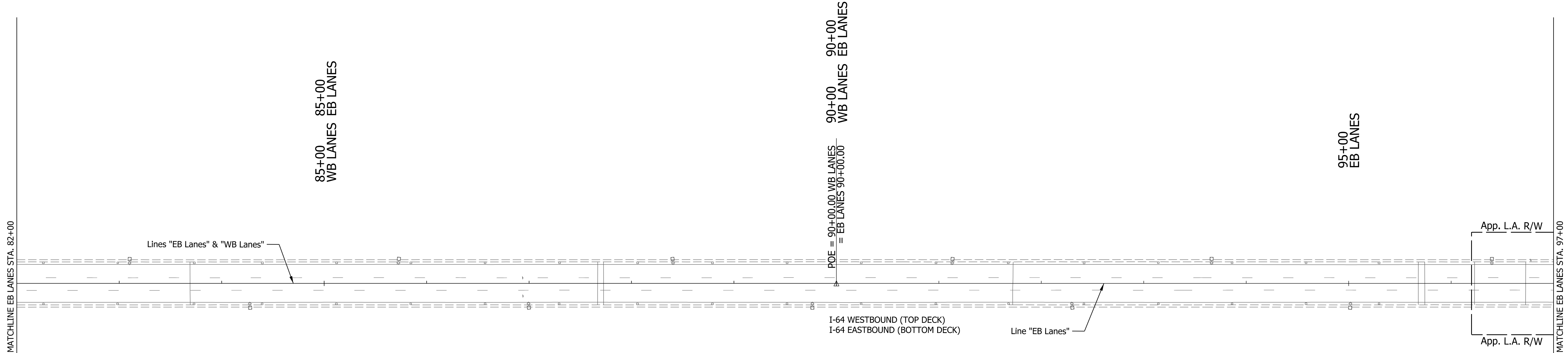
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| DESIGNED: SKW | 4/1/21 | DRAWN: KLO | 4/1/21 | |
| CHECKED: JFK | 4/6/21 | CHECKED: JFK | 4/6/21 | |

INDIANA
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL PLAN
STA. 69+00 TO STA. 82+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| | 11 of 28 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

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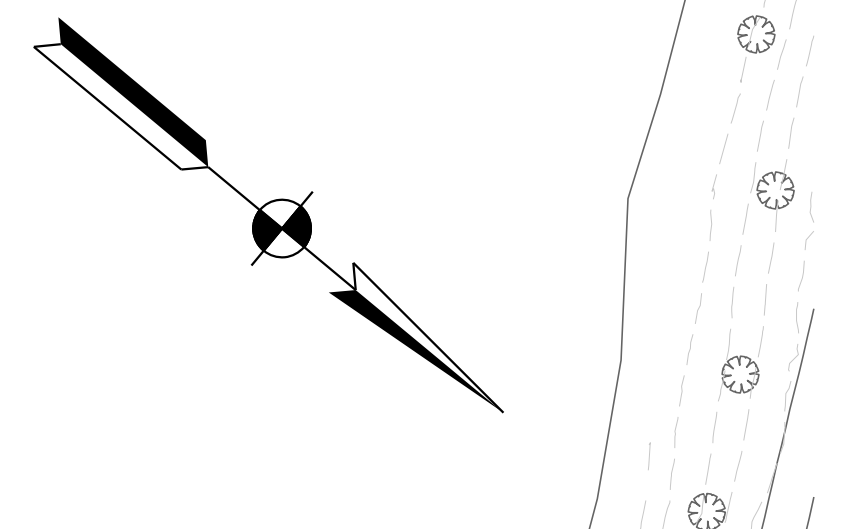


Erosion Control Notes:

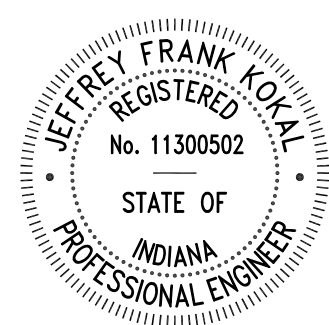
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LEGEND

| | | | |
|--|-------------------------------------|--|---------------------------------------|
| | Sheet Flow Indicator | | Perimeter Protection; Filter Sock |
| | Temporary Construction Laydown Area | | Temporary Check Dam; Traversable |
| | Temporary MOT Construction Area | | Perimeter Protection; Silt Fence |
| | Temporary Construction Access Area | | Temporary Check Dam; Revetment Riprap |
| | Utility Construction Area | | |



NOT FOR CONSTRUCTION

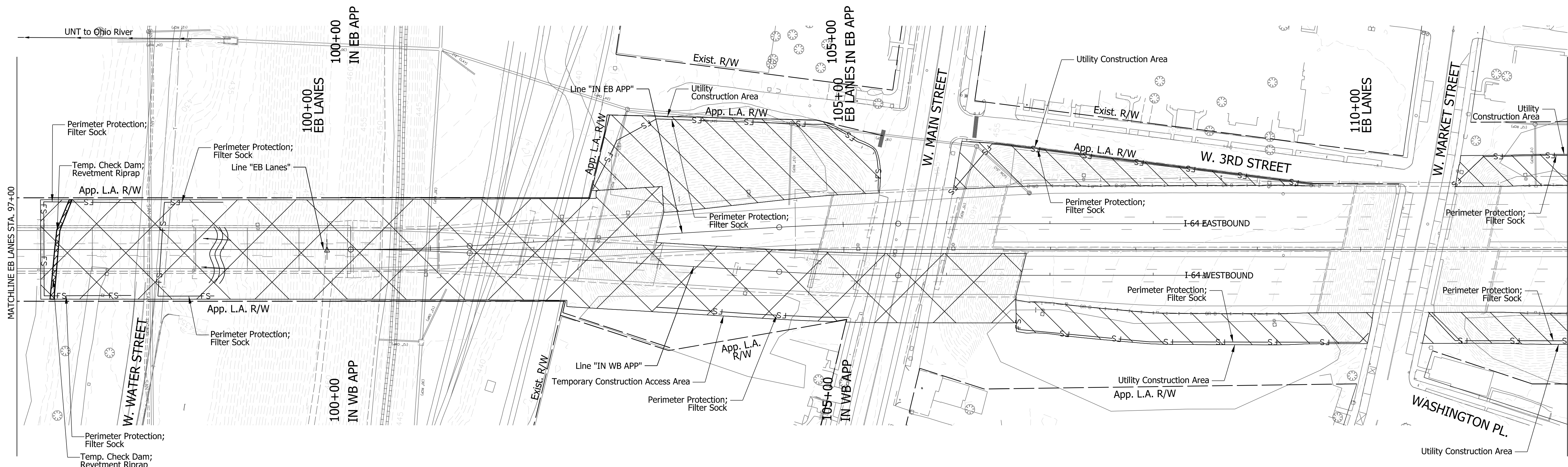


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| CHECKED: JFK | 4/6/21 | CHECKED: JFK | 4/6/21 | |

INDIANA
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL PLAN
STA. 82+00 TO STA. 97+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| | 12 of 28 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

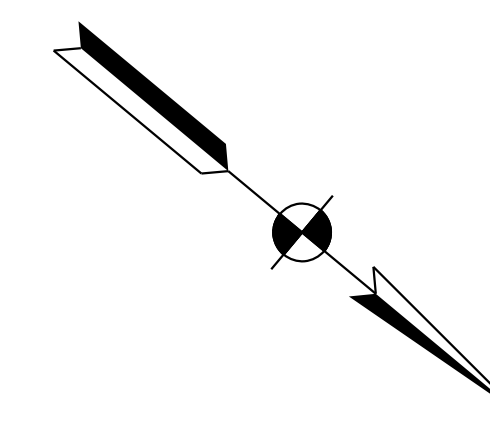


Erosion Control Notes:

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LEGEND

| | | | |
|--|-------------------------------------|--|---------------------------------------|
| | Sheet Flow Indicator | | Perimeter Protection; Filter Sock |
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| | Temporary MOT Construction Area | | Perimeter Protection; Silt Fence |
| | Temporary Construction Access Area | | Temporary Check Dam; Revetment Riprap |
| | Utility Construction Area | | |



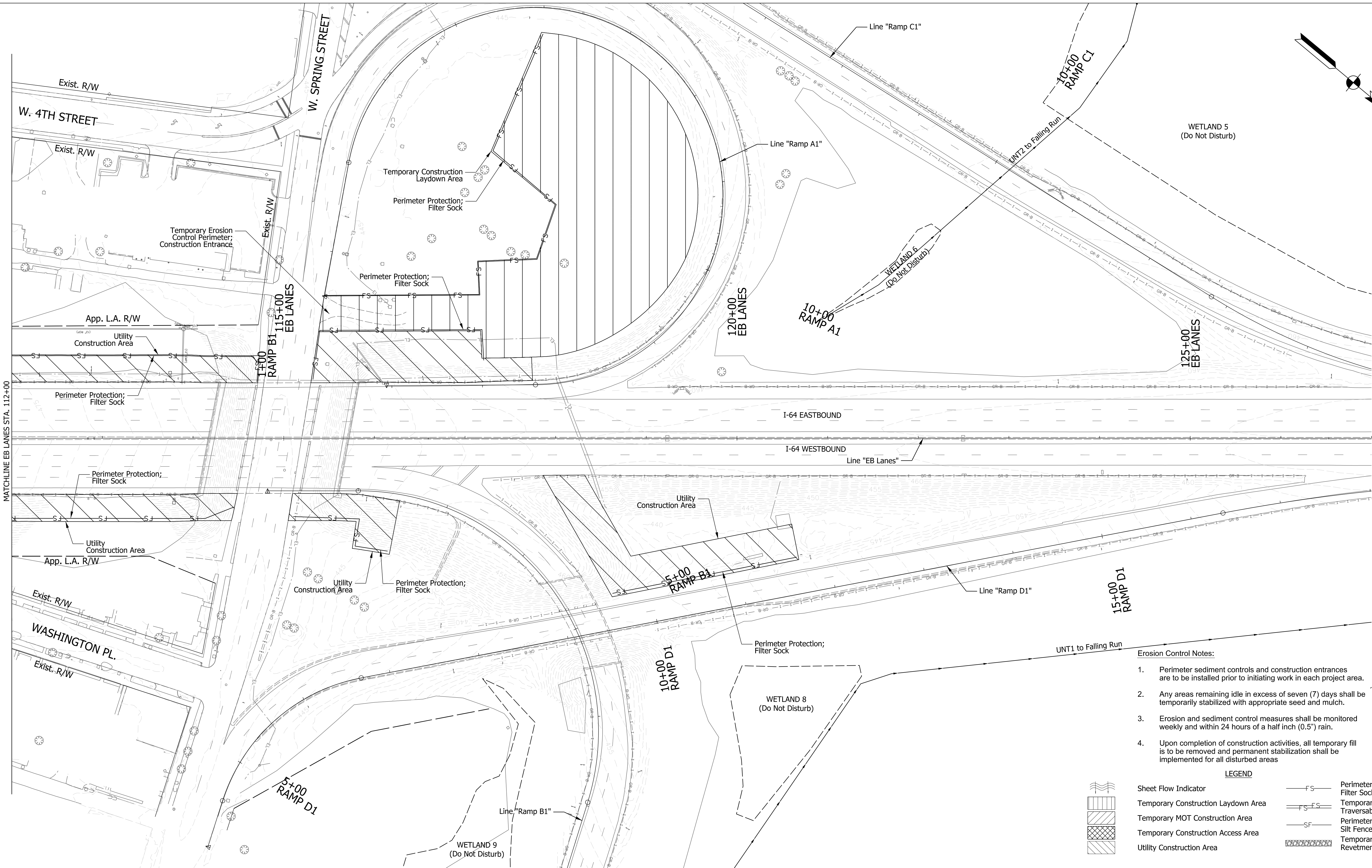
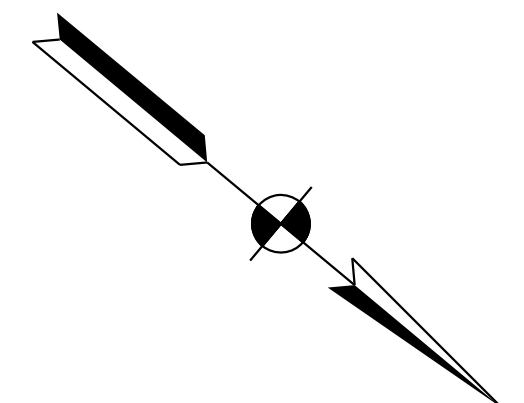
NOT FOR CONSTRUCTION



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| DESIGNED: SKW | 4/1/21 | DRAWN: KLO | 4/1/21 | |
| CHECKED: JFK | 4/6/21 | CHECKED: JFK | 4/6/21 | |

INDIANA
DEPARTMENT OF TRANSPORTATION
EROSION CONTROL PLAN
STA. 97+00 TO STA. 112+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| | 13 of 28 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

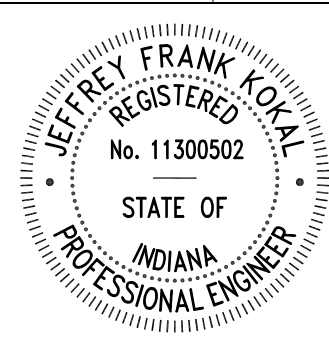


- Erosion Control Notes:**
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LEGEND

| | | | |
|--|-------------------------------------|--|---------------------------------------|
| | Sheet Flow Indicator | | Perimeter Protection; Filter Sock |
| | Temporary Construction Laydown Area | | Temporary Check Dam; Traversable |
| | Temporary MOT Construction Area | | Perimeter Protection; Silt Fence |
| | Temporary Construction Access Area | | Temporary Check Dam; Retention Riprap |
| | Utility Construction Area | | |

NOT FOR CONSTRUCTION



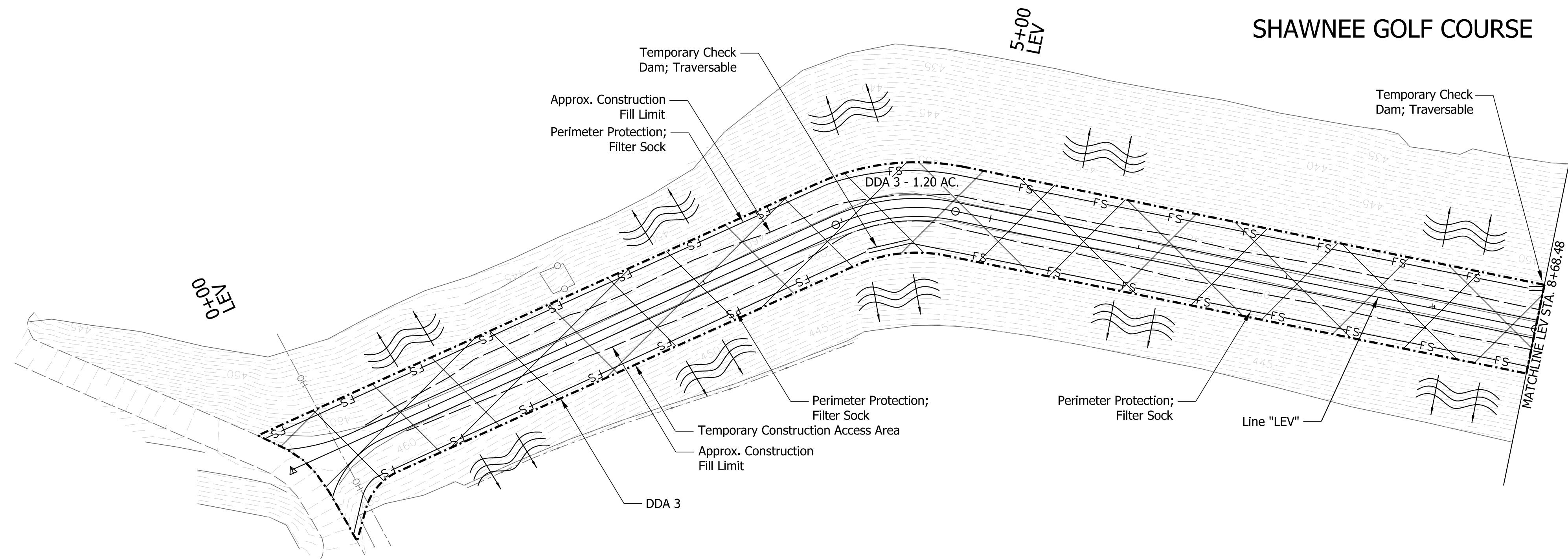
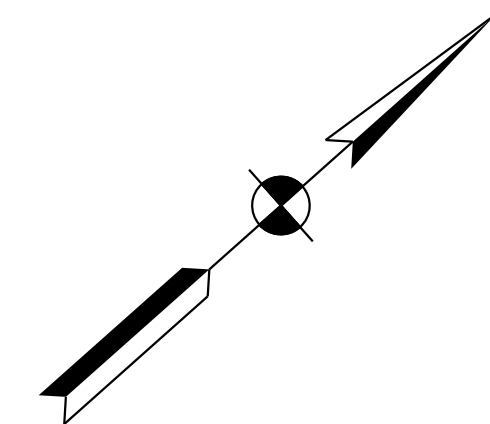
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| CHECKED: JFK | 4/6/21 | CHECKED: JFK | 4/6/21 | |

INDIANA
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL PLAN
STA. 112+00 TO STA. 127+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| | 14 of 28 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

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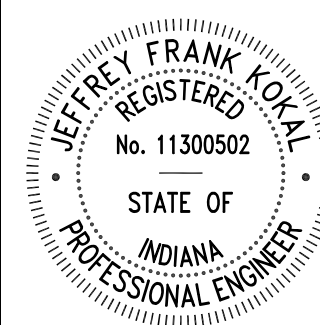
Erosion Control Notes:

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LEGEND

| | | | |
|--|-------------------------------------|--|---------------------------------------|
| | Sheet Flow Indicator | | Perimeter Protection; Filter Sock |
| | Temporary Construction Laydown Area | | Temporary Check Dam; Traversable |
| | Temporary MOT Construction Area | | Perimeter Protection; Silt Fence |
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| | Utility Construction Area | | |

NOT FOR CONSTRUCTION

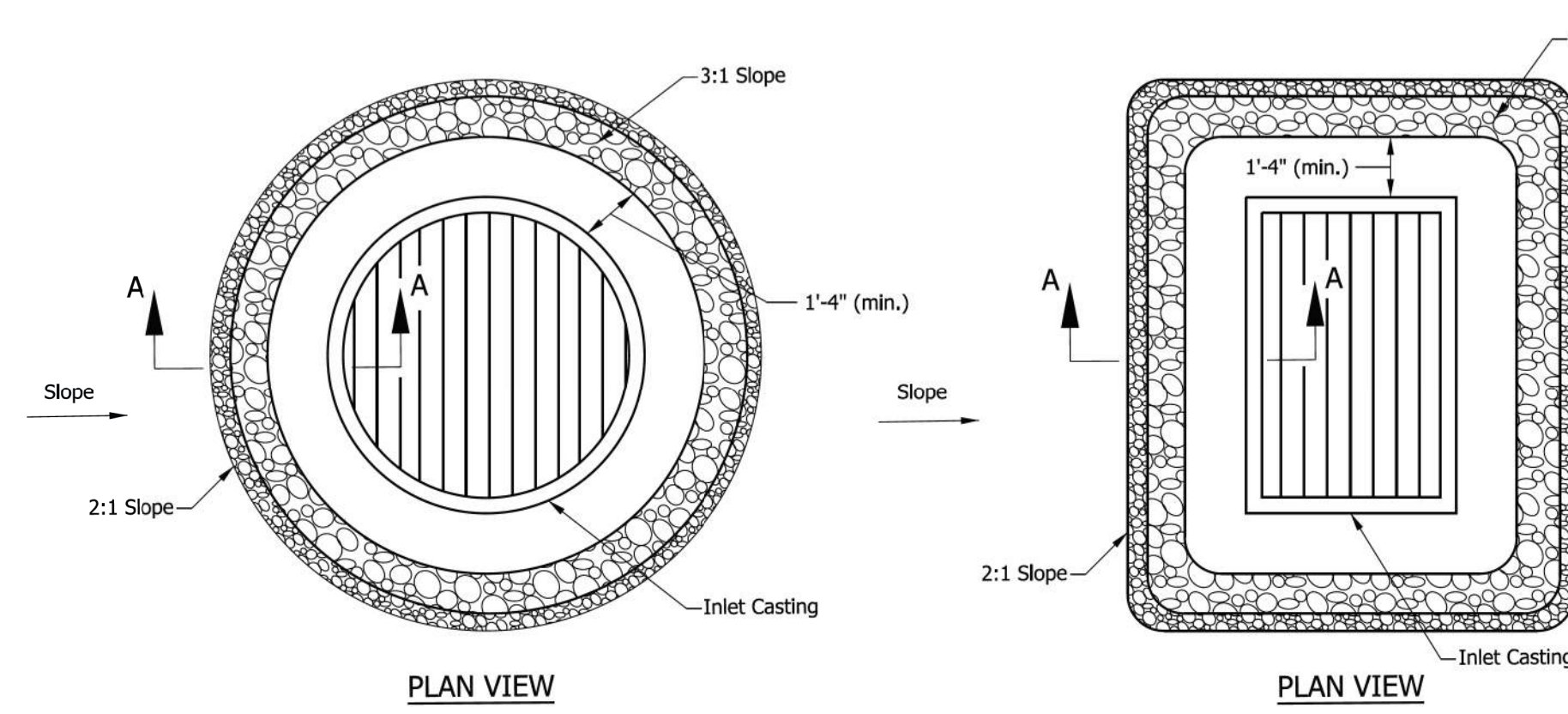


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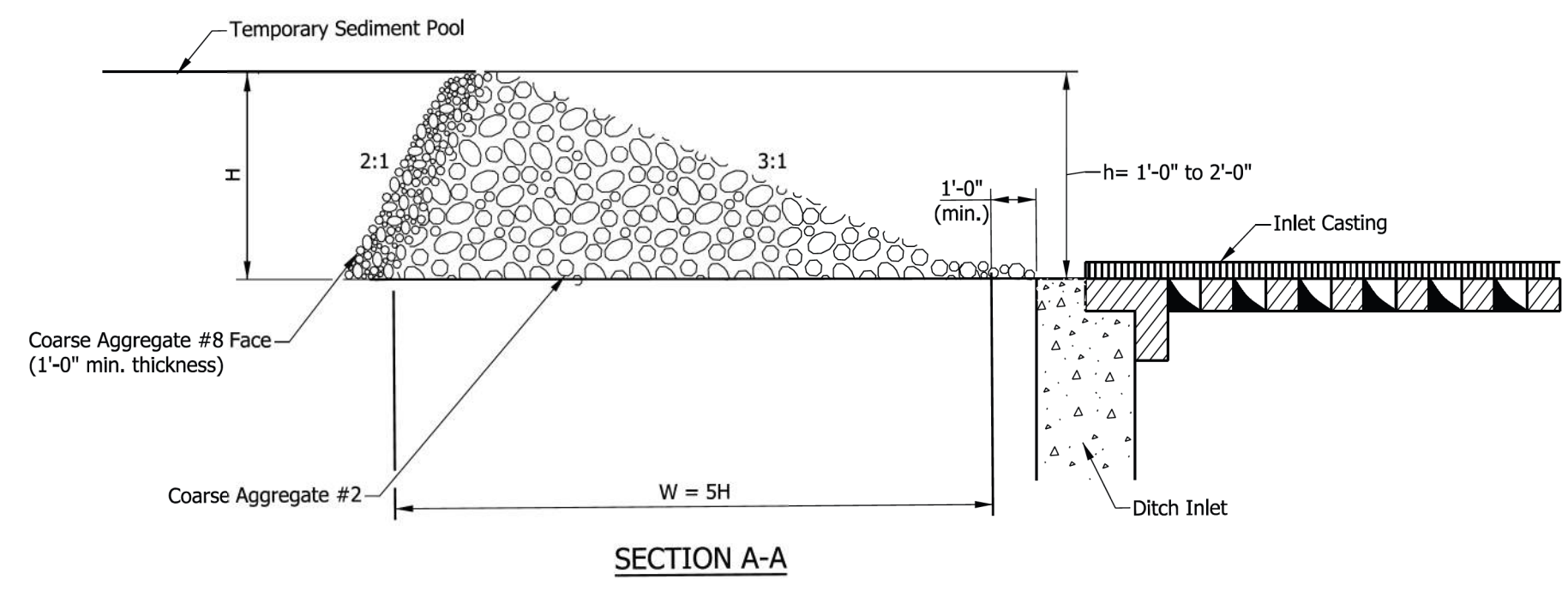
INDIANA
DEPARTMENT OF TRANSPORTATION

**EROSION CONTROL PLAN
STA. 0+00 TO STA. 8+68.48**

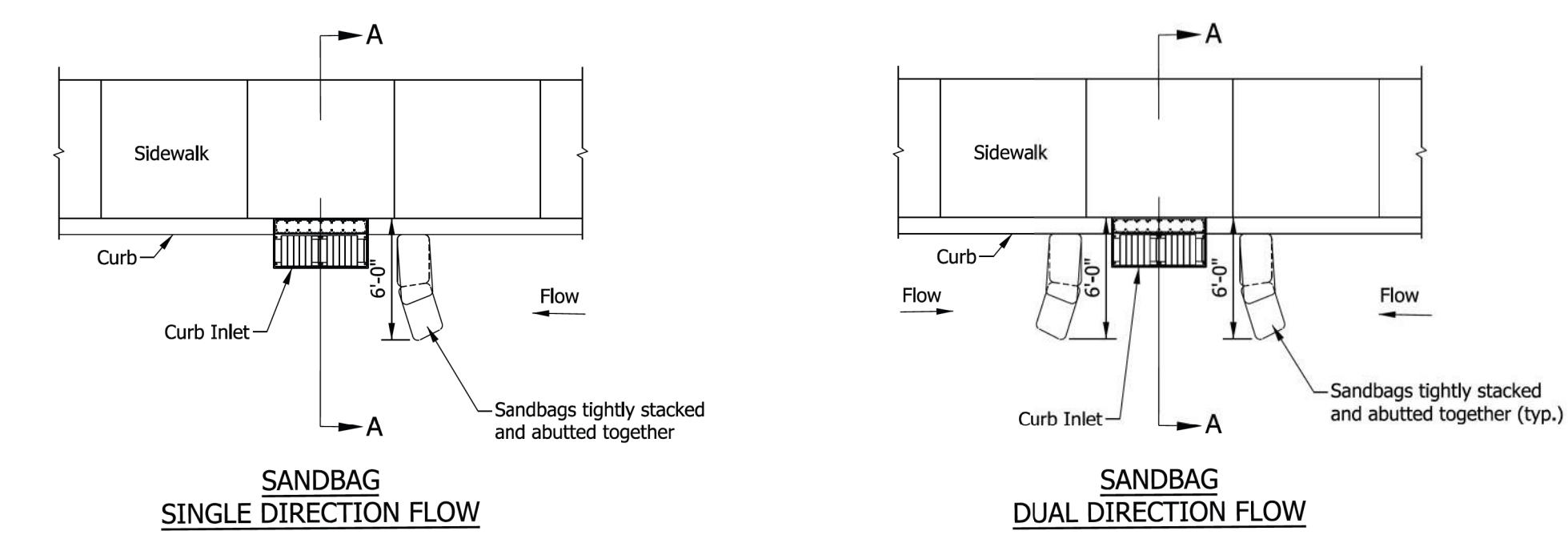
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|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| | 15 of 28 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |



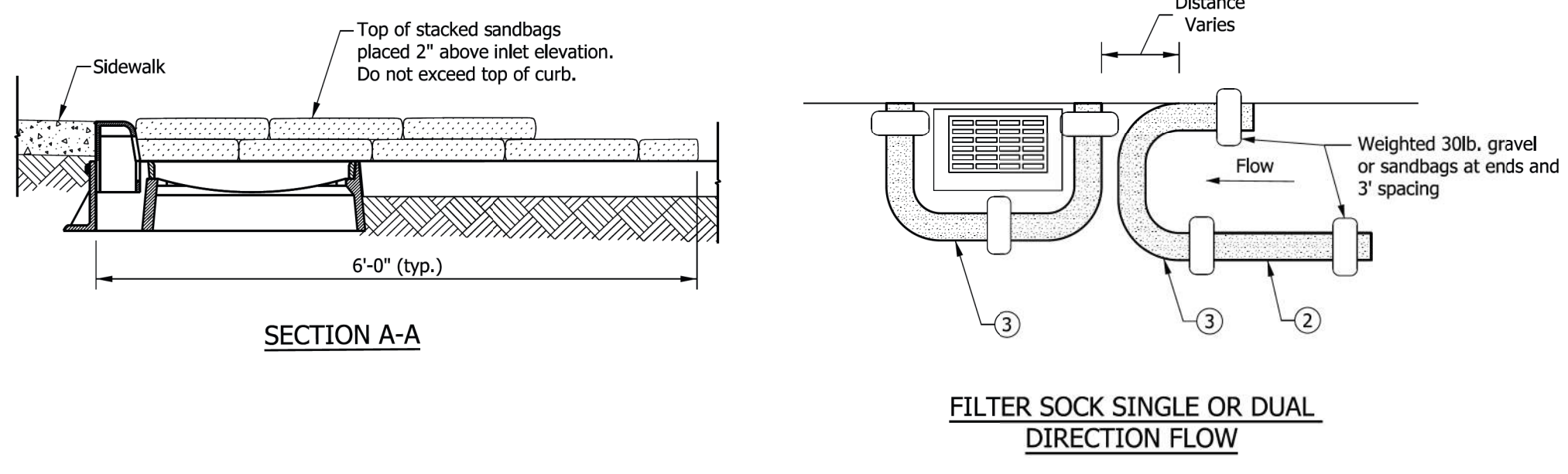
- NOTES:**
1. $H = h + 0.25$, ft
 2. Weight of coarse aggregate #2, Tons: $(6.28/27)(0.67^2 + 2.5H)(1 + 3H + \frac{1}{2} \text{inlet width})(0.6)$
 3. Weight of coarse aggregate #8, Tons: $(6.28/27)(H)(1 + 4H + \frac{1}{2} \text{inlet width})(0.6)$



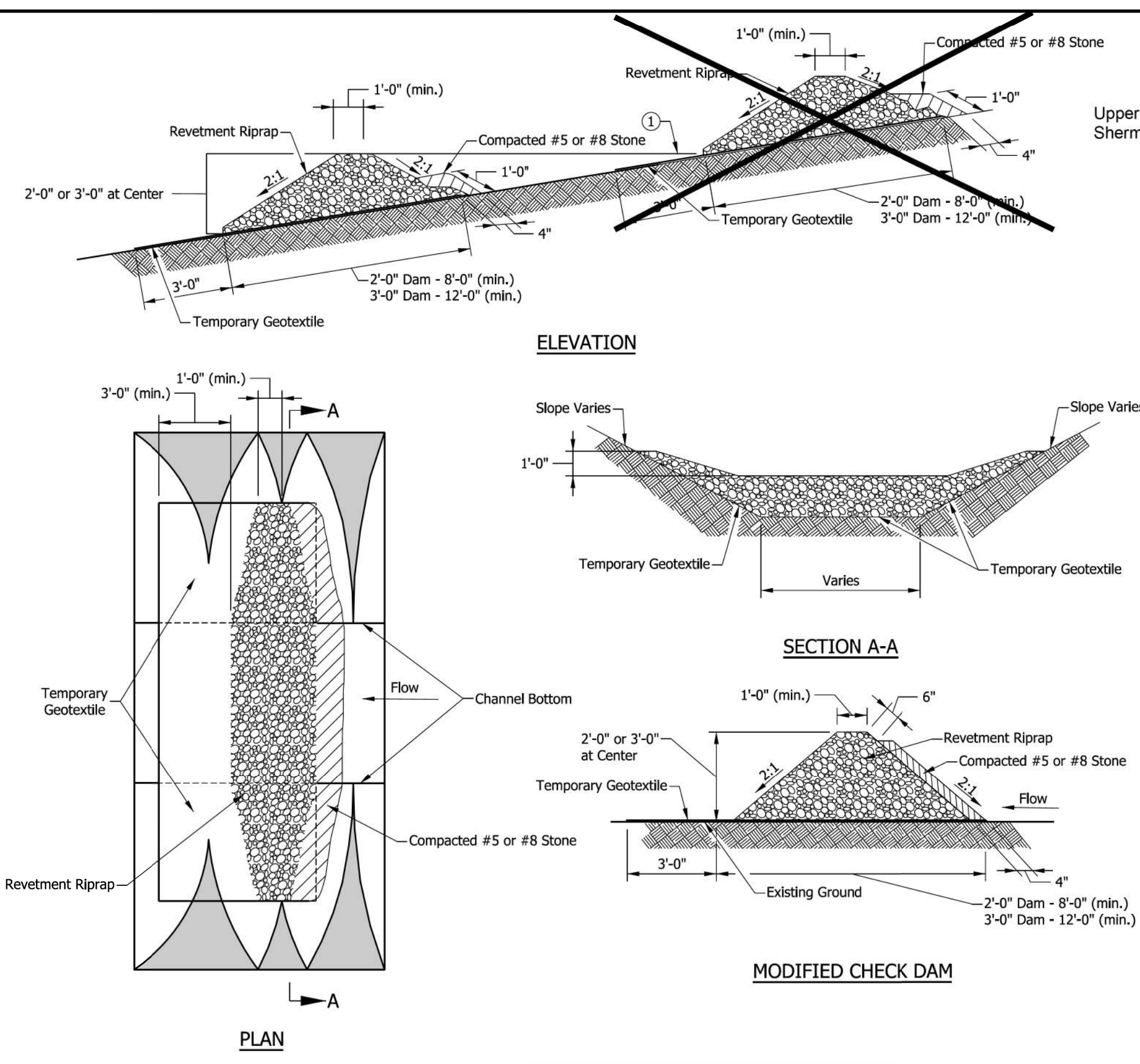
INDIANA DEPARTMENT OF TRANSPORTATION
 TEMPORARY INLET PROTECTION, GRAVEL RING
 SEPTEMBER 2019
 STANDARD DRAWING NO. E 205-TECD-03



- NOTES:**
1. Curb inlet protection shall be used within a lane or shoulder only when closed to traffic. Curb inlet protection shall not cause water to encroach upon a lane open to traffic.
 2. May be used in conjunction with drain inlet protection as a best management practice (BMP) in combination with bag or drop inlet protection inserts.
 3. Inlet protection shall be used within a lane or shoulder only when closed to traffic. Inlet protection shall not cause water to encroach upon a lane open to traffic.



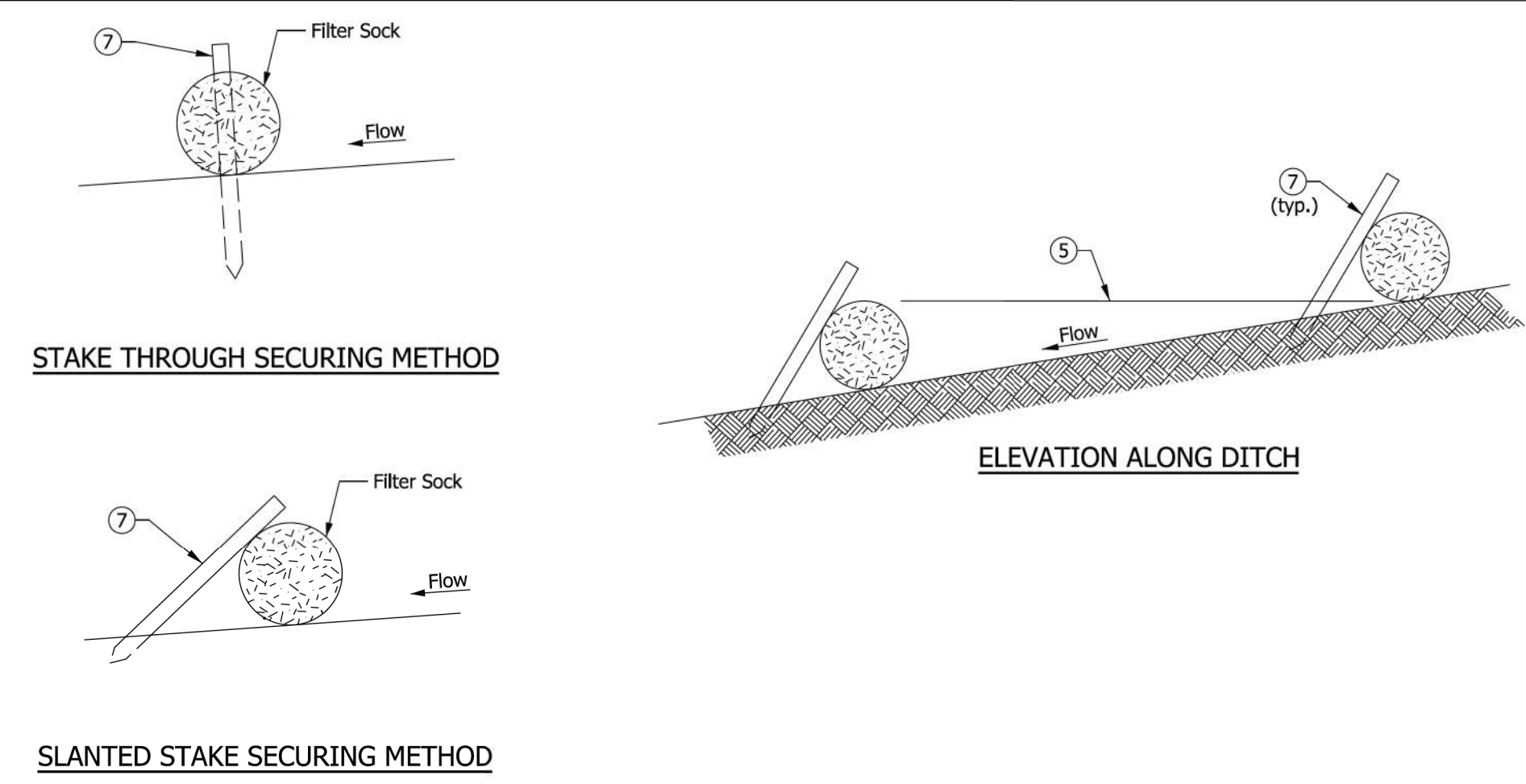
INDIANA DEPARTMENT OF TRANSPORTATION
 TEMPORARY CURB INLET PROTECTION
 SEPTEMBER 2019
 STANDARD DRAWING NO. E 205-TECD-05



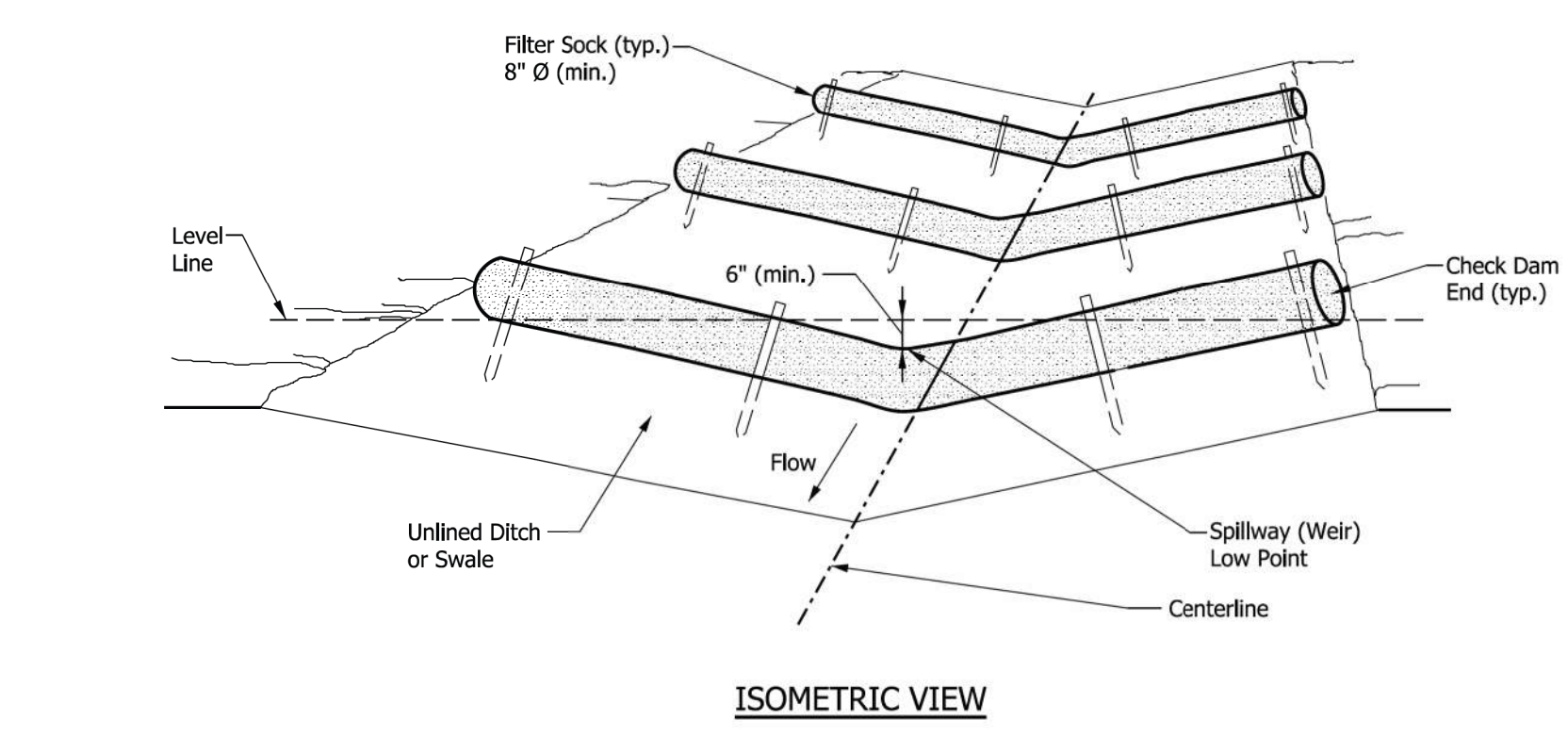
Upper or second check dam not required for the Sherman Minton Corridor Project.

Note:
 Use one riprap check dam in locations as shown on the plan sheets for the Sherman Minton Corridor Project.

INDIANA DEPARTMENT OF TRANSPORTATION
 TEMPORARY CHECK DAM, REVETMENT RIPRAP
 SEPTEMBER 2019
 STANDARD DRAWING NO. E 205-TECD-06

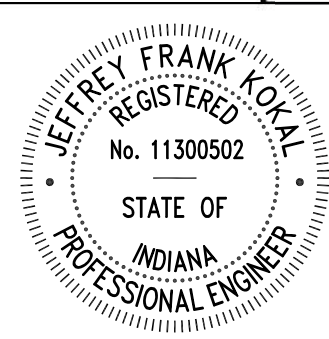


- NOTES:**
1. Fiber roll may be substituted for filter sock.
 2. Check dams shall be placed perpendicular to the flow of water.
 3. Check dam ends shall be positioned as shown such that storm water flows over the weir low point and does not flow around the ends.
 4. Check dams shall remain in place until all upstream areas become stable.
 5. Check dams shall be spaced such that the top of the downstream check dam is at the same elevation as the toe of the adjacent upstream check dam.
 6. Filter sock shall be secured as shown or in accordance with the manufacturer's instructions.
 7. Stake angle and length shall be sufficient to wedge filter sock to the ground to prevent movement and undercutting.



INDIANA DEPARTMENT OF TRANSPORTATION
 TEMPORARY CHECK DAM, TRAVERSABLE, LOW PROFILE
 SEPTEMBER 2019
 STANDARD DRAWING NO. E 205-TECD-07

NOT FOR CONSTRUCTION

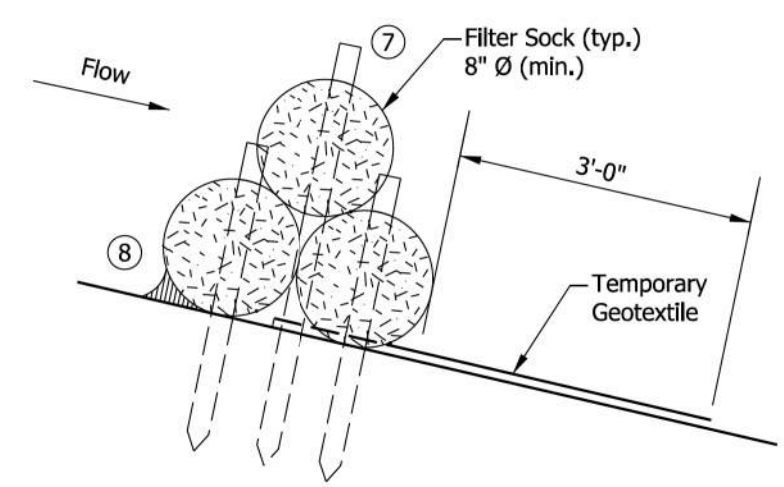


| | | | |
|--------------------------|----------------------------|-----------------|--------|
| RECOMMENDED FOR APPROVAL | <i>[Signature]</i> 5-13-21 | DESIGN ENGINEER | DATE |
| DESIGNED: SKW | 4/1/21 | DRAWN: KLO | 4/1/21 |
| CHECKED: JFK | 4/6/21 | CHECKED: JFK | 4/6/21 |

INDIANA DEPARTMENT OF TRANSPORTATION
 EROSION CONTROL PLAN
 STANDARD DRAWINGS

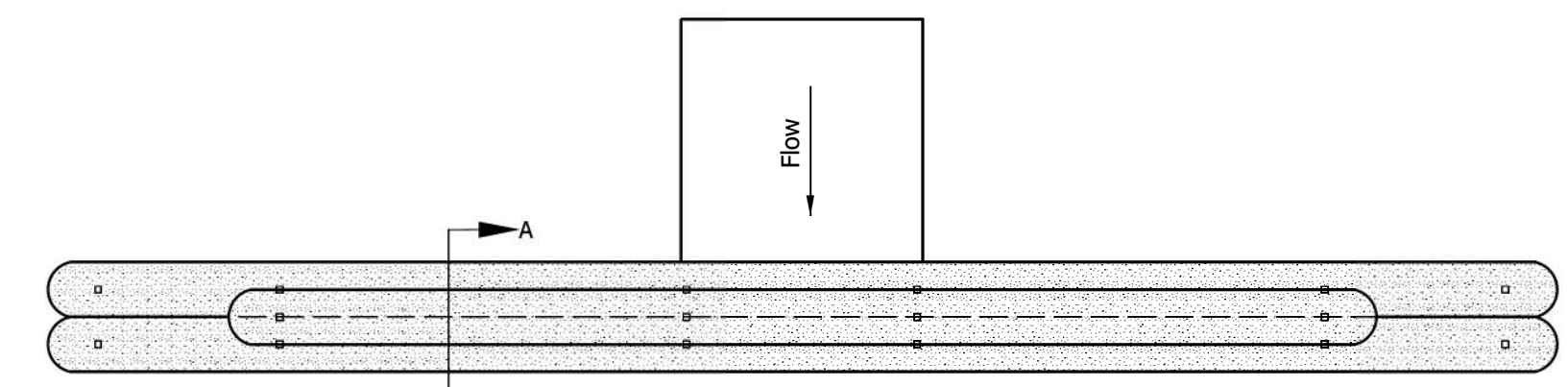
| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| N/A | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| | 16 of 28 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

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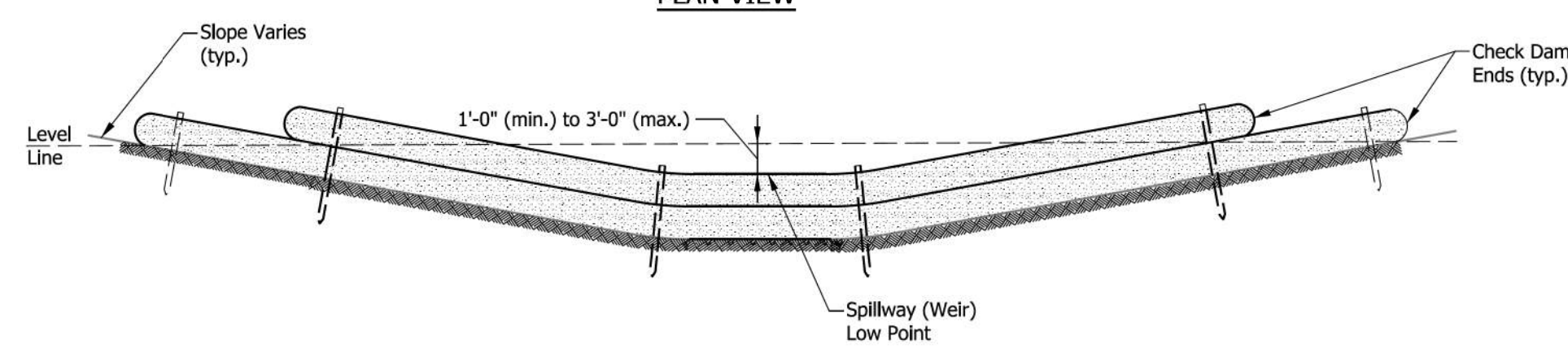


SECTION A-A

- NOTES:**
1. Fiber roll may be substituted for filter sock.
 2. Check dams shall be placed perpendicular to the flow of water.
 3. Check dam ends shall be positioned as shown such that storm water flows over the weir low point and does not flow around the ends.
 4. Check dams shall remain in place until all upstream areas become stable.
 5. Check dams shall be spaced such that the top of the downstream check dam is at the same elevation as the toe of the adjacent upstream check dam.
 6. Filter sock shall be secured as shown or in accordance with the manufacturer's instructions.
 7. Stake length shall be sufficient to wedge filter sock to the ground to prevent movement and undercutting.
 8. When undercutting is identified, compacted #5 or #8 stone shall be placed as shown.

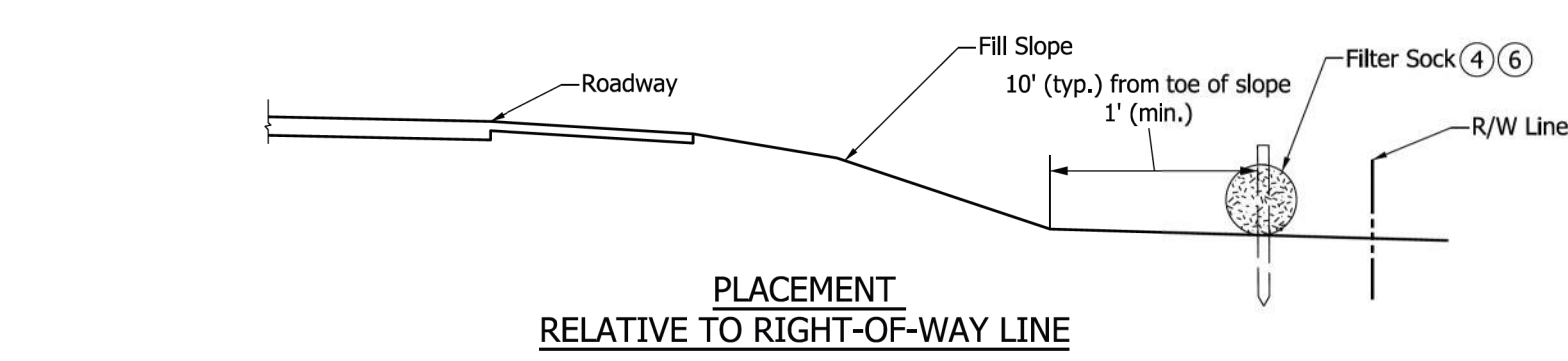


TRAVERSABLE CHECK DAM
PLAN VIEW

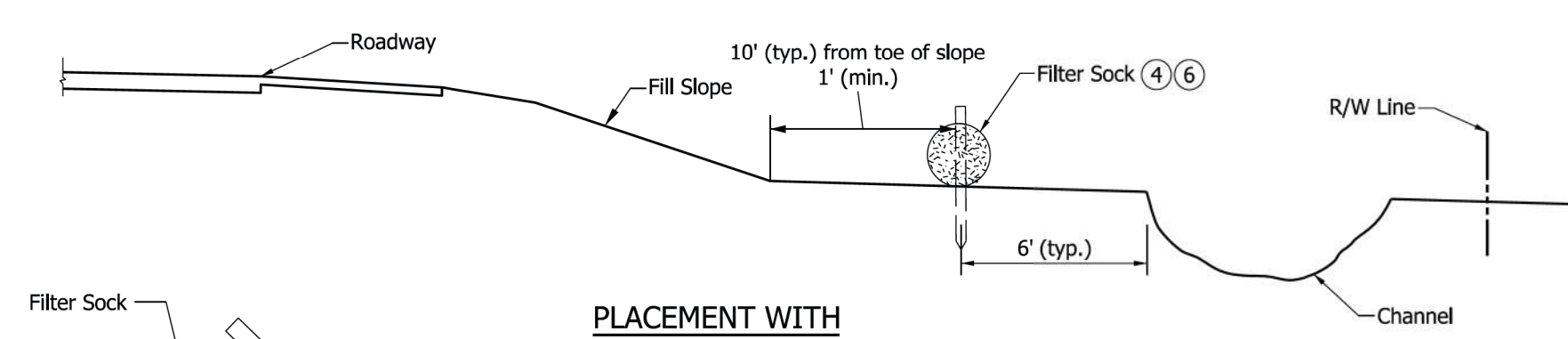


TRAVERSABLE CHECK DAM
ELEVATION

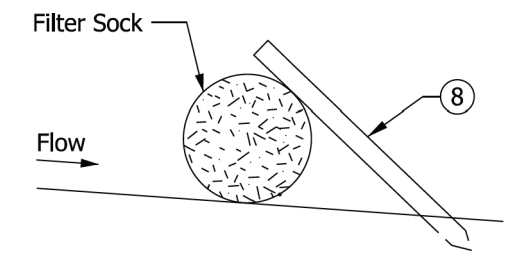
INDIANA DEPARTMENT OF TRANSPORTATION
 TEMPORARY CHECK DAM,
 TRAVERSABLE
 SEPTEMBER 2019
 STANDARD DRAWING NO. E 205-TECD-08



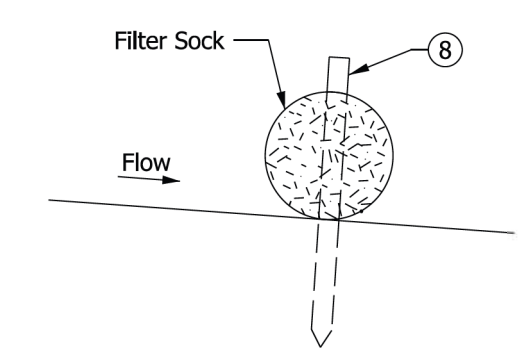
PLACEMENT
RELATIVE TO RIGHT-OF-WAY LINE



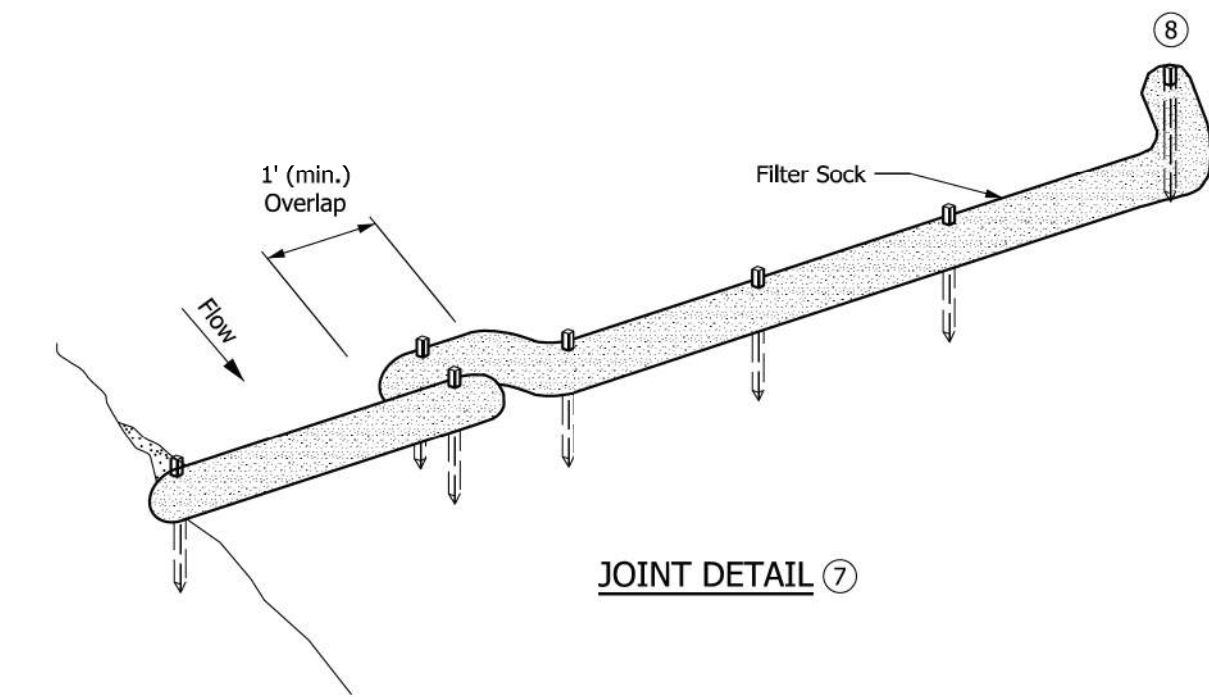
PLACEMENT WITH
CHANNEL IN RIGHT OF WAY



SLANTED STAKE
SECURING METHOD



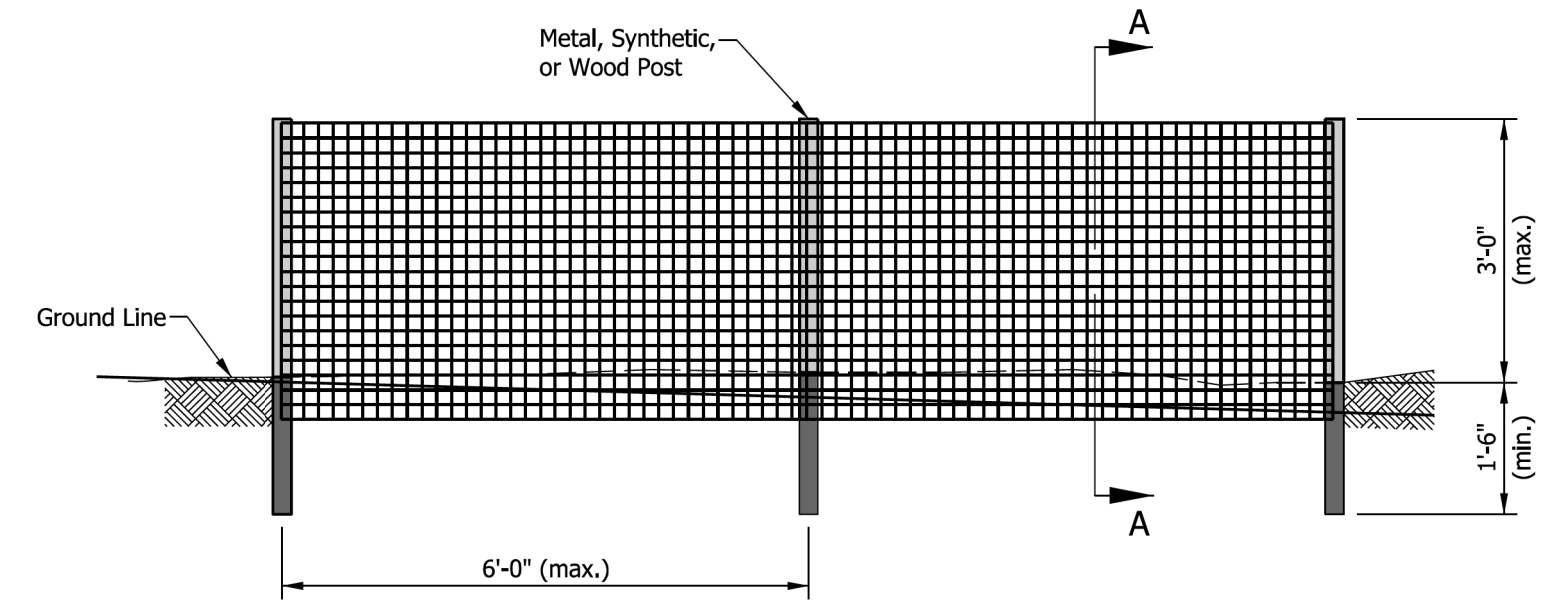
STAKE THROUGH
SECURING METHOD



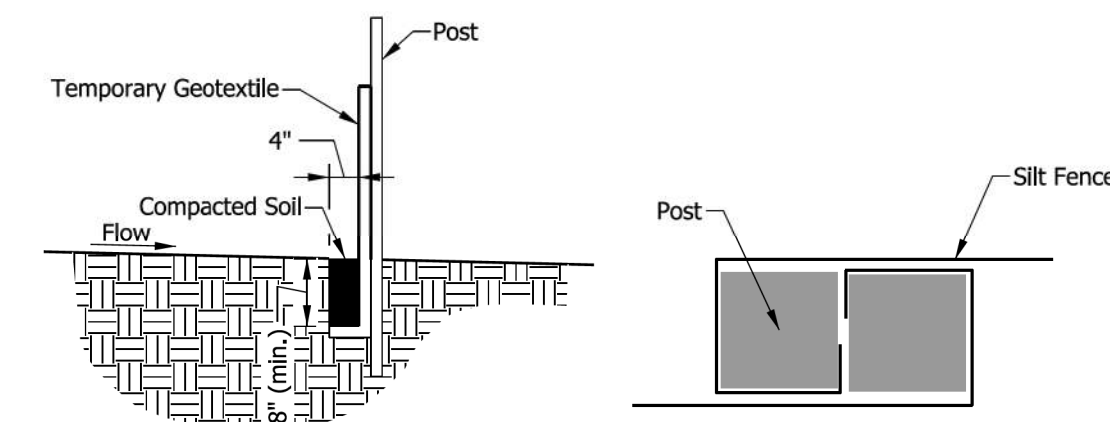
JOINT DETAIL

- NOTES:**
1. Fiber roll may be substituted for filter sock.
 2. Filter sock shall be installed as shown or in accordance with manufacturer's recommendations.
 3. Filter sock diameter shall be as required by design based on watershed area. Use 6" diameter filter sock for Sherman Minton Corridor Project.
 4. Filter sock shall be placed perpendicular to the flow of water.
 5. Filter sock shall be secured as shown or in accordance with the manufacturer's instructions.
 6. Filter sock does not require staking when fill slope is less than or equal to 12%, except when fill slope is below the Q_{100} water surface elevation and flood prone area.
 7. Filter socks shall be secured in locations below the Q_{100} water surface elevation and flood prone location. Filter sock end shall be secured using the stake through method. Intermediate points may be secured using either the stake through or slanted stake method.
 8. Stake angle and length shall be sufficient to wedge filter sock to the ground to prevent movement and undercutting.

INDIANA DEPARTMENT OF TRANSPORTATION
 PERIMETER PROTECTION,
 FILTER SOCK
 SEPTEMBER 2019
 STANDARD DRAWING NO. E 205-TECD-10



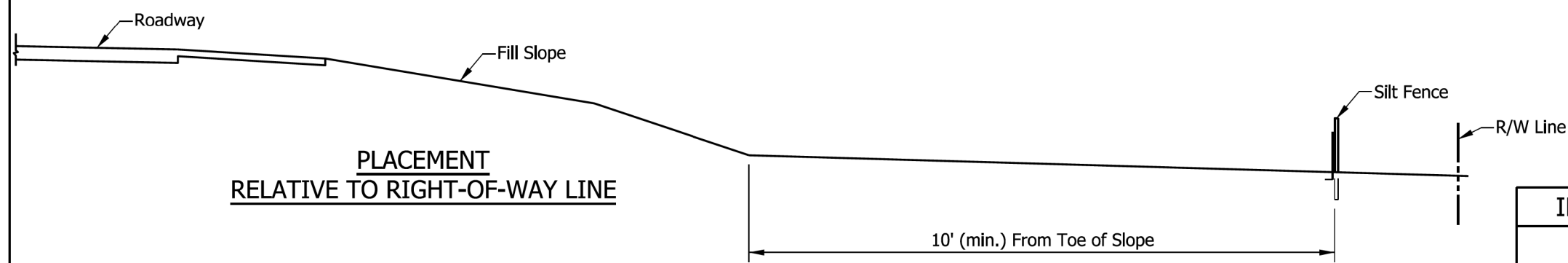
ELEVATION



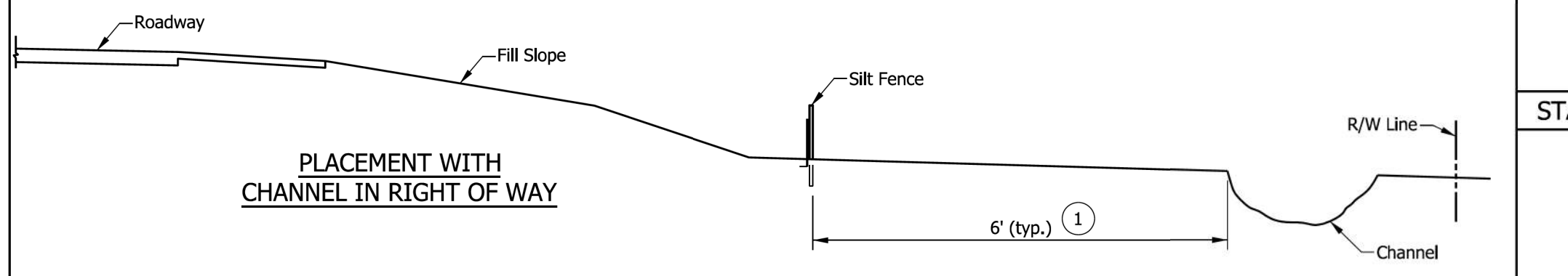
SECTION A-A

JOINT DETAIL
PLAN VIEW

- NOTES:**
1. Dimensions will vary based on right-of-way availability. Silt fence shall be placed as close as possible to the edge of construction limits.
 2. The spacing of the tiebacks shall equal the spacing of the posts. Additional post depth or tiebacks may be required in unstable soils.
 3. Filter Sock shall be used instead of silt fence at or below Q_{100} .

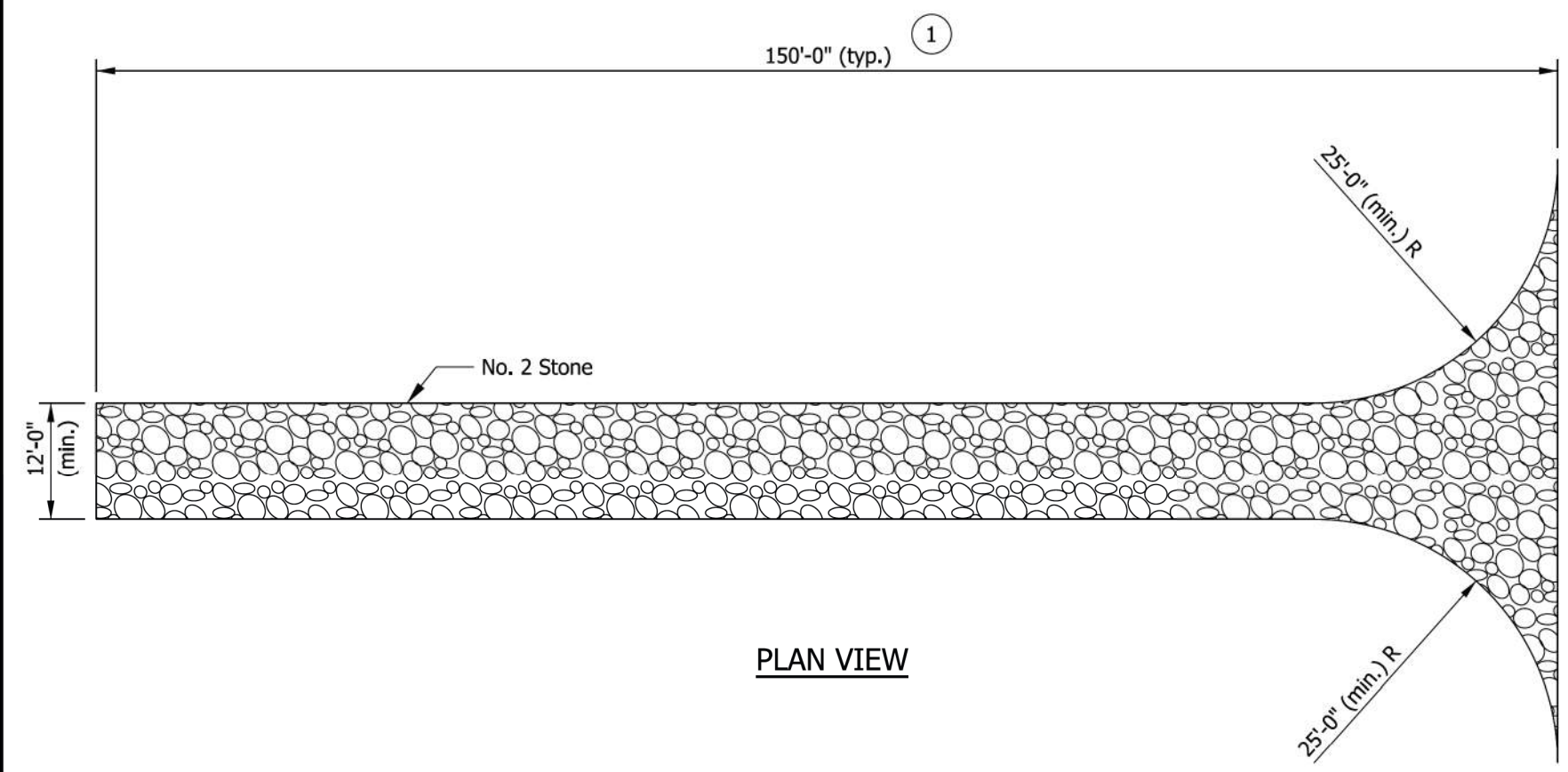


PLACEMENT
RELATIVE TO RIGHT-OF-WAY LINE

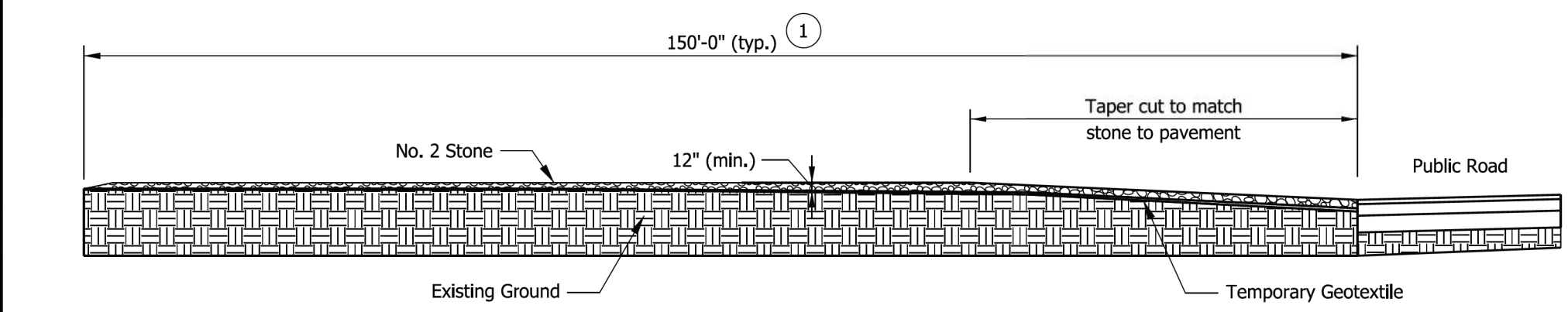


PLACEMENT WITH
CHANNEL IN RIGHT OF WAY

INDIANA DEPARTMENT OF TRANSPORTATION
 PERIMETER PROTECTION,
 SILT FENCE
 SEPTEMBER 2019
 STANDARD DRAWING NO. E 205-TECD-11



PLAN VIEW

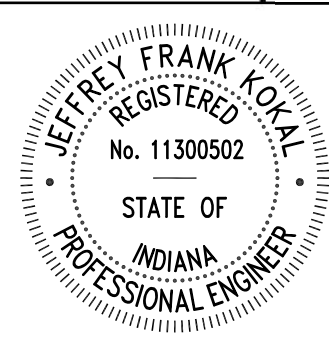


PROFILE VIEW

- NOTE:**
1. May be reduced as justified by site conditions, but shall not be less than 50 ft.

INDIANA DEPARTMENT OF TRANSPORTATION
 TEMPORARY EROSION CONTROL PERIMETER
 CONSTRUCTION ENTRANCE
 SEPTEMBER 2019
 STANDARD DRAWING NO. E 205-TECD-12

NOT FOR CONSTRUCTION



| | | | |
|--------------------------|----------------------------|-----------------|--------|
| RECOMMENDED FOR APPROVAL | <i>[Signature]</i> 5-13-21 | DESIGN ENGINEER | DATE |
| DESIGNED: SKW | 4/1/21 | DRAWN: KLO | 4/1/21 |
| CHECKED: JFK | 4/6/21 | CHECKED: JFK | 4/6/21 |

INDIANA DEPARTMENT OF TRANSPORTATION
 EROSION CONTROL PLAN
 STANDARD DRAWINGS

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| N/A | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
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| CONTRACT | PROJECT |
| B-40719 | 1702255 |

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| | |
|----------|-------------|
| PROJECT | DESIGNATION |
| 1702255 | 1702255 |
| CONTRACT | |
| B-40719 | |

INDIANA DEPARTMENT OF TRANSPORTATION



MAINTENANCE OF TRAFFIC PLANS BUILDABLE UNIT 2B

ROUTE: I-64 AT: SHERMAN MINTON BRIDGE
PROJECT NO. 1702255 P.E.

NO ADDITIONAL RIGHT OF WAY
REQUIRED FOR THIS PROJECT

R/W
1702255 CONST.

Project Description: Ramp Modifications for the Rehabilitation of Sherman Minton Bridge
over Ohio River and associated Indiana and Kentucky Approach Bridges
Section 2, T-3-S, R-6-E, New Albany Township, Floyd County, Indiana



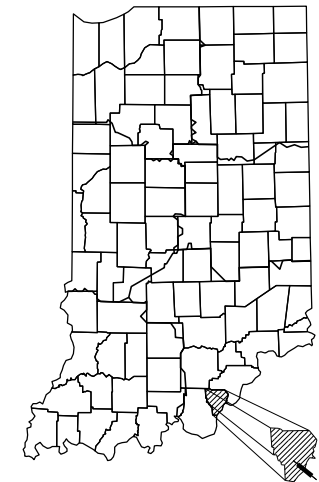
SHERMAN MINTON BRIDGE
REHABILITATION

TRAFFIC DATA (I-64)

| | | | |
|--------------------------|--------|-----------------------------|--------------------|
| A.A.D.T. | (2018) | 45,200 V.P.D. (EB) | 44,800 V.P.D. (WB) |
| A.A.D.T. | (2031) | 45,490 V.P.D. (EB) | 42,500 V.P.D. (WB) |
| D.H.V. | (2031) | 4,090 V.P.H. (EB) | 3,830 V.P.H. (WB) |
| DIRECTIONAL DISTRIBUTION | | 50.3 % EB | |
| TRUCKS | | 11 % A.A.D.T. 6 % D.H.V. | |

DESIGN DATA

| | |
|---------------------------|-----------------------------------|
| DESIGN SPEED | SEE GENERAL NOTES AND INDEX SHEET |
| PROJECT DESIGN CRITERIA | 3R (FREEWAY) |
| FUNCTIONAL CLASSIFICATION | PRINCIPAL ARTERIAL |
| RURAL/URBAN | URBAN |
| TERRAIN | LEVEL |
| ACCESS CONTROL | FULL |



PROJECT LOCATION SHOWN BY
FLOYD COUNTY

LATITUDE: 38°16'43" N LONGITUDE: 85°49'20" W



SCALE: 1" = 2000'

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS



A-30

| | | |
|--------------------------------------|--------------------|--------------------------------|
| PLANS PREPARED BY: | Jacobs Engineering | (314) 335-4000 PHONE NUMBER |
| CERTIFIED BY: | | DATE |
| RECOMMENDED FOR LETTING: | | DATE |
| INDIANA DEPARTMENT OF TRANSPORTATION | | |

| | |
|----------------|---------|
| DESIGNATION | |
| 1702255 | |
| BUILDABLE UNIT | SHEETS |
| 2B | 1 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

UTILITIES

Kentucky Utilities

| | | | |
|--|---|--|--|
| Communication AT&T - Distribution Scott Roche 3719 Bardstown Rd Louisville, KY 40218 (502) 348-4528 SR8832@att.com | Gas and Electric Louisville Gas and Electric Caroline Justice 820 W. Broadway Louisville, KY 40202 (502) 627-3708 Caroline.Justice@LGE-KU.com | Water and Sewer Louisville Water Eric Walls 550 S. Third Street Louisville, KY 40202 (502) 333-4858 ewalls@lwcky.com | MSD David Mulloy 700 W. Liberty Street Louisville, KY 40203 (502) 540-6615 david.mulloy@louisvillemsd.org |
|--|---|--|--|

Indiana Utilities

| | | |
|---|--|--|
| Gas and Electric Centerpoint Energy Distribution Timmy Langston (812) 348-6703 timothy.langston@centerpointenergy.com | Water and Sewer IN American Water inutilitycoordination@amwater.com | Local Housing Authority - New Albany Dwayne Gadd (812) 948-2319 dgadd@nahain.com |
| Centerpoint Energy Transmission Raj Brahmabhatt, PE 28100 Torch Pkwy Warrenville, IL 60555 (630) 225-6086 rbrahmabhatt@enengineering.com | Communication Centurylink (Lumen) Rob Smith 1409 E. Grace Street Rensselaer, IN 47978 relocations@Lumen.com Nationalrelo@centurylink.com | City of New Albany Scott Wilkinson (812) 948-5320 swilkinson@cityofnewalbany.com |
| Duke Energy dei-dline-coord@duke-energy.com dei-tline-coord@duke-energy.com | MCI investigations@verizon.com | INDOT INDOT ITS Drew Sorenson 8620 E. 21st Street Indianapolis, IN 46219 ITS_Locates@indot.in.gov |
| Duke Energy Telecom Chris Gere (980) 373-6842 Chris.Gere@duke-energy.com | Zayo Waylon Higgins 9209 Castlegate Drive Indianapolis, IN 46256 INKY_relo@zayo.com | INDOT Traffic Signal Steve Gann (812) 524-3777 sgann@indot.in.gov |
| | Windstream Jeremy Smiddle (618) 383-4032 Jeremy.smiddle@windstream.com | |



INDIANA UNDERGROUND
1-800-382-5544 OR CALL 811
24 HOURS A DAY 7 DAYS A WEEK

Phase 1

Construction Activities

- 1) Remove concrete median barrier between EB and WB I-64 Sta. 122+50 to Sta. 127+25 on the Indiana side of the Sherman Minton Bridge and construct crossover.
- 2) Construct crossover on the Kentucky side of the Sherman Minton Bridge between EB and WB I-64.
- 3) Complete Sherman Minton Bridge repairs and associated I-64 roadway work on the 2 closed left lanes of EB I-64 (Lower Deck).

Traffic Revisions

- 1) Close 1 lane of WB I-64 to allow for 1 lane of EB I-64 in the WB I-64 lanes.
- 2) Utilize crossovers and shift 1 lane of EB I-64 to WB I-64.
- 3) Close 2 left lanes of EB I-64 across the Sherman Minton Bridge.
- 4) Utilize nighttime lane closures to close the lower deck of the Sherman Minton Bridge. Use INDOT Standard Drawing E 801-TCLC-01 and close the right lane of EB I-64 beginning at Sta. 198+00 "B". Maintain exit 123 to Spring Street. Close EB I-64 On-ramp at Spring Street.

Phase 2

Construction Activities

- 1) Maintain crossovers constructed in Phase 1.
- 2) Complete Sherman Minton Bridge repairs and associated I-64 roadway work on the remaining right lane of EB I-64 (Lower Deck).

Traffic Revisions

- 1) Maintain 1 lane of EB I-64 traffic in the WB direction from Phase 1.
- 2) Shift 1 lane of EB I-64 traffic across the Sherman Minton Bridge to the newly constructed EB lanes from Phase 1.
- 3) Close 2 right lanes of EB I-64 across the Sherman Minton Bridge.
- 4) Utilize nighttime lane closures to close the lower deck of the Sherman Minton Bridge. Use INDOT Standard Drawing E 801-TCLC-01 and close the right lane of EB I-64 beginning at Sta. 198+00 "B". Maintain exit 123 to Spring Street. Close EB I-64 On-ramp at Spring Street.

GENERAL NOTES

- 1) Traffic shall be maintained in accordance with the Indiana Manual on Uniform Traffic Control Devices (2011 Edition), the InDOT Construction Standards and Project Technical Provisions. Daytime and nighttime shoulder closures, lane closures, and other temporary operations not detailed in the traffic control plans shall follow the standard details found in the InDOT Manual on Uniform Traffic Control Devices (2011 Edition).
- 2) Along I-64 a 15-mile-per-hour speed reduction within the work zone shall be posted when a single 11 foot lane is maintained in any direction. A 10 mile-per-hour speed reduction within the work zone shall be posted in all other instances along I-64.
- 3) The minimum MOT lane width shall be 11 feet on I-64, I-264, and I-265.
- 4) The minimum MOT lane width for multiple lane ramps shall be 11.5 feet.
- 5) The minimum MOT lane width for single lane ramps shall be 16 feet.
- 6) The minimum MOT lane width on local streets shall match existing lane widths up to 11 feet.
- 7) The minimum MOT lane width within multi-lane shifts on interstate mainline and ramps shall be 13 feet.
- 8) A minimum horizontal clearance of 1 foot between barrier and edge of travel lane shall be provided.
- 9) Opposing traffic lanes of I-64, I-264, or I-265 traffic shall be separated by traffic barrier if the existing open median width is reduced or otherwise cannot be provided.
- 10) For TTB used to separate two directions of traffic in a crossover, glare screens shall be used to eliminate headlight glare.
- 11) Design-Build Contractor shall maintain positive drainage on all active travel lanes during Construction Work.
- 12) Barriers shall not impede snow removal operations outside the limits of the Project. To facilitate snow removal operations by INDOT and KYTC, from November 1 through March 31 each year, Design-Build Contractor shall maintain open areas along interstate routes and ramps matching existing shoulder widths. Within the limits of the Project, shoulders may be reduced to the widths shown in the Design-Build Contractor's approved MOT plans, while work is progressing.
- 13) Design-Build Contractor shall remove all existing rumble strips and replace with asphalt pavement prior to moving any traffic onto shoulders. Vehicles cannot straddle rumble strips at any point of construction. Before substantial project completion, all rumble strips shall be replaced in their existing location.
- 14) Existing striping which conflicts with temporary markings within 12-feet of construction shall be water blasted and removed prior to any traffic shifts. All areas shown in the Design-Build Contractor's approved MOT plans shall be returned to their original configuration prior to Final Acceptance.
- 15) All existing raised pavement markers within 12-feet of construction shall be disabled prior to any traffic shifts.
- 16) Temporary minimum vertical clearance shall be 14'-6" for I-64 during construction.
- 17) Access to all businesses and residences shall be maintained at all times.
- 18) Design-Build Contractor shall maintain existing or detoured pedestrian and/or bicycle access on all sidewalks, trails, transit facilities and at all intersections that are open to traffic. Design-Build Contractor shall also maintain safe access and passage for all pedestrian facilities.
- 19) Design-Build Contractor shall provide an alternate accessible pedestrian route that complies with the MUTCD, PROWAG, and ADA requirements where accessible pedestrian and/or bicycle routes are allowed to be closed by Design-Build Contractor during construction.
- 20) Design-Build Contractor shall not park motor vehicles or construction vehicles, locate construction signs, or place equipment on a pedestrian sidewalk or path open to pedestrian traffic; or use the pedestrian sidewalk or path for loading operations, stockpiling of materials, or allowing demolished or spoil materials to be deposited on the surface of a pedestrian and/or bicycle sidewalk or path.
- 21) Design-Build Contractor shall provide temporary access for the Shawnee Golf Course through the Work area under the Kentucky Approach Bridge at all times. If Work areas encroach upon the golf course access, a minimum clear width of 15 feet wide by 16 feet high shall be provided.
- 22) All temporary golf course access points shall be coordinated with the management of the Shawnee Golf Course a minimum of 14 days prior to implementation of temporary access.
- 23) Design Build Contractor shall always maintain two I-64 traffic lanes in each direction on the Indiana Approach Bridges, the Sherman Minton Bridge, and the Kentucky Approach Bridge. However, I-64 may be reduced to one lane in each direction during off-peak hours per Table 12-1 of the Project Technical Provisions.

INDEX

| SHEET NO. | DRAWINGS INDEX |
|-----------|--|
| 1 | TITLE |
| 2 | INDEX AND GENERAL NOTES |
| 3 | MAINTENANCE OF TRAFFIC - TYPICALS - PHASE 1 |
| 4 | MAINTENANCE OF TRAFFIC - TYPICALS - PHASE 2 |
| 5 - 18 | MAINTENANCE OF TRAFFIC - I-64 PLAN SHEETS - PHASE 1 |
| 19 - 23 | MAINTENANCE OF TRAFFIC - I-64 PLAN SHEETS - PHASE 2 |
| 24 | MAINTENANCE OF TRAFFIC - DETAIL - KENTUCKY CROSSOVER |
| 25 - 33 | MAINTENANCE OF TRAFFIC - CROSS SECTIONS - KENTUCKY CROSSOVER |
| 34 - 43 | MAINTENANCE OF TRAFFIC - CROSS SECTIONS - INDIANA CROSSOVER |

DESIGN SPEEDS

| Roadway | Posted Speed | Construction Speed |
|-------------------------------------|--------------|--------------------|
| I-64 | 55 MPH | 45 MPH* |
| I-65 | 65 MPH | 65 MPH |
| I-264 | 55 MPH | 55 MPH |
| I-265 | 65 MPH | 65 MPH |
| Ramp A (I-64/I-265) | 40 MPH | 30 MPH |
| Ramp C (I-64/I-265) | 40 MPH | 30 MPH |
| Ramp EB-R-FUT/ SW-R (I-265/I-65) | 45 MPH | 35 MPH |
| Ramp A1 | 25 MPH | 25 MPH |
| Ramp B1 | 25 MPH | 25 MPH |
| Ramp C1 | 40 MPH | 40 MPH |
| Ramp D1 | 45 MPH | 45 MPH |
| Ramp W-S (I-64/I-264) | 50 MPH | 50 MPH |
| Ramp PR S-W (I-264/I-64) | 45 MPH | 45 MPH |
| Ramp E-S (I-64/I-264) | 45 MPH | 45 MPH |
| Ramp S-E (I-264/I-64) | 45 MPH | 45 MPH |
| Local Streets | 25 MPH | 25 MPH |

*See General Note 2.

INDOT STANDARD DRAWINGS

INDOT Standard Drawings (2020) used:

| | |
|------------|----------------------------------|
| E 715-SLDR | Slotted Drain Pipe |
| E 720-EDCA | Earth Ditch Casting Type 7 |
| E 720-INST | Inlet Standard |
| E 801-TCCO | Temporary Construction Crossover |
| E 801-TCDV | Traffic Control Devices |
| E 801-TCLC | Traffic Control Lane Closures |
| E 801-TCLG | Traffic Control Legend |
| E 801-TCSC | Traffic Control Shoulder Closure |
| E 801-TCSN | Traffic Control Signs |
| E 808-DLIM | Dotted Line Markings |



A-31

| | | |
|--------------------------|-----------------|----------------------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 4/20/21 | DRAWN: DPS 4/20/21 |
| CHECKED: AWL | 4/23/21 | CHECKED: AWL 4/23/21 |

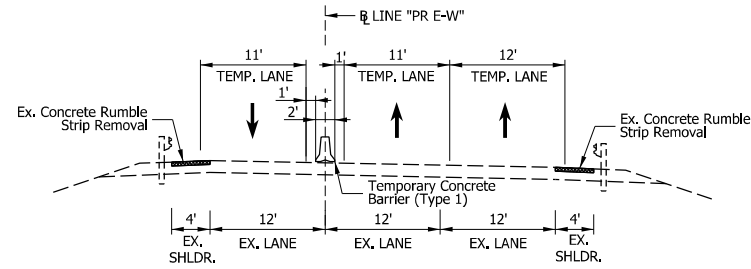
INDIANA
DEPARTMENT OF TRANSPORTATION

INDEX AND
GENERAL NOTES

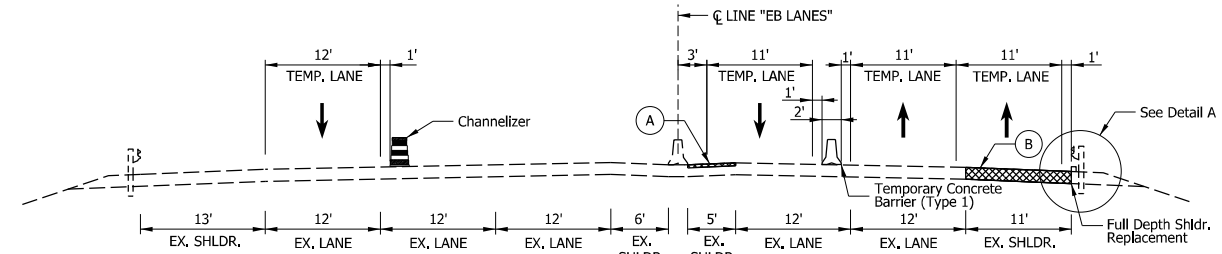
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| HORIZONTAL SCALE | BRIDGE FILE |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 2 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

A SHOULDER STRENGTHENING
 1.5" Mill; Overlay with
 165 lbs/syd QC/QA-HMA, 4, 76, Surface, 9.5 mm

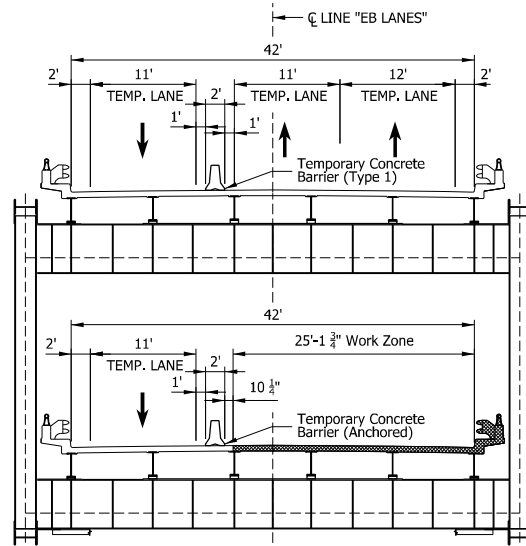
B FULL DEPTH SHOULDER REPLACEMENT
 165 lbs/syd QC/QA-HMA, 4, 76, Surface, 9.5 mm on
 275 lbs/syd QC/QA-HMA, 4, 76, Intermediate, 19.0 mm on
 1100 lbs/syd QC/QA-HMA, 4, 64, Base, 25.0 mm on
 Subgrade Treatment, Type ID



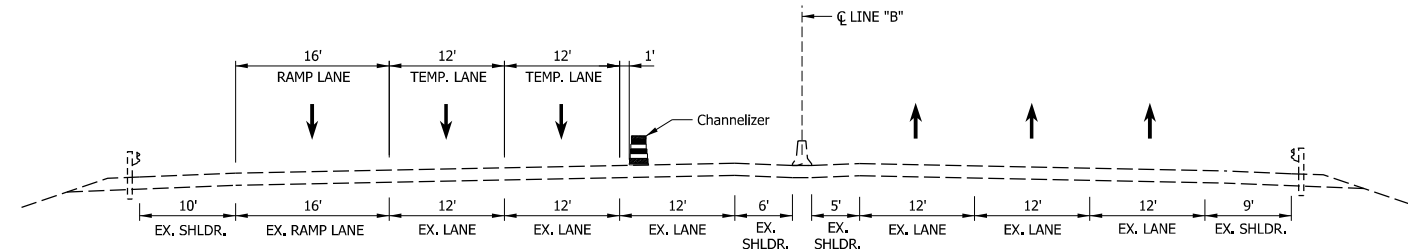
I-64 WB SECTION
 STA. 361+00 "PR E-W"



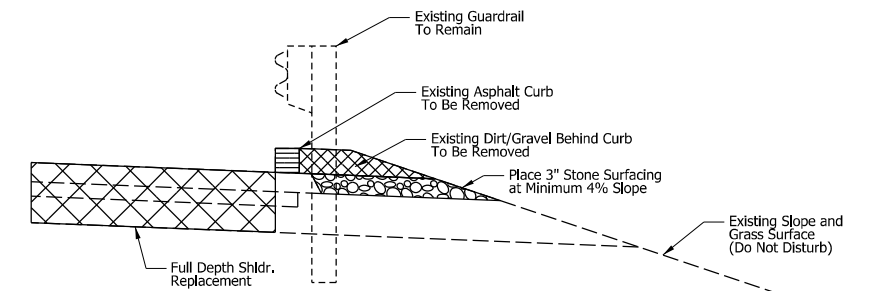
I-64 SECTION
 STA. 120+00 "EB LANES"



BRIDGE SECTION
 STA. 90+00 "EB LANES"



I-64 SECTION
 STA. 134+00 "B"



DETAIL A
 STA. 117+20 TO STA. 126+28 "EB LANES"

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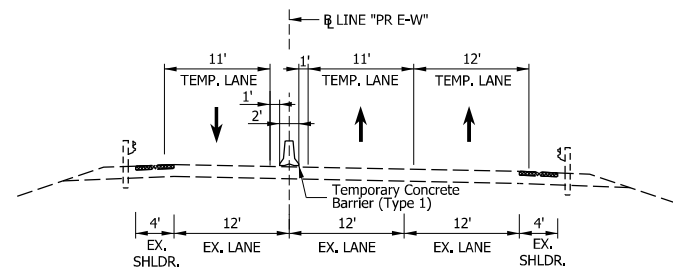
A-32

| | | |
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| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 3/22/21 | DRAWN: DPS 3/22/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

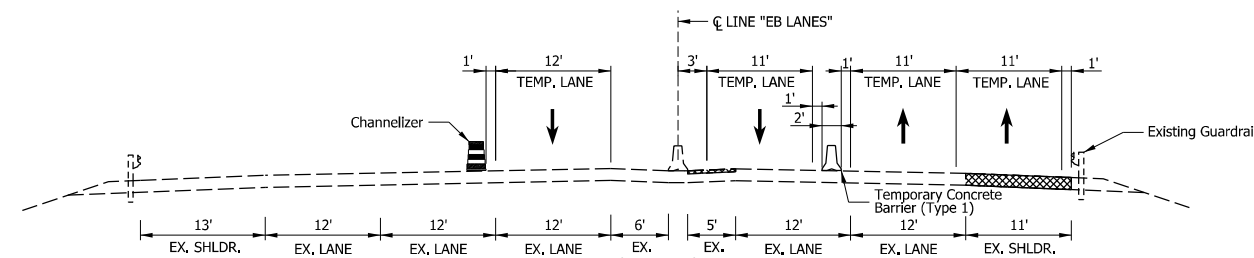
INDIANA
 DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
 MOT PHASE 1
 TYPICAL SECTIONS

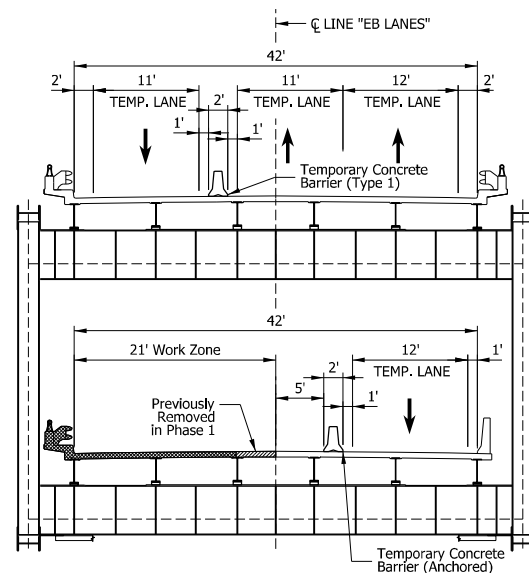
| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=10' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 3 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |



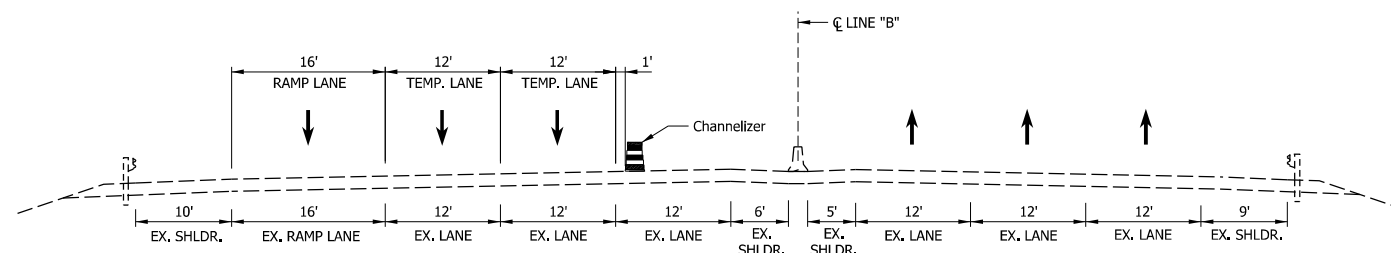
I-64 WB SECTION
STA. 361+00 "PR E-W"



I-64 SECTION
STA. 120+00 "EB LANES"



BRIDGE SECTION
STA. 90+00 "EB LANES"



I-64 SECTION
STA. 134+00 "B"

Plot: 16-JUN-2021 08:51 AM



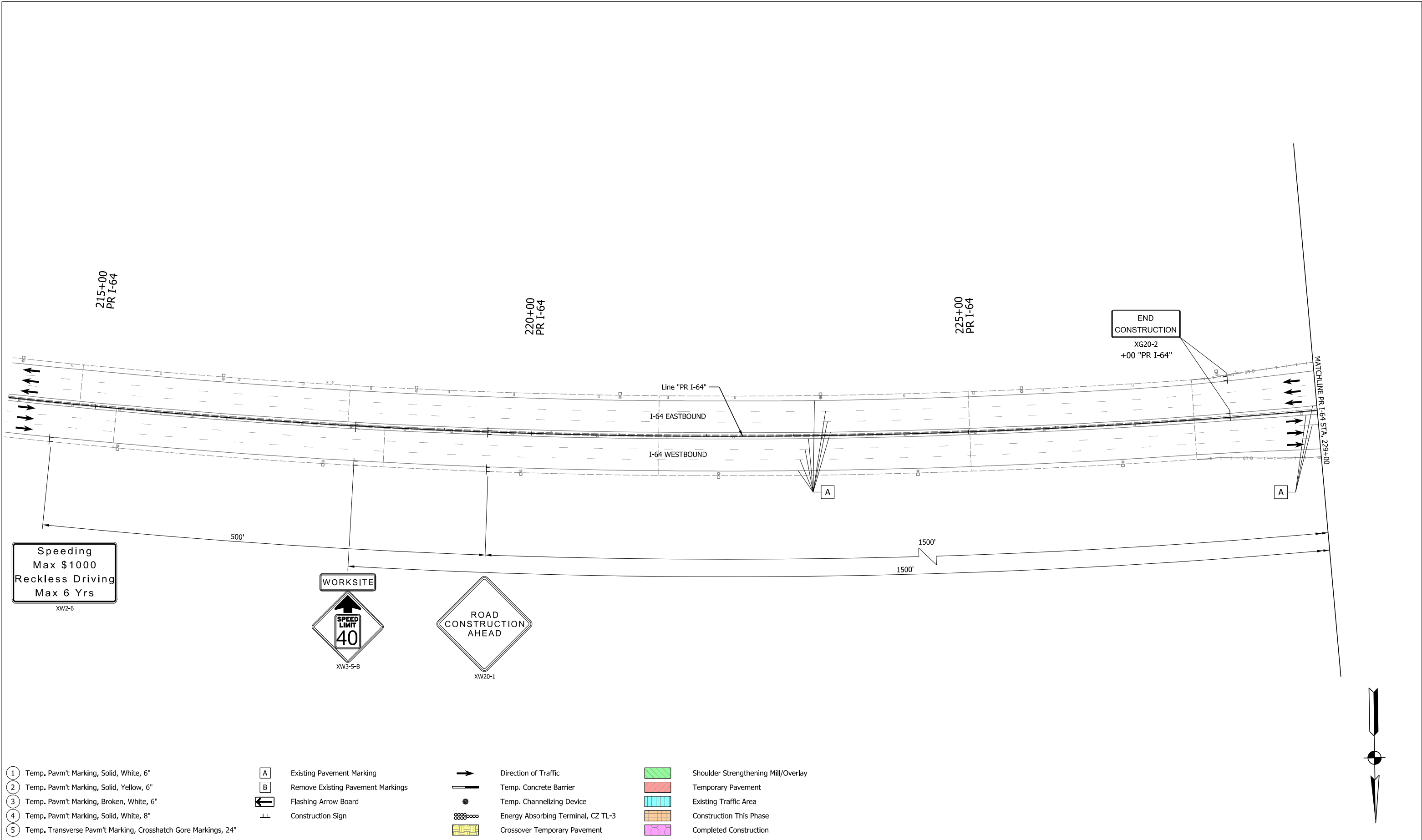
A-33

| | | |
|--------------------------|-----------------|----------------------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 3/22/21 | DRAWN: DPS 3/22/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 2
TYPICAL SECTIONS

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=10' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 4 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |



- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



Plot: 16-JUN-2021 08:51 AM



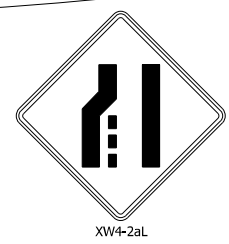
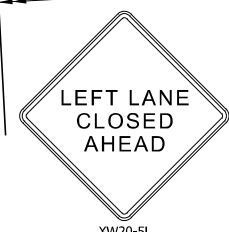
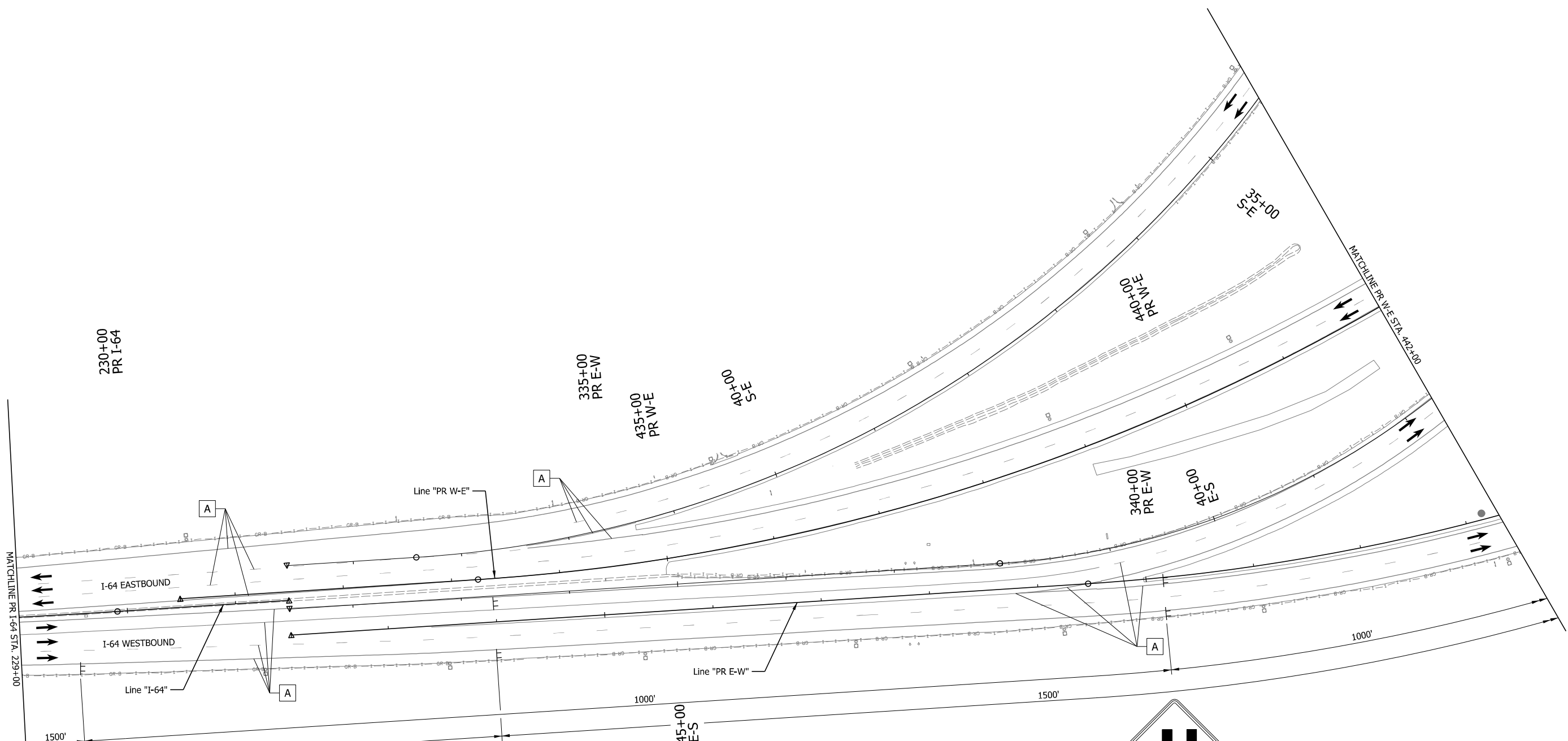
A-34

| | | |
|--------------------------|-----------------|----------------------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 3/11/21 | DRAWN: DPS 3/11/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 215+00 TO STA. 229+00**

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 5 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |



- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



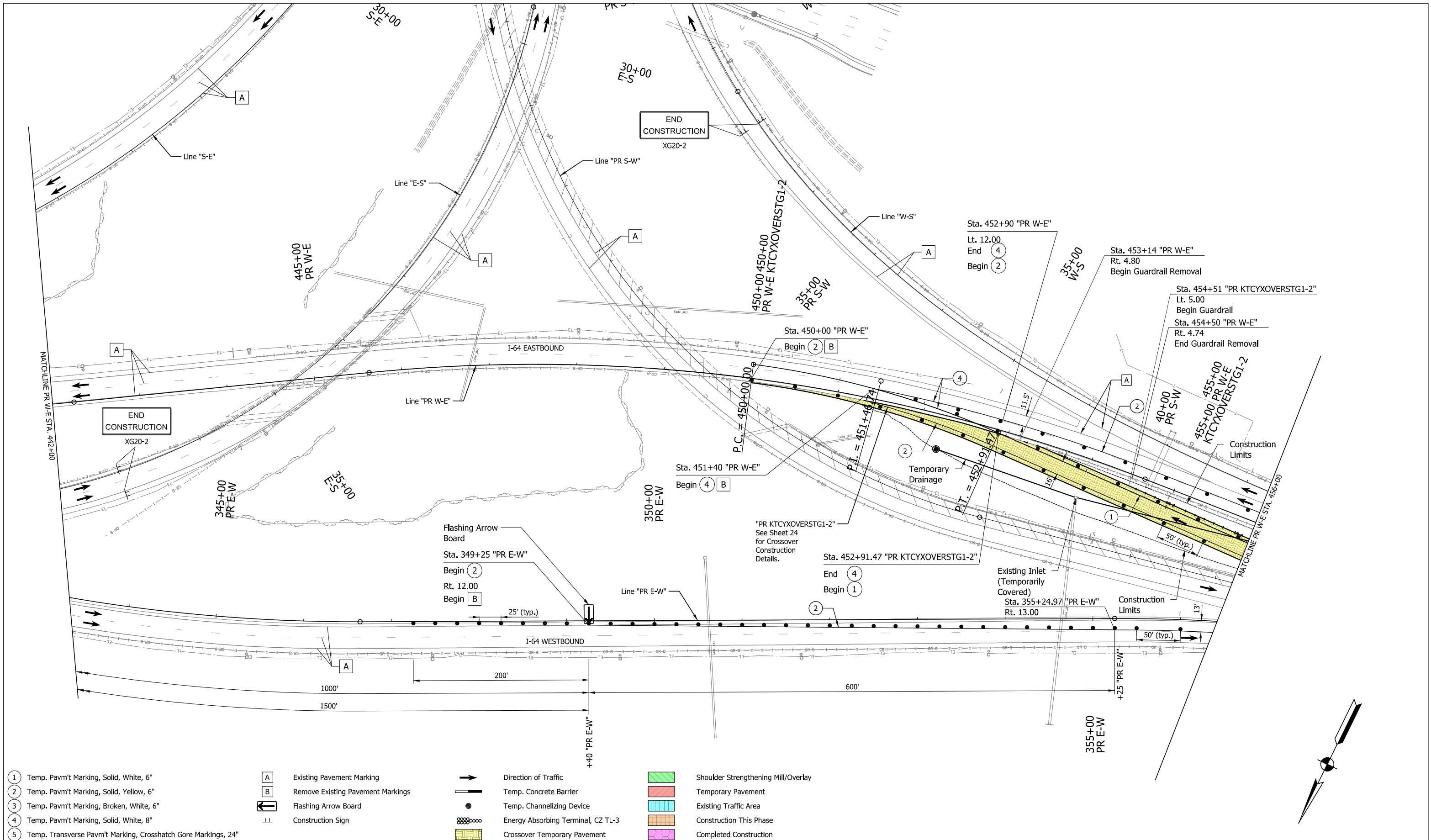
A-35

| | | |
|--------------------------|-----------------|--------------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 3/11/21 | DRAWN: DPS |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL |
| | | 3/11/21 |
| | | 3/26/21 |

INDIANA DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC MOT PHASE 1
 STA. 229+00 TO STA. 442+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 6 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 16-JUN-2021 08:52 AM

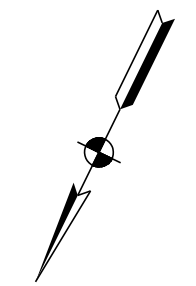


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



Plot: 16-JUN-2021 08:53 AM



A-36

| | | |
|--------------------------|-----------------|----------------------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 3/11/21 | DRAWN: DPS 3/11/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 442+00 TO STA. 456+00**

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 7 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Sta. 457+09 "PR-KTCYXOVERSTG1-2"
 Begin 1284' Rad.
 Rt. 16.00
 Begin 1300' Rad.
 Rt. 18.00
 Begin 1302' Rad.
 Temp. Concrete Barrier

Sta. 42+44 "PR S-W"
 Lt. 7.50
 Begin Guardrail Removal

Sta. 357+79 "PR E-W"
 Lt. 33.07
 Begin 1300' Rad.

"PR KTCYXOVERSTG1-2"
 See Sheet 24
 for Crossover
 Construction
 Details

Temporary
 Drainage
 Line "PR W-E"

MATCHLINE PR W-E STA. 456+00

Sta. 59+37 "EB LANES"
 Rt. 5.78
 Begin Shift

Sta. 59+13 "EB LANES"
 Lt. 5.70
 Begin [B]

Sta. 359+46 "PR E-W"
 Lt. 20.91
 Begin Shift
 Construction
 Limits

Sta. 358+00 "PR E-W"
 Rt. 13.00
 Begin 2000' Rad.

End [2]
 Begin [4]
 Lt. 2.77
 End [1]

Begin [4]
 Rt. 25.00
 Begin [1]

Sta. 41+91 "PR S-W"
 Rt. 19.45
 Begin 3000' Rad.

Begin [4] [B]
 Sta. 456+55 "PR-KTCYXOVERSTG1-2"
 Rt. 18.00
 Begin Temp. Concrete Barrier

Sta. 41+25 "PR S-W"
 Rt. 5.35
 Begin 4250' Rad.

Begin [2]

40+00
 W-S

Sta. 59+13 "EB LANES"
 Lt. 5.70
 Begin [B]

Sta. 359+46 "PR E-W"
 Lt. 20.91
 Begin Shift
 Construction
 Limits

Sta. 358+00 "PR E-W"
 Rt. 13.00
 Begin 2000' Rad.

End [2]
 Begin [4]
 Lt. 2.77
 End [1]

Begin [4]
 Rt. 25.00
 Begin [1]

Sta. 41+91 "PR S-W"
 Rt. 19.45
 Begin 3000' Rad.

Begin [4] [B]
 Sta. 456+55 "PR-KTCYXOVERSTG1-2"
 Rt. 18.00
 Begin Temp. Concrete Barrier

Sta. 41+25 "PR S-W"
 Rt. 5.35
 Begin 4250' Rad.

Begin [2]

60+00
 EB LANES

60+00
 EB LANES

Sta. 61+67 "EB LANES"
 Lt. 17.80
 Begin Shift
 Begin [1] [B]

Sta. 62+67 "EB LANES"
 Lt. 6.00
 Begin Temp Conc Barrier
 Lt. 8.00
 End Shift
 Lt. 19.00
 End Shift

Sta. 359+97 "PR E-W"
 Lt. 2.00
 End 1300' Rad.
 Continue [2]
 Lt. 0.00'
 Continue Temp Conc. Barrier
 Rt. 2.00
 Continue [2]

Sta. 361+04 "PR E-W"
 Lt. 13.00
 End Shift [1]

Sta. 360+59 "PR E-W"
 Lt. 16.05
 End Guardrail
 End Guardrail Removal
 Match Existing Guardrail

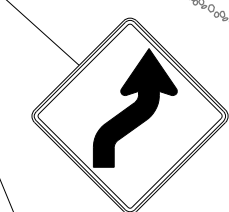
Sta. 359+80 "PR E-W"
 Rt. 13.00
 End 2000' Rad.
 End 1849' Rad.
 End [4]
 Begin [3]

Sta. 359+00 "PR E-W"
 Rt. 9.58
 End 3000' Rad.
 Begin 1849' Rad.
 Lt. 2.35
 End 4250' Rad.
 Begin 1150' Rad.

Sta. 359+00 "PR E-W"
 Rt. 9.58
 End 3000' Rad.
 Begin 1849' Rad.
 Lt. 2.35
 End 4250' Rad.
 Begin 1150' Rad.

Sta. 359+00 "PR E-W"
 Rt. 9.58
 End 3000' Rad.
 Begin 1849' Rad.
 Lt. 2.35
 End 4250' Rad.
 Begin 1150' Rad.

Sta. 359+00 "PR E-W"
 Rt. 9.58
 End 3000' Rad.
 Begin 1849' Rad.
 Lt. 2.35
 End 4250' Rad.
 Begin 1150' Rad.



XW1-4R
 +00

Sta. 361+04 "PR E-W"
 Lt. 13.00
 End Shift [1]

Sta. 360+59 "PR E-W"
 Lt. 16.05
 End Guardrail
 End Guardrail Removal
 Match Existing Guardrail

Sta. 359+80 "PR E-W"
 Rt. 13.00
 End 2000' Rad.
 End 1849' Rad.
 End [4]
 Begin [3]

Sta. 359+00 "PR E-W"
 Rt. 9.58
 End 3000' Rad.
 Begin 1849' Rad.
 Lt. 2.35
 End 4250' Rad.
 Begin 1150' Rad.

Sta. 359+00 "PR E-W"
 Rt. 9.58
 End 3000' Rad.
 Begin 1849' Rad.
 Lt. 2.35
 End 4250' Rad.
 Begin 1150' Rad.

Sta. 359+00 "PR E-W"
 Rt. 9.58
 End 3000' Rad.
 Begin 1849' Rad.
 Lt. 2.35
 End 4250' Rad.
 Begin 1150' Rad.

Sta. 359+00 "PR E-W"
 Rt. 9.58
 End 3000' Rad.
 Begin 1849' Rad.
 Lt. 2.35
 End 4250' Rad.
 Begin 1150' Rad.

Sta. 359+00 "PR E-W"
 Rt. 9.58
 End 3000' Rad.
 Begin 1849' Rad.
 Lt. 2.35
 End 4250' Rad.
 Begin 1150' Rad.

Sta. 359+00 "PR E-W"
 Rt. 9.58
 End 3000' Rad.
 Begin 1849' Rad.
 Lt. 2.35
 End 4250' Rad.
 Begin 1150' Rad.

Sta. 359+00 "PR E-W"
 Rt. 9.58
 End 3000' Rad.
 Begin 1849' Rad.
 Lt. 2.35
 End 4250' Rad.
 Begin 1150' Rad.

Sta. 359+00 "PR E-W"
 Rt. 9.58
 End 3000' Rad.
 Begin 1849' Rad.
 Lt. 2.35
 End 4250' Rad.
 Begin 1150' Rad.

Sta. 359+00 "PR E-W"
 Rt. 9.58
 End 3000' Rad.
 Begin 1849' Rad.
 Lt. 2.35
 End 4250' Rad.
 Begin 1150' Rad.

Sta. 359+00 "PR E-W"
 Rt. 9.58
 End 3000' Rad.
 Begin 1849' Rad.
 Lt. 2.35
 End 4250' Rad.
 Begin 1150' Rad.

- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- [A] Existing Pavement Marking
- [B] Remove Existing Pavement Markings
- [Flashing Arrow Board]
- [Construction Sign]

- [Arrow] Direction of Traffic
- [Barrier] Temp. Concrete Barrier
- [Device] Temp. Channelizing Device
- [Terminal] Energy Absorbing Terminal, CZ TL-3
- [Crossover] Crossover Temporary Pavement

- [Green Hatched] Shoulder Strengthening Mill/Overlay
- [Red Hatched] Temporary Pavement
- [Blue Hatched] Existing Traffic Area
- [Orange Hatched] Construction This Phase
- [Purple Hatched] Completed Construction



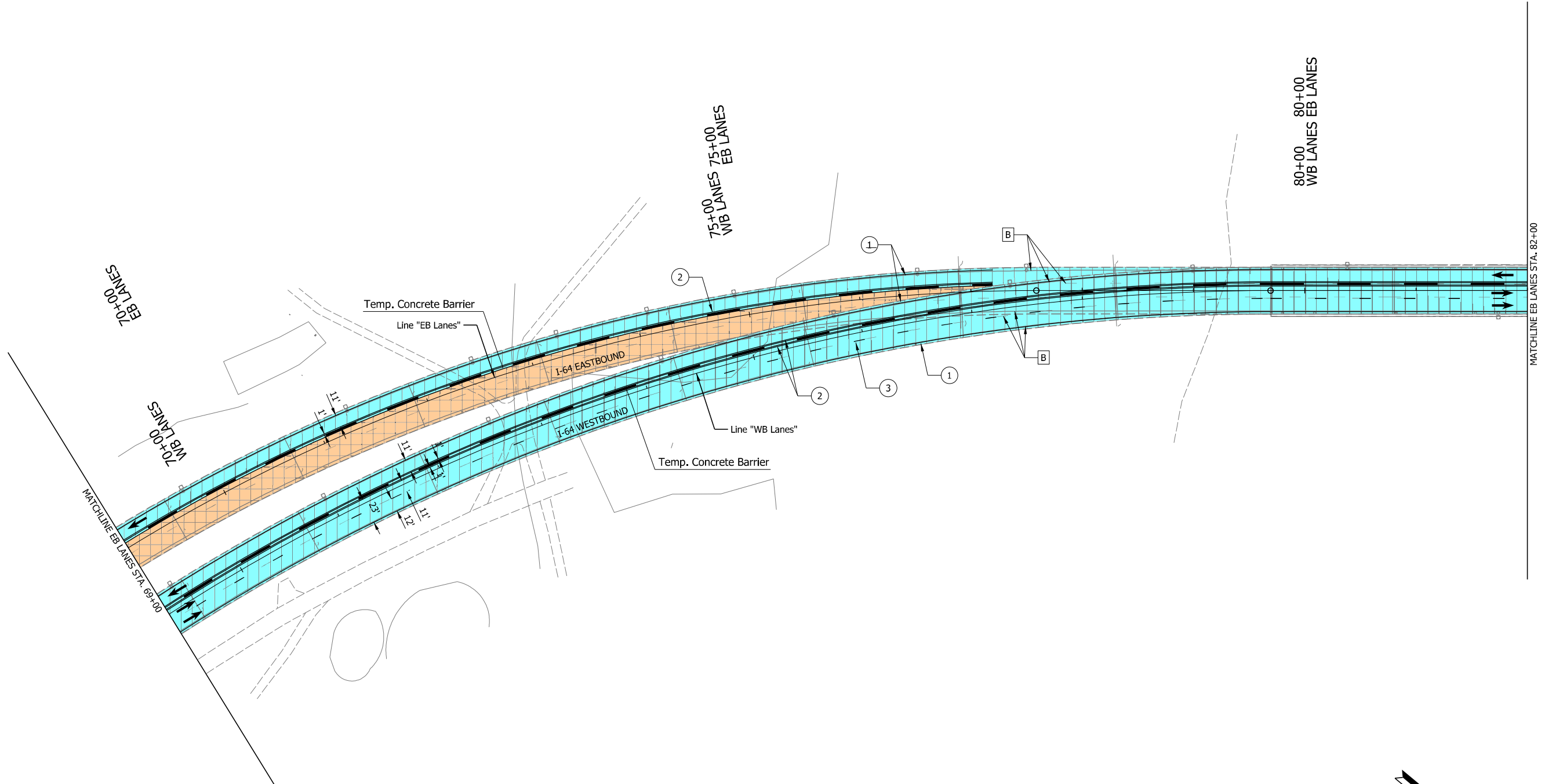
A-37

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| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 3/11/21 | DRAWN: DPS 3/11/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

INDIANA
 DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC
 MOT PHASE 1
 STA. 456+00 TO STA. 69+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 8 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 17-JUN-2021 1:16:54 PM

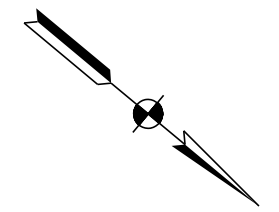


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ↔ Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ⊘ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction



Plot: 16-JUN-2021 08:54 AM



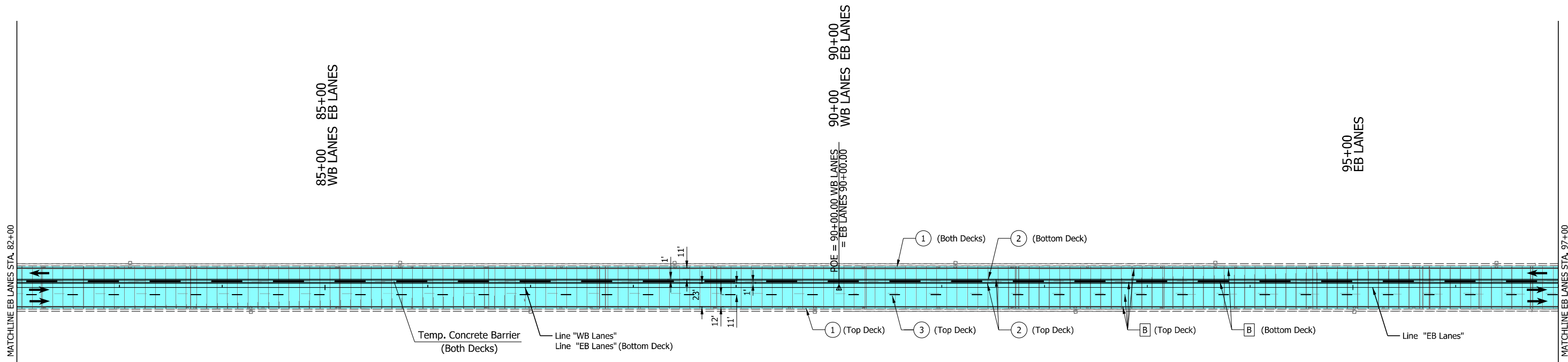
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| | | |
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| DESIGNED: DPS | 3/11/21 | DRAWN: DPS 3/11/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 69+00 TO STA. 82+00**

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 9 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

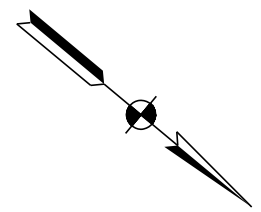


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ↔ Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ⊘ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction



Plot: 16-JUN-2021 08:54 AM



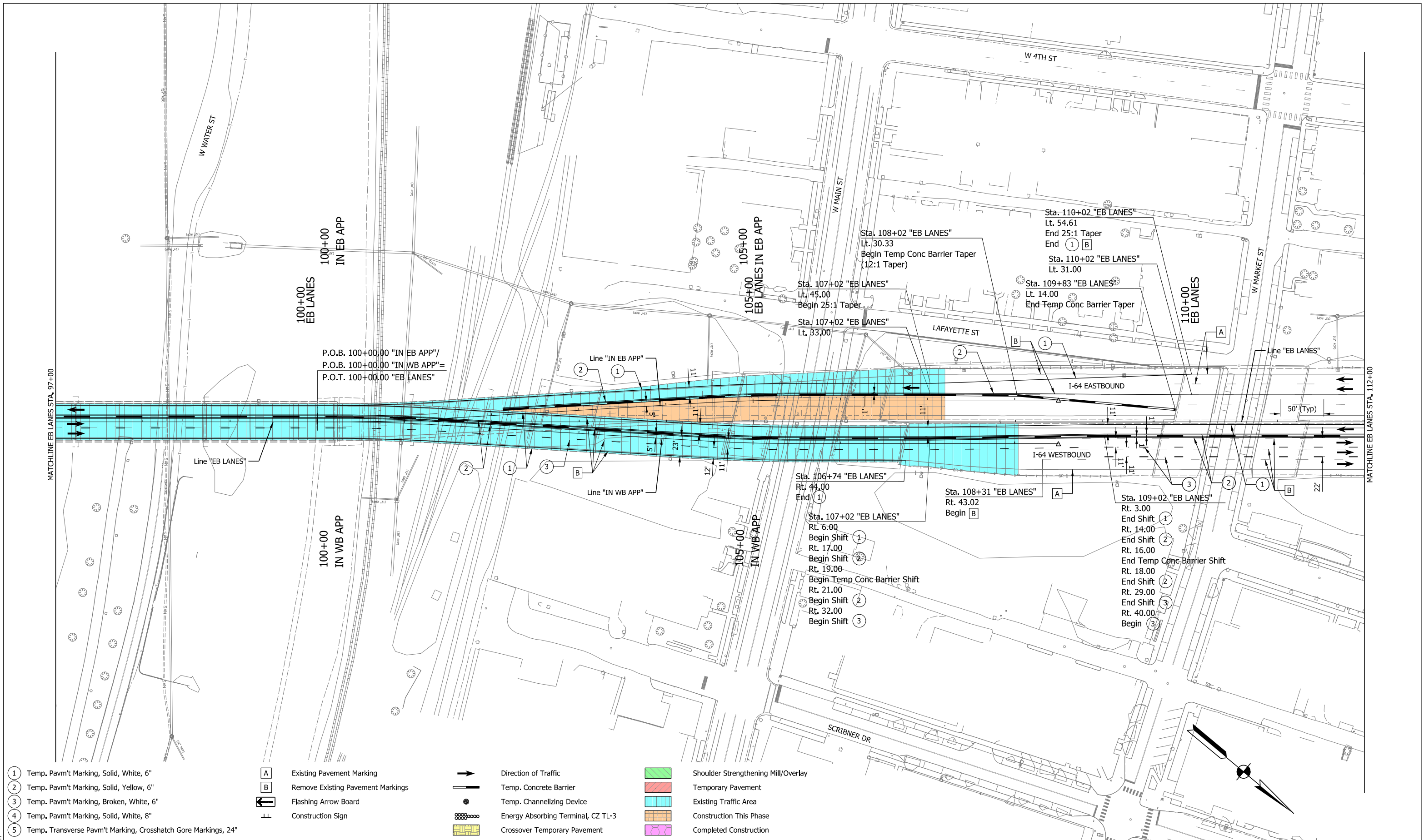
A-39

| | | |
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| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 3/11/21 | DRAWN: DPS 3/11/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 82+00 TO STA. 97+00**

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 10 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |



- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ↔ Flashing Arrow Board
- LL Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



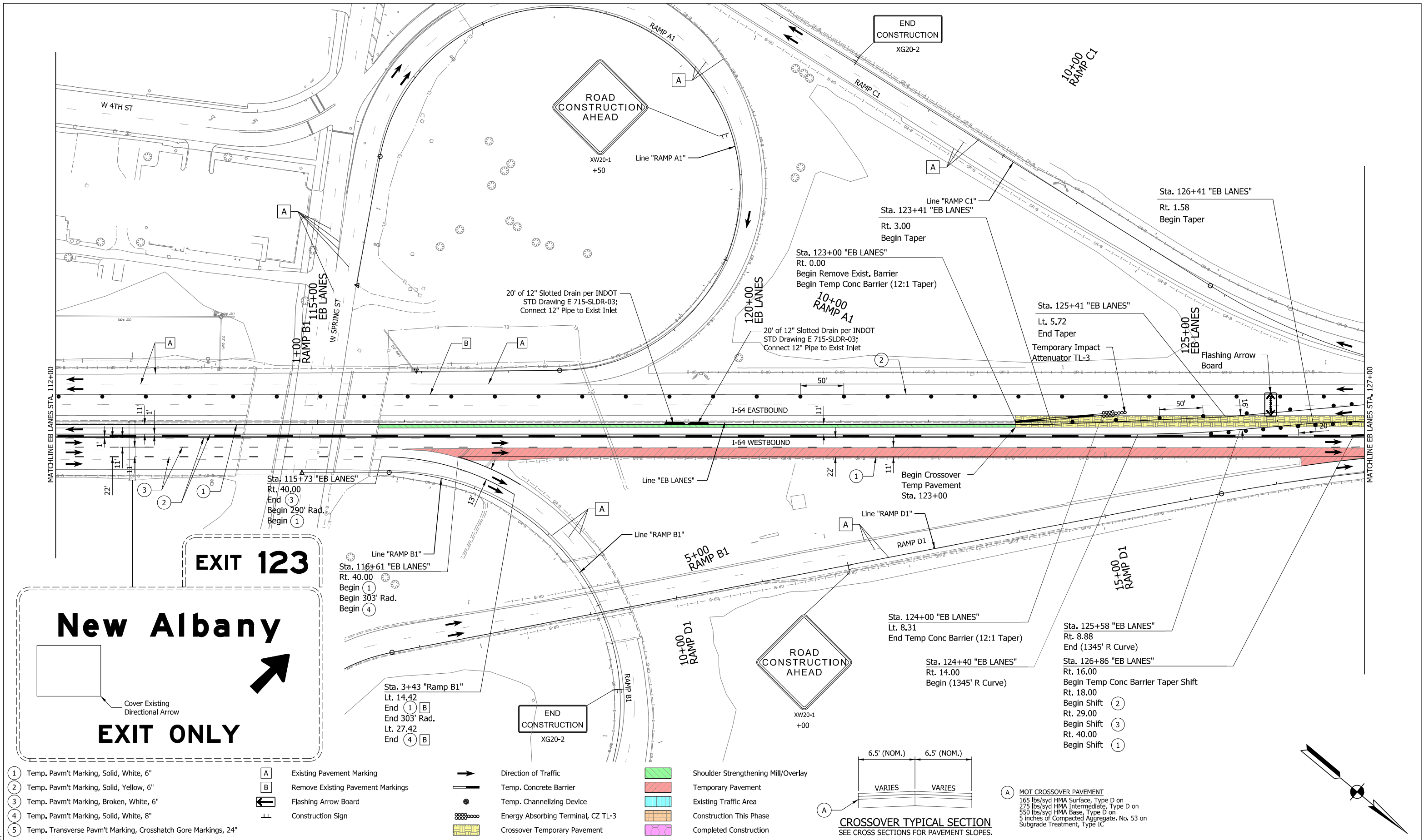
A-40

| | | |
|--------------------------|-----------------|----------------------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 3/11/21 | DRAWN: DPS 3/11/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

INDIANA DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC MOT PHASE 1
 STA. 97+00 TO STA. 112+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 11 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 16-JUN-2021 08:55 AM

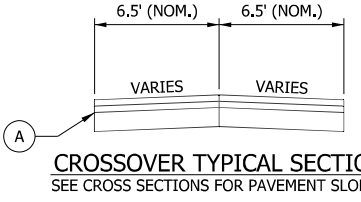


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



A MOT CROSSOVER PAVEMENT
 165 lbs/syd HMA Surface, Type D on
 275 lbs/syd HMA Intermediate, Type D on
 550 lbs/syd HMA Base, Type D on
 5 inches of Compacted Aggregate, No. 53 on
 Subgrade Treatment, Type IC

Plot: 16-JUN-2021 08:56 AM



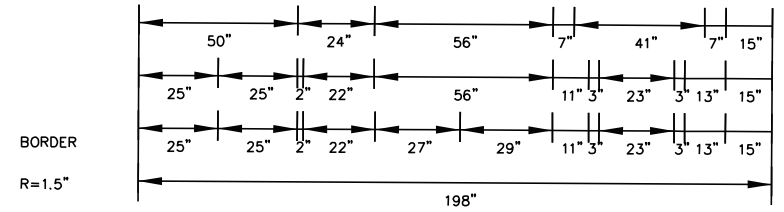
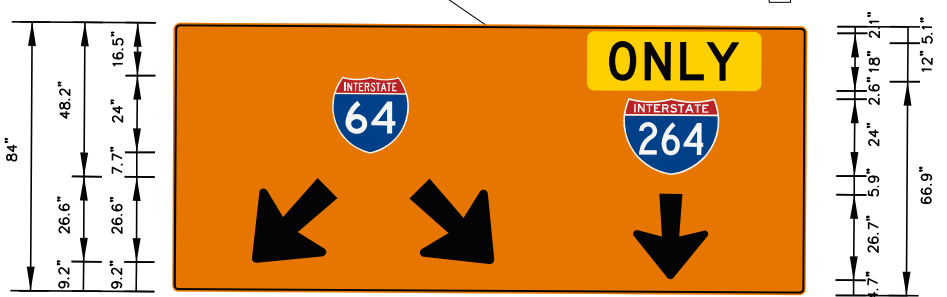
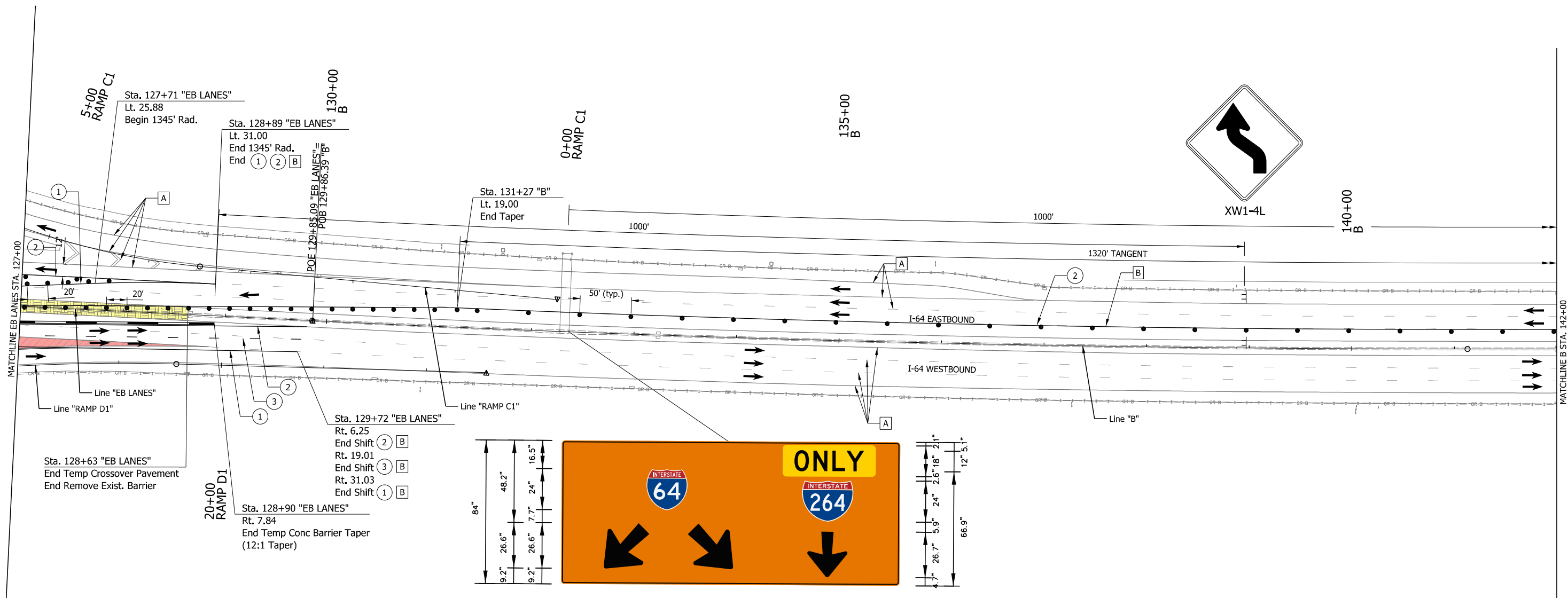
A-41

| | | |
|--------------------------|-----------------|----------------------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 3/11/21 | DRAWN: DPS 3/11/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 112+00 TO STA. 127+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 12 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |



- ① Temp. Pavm't Marking, Solid, White, 6"
 - ② Temp. Pavm't Marking, Solid, Yellow, 6"
 - ③ Temp. Pavm't Marking, Broken, White, 6"
 - ④ Temp. Pavm't Marking, Solid, White, 8"
 - ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"
- A Existing Pavement Marking
 - B Remove Existing Pavement Markings
 - Flashing Arrow Board
 - Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement
- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction

A-42

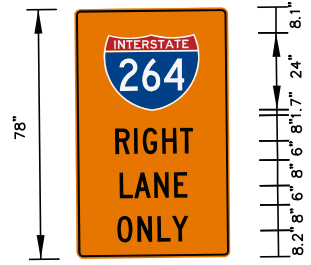


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|--------------------------|-----------------|----------------------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 3/11/21 | DRAWN: DPS 3/11/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

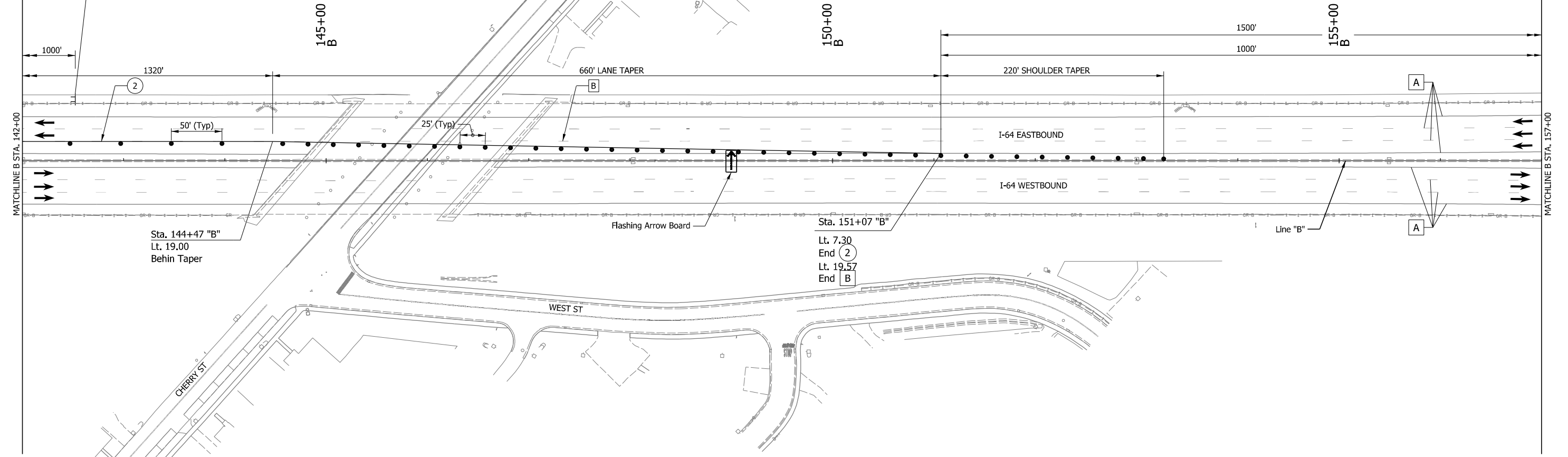
INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC MOT PHASE 1
 STA. 127+00 TO STA. 142+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | DESIGNATION |
| VERTICAL SCALE | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 13 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 17-JUN-2021 16:54 PM



BORDER
 R=1.5"
 TH=0.63"
 IN=0.47"
 Panel Style: construction_guide.ssi
 M.U.T.C.D.: 2009 Edition

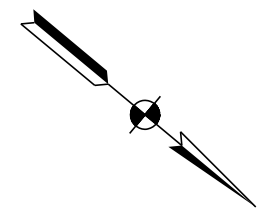


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



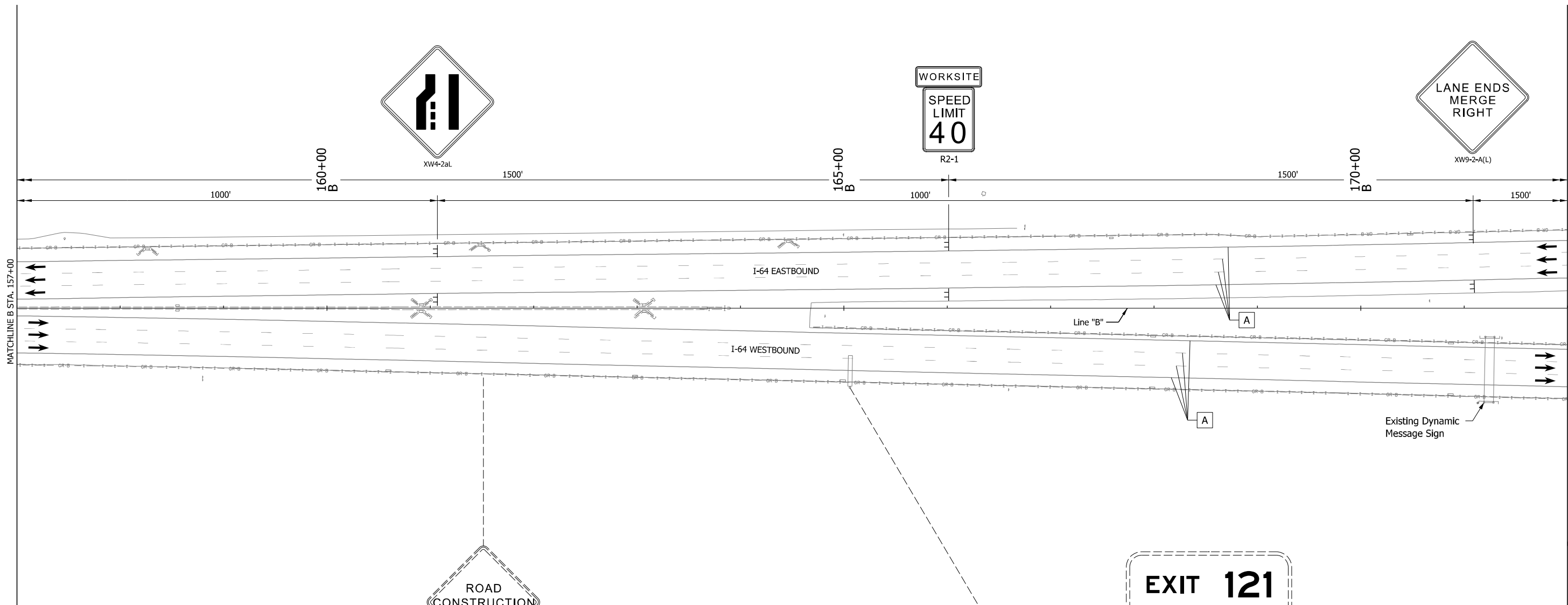
A-43

| | | |
|--------------------------|-----------------|----------------------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 3/11/21 | DRAWN: DPS 3/11/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

INDIANA
 DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC
 MOT PHASE 1
 STA. 142+00 TO STA. 157+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 14 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 17-JUN-2021 16:55 PM

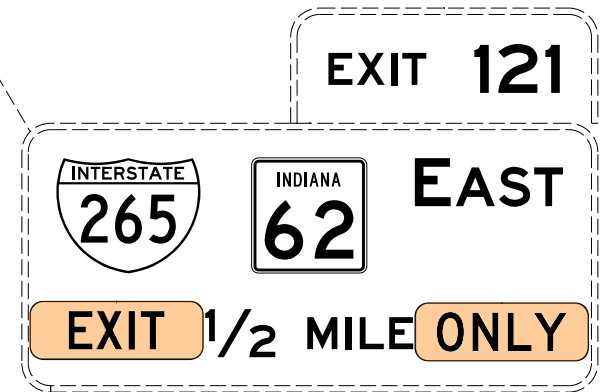


- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

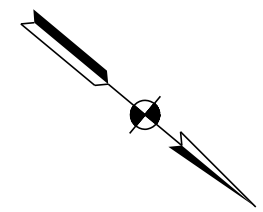
- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



NOTE:
1. Sign modifications to remain from previous Phase.



Plot: 16-JUN-2021 08:58 AM

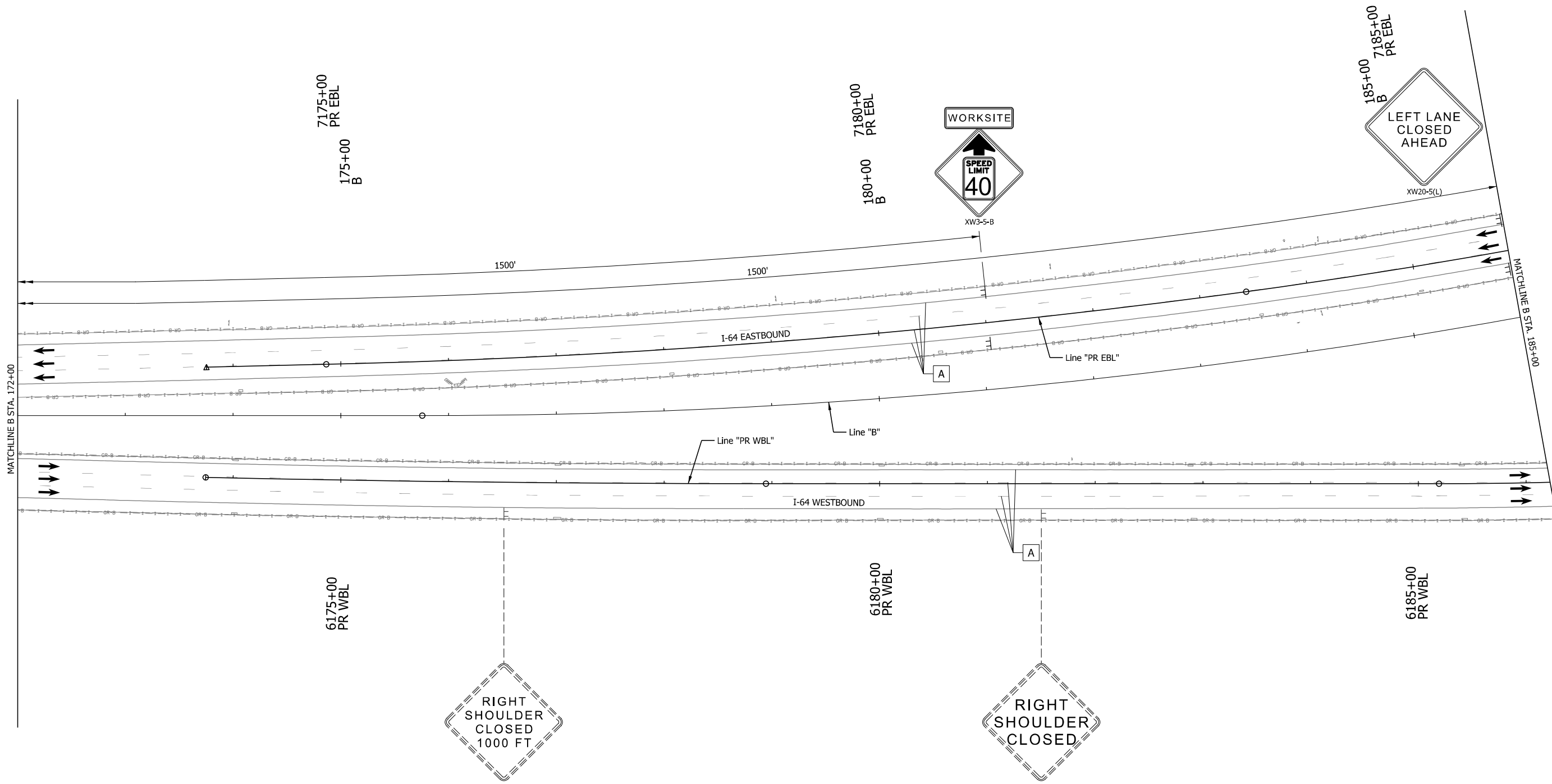


A-44

| | | |
|--------------------------|-----------------|----------------------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 3/11/21 | DRAWN: DPS 3/11/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC MOT PHASE 1
STA. 147+00 TO STA. 172+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 15 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

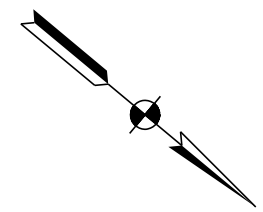


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



A-45

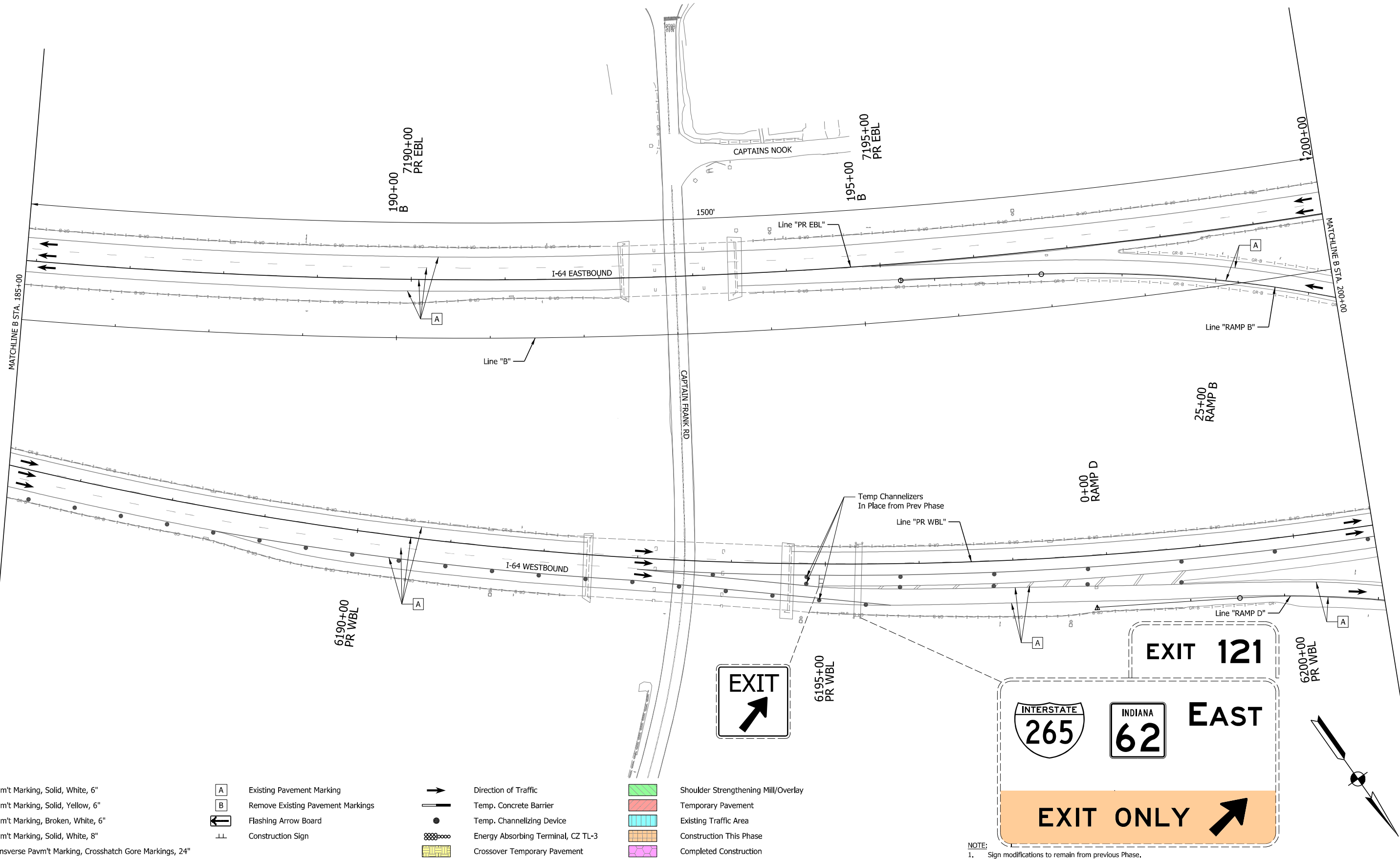
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|--------------------------|-----------------|----------------------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 3/11/21 | DRAWN: DPS 3/11/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 172+00 TO STA. 185+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 16 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 16-JUN-2021 08:59 AM



- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction

NOTE:
1. Sign modifications to remain from previous Phase.

Plot: 16-JUN-2021 08:59 AM



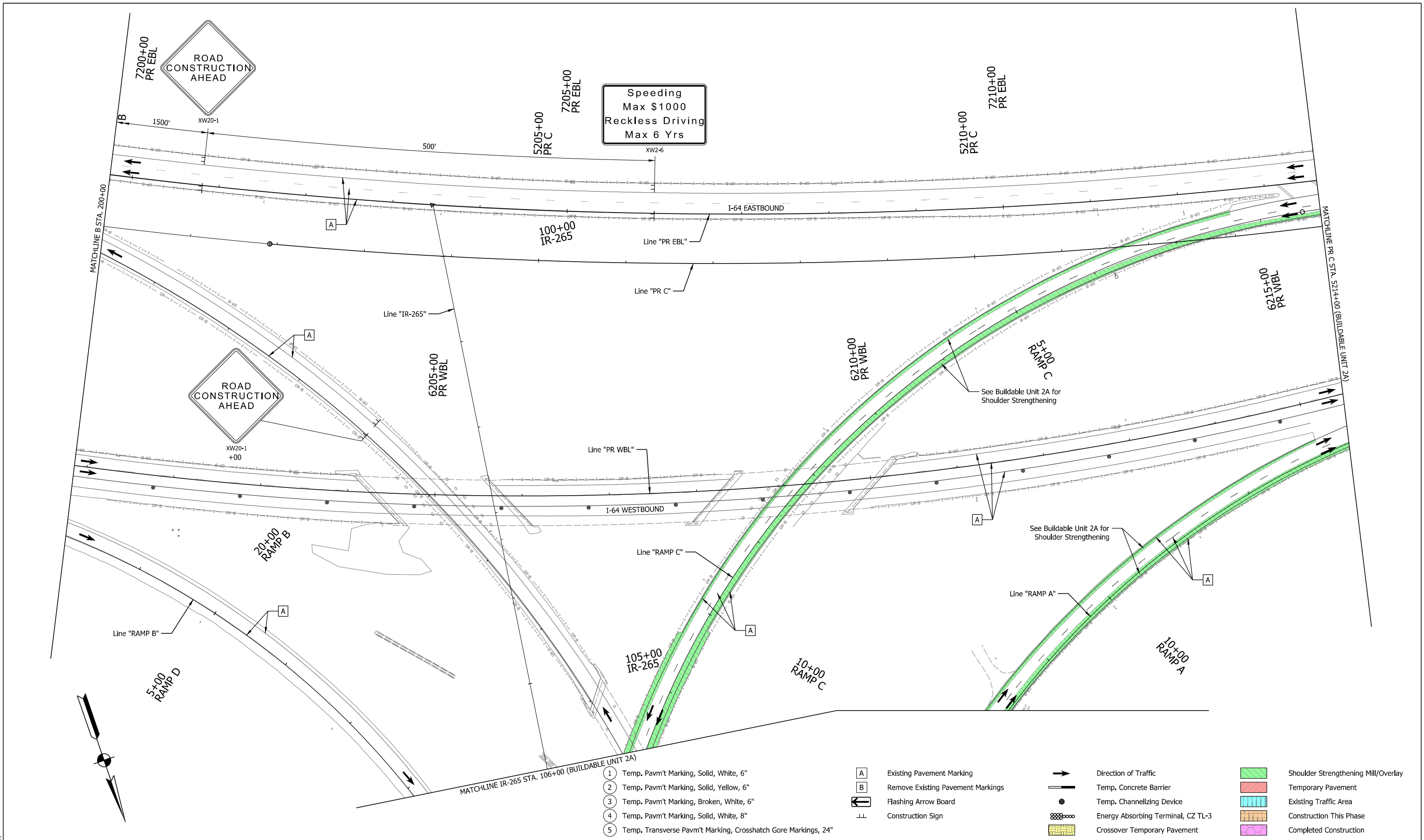
A-46

| | | |
|--------------------------|-----------------|----------------------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 3/11/21 | DRAWN: DPS 3/11/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 185+00 TO STA. 200+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 17 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |



Speeding
Max \$1000
Reckless Driving
Max 6 Yrs

ROAD
CONSTRUCTION
AHEAD

ROAD
CONSTRUCTION
AHEAD

- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



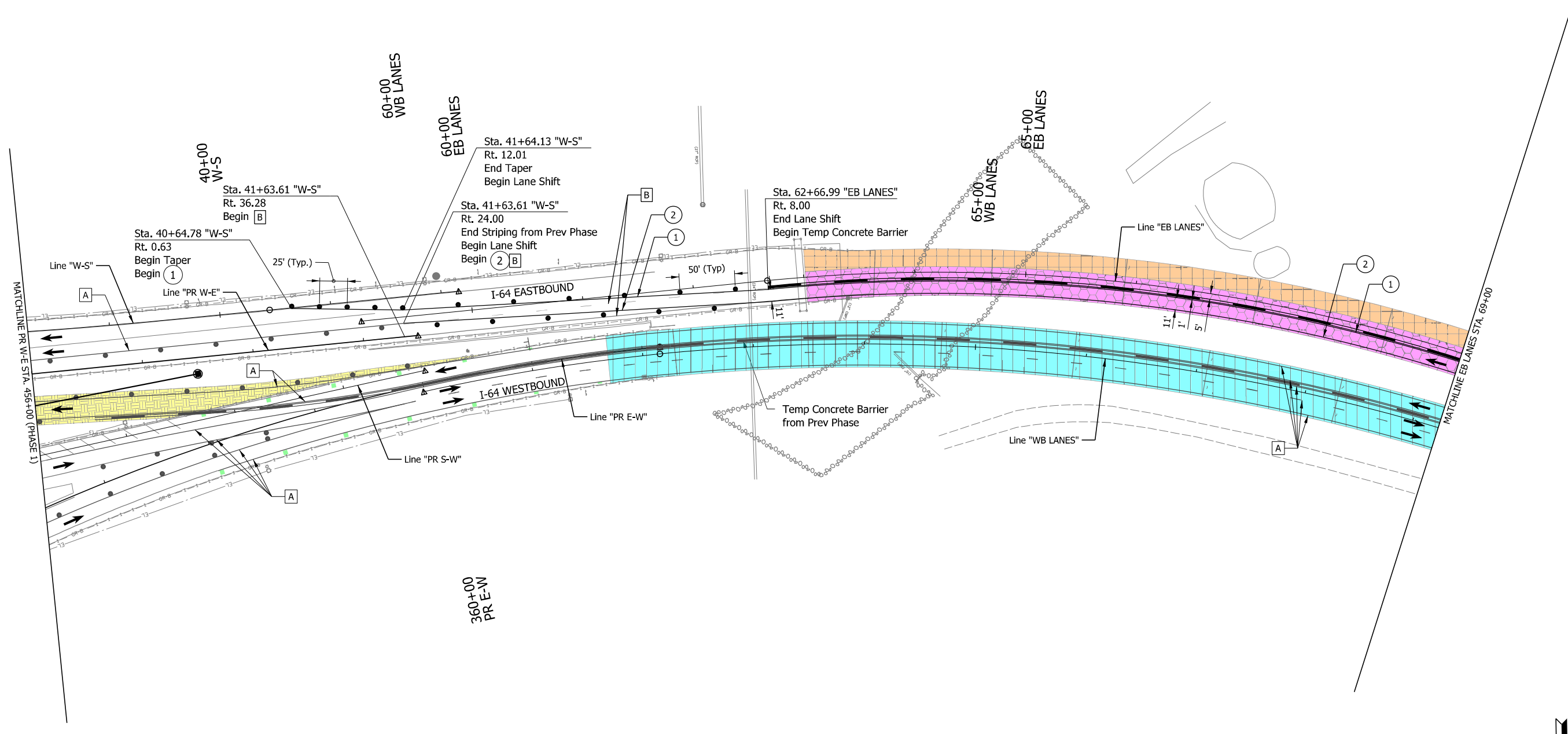
A-47

| | | |
|--------------------------|-----------------|----------------------|
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| DESIGNED: DPS | 3/11/21 | DRAWN: DPS 3/11/21 |
| CHECKED: AWL | 3/26/21 | CHECKED: AWL 3/26/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 200+00 TO STA. 5214+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 18 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 16-JUN-2021 09:00 AM

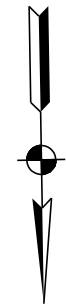


- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



A-48

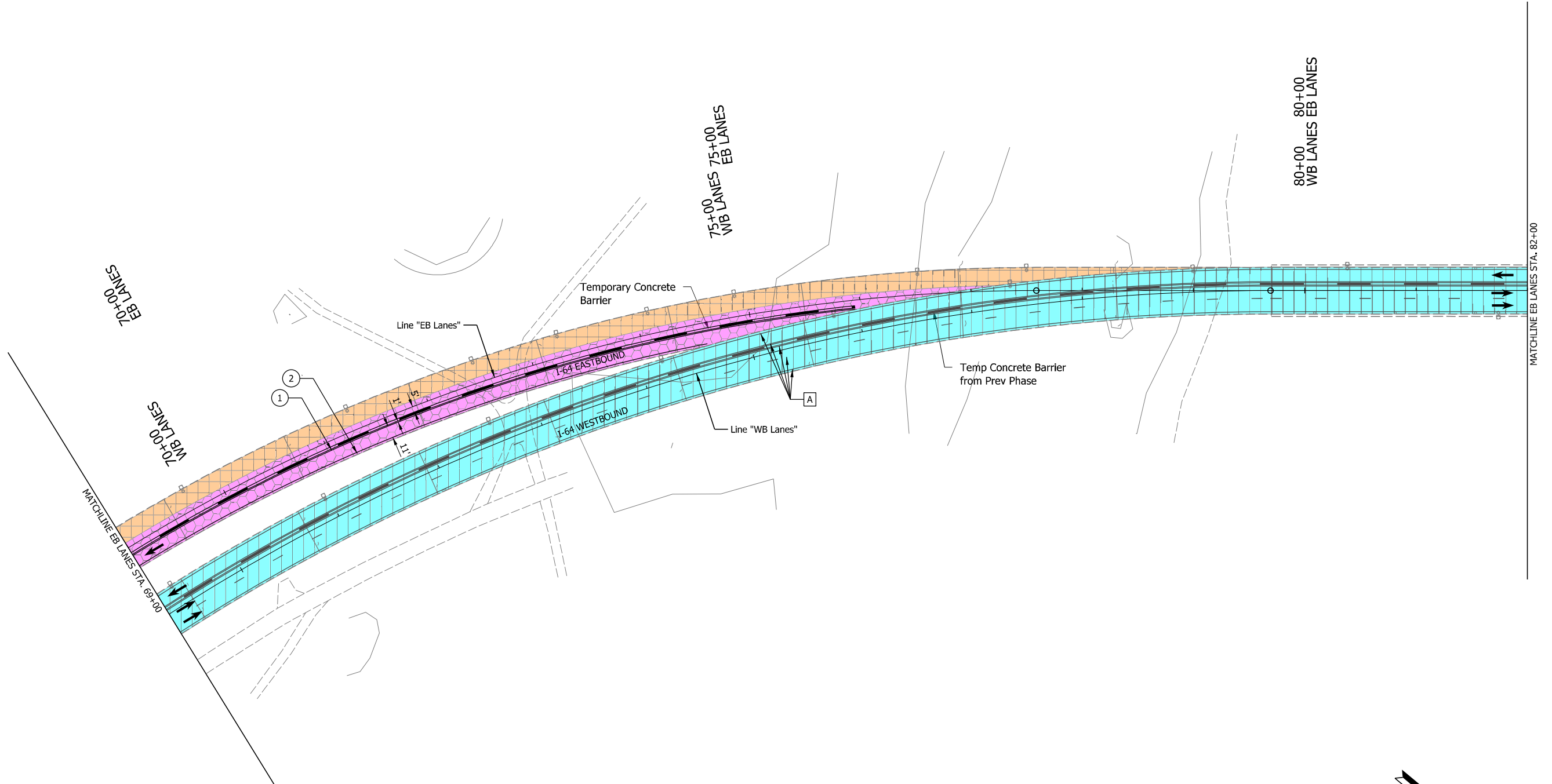
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| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 4/20/21 | DRAWN: DPS |
| CHECKED: AWL | 4/23/21 | CHECKED: AWL |

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 2
STA. 456+00 TO STA. 69+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 19 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 17-JUN-2021 16:55 PM



- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



A-49

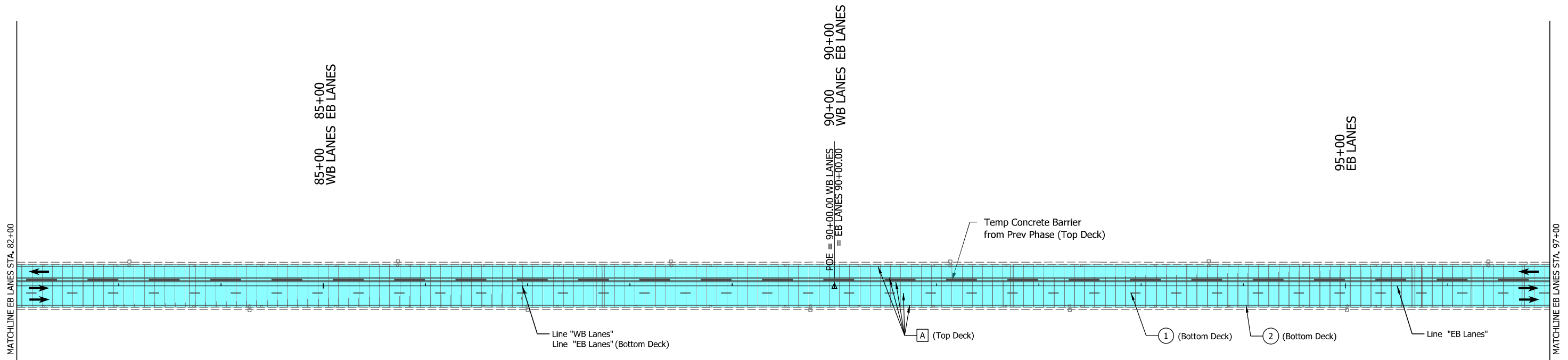
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| DESIGNED: DPS | 4/20/21 | DRAWN: DPS 4/20/21 |
| CHECKED: AWL | 4/23/21 | CHECKED: AWL 4/23/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 2
STA. 69+00 TO STA. 82+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 20 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 16-JUN-2021 09:01 AM

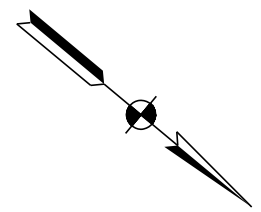


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



Plot: 16-JUN-2021 09:01 AM



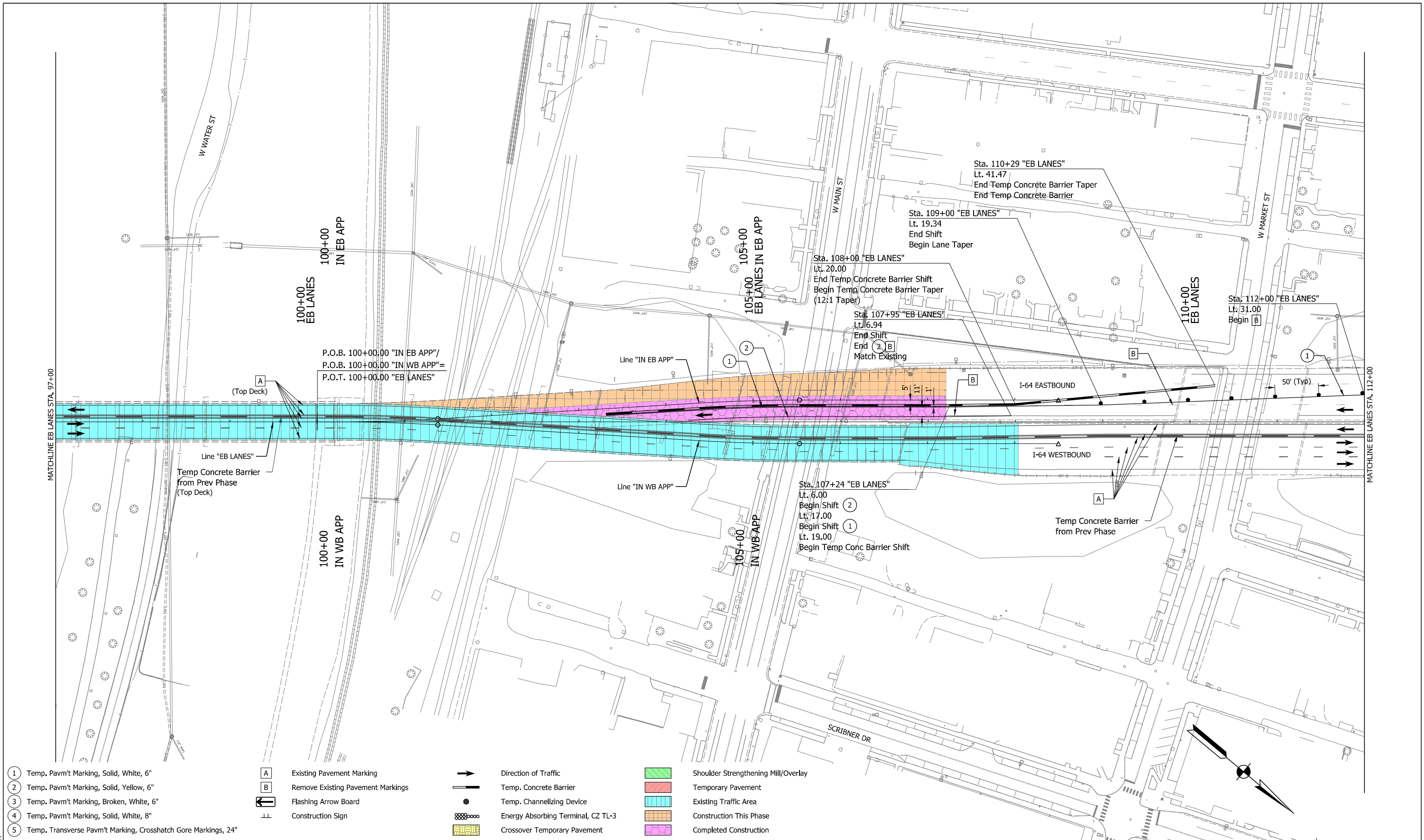
A-50

| | | |
|--------------------------|-----------------|--------------|
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| DESIGNED: DPS | 4/20/21 | DRAWN: DPS |
| CHECKED: AWL | 4/23/21 | CHECKED: AWL |
| | | 4/23/21 |

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 2
STA. 82+00 TO STA. 97+00

| | | |
|------------------|-------------|-------|
| HORIZONTAL SCALE | BRIDGE FILE | |
| 1"=50' | | |
| VERTICAL SCALE | DESIGNATION | |
| | 1702255 | |
| BUILDABLE UNIT | SHEETS | |
| 2B | 21 | of 43 |
| CONTRACT | PROJECT | |
| B-40719 | 1702255 | |



- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Concrete Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



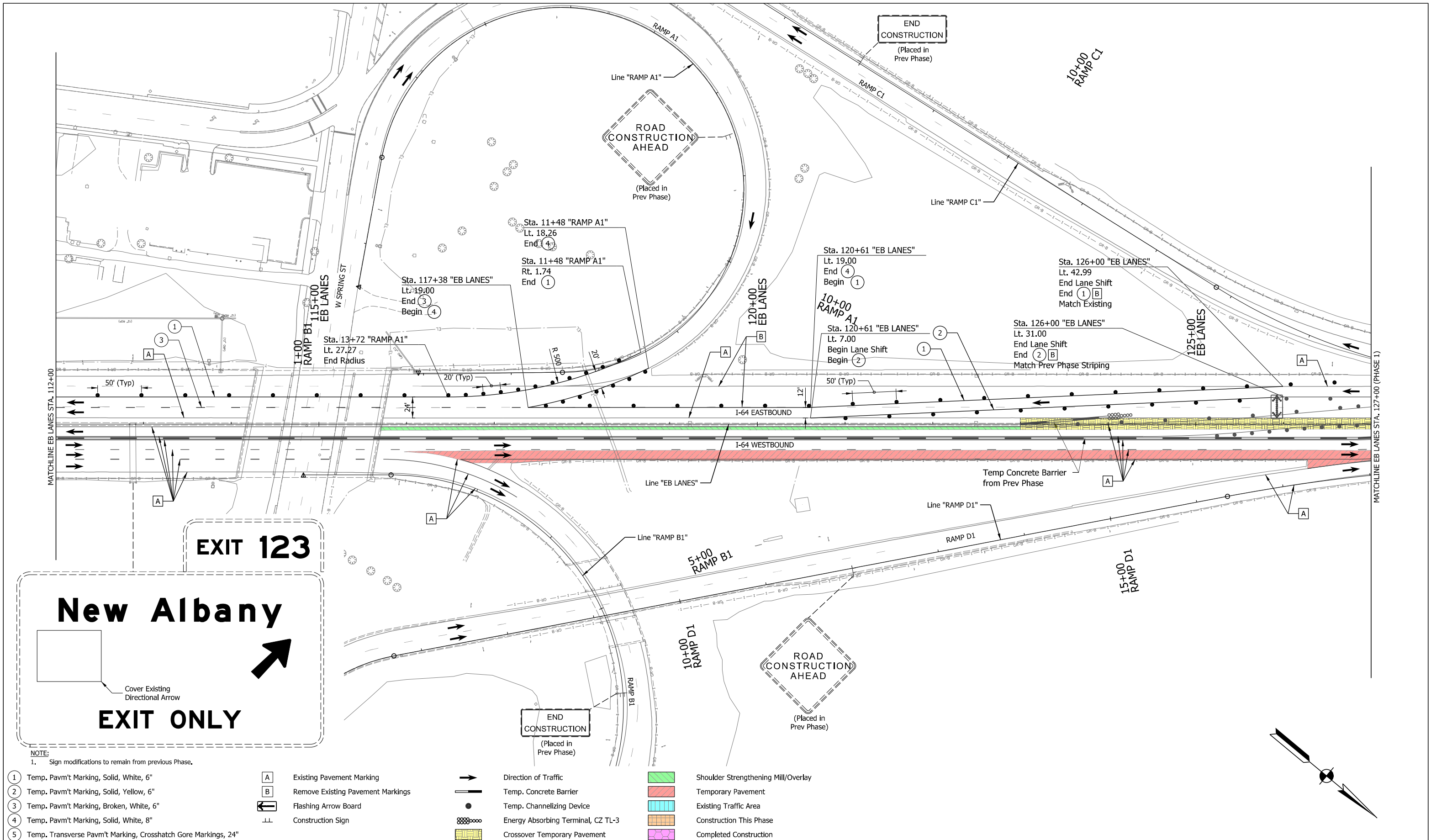
A-51

| | | |
|--------------------------|-----------------|----------------------|
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| DESIGNED: DPS | 4/20/21 | DRAWN: DPS 4/20/21 |
| CHECKED: AWL | 4/23/21 | CHECKED: AWL 4/23/21 |

INDIANA DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC MOT PHASE 2
 STA. 97+00 TO STA. 112+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | DESIGNATION |
| VERTICAL SCALE | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 22 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 16-JUN-2021 09:02 AM



EXIT 123

New Albany

Cover Existing Directional Arrow

EXIT ONLY

NOTE:
1. Sign modifications to remain from previous Phase.

- | | | | |
|--|-------------------------------------|------------------------------------|-------------------------------------|
| ① Temp. Pavm't Marking, Solid, White, 6" | A Existing Pavement Marking | Direction of Traffic | Shoulder Strengthening Mill/Overlay |
| ② Temp. Pavm't Marking, Solid, Yellow, 6" | B Remove Existing Pavement Markings | Temp. Concrete Barrier | Temporary Pavement |
| ③ Temp. Pavm't Marking, Broken, White, 6" | Flashing Arrow Board | Temp. Channelizing Device | Existing Traffic Area |
| ④ Temp. Pavm't Marking, Solid, White, 8" | Construction Sign | Energy Absorbing Terminal, CZ TL-3 | Construction This Phase |
| ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24" | | Crossover Temporary Pavement | Completed Construction |



A-52

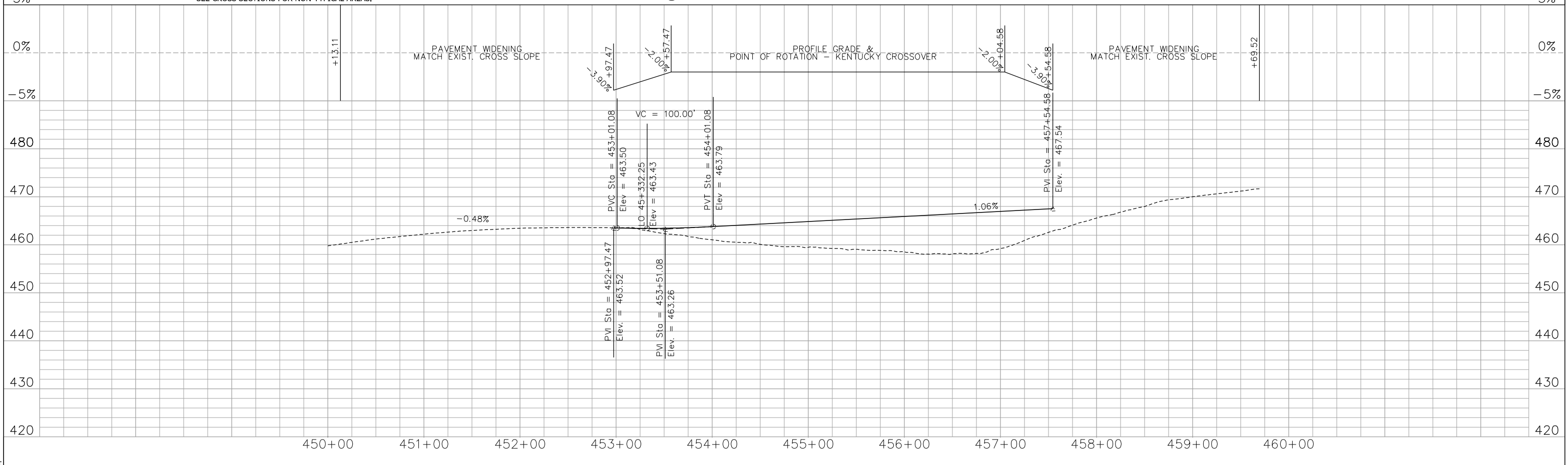
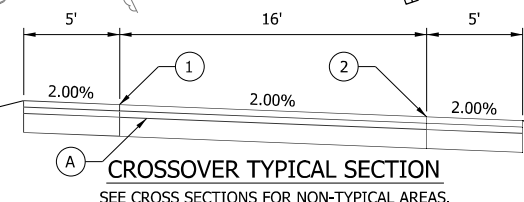
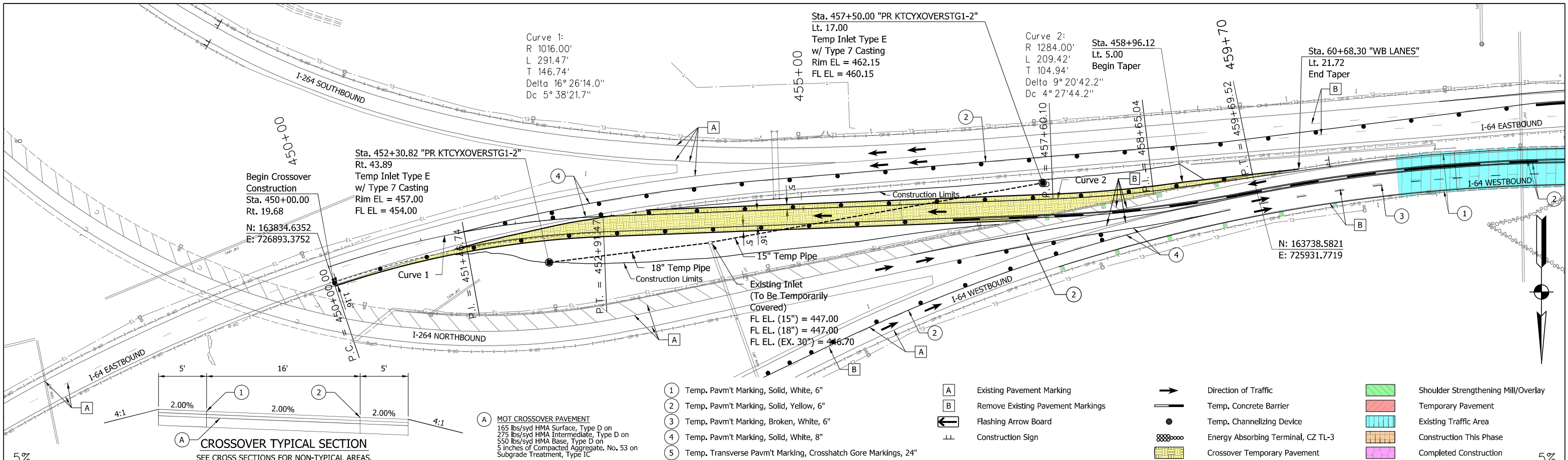
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| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: DPS | 4/20/21 | DRAWN: DPS 4/20/21 |
| CHECKED: AWL | 4/23/21 | CHECKED: AWL 4/23/21 |

INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC MOT PHASE 2
STA. 112+00 TO STA. 127+00

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 23 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

Plot: 16-JUN-2021 09:02 AM



Plot: 17-JUN-2021 16:56 PM



A-53

| | | |
|--------------------------|-----------------|--------------|
| RECOMMENDED FOR APPROVAL | DESIGN ENGINEER | DATE |
| DESIGNED: KLO | 4/20/21 | DRAWN: KLO |
| CHECKED: AWL | 4/23/21 | CHECKED: AWL |

INDIANA
 DEPARTMENT OF TRANSPORTATION

KENTUCKY CROSSOVER
 PLAN & PROFILE SHEET

| | |
|------------------|-------------|
| HORIZONTAL SCALE | BRIDGE FILE |
| 1"=50' | |
| VERTICAL SCALE | DESIGNATION |
| 1"=10' | 1702255 |
| BUILDABLE UNIT | SHEETS |
| 2B | 24 of 43 |
| CONTRACT | PROJECT |
| B-40719 | 1702255 |

APPENDIX B: IPaC Coordination

Susan Castle

Subject: FW: Lead Des. No. 1702255, Sherman Minton Renewal Project, Floyd County, IN and Jefferson County, KY

From: Hinkle, Meghan <MHinkle@indot.IN.gov>
Sent: Wednesday, May 19, 2021 1:08 PM
To: Susan Castle <susanc@metricenv.com>
Cc: Miller, Brandon <BraMiller1@indot.IN.gov>; Bales, Ronald <rbales@indot.IN.gov>; Corbin, Daniel <DCorbin@indot.IN.gov>; Samantha Wickizer <samanthaw@metricenv.com>; Tony Miller <tonym@metricenv.com>
Subject: RE: Lead Des. No. 1702255, Sherman Minton Renewal Project, Floyd County, IN and Jefferson County, KY

Susan,

I also spoke with USFWS and they are in agreement with my thought process. In the environmental re-evaluation document just state that re-coordination was not required with the IN Section 7 determination because all new impacts were on in Kentucky. Re-coordination occurred with KY DOT to update their Section 7 determination.

Let me know if you have any additional questions.

Meghan Hinkle

Major Projects / LPA Review Liaison
Environmental Services Division
Indiana Department of Transportation
100 N Senate Ave N758-ES
Indianapolis, IN 46204-2216
New Work Cell: 317-416-6649
Email: MHinkle@indot.IN.gov



From: Susan Castle <susanc@metricenv.com>
Sent: Wednesday, May 19, 2021 1:02 PM
To: Hinkle, Meghan <MHinkle@indot.IN.gov>
Cc: Miller, Brandon <BraMiller1@indot.IN.gov>; Bales, Ronald <rbales@indot.IN.gov>; Corbin, Daniel <DCorbin@indot.IN.gov>; Samantha Wickizer <samanthaw@metricenv.com>; Tony Miller <tonym@metricenv.com>
Subject: RE: Lead Des. No. 1702255, Sherman Minton Renewal Project, Floyd County, IN and Jefferson County, KY

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Hi Meghan,

Great! Thank you very much.



Susan Castle

Senior Scientist

O 317.608.2730

M 317.379.3649

6958 Hillsdale Court
Indianapolis, IN 46250
www.metricenv.com

Certified DBE/MBE/SBE Company
Indiana | Kentucky | Ohio | West Virginia

From: Hinkle, Meghan <MHinkle@indot.IN.gov>
Sent: Wednesday, May 19, 2021 12:21 PM
To: Susan Castle <susanc@metricenv.com>
Cc: Miller, Brandon <BraMiller1@indot.IN.gov>; Bales, Ronald <rbales@indot.IN.gov>; Corbin, Daniel <DCorbin@indot.IN.gov>; Samantha Wickizer <samanthaw@metricenv.com>; Tony Miller <tonym@metricenv.com>
Subject: RE: Lead Des. No. 1702255, Sherman Minton Renewal Project, Floyd County, IN and Jefferson County, KY

Good Afternoon,

See my responses below in green.

Let me know if you have any additional questions.

Thanks,

Meghan Hinkle

Major Projects / LPA Review Liaison
Environmental Services Division
Indiana Department of Transportation
100 N Senate Ave N758-ES
Indianapolis, IN 46204-2216
New Work Cell: 317-416-6649
Email: MHinkle@indot.IN.gov



From: Susan Castle <susanc@metricenv.com>
Sent: Wednesday, May 19, 2021 11:11 AM
To: Hinkle, Meghan <MHinkle@indot.IN.gov>
Cc: Miller, Brandon <BraMiller1@indot.IN.gov>; Bales, Ronald <rbales@indot.IN.gov>; Corbin, Daniel <DCorbin@indot.IN.gov>; Samantha Wickizer <samanthaw@metricenv.com>; Tony Miller <tonym@metricenv.com>
Subject: RE: Lead Des. No. 1702255, Sherman Minton Renewal Project, Floyd County, IN and Jefferson County, KY

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Hi Meghan,

The Consistency Letter was prepared for the entire project which includes Indiana and Kentucky. **Yes the entire project was discussed in the IPaC submittal because it is one project, however, the Rangewide Programmatic Agreement (IPaC) only applies to the IN footprint portion of the project. The IN project scope has not changed. It sounds like only the KY scope has changed and they have a different Section 7 coordination process.**

The determination key questions that will be answered differently than previously submitted for IPaC Record Locator #616-18059899 are as follows:

#5: Does the project include *any* activities that are **greater than 300 feet** from existing road/rail surfaces? This answer will change from “No” to “Yes” *Due to stabilizing the top of the levee for construction access using a geotextile overlaid with 12 inch coarse aggregate from the Shawnee Golf Course entrance road to the Kentucky approach bridges (approx. 1,440 ft.). This levee access on the KY side of the project is greater than 300 feet from the existing road/rail surfaces. This is the KY side not the IN side. And if this response changes, IPaC cannot be completed.*

#14: Does the project include wetland or stream protection activities associated with compensatory wetland mitigation? This answer will change from “No” to “Yes” *We are going to have to mitigate for the wetland impacts on the KY side (0.48 acres) via mitigation banking. This is the KY side not the IN side.*

#22: Will the project involve the use of **temporary** lighting *during* the active season? This answer was “No” and likely will be changed to “Yes”; however, please see the response from the designer below. **Based on the designers response below I do not see a concern with USFWS coordination or a need to update IPaC.**

The temporary lighting we are using basically matches the existing lighting currently being used on the bridge. We may move a few fixtures to accommodate the painters platform but there were existing light fixtures in place that have functioned for the past few decades.

Per Section 11.5.3.1:

- *Design-Build Contractor shall maintain required highway illumination levels for all open traffic movements and corresponding operational lighting during construction of the Project, whether by maintenance of existing lighting or by installation of temporary lighting, until new lighting fixtures are installed and operational.*
- *Temporary lighting shall achieve existing lighting illumination levels as a minimum, including Average Maintained Horizontal Illuminance, Minimum Illuminance, and Average/Minimum Uniformity Ratio.*

The designer said he could answer it either way (“no” or “yes”). Again, normal or permanent temporary lighting will be in place each evening but matches the existing lighting. I’m sure Kokosing will occasionally use constructing lighting that focuses on a given task or activity along the corridor, just like any other large construction project that requires nighttime activities. When they do this, there will already be roadway lighting in use which is the existing lighting on the bridge.

If you would like to call me to discuss, please feel free to call my Cell Phone 317-379-3649 or if you’d like to have a Teams Meeting, please let me know.

Thank you for your assistance.



Susan Castle
Senior Scientist

From: [Logsdon, Andrew M \(KYTC\)](#)
To: [Samantha Wickizer](#)
Cc: [Day, Jana \(KYTC\)](#); [Tony Miller](#); [Kokal, Jeff](#)
Subject: Re: I-64 Sherman Minton KY Site Visit 3-19-2021 Meeting Summary
Date: Wednesday, May 26, 2021 6:49:19 PM
Attachments: [image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)
[image007.png](#)
[image008.png](#)
[image009.png](#)
[image010.png](#)
[image011.png](#)
[image001.png](#)

You guys can handle it on your end.
The gray bat and NLEB letters shouldn't need to be redone

Thanks

From: Samantha Wickizer <samanthaw@metricenv.com>
Sent: Wednesday, May 26, 2021 6:10 PM
To: Logsdon, Andrew M (KYTC) <Andrew.Logsdon@ky.gov>
Cc: Day, Jana (KYTC) <jana.day@ky.gov>; Tony Miller <tonym@metricenv.com>; Kokal, Jeff <Jeff.Kokal@jacobs.com>
Subject: RE: I-64 Sherman Minton KY Site Visit 3-19-2021 Meeting Summary

Hello Andrew,

Thank you and yes it did and I really appreciate the follow-up.

It sounds, as I suspected, just a very different way in which the two states handle this process, we just needed the verification on our end that everything is fully satisfied from the KYTC perspective even with the additional access area.

There will still be no impacts to the river.

Would you like for us to provide the updated species list, or will you handle that on your end?



Samantha Wickizer, CESSWI
Technical Consultant/ Stormwater Specialist

M 317.608.2798

6958 Hillsdale Court

Indianapolis, IN 46250
www.metricenv.com

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Indiana | Kentucky | Ohio | West Virginia

From: Logsdon, Andrew M (KYTC) <Andrew.Logsdon@ky.gov>
Sent: Wednesday, May 26, 2021 8:44 AM
To: Samantha Wickizer <samanthaw@metricenv.com>
Cc: Day, Jana (KYTC) <jana.day@ky.gov>; Tony Miller <tonym@metricenv.com>; Kokal, Jeff <Jeff.Kokal@jacobs.com>
Subject: Re: I-64 Sherman Minton KY Site Visit 3-19-2021 Meeting Summary

Samantha,

We don't usually re-up the IPaC every 90 days and our field office and FHWA has never had an issue with that.

Since there was a minor addition to the impact area I can see the utility in doing a new IPaC entry but the new access along the levee didn't change any call we made in regards to section 7 but since Indiana is asking about it let's just do what they want and complete a new IPaC species list and attach it to our no effect finding. The call we made on each listed species hasn't changed. It won't unless there are impacts to the river.

The existing facility is an interstate bridge in a urban area and temporary lighting during construction will have no impact on bat foraging.

Did this get your questions answered?

From: Samantha Wickizer <samanthaw@metricenv.com>
Sent: Tuesday, May 25, 2021 3:06 PM
To: Logsdon, Andrew M (KYTC) <Andrew.Logsdon@ky.gov>
Cc: Day, Jana (KYTC) <jana.day@ky.gov>; Tony Miller <tonym@metricenv.com>; Kokal, Jeff <Jeff.Kokal@jacobs.com>
Subject: RE: I-64 Sherman Minton KY Site Visit 3-19-2021 Meeting Summary

Good afternoon Andrew,

Could you please provide a response to my email below at your earliest convenience.

Thank you,



Samantha Wickizer, CESSWI
Technical Consultant/ Stormwater Specialist

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From: Samantha Wickizer
Sent: Wednesday, May 19, 2021 4:48 PM
To: Logsdon, Andrew M (KYTC) <Andrew.Logsdon@ky.gov>
Cc: Day, Jana (KYTC) <jana.day@ky.gov>; Tony Miller <tonym@metricenv.com>
Subject: RE: I-64 Sherman Minton KY Site Visit 3-19-2021 Meeting Summary

Good afternoon Andrew,

Thank you so much for your continued help on this issue.

We included your original communications in March regarding this as a piece of our environmental permit applications. There were comments in the IFA review process that the IPaC that was provided was only good for 90 days and did not encompass the additional area of the project associated with the access on the levee.

Is there additional coordination that needs to occur for USFWS/IPaC documentations as it relates to the use of the levee access road, wetland impacts, or potential for 'temporary' lighting on the Kentucky side of the project?

These were concerns outlined by INDOT, but as they are on the KY side of the project they have stated that as long as we document in the additional information submittal for the CE and our coordination with you, that will suffice for them.

I have attached our communications with INDOT for your reference.

Please advise.



Samantha Wickizer, CESSWI
Technical Consultant/ Stormwater Specialist

M 317.608.2798

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Indianapolis, IN 46250
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United States Department of the Interior



FISH AND WILDLIFE SERVICE
Kentucky Ecological Services Field Office
J C Watts Federal Building, Room 265
330 West Broadway
Frankfort, KY 40601-8670
Phone: (502) 695-0468 Fax: (502) 695-1024
<http://www.fws.gov/frankfort/>

In Reply Refer To:

May 27, 2021

Consultation Code: 04EK1000-2021-SLI-0883

Event Code: 04EK1000-2021-E-03040

Project Name: Sherman Minton Bridge Revised

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

Your concern for the protection of endangered and threatened species is greatly appreciated. The purpose of the Endangered Species Act (ESA) of 1973, as amended (16 U.S.C. 1531 et seq.) (ESA) is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. The species list attached to this letter fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the ESA to provide information as to whether any proposed or listed species may be present in the area of a proposed action. This is not a concurrence letter; additional consultation with the Service may be required.

The Information in Your Species List:

The enclosed species list identifies federal trust species and critical habitat that may occur within the boundary that you entered into IPaC. For your species list to most accurately represent the species that may potentially be affected by the proposed project, the boundary that you input into IPaC should represent the entire “action area” of the proposed project by considering all the potential “effects of the action,” including potential direct, indirect, and cumulative effects, to federally-listed species or their critical habitat as defined in 50 CFR 402.02. This includes effects of any “interrelated actions” that are part of a larger action and depend on the larger action for their justification and “interdependent actions” that have no independent utility apart from the action under consideration (e.g.; utilities, access roads, etc.) and future actions that are reasonably certain to occur as a result of the proposed project (e.g.; development in response to a new road). If your project is likely to have significant indirect effects that extend well beyond the project footprint (e.g., long-term impacts to water quality), we highly recommend that you

coordinate with the Service early to appropriately define your action area and ensure that you are evaluating all the species that could potentially be affected.

We must advise you that our database is a compilation of collection records made available by various individuals and resource agencies available to the Service and may not be all-inclusive. This information is seldom based on comprehensive surveys of all potential habitats and, thus, does not necessarily provide conclusive evidence that species are present or absent at a specific locality. New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list.

Please note that “critical habitat” refers to specific areas identified as essential for the conservation of a species that have been designated by regulation. Critical habitat usually does not include all the habitat that the species is known to occupy or all the habitat that may be important to the species. Thus, even if your project area does not include critical habitat, the species on the list may still be present.

Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the ESA, the accuracy of this species list should be verified after 90 days. The Service recommends that verification be completed by visiting the ECOS-IPaC website at regular intervals during project planning and implementation for updates to species lists and associated information. To re-access your project in IPaC, go to the IPaC web site (<https://ecos.fws.gov/ipac/>), select “Need an updated species list?”, and enter the consultation code on this letter.

ESA Obligations for Federal Projects:

Under sections 7(a)(1) and 7(a)(2) of the ESA and its implementing regulations (50 CFR 402 et seq.), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

If a Federal project (a project authorized, funded, or carried out by a federal agency) may affect federally-listed species or critical habitat, the Federal agency is required to consult with the Service under section 7 of the ESA, pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at: <http://www.fws.gov/endangered/esa-library/pdf/TOC-GLOS.PDF>

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). Recommended contents of a Biological Assessment are described at 50 CFR 402.12. For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat.

ESA Obligations for Non-federal Projects:

Proposed projects that do not have a federal nexus (non-federal projects) are not subject to the obligation to consult under section 7 of the ESA. However, section 9 of the ESA prohibits certain activities that directly or indirectly affect federally-listed species. These prohibitions apply to all individuals subject to the jurisdiction of the United States. Non-federal project proponents can request technical assistance from the Service regarding recommendations on how to avoid and/or minimize impacts to listed species. The project proponent can choose to implement avoidance, minimization, and mitigation measures in a proposed project design to avoid ESA violations.

Additional Species-specific Information:

In addition to the species list, IPaC also provides general species-specific technical assistance that may be helpful when designing a project and evaluating potential impacts to species. To access this information from the IPaC site (<https://ecos.fws.gov/ipac/>), click on the text “My Projects” on the left of the black bar at the top of the screen (you will need to be logged into your account to do this). Click on the project name in the list of projects; then, click on the “Project Home” button that appears. Next, click on the “See Resources” button under the “Resources” heading. A list of species will appear on the screen. Directly above this list, on the right side, is a link that will take you to pdfs of the “Species Guidelines” available for species in your list. Alternatively, these documents and a link to the “ECOS species profile” can be accessed by clicking on an individual species in the online resource list.

Next Steps:

Requests for additional technical assistance or consultation from the Kentucky Field Office should be submitted following guidance on the following page <http://www.fws.gov/frankfort/PreDevelopment.html> and the document retrieved by clicking the “outline” link at that page. When submitting correspondence about your project to our office, please include the Consultation Tracking Number in the header of this letter. (There is no need to provide us with a copy of the IPaC-generated letter and species list.)

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Kentucky Ecological Services Field Office

J C Watts Federal Building, Room 265

330 West Broadway

Frankfort, KY 40601-8670

(502) 695-0468

Project Summary

Consultation Code: 04EK1000-2021-SLI-0883

Event Code: 04EK1000-2021-E-03040

Project Name: Sherman Minton Bridge Revised

Project Type: BRIDGE CONSTRUCTION / MAINTENANCE

Project Description: Kentucky side of the Sherman Minton Bridge corridor construction project

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@38.274690050000004,-85.81584880084019,14z>



Counties: Jefferson County, Kentucky

Endangered Species Act Species

There is a total of 14 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 3 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

| NAME | STATUS |
|---|------------|
| <p>Gray Bat <i>Myotis grisescens</i></p> <p>No critical habitat has been designated for this species.</p> <p>This species only needs to be considered under the following conditions:</p> <ul style="list-style-type: none">▪ The project area includes potential gray bat habitat. <p>Species profile: https://ecos.fws.gov/ecp/species/6329</p> <p>General project design guidelines: https://ecos.fws.gov/docs/tess/ipac_project_design_guidelines/doc6422.pdf</p> | Endangered |
| <p>Indiana Bat <i>Myotis sodalis</i></p> <p>There is final critical habitat for this species. The location of the critical habitat is not available.</p> <p>This species only needs to be considered under the following conditions:</p> <ul style="list-style-type: none">▪ The project area includes 'potential' habitat. All activities in this location should consider possible effects to this species. <p>Species profile: https://ecos.fws.gov/ecp/species/5949</p> <p>General project design guidelines: https://ecos.fws.gov/docs/tess/ipac_project_design_guidelines/doc6422.pdf</p> | Endangered |
| <p>Northern Long-eared Bat <i>Myotis septentrionalis</i></p> <p>No critical habitat has been designated for this species.</p> <p>This species only needs to be considered under the following conditions:</p> <ul style="list-style-type: none">▪ The specified area includes areas in which incidental take would not be prohibited under the 4(d) rule. For reporting purposes, please use the "streamlined consultation form," linked to in the "general project design guidelines" for the species. <p>Species profile: https://ecos.fws.gov/ecp/species/9045</p> <p>General project design guidelines: https://ecos.fws.gov/docs/tess/ipac_project_design_guidelines/doc6422.pdf</p> | Threatened |

Clams

| NAME | STATUS |
|--|------------|
| <p>Clubshell <i>Pleurobema clava</i> Population: Wherever found; Except where listed as Experimental Populations No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/3789 General project design guidelines: https://ecos.fws.gov/docs/tess/ipac_project_design_guidelines/doc5639.pdf</p> | Endangered |
| <p>Fanshell <i>Cyprogenia stegaria</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/4822 General project design guidelines: https://ecos.fws.gov/docs/tess/ipac_project_design_guidelines/doc5639.pdf</p> | Endangered |
| <p>Northern Riffleshell <i>Epioblasma torulosa rangiana</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/527 General project design guidelines: https://ecos.fws.gov/docs/tess/ipac_project_design_guidelines/doc5639.pdf</p> | Endangered |
| <p>Orangefoot Pimpleback (pearlymussel) <i>Plethobasus cooperianus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/1132 General project design guidelines: https://ecos.fws.gov/docs/tess/ipac_project_design_guidelines/doc5639.pdf</p> | Endangered |
| <p>Pink Mucket (pearlymussel) <i>Lampsilis abrupta</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/7829 General project design guidelines: https://ecos.fws.gov/docs/tess/ipac_project_design_guidelines/doc5639.pdf</p> | Endangered |
| <p>Rabbitsfoot <i>Quadrula cylindrica cylindrica</i> There is final critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/5165 General project design guidelines: https://ecos.fws.gov/docs/tess/ipac_project_design_guidelines/doc5639.pdf</p> | Threatened |
| <p>Ring Pink (mussel) <i>Obovaria retusa</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/4128 General project design guidelines: https://ecos.fws.gov/docs/tess/ipac_project_design_guidelines/doc5639.pdf</p> | Endangered |
| <p>Rough Pigtoe <i>Pleurobema plenum</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/6894 General project design guidelines: https://ecos.fws.gov/docs/tess/ipac_project_design_guidelines/doc5639.pdf</p> | Endangered |
| <p>Sheepnose Mussel <i>Plethobasus cyphus</i> No critical habitat has been designated for this species.</p> | Endangered |

| NAME | STATUS |
|--|------------|
| Species profile: https://ecos.fws.gov/ecp/species/6903 General project design guidelines: https://ecos.fws.gov/docs/tess/ipac_project_design_guidelines/doc5639.pdf | |
| Spectaclecase (mussel) <i>Cumberlandia monodonta</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/7867 General project design guidelines: https://ecos.fws.gov/docs/tess/ipac_project_design_guidelines/doc5639.pdf | Endangered |

Flowering Plants

| NAME | STATUS |
|--|------------|
| Running Buffalo Clover <i>Trifolium stoloniferum</i> Population: No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/2529 | Endangered |

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

From: [Hokanson, Shawn M \(EEC\)](#)
To: [Samantha Wickizer](#); [Vogeler, Samantha N \(EEC\)](#)
Cc: [Amy Smith](#); [Brant Mercer](#); [Gary Obert](#); [Kokal, Jeff](#); [Morris, Daniel](#); [Tony Miller](#); [Vince Epps](#); [vpm@kokosing.biz](#); [Michels, Adam C \(KYTC\)](#); [Bowman, Sandra A](#); [Corbin, Daniel](#); [Curry, Jennifer](#); [Greer, Daryl J \(KYTC\)](#); [dav.kessinger@mbakerintl.com](#); [gcouch@indot.in.gov](#); [Heustis, Ronald](#); [kahrenholtz@kaskaskiaeng.com](#); [LKANG@indot.in.gov](#); [Logsdon, Andrew M \(KYTC\)](#); [mhamman@mbakerintl.com](#); [Pusti, Mary](#); [Meredith, Royce J \(KYTC\)](#); [Slaymon, Shawn](#); [SMCPDBBV@indot.in.gov](#); [srubin@indot.in.gov](#); [Foreman, Tim E \(KYTC\)](#); [tobias.randolph@parsons.com](#); [Virginia Flynn](#); [wendy.vachet@mbakerintl.com](#)
Subject: RE: I-64 Sherman Minton Bridge KY Floodway permit follow-up
Date: Monday, March 1, 2021 11:47:32 AM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)

The project is exempt from KY permitting but does have to meet the FEMA requirements (CLOMR/LOMR if applicable). They would not have to request CLOMR/LOMR for the temporary riprap to allow access.

Shawn Hokanson
502-782-6977

From: Samantha Wickizer [mailto:samanthaw@metricenv.com]
Sent: Monday, March 1, 2021 11:44 AM
To: Hokanson, Shawn M (EEC) <Shawn.Hokanson@ky.gov>; Vogeler, Samantha N (EEC) <samantha.vogeler@ky.gov>
Cc: Amy Smith <amys@metricenv.com>; Brant Mercer <bdm@kokosing.biz>; Gary Obert <geo@kokosing.biz>; Kokal, Jeff <Jeff.Kokal@jacobs.com>; Morris, Daniel <Daniel.Morris@jacobs.com>; Tony Miller <tonym@metricenv.com>; Vince Epps <vincee@metricenv.com>; vpm@kokosing.biz; Michels, Adam C (KYTC) <Adam.Michels@ky.gov>; Bowman, Sandra A <SBowman@indot.IN.gov>; Corbin, Daniel <DCorbin@indot.IN.gov>; Curry, Jennifer <JCurry1@indot.IN.gov>; Greer, Daryl J (KYTC) <Daryl.Greer@ky.gov>; dav.kessinger@mbakerintl.com; gcouch@indot.in.gov; Heustis, Ronald <RHEUSTIS@indot.IN.gov>; kahrenholtz@kaskaskiaeng.com; LKANG@indot.in.gov; Logsdon, Andrew M (KYTC) <Andrew.Logsdon@ky.gov>; mhamman@mbakerintl.com; Pusti, Mary <Mary.Pusti@mbakerintl.com>; Meredith, Royce J (KYTC) <Royce.Meredith@ky.gov>; Slaymon, Shawn <SSlaymon@indot.IN.gov>; SMCPDBBV@indot.in.gov; srubin@indot.in.gov; Foreman, Tim E (KYTC) <Tim.Foreman@ky.gov>; tobias.randolph@parsons.com; Virginia Flynn <VFlynn@kaskaskiaeng.com>; wendy.vachet@mbakerintl.com
Subject: I-64 Sherman Minton Bridge KY Floodway permit follow-up

Hello Shawn,

Thanks again for your attendance of our meeting last week regarding this project.

During our discussions relating to the KY Floodway permit, it was indicated that the project may qualify for an exemption due to being DOT owned, but you wanted to double-check whether temporary work is covered by the exemption.

Have you had the chance to look into this or can you provide additional guidance on this matter?

Thank you,

Samantha Wickizer, CESSWI
Project Scientist



Phone: 317.608.2798 Email: samanthaw@metricenv.com
6971 Hillsdale Court, Indianapolis, IN 46250



www.metricenv.com

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APPENDIX C:
Section 106 of the National Historic
Preservation Act



ANDY BESHEAR
GOVERNOR

TOURISM, ARTS AND HERITAGE CABINET
KENTUCKY HERITAGE COUNCIL
THE STATE HISTORIC PRESERVATION OFFICE

MICHAEL E. BERRY
SECRETARY

JACQUELINE COLEMAN
LT. GOVERNOR

410 HIGH STREET
FRANKFORT, KENTUCKY 40601
(502) 564-7005
www.heritage.ky.gov

CRAIG A. POTTS
EXECUTIVE DIRECTOR &
STATE HISTORIC
PRESERVATION OFFICER

07/27/2021

Danny Peake, Director
Division of Environmental Analysis
Kentucky Transportation Cabinet
200 Mero Street,
Frankfort, KY 40622

RE: Sherman Minton/I-64 Bridge Project Determination of Effect
Louisville, Jefferson County, Kentucky and Floyd County, Indiana
KYTC Item No. 5-10027

Dear Mr. Peake:

Thank you for your submittal of a letter report for the above-referenced undertaking. We understand the purpose of this project is to address deteriorating structural conditions on the Sherman Minton bridge, carrying I-64 south over the Ohio in Louisville, Kentucky. Associated work will include bridge deck repair, rehab or replacement of steel and hanger cables, painting, and other minor details.

Our office has worked extensively with KYTC on this project, culminating in a site visit to discuss the project scope in June of 2021. Since that time, a cultural historic has been prepared for this undertaking, however, due to time restraints, the project is set to begin construction immediately. Therefore, consultation for this project has been unique, and remains ongoing.

Based on the findings of the consultant in the report, our office is prepared to offer a **revised conditional finding of No Adverse Effect** to allow construction to commence. This conditional finding hinges upon the following:

- A revised Cultural Historic that addresses the issues outlined via email on July 27th, 2021 is submitted and accepted by our office no later than six months from the date of this letter. Our office is withholding comment on eligibility discussions included within the report until the revised copy is received. This will include a paper copy of the revised report, if requested by our office.
- Survey forms, likely to be submitted by a different consultant, are submitted and accepted by our office for all resources within the APE no later than one year from the date of this letter. This should be coordinated with our office, and follow the guidelines discussed via email on July 27th, 2021. This will include a paper copy of the survey forms, if requested by our office.

**RE: Sherman Minton/I-64 Bridge Project Determination of Effect in Louisville, Jefferson County, Kentucky and Floyd County, Indiana
KYTC Item No. 5-10027**

- Use of the NRHP-eligible Shawnee Golf Course, Shawnee Residential Historic District and Northwestern Parkway are temporary, with any damages to the roadway caused by construction vehicles to be repaired by the end of the project.
- Use of the NRHP-eligible Louisville Floodwall levee for construction vehicle access shall protect the resource from adverse effect, with no more than one foot of topsoil removed from the levee, before the aggregate and geotextile road is set up. All area of removed matter shall be returned to the state in which it was found at the end of the project duration, conforming to pre-existing dimensions including height, width, and slope of the levee, as this will be temporarily altered by the construction of a road for contractor access. This shall be confirmed with SHPO at the end of this undertaking.

We look forward to additional consultation on this project. Should you have any questions, please contact Gabrielle Fernandez of my staff at Gabrielle.Fernandez@ky.gov.

Sincerely,



Craig A. Potts,
Executive Director and
State Historic Preservation Officer

CP: gf, KHC # 61847
cc. Amanda Abner, DEA



An Equal Opportunity Employer



July 9, 2021

Craig A. Potts
State Historic Preservation Officer
Kentucky Heritage Council
410 High Street
Frankfort, KY 40601

RE: Additional Project Information, NRHP Evaluation, and Effects Determination
Recommendations
Sherman-Minton Bridge (NBI No. I64-123-04691D) Renewal Project
Kentucky Item ID No. 5-10027
Floyd County, Indiana and Jefferson County, Kentucky

Dear Mr. Potts,

The Indiana Department of Transportation (INDOT) and the Kentucky Transportation Cabinet (KYTC) jointly propose to undertake a bridge improvement project for the Sherman-Minton Bridge, which carries Interstate 64 (I-64) and United States Highway 150 (US 150) over the Ohio River between Indiana and Kentucky. Kokosing Construction Company, Inc., is under contract with the Indiana Finance Authority to complete the project. Metric Environmental, LLC, has been subcontracted to complete the environmental and Section 106 documentation for the project.

The intent of this letter is two-fold. It is to provide a summary of the previously supplied information about this project, as well as to give additional information on recent project activities and updated design plans. Its second purpose is to provide a historic context, identification of historic resources within the Area of Potential Effects (APE) approved by the Kentucky SHPO's office on June 3, 2021, and to conclude by discussing the undertaking's potential effects on historic properties in the APE that we are reasonably able to foresee.

Project Description

The need for this project is due to the Sherman-Minton Bridge's deteriorated structural condition. Bridge inspection reports conducted in 2019 noted the evidence of cracking, spalling, road salt damage, and corrosion to the bridge deck; paint failure throughout the structure has left it unprotected and susceptible to corrosion; cable hangers and connectors exhibit surface corrosion and displacement; steel stringers have heavy corrosion and section loss; and the bridge substructure shows evidence of minor cracking and spalling in the piers and abutments. The purpose of the project is to extend the service life of the Sherman-Minton Bridge and its associated Indiana and Kentucky approach bridges. Completed in 1962, the double-decked Sherman-Minton Bridge carries six lanes of traffic over the Ohio River, and it is the most heavily travelled crossing of the Ohio River in Kentucky with an average daily traffic count of 90,000.

The proposed project consists of rehabilitating and painting the Sherman-Minton Bridge. The bridge crossing includes five ancillary bridge structures (four of which are in Indiana), and the project scope includes replacement or refurbishing all the bridge decks, rehabilitation or replacement of structural steel elements and hanger cables, new lighting, drainage repairs, and painting of the steel components. Project limits on the Sherman-Minton Bridge extend 0.32 miles south of the Elm Street south on-ramp in New Albany, Indiana, to 0.45 miles from the I-64 south off-ramp in Louisville, Kentucky.

The single ancillary bridge project in Kentucky concerns Bridge No. 056B00161N, a 27-span steel girder bridge that acts as both the east and west bound approach to the Sherman-Minton Bridge. The bridge has a curb-to-curb roadway width of 42 feet with three-foot shoulders, and it carries three northbound and three southbound lanes. Proposed work to this bridge includes demolition of the existing bridge decks, cleaning the girders, installing new connectors, casting a new reinforced concrete bridge deck and railings. The existing concrete approach slabs will also be replaced with new reinforced concrete approach slabs. Structural repairs to the bridge would address any cracks in steel members, repair or replacement of lateral restraints, and replacement of deteriorated bolts and rivets. Cleaning and painting work includes removal of the existing paint and application of new on all steel members of the bridge. Proposed substructure work to the bridge consists of concrete repairs to the pier caps and columns, with reinforcement repairs performed to the columns as needed. The bridge's dimensions will not change because of the proposed undertaking, nor will right-of-way acquisition be needed.

Maintenance of Traffic (MOT)

The MOT for the project will require one eastbound (EB) and one westbound (WB) lane will be closed throughout construction. Open travel lanes will shift location on the Sherman Minton Bridge during construction. Two EB and two WB travel lanes will remain open for cross-river traffic and existing access ramps will remain open except for the following allowances:

- 180 nights per construction year during which two EB and two WB lanes and associated access ramps will be closed each night approximately from 9 pm to 4 am and 10 pm to 5 am, respectively. Cross-river traffic will be maintained with one EB and one WB travel lane open and a temporary crossover lane in Kentucky for I-64 WB to merge with I-264.
- Short-term closure of the Sherman Minton Bridge will be allowed for one nine consecutive day period and up to three weekend closures during each construction year; excluding holidays and community events detailed below. During the short-term bridge closure, all Interstate 64 (US 150) cross-river traffic will be diverted to detour routes.

I-64 EB & WB MOT – Shoulder rehabilitation for travel lane reconfiguration and Indiana Crossover; no pavement footprint widening.

I-64/I-265 Interchange MOT – Mill and fill shoulder rehabilitation to existing pavement footprint on EB to NB and SB to WB ramps; existing one-lane ramps will be restriped and converted to two-lane ramps.

I-65 /I-265 Interchange MOT – Mill and fill shoulder rehabilitation to existing pavement footprint on EB to SB ramp; existing one-lane ramp will be restriped and converted to a two-lane ramp.

Provisions are included for local traffic access and through-traffic dependent businesses by retaining existing access ramps in Indiana and Kentucky; through-traffic dependent businesses by maintaining cross-river travel lanes in both directions; public notification, signage according to MOT, and posting requirements during construction; and alternative routes that remain within the interstate system for local river crossings. Both I-264 and I-64 through traffic may use I-65 paired Kennedy/Lincoln bridges. Local access to west Louisville could follow the same alternate route or remain on the local arterial network.

Construction is slated to begin in the 2021 season and is anticipated to take 2 to 3 years to complete. Please refer to Appendix A for project location maps, and Appendix B for project plans and MOT plans.

Efforts to Identify Historic Properties: 2018-2020

The environmental coordination for the project began in 2018, including Section 106 investigations. An early coordination letter was sent to identified consulting parties on December 11, 2018. In a letter dated January 7, 2019, the Indiana SHPO acknowledged receipt of the early coordination letter. The SHPO then indicated that, because the project met the criteria as a “minor project” under the Indiana Minor Projects Programmatic Agreement, the Indiana SHPO did not plan on participating in the Section 106 consultation for the undertaking. On May 7, 2020, the INDOT Cultural Resources Office issued clearance for both historic property and archaeological investigations under their Minor Projects Programmatic Agreement. In a letter dated December 20, 2018, the Kentucky Heritage Council (KHC), or SHPO, acknowledged receipt of the early coordination letter and inclusion of a proposed “worst case” Area of Potential Effects (APE) that included six possible MOT alternatives. The KHC stated the APE appeared to be adequate for the project, and they also noted the presence of the National Register of Historic Places (NRHP) listed Shawnee Park and Portland Historic District as within the proposed APE.

On March 2, 2020, the KYTC issued consulting party invitations to the following parties:

- Greg Fischer, Mayor, City of Louisville
- Layla George, President and CEO, Olmsted Parks Conservancy, Louisville
- Jay Gulicke, Chair, Olmsted Parks Conservancy, Louisville
- Donna Purvis, Councilwoman, Louisville Metro Council District 5

The letter included a project overview including the purpose and need, and explained the process for participating as a consulting party for the undertaking. No responses were received.

Above Ground Investigation

On March 26, 2020, the KYTC provided the KHC a report prepared by their office with assistance from staff at Michael Baker International, Inc. (Michael Baker), that listed the eligibility and effects determinations for above-ground resources in the proposed APE, which by that point had been narrowed

down to four possible MOT alternatives. Included with the report was a cover letter signed by Daniel R. Peake of KYTC requesting the KHC to review and comment upon the possible effects for each of the four remaining MOT options to help KYTC and the design-build team choose the final preferred alternative. In the same letter, KYTC, on behalf of the Federal Highway Administration (FHWA), expressed their opinion that the undertaking, as proposed, would have “No Adverse Effect” on any of the NRHP eligible or listed properties within the proposed APE. Mr. Peake stated they would present the preferred MOT alternative to the KHC once it had been selected to determine if additional consultation is warranted. Lastly, Mr. Peake’s letter concluded by requesting the KHC issue a conditional determination of “No Adverse Effect” for the undertaking.

The report included in the March 26, 2020, mailing to the KHC that was prepared by Michael Baker discussed the following four proposed MOT routes (note: MOT options 3 and 6 were the two options that had been removed from consideration):

MOT 1 – Two lanes open, both decks/directions (existing EB and WB decks). Consists of three phases; travel lanes shift for each phase; 1-lane closure for both EB and WB during all phases, 2-lanes maintained for both EB and WB during all phases, maintains east/west river crossing, no access changes required, and traffic lanes adjacent to construction work zone. MOT 1 has the fewest/lowest temporary changes from existing conditions for access, east/west crossings, diverted traffic, congestion, user cost, driver expectation, community and EJ impacts, and the most public support. However, MOT 1 also has the highest duration and estimated project cost, and reduced safety with traffic adjacent to the work zone.

MOT 2 – One lane open, both decks/directions (EB and WB decks). Consists of two phases; travel lanes shift for each phase, 2-lane closures for both EB and WB during all phases, 1-lane maintained for both EB and WB during all phases, Kentucky crossover for I-64 WB merge with I-264, maintains east/west river crossing, no access changes required, and traffic lanes adjacent to construction work zone. MOT 2 has the fewer/lower temporary changes from existing conditions for access, east/west crossings, diverted traffic, travel time, congestion, estimated project cost, user cost, driver expectation, community and EJ impacts, shorter duration, and more favored public comments. However, MOT 2 also has reduced safety with traffic adjacent to the work zone.

MOT 4 – Reversible center lane (AM-EB / PM-WB) and one-way EB/WB lanes open on one deck. Consists of two phases; Phase 1 - lower deck closed / Phase 2 - upper deck closed, traffic lanes separated from construction work zone, AM (midnight to noon) – two (2) EB lanes and one (1) WB lane open during all phases, PM (midnight to noon) – one (1) EB lanes and two (2) WB lanes open during all phases, minimal center lane transition with moveable barrier, maintains east/west river crossing, access changes required, and a Kentucky crossover for I-64 WB merge with I-264. MOT 4 has lower temporary changes from existing conditions for access, diverted traffic, travel time, congestion, user cost, driver expectation, separated traffic and work zone, community and EJ impacts, and the favorable public support. The one-deck MOT options (3, 4, and 6) have similar duration, estimated project cost, and separate traffic from the work zone, however, MOT 4 retains open east/west crossings, the movable barrier can be operated during open traffic conditions (no temporary closure), and with some uncertainty for driver expectation.

MOT 5 – Short-term closure of the bridge and all six-lanes. Closes east/west river crossing, consists of one phase, access changes required, all traffic diverted, detour route: I-265 to I-65, and concurrent access ramp modifications. MOT5 (Full Duration Closure) has the most/highest temporary changes from existing conditions for access, east/west crossings, diverted traffic, travel time, congestion, user cost, driver expectation, community and EJ impacts, and little to no public support. Short-term use of MOT 5 may be considered based upon constructability requirements, as warranted. May be considered based upon constructability requirements or as warranted to keep the Sherman Minton Bridge safely in-service during construction.

The report included a revised APE that included a proposed APE for each MOT option (1, 2, 4, and 5), as well as an inventory of previously identified historic sites within the APEs of all four MOT options. The KHC is the repository of previous survey data, and Michael Baker submitted a request to that office for this data that pertained to the APEs of the four MOT options. Please refer to Appendix C for a copy of the March 26, 2020, letter from the KYTC to the KHC containing the survey data inventory.

The report discussed anticipated project impacts to the identified historic resources and stated the undertaking would pose no direct impacts to them. Additionally, the potential effect of diverted traffic causing congestion would be alleviated by the proximity of several other local bridges, as well as by the arterial grid street network of western Louisville, which was noted to currently operate under capacity. Of all the resources listed, the report listed the Shawnee Golf Course as having the largest potential impact due to its location at the base of the Sherman Minton Bridge and its approach. The report concluded by summarizing the undertaking's effects for each MOT alternative upon the Olmsted Park System of Louisville Historic District (HD), using the examples of adverse effects listed in 36 CFR Part § 800.5(a)(2). The report concluded that the anticipated project impacts would introduce visual and audible elements to the Olmsted Park System of Louisville HD within the proposed APE, but the effects would be temporary and minimal, and would not diminish their integrity. Thus, the appropriate finding put forward by the report for the undertaking was "No Adverse Effect."

In a letter dated March 30, 2020, Craig Potts of the KHC replied to the KYTC's March 26, 2020, correspondence and stated that due to time constraints, the KHC would agree to provide a conditional determination of "No Adverse Effect" but their concurrence was contingent upon the final choice of a project alternative along with a finalized effects analysis and effect finding, and the subsequent requirement of KYTC to notify the appropriate consulting parties.

Archaeological Investigation

On March 26, 2020, archaeologists from Wood Environment & Infrastructure Solutions, Inc. (Wood) (Reynolds & Kelly, 2020) submitted a Phase I archaeological survey report for the undertaking to KYTC (KY OSA registration No.: FY20-10684). The Phase I survey consisted of an approximate 2.5 acres (1.01 hectares) of area beneath the Kentucky approach to the Sherman Minton Bridge within the Shawnee Golf Course. Field methods consisted of pedestrian survey and visual inspection of the entire APE and shovel test probes were excavated to a depth of 1 meter. Deeper soils were examined through the excavation of auger probes into the base of selected shovel test probe sites to no more than 50 meters. A total of 28

shovel test probes and six bucket augers were excavated within the APE. Two newly recorded archaeological sites were identified during field work, but due to heavy soil disturbance and a lack of significant artifact concentrations at either site, the archaeologists recommend neither site as NRHP eligible. Their report concluded that no further archaeological investigations in the APE were recommended.

In a letter dated March 27, 2020, Mr. Peake of the KYTC submitted a summary of the archaeologists finding and recommendations to the KHC and requested a conditional concurrence of “No Historic Properties Affected” for archaeological resources. In a letter dated March 27, 2020, Mr. Potts of the KHC concurred with the finding, conditional upon receiving the Phase I report no later than June 30, 2020, and that if the APE should be modified, the KHC would be consulted to determine if additional archaeological work may be required.

In a letter dated July 15, 2020, Mr. Peake KYTC submitted an electronic copy of the revised Phase I archaeology report, along with KYTC’s comments requesting revisions that had been made to the revised report. In a letter dated July 27, 2020, Mr. Potts of the KHC acknowledged receipt of the revised Phase I report and agreed with the recommendations of the archaeologists. The KHC issued their concurrence with the KYTC’s finding of “No Historic Properties Affected” for archeological resources for this undertaking.

The Categorical Exclusion, Level 4 document was approved by the Federal Highway Administration on October 7, 2020.

Please refer to Appendix C for Section 106 coordination and correspondence.

Efforts to Identify Historic Properties: 2020-Present

In August 2020, project team leaders from Indiana and Kentucky determined their preferred MOT alternative to be MOT 1, which consists of three phases with travel lane shifts for each phase. MOT 1 ensures that two lanes will remain open on both the eastbound (EB) and westbound (WB) decks during all phases; there will be one lane closure for both EB and WB during all phases; an east/west river crossing will be maintained; no access changes will be required; and there will be traffic lanes adjacent to construction work zones. The preferred alternative was the culmination of months of input from community leaders, business owners, and other stakeholders gathered at multiple public meetings.

On December 18, 2020, the Indiana Finance Authority selected Kokosing Construction Company, Inc., as the design-build contractor for the undertaking. Stage 3 plans for MOT maintenance were issued to INDOT on June 16, 2021. Refer to Appendix B for a copy of the MOT plans.

Since the submittal of the eligibility and effects determinations for above-ground resources report prepared by Michael Baker and the Phase I archaeology report prepared by Wood in March 2020, the project scope was modified to include the creation of a temporary access road that would be installed on an existing levee southwest of the bridge in Louisville, located within the NRHP-eligible Shawnee Golf Course. The temporary road is required for construction access to the approach bridges. The access road

would service construction vehicles for access to the Kentucky approach bridges. As proposed, truck traffic would enter the Shawnee Golf Course from the intersection of the Northwestern Parkway at Bank Street. There is an existing paved golf course road at this location that will allow access from Bank Street to the levee. The proposed access road would be constructed of temporary gravel aggregate over geotextile fabric, and it would measure 12 feet wide, one foot thick, and 1,440 feet long. Total temporary right-of-way needed for the access road is approximately 6.77 acres. Kokosing has devised the following schedule for construction traffic utilizing the access road, broken into four phases:

1. Construction of the access road and aggregate pads under the approach spans
 - Approximately 20 loads of stone / day for 4 days
 - 1 - Cat D6 Dozer stripping topsoil / spreading stone for 4 days
 - 1 - 72" smooth drum roller compacting stone for 4 days
 - Occasional pickup trucks in and out of the site
2. Normal day to day operations throughout the project
 - Water Truck making passes as needed for dust control
 - Occasional Pickup trucks in and out of the site
 - 1 to 2 concrete trucks with light loads (2 CY to 3 CY) for pier cap patching
 - Occasional low boy trailer bringing equipment as needed in and out for KCC and North Star (this will be infrequent after the initial mobilization)
3. Hydro demolition activities
 - 1 Additional water truck emptying frac tanks from hydro-demo operation and hauling offsite
 - Occasional pickup trucks in and out of the site
4. Removal of the access road and aggregate pads and subsequent stabilization
 - 1 – Track-hoe loading dump trucks for 4 days
 - Approximately 20 dump truck loads / day for 4 days
 - 1 – D6 Dozer for 2 days redressing the levee
 - Occasional pickup trucks in and out of the site
 - 1 - Hydro seeder for 1 day to stabilize the levee once we are complete

Refer to Appendix B for a copy of the proposed levee access road plans.

Thus, because of this subsequent design change to KY Item No. 5-64, the original Area of Potential Effect (APE) has been widened as part of the “reasonable and good faith effort” that is required by the Section 106 process to identify historic properties (36 CFR Part § 800.4(b)(1)).

Archaeological Investigation

Karen Garrard, a Qualified Professional historian who meets the Secretary of the Interior’s Professional Qualifications Standards for Section 106 work per 36 CFR Part § 61, reviewed the proposed newly added project area by conducting a literature review. The expanded APE required for a temporary access road encompasses an additional approximate 6.77 acres located immediately adjacent and southwest of the

area examined during the original archaeological investigation for the project in 2020. Based on review of the documentation Metric received regarding the previous investigation, there are no previously recorded archaeological resources within the additional acreage nor any sites eligible for listing in the NRHP within its vicinity.

Dr. Garrard conducted a literature review utilizing online information from the United States Department of Agriculture, Natural Resource Conservation Service Web Soil Survey, as well as historical county soil surveys. Mapped soils within the additional acreage consist entirely of Urban land-Udorthents complex, smoothed, 0 to 50 percent slopes; udorthents comprise a deep and very deep mixture of geologic and artificial materials that have been graded and smoothed. The additional 6.77 acres have been heavily disturbed from the levee's construction, and the levee itself is composed of artificial fill. For these reasons, Dr. Garrard determined no archaeological investigations were warranted as the area within the expanded APE was not considered to have the potential to contain archaeological resources.

An archaeological summary report was submitted to the KYTC in early May 2021, and on May 13, 2021, the KYTC agreed with the findings, and recommended that a "No Historic Properties Affected" determination was still appropriate for the undertaking. On May 17, 2021, the KHC also gave their concurrence with this determination. In a letter dated May 18, 2021, Daniel Corbin of the Indiana Finance Authority (IFA) notified Vince Martini of Kokosing Construction Company, Inc., that the archaeological investigation for the revised APE had been cleared. Mr. Corbin stated that if the project footprint changes then IFA should be provided a new Section 106 review for archaeology. Refer to Appendix C for copies of this correspondence.

Above Ground Investigation

Candace Hudziak, a Qualified Professional historian who meets the Secretary of the Interior's Professional Qualifications Standards for Section 106 work per 36 CFR Part § 61, conducted a literature review of the project area and established a revised APE to include the addition of a levee access road. The revised APE was submitted to KYTC on May 24, 2021, for review and submittal to the KHC for their review and comment. In a letter dated June 3, 2021, Mr. Potts of KHC acknowledged receipt of the revised APE from the KYTC and expressed their office's concurrence with the APE's adequacy for this undertaking. Mr. Potts stated the KHC "strongly suggest(s) avoidance/minimization to avoid the NRHP-eligible or listed resources within the proposed APE" to avoid adverse effects to those resources.

In preparation of a historic property report Ms. Hudziak encountered a discrepancy regarding the Shawnee Golf Course as being officially listed as part of the Olmsted Park System of Louisville HD. In documentation Metric received regarding previous investigations done by cultural resources consultants on behalf of the Sherman Minton Renewal Project conducted in 2020, the Shawnee Golf Course north of Shawnee Park (and located beneath the Sherman Minton Bridge) was included in the NRHP boundary of the Olmsted Park System of Louisville HD, which was listed on May 17, 1982 (NPS No. 82002715). However, upon further investigation it was discovered the golf course was not included within the NRHP nomination application's boundary for Shawnee Park within the HD.

On Jan 28, 2021, Ms. Hudziak of Metric contacted Lisa Thompson (National Register Coordinator; lisam.thompson@ky.gov), Candi Rinehart (Survey Coordinator; candicea.rinehart@ky.gov) and Karen Stevens (Database Coordinator; karen.stevens@ky.gov), all with the KHC, to confirm if the golf course is currently listed on the NRHP as part of the Olmsted HD. On Jan 28, 2021, Karen Stevens via email stated that within their organization's GIS the golf course is included within the NRHP boundary; however, she noted the topographic map included with the NRHP application submitted to the NPS in 1981 does not include the golf course. Ms. Stevens attached the complete NRHP application to her email, which contained photographs and maps. The map submitted for Shawnee Park does not have the golf course outlined within the boundary.

Also on Jan 28, 2021, Lisa Thompson from the KHC sent a link via email to the complete NR application on the National Archives website (<https://catalog.archives.gov/id/123850704>), which included a map image showing the NRHP boundary for Shawnee Park that did not include the golf course. On Jan 29, 2021, Lisa Thompson replied via email and reiterated that the correct boundary for Shawnee Park is the boundary found in the NRHP application.

On June 29, 2021, an on-site meeting was held to discuss potential impacts to historic properties within the APE. In attendance were Royce Meredith, Dave Harmon, Amanda Abner, and Connor Ouellette of the KYTC; Craig Potts and Gabrielle Fernandez of the KHC; Brad Young of Kokosing Construction Company; Samantha Wickizer, Beth Hillen, and Candy Hudziak of Metric Environmental; and Nathan Maiwald and Chris Gayheart of Louisville Metro Parks. At the meeting, Mr. Potts and Ms. Fernandez of the KHC stated that the levee had recently been determined NRHP eligible and should now be considered a historic resource. The levee was determined eligible as part of Louisville Reach section of the Louisville Metro Flood Protection System (LMFPS), which includes floodwall, levee, and 11 pump stations and associated structures under Criterion A for its association with flood control and community planning in Louisville, with a recommended period of significance of 1947 to 1956. Mr. Potts also indicated that the Shawnee neighborhood may also be NRHP eligible as a historic district. Additionally, Ms. Abner of the KYTC stated the Shawnee Golf Course has also been determined NRHP eligible as part of the Olmsted Park System of Louisville HD in the process of a previous Section 106 investigation.

Please refer to Appendix C for copies all correspondence discussed above.

The following is a brief historic context of the project area, an NRHP evaluation of historic properties both listed and eligible for NRHP listing within the APE, and a discussion of the undertaking's effects upon them.

Historic Context

Founded upon the Falls of the Ohio, the first successful attempt to establish a permanent settlement that became Louisville was made in 1779. The years between 1780 and 1800, which were marked by both the Revolutionary War military and Native American skirmishes in the area, slowed the burgeoning town's development, but The Ohio River, a major artery of commerce, kept the dream alive for Louisville's early speculators and developers. Robert Fulton's patented steamboat design ushered in a new era for all Ohio River port towns, including Louisville, after 1810 and attracted the people and commerce that transformed it into a bustling city by 1830. With an economy led by river trade the city's development

initially stayed close to the riverfront, across from the equally booming Indiana town of Jeffersonville. The Falls dictated that most goods had to be offloaded from the boats and transported by land around this treacherous break in the river, and thus Louisville attracted the warehouses, insurance brokers, bankers and merchants who accommodated the shippers' needs.

At the same time, the community of Portland, which lay downstream of the Falls west of Louisville, was also being developed. Portland was platted in 1814 and again in 1817 due to rapid growth, and many of its first residents were French immigrants who established the town's first businesses and the area's first Catholic church, called Church of Our Lady. For those traveling upstream on the Ohio Portland's wharf became the most convenient spot to unload passengers and cargo before reaching the Falls in the same way Louisville was the stopping point for boats traveling down river. Portland boasted a wharf that rivaled neighboring Louisville's in size, to the latter's great agitation. In 1833 the Louisville and Portland Canal was completed that bypassed the Falls and, in theory, no longer would necessitate unloading boats to pass through. In practice, however, the canal proved too small for larger boats, which frequently ran aground there. Attempts were made to widen the canal to some success, but by the time the canal was useful for all boats the advent of railroad travel was on the horizon, signaling the end of river commerce's dominance. Louisville annexed Portland in 1852, becoming Louisville's first annexation. By the mid-1800s an influx of Irish and German immigrants recreated both settlements to reflect their own traditions and customs. The Portland neighborhood continued to grow and thrive into the 1900s and was primarily favored by working class families due to its proximity to the river front, where workers initially worked the docks and then later transitioned to factory labor.

Just as Louisville was beginning to bolster its river transport with railroad connections to the south the Civil War brought tumult to its local economy. The city was stuck between a rock and a hard place: the Confederates enforced a southern blockade of Louisville, and Union contracts for goods and services barely materialized due to the distrusted loyalties of Louisvillians. When the war was over Louisville emerged undamaged by battles but its reliance on river commerce, which was losing the battle against rail transport, greatly hamstrung its local economy. The railways the city did have traveled south, but the Civil War's devastation of many southern cities made the South economically weak for years to come. Unlike the Indiana cities of Jeffersonville and Clarksville across the river, Louisville's leaders had been slow to attract manufacturing in favor of bolstering river commerce, but after the War new realities rapidly changed their disposition. Throughout the rest of the 1860s and 70s capital was raised for the establishment of hundreds of new factories that made everything from agricultural implements to furniture. Between 1870 and 1880 manufacturing businesses grew from 650 to just over 1,100, and the city's economy was flying once again.¹

Suburban growth began to occur outside of the original section of Louisville near the waterfront in the late 1800s, first to the east and south. This was enabled by the infrastructure upgrades that had begun happening in the city, such as electric streetcars replacing horse-drawn trolleys. The ability to lay down tracks rather than use existing roads for horse-drawn trolleys meant a greater freedom of movement in

¹ George H. Yater, "Louisville: A Historical Overview," in *The Encyclopedia of Louisville*, John E. Kleber, ed. (Louisville, KY: University of Kentucky Press, 2001), xxii.

designing routes. The reliability, affordability, and convenience led to explosion of streetcar growth in urban centers over the next twenty years. Their popularity meant that wherever new tracks were put down, development soon followed. In the meantime, the once-fashionable older neighborhoods of downtown Louisville that were now increasingly suffering from air and noise pollution that resulted from rapid industrial growth concentrated near the river. Real estate developers wasted no time in buying farmland on the outskirts of town and developing it for residences. These “streetcar suburbs” were tailored to those with the means to live outside of the city, and included houses on large lots in secluded, bucolic, and rural settings.

Rapid industrialization also prompted a group of Louisville residents to begin lobbying the city in the late 1880s for the systematic development of public parks and greenspace, a growing movement happening in larger cities across the country whose residents were endeavoring their leaders to plan for future growth, rather to react after the fact. Urban planners often subscribed to the popular aesthetic of the day known as the City Beautiful movement, which stressed the importance of green space, nature, and beauty in the built environment for its citizens. Parks, sweeping boulevards, and access by the public to natural landscapes such as woodlands and water were often outcomes of City Beautiful initiatives.

In 1891 Louisville’s Board of Park Commissioners hired well-known landscape architect Fredrick Law Olmsted to create and design a park system for the city. In 1892 Olmsted presented his design, which called for the creation of Shawnee, Cherokee, and Iroquois Parks, each on the outskirts of the city at that time. The parks were to be connected by broad, tree-lined roads named Algonquin, Southwestern, Northwestern, Eastern, and Southern Parkway that together totaled twenty-six miles of new roadway. Olmsted envisioned each park to have a distinct character, wherein Cherokee Park in the posh eastern Louisville suburbs would be formally landscaped; Iroquois Park in the southern and more outlying part of the city would be kept rugged and forested; and Shawnee Park located adjacent to the Ohio River in western Louisville would be a grassy, scenic spot for recreational and athletic past times. Olmsted’s parkways were to make a circuit between the three parks. Construction began in 1892, and by 1895 all three parks had been developed. Work on the parkways continued into the 1930s, with approximately 15 miles of new boulevards built; however, several sections were never completed. Portions of Shawnee Park and Northwestern Parkway lie within the project’s proposed APE.

Work on Olmsted’s design began in 1892, but because of land acquisition difficulties with the property owners Shawnee Park was one of the last to be developed. Bounded by the Ohio River, West Broadway Street, and Southwestern and Northwestern Parkways, the park contains 180 acres. It features a great lawn, promenades, beaches, a bandstand, and a boat ramp, and in 1927 an 18-hole golf course was added to the north end of the park. In 1911 the city limited the areas where African Americans could go in Shawnee Park and shortly thereafter the city purchased land south of the park to create a park for African Americans. In 1922 the new 61-acre Chickasaw Park, designed by the Olmsted Brothers, opened, and the Parks Board designated it for use by African Americans. In 1924 the Parks Board passed a resolution restricting the larger city parks, such as Shawnee Park, to whites only. As a result, African Americans had access to roughly 120 acres of public park spread across five parks in Louisville, nearly half of which was

contained within a single park – Chickasaw. It would remain this way until Louisville mayor Andrew Broaddus desegregated all city parks in 1955.²

Prior to 1890 the area surrounding Shawnee Park was primarily farmland, and much of it was owned by tobacco farmers.³ However, shortly after Shawnee Park opened savvy real estate developers convinced landowners to sell, and then parceled lots for residential use and advertised them to home buyers interested in suburban living. This would be true for the areas near Iroquois and Cherokee Parks, as well, which were also developed into suburban enclaves. The housing boom near Shawnee Park eventually blossomed into a neighborhood, which was noted for its large houses on manicured lots, with large shade trees lining the streets. The city annexed Shawnee Park and adjacent blocks in 1895, and subsequently the city extended streetcar lines to the neighborhood.⁴ Once this happened residential growth along Northwestern Parkway was all but assured, as residents there could now access municipal utilities such as gas or electric lighting (including streetlights), indoor plumbing and treated water from the city's waterworks, and other services such as police and fire assistance. Assisting the suburban exodus were Olmsted's new boulevards, which in some cases meant upgrading existing roads. Such was the case with Northwestern Parkway, which was new alignment from Shawnee Park until reaching the Portland neighborhood, at which point it became the route formerly known as Third Street. Northwestern Parkway begins at Shawnee Park and travels around the tip of Louisville until terminating at a flood wall near the Fourteenth Street Bridge. Olmsted designed Northwestern Parkway to connect to Cherokee Park but that section was never constructed.⁵ The western section of Northwestern Parkway travels through residential neighborhoods that are located within the project's APE, including the NRHP listed Portland Historic District, with houses built primarily in the late 1800s and early 1900s.

By 1900 three railroad bridges had been erected across the Ohio River, and ferries managed river crossings for all other traffic as they had done since the early 1800s. As automobile use proliferated in the early 1900s, however, popular support for an automobile crossing of the river gained steam. In November 1912, the Kentucky & Indiana Terminal Bridge between New Albany and the Portland neighborhood of Louisville was completed. This bridge was primarily a railroad bridge, but with wagon roads flanking each side of the tracks that travelers paid a toll to use. By the 1920s more cars than horse-drawn wagons were using the side roads on the bridge. This bridge remained open to automobiles until 1979. For Louisville residents who lived farther east and wished to travel to Jeffersonville, however, the Kentucky & Indiana Terminal Bridge did not fit the bill. Public pressure to build a new automobile-only bridge increased and led to the City of Louisville to finance construction of such a bridge through a bond issue. In November 1929 the Municipal Bridge, which was renamed the George Rogers Clark Memorial Bridge in 1949, opened for

² Jane Futrell Winslow, "Chickasaw: An Olmsted Park Built for African Americans," (College Station, TX: Texas A&M University, Department of Landscape Architecture & Urban Planning), available under https://laup.arch.tamu.edu/about/news/BHM/Chickasaw_Winslow,-JF.pdf [accessed July 2021].

³ John C. Pillow, "Shawnee: A Place in Time," *Louisville Courier-Journal*, 1989, available under <https://louisville.edu/cepm/westlou/west-louisville-general/a-place-in-time-courier-journals-history-of-neighborhoods/> [accessed July 2021].

⁴ Ibid.

⁵ Marty Poynter Hedgepeth, "Olmsted Park System of Louisville," National Register of Historic Places Inventory Nomination Form," Louisville, Jefferson County, Kentucky, National Register No. 82002715.

motorists and a toll remained in place until 1946, when the bonds were paid off.⁶ The bridge's traffic merged onto 2nd Street on the Louisville side. That same year another major infrastructure project -- the McAlpine Lock and Dam -- was completed after numerous years of construction. The federally funded project installed a series of locks and dams near Shippingport Island to always ensure a minimum river depth of nine feet to aid river transport.

In January 1937, during the depths of the Great Depression, the entire Ohio Valley was hit by the worst flood in recorded history. Finally on January 27 the Ohio River crested at 57 feet and did not fall back to its normal level until mid-February. Remembered forever after as the "Great Flood," it flooded roughly three-quarters of the city, displaced hundreds of thousands of its residents, caused ninety deaths, and over \$50 million in property damage in Louisville.⁷ The areas closest to the river, such as in the APE, bore the brunt of the flooding. Buildings in the Shawnee and Portland neighborhoods were inundated with water for weeks and caused numerous of them to be destroyed as a result.

In the period after the 1937 flood, the United States Army Corps of Engineers (USACE) undertook a wider and more pro-active role in flood control and protection. Beginning in the late 1940s the Louisville District of the USACE oversaw construction of 4.5 miles of concrete wall in the downtown and 12.5 miles of 30-foot-tall earthen levees near the waterfront, as well as 13 pumping stations that was completed in 1957.⁸ An approximate 1,400-foot section of earthen levee built during the USACE is in the APE south of the Sherman Minton Bridge. Additionally, because of the Great Flood and subsequent flood wall construction several streets close to the waterfront in the Portland area, such as Water and Front streets, are no longer extant.

By 1952 the Clark Memorial Bridge was experiencing peak capacity and causing traffic congestion, but residents balked at reinstating tolls on the Clark Memorial Bridge to fund building a new bridge. In 1955 the federal government agreed to build a new bridge between New Albany and Louisville as part of construction of Interstate 64, as well as a bridge between Jeffersonville and Louisville as part of the construction of Interstate 65, of which they would pay 90 percent and Indiana and Kentucky would pay the remaining 10 percent for the New Albany and Jeffersonville bridges, respectively. Louisville engineering firm Hazelet and Erdal was chosen in 1956 to construct the New Albany span, which consisted of a double-decker two-span steel through arch bridge, with a total length of 2,054 feet. The new structure was completed in 1962, and it was named in honor of Floyd County, Indiana, native son Sherman Minton, who served as United States Senator from 1934 to 1940 and Associate Justice of the Supreme Court from 1949 to 1956. Later that same year the American Institute of Steel Construction named the Sherman Minton Bridge the most beautiful long-span bridge in the nation for 1961.⁹ Hazelet and Erdal was once again chosen to design the Jeffersonville bridge, which they came up with a six-lane, single-deck cantilevered through truss bridge that was completed in 1963, and formally dedicated in December as the John Fitzgerald Kennedy Memorial Bridge.

⁶ Carl E. Kramer, "Bridges, Automobiles," in *The Encyclopedia of Louisville*, 123

⁷ Yater, "Louisville: A Historical Overview," in *The Encyclopedia of Louisville*, xxvi-xxvii.

⁸ Charles E. Parrish, "Floods and Flood Control," in *The Encyclopedia of Louisville*, 297.

⁹ *Ibid.*, 124.

Post-War suburban development in Louisville continued to expand, primarily to the south and east, aided in part by construction of interstate highways in that period. The Portland neighborhood, historically working class, remained similar in character, although the demographics became less white and more minority there. The affluent Shawnee neighborhood along Northwestern Parkway, developed in the 1890s and early 1900s, began to become more middle-class by the 1940s as wealthier families left for newer suburbs. The inhabitants were still primarily white, but south of that area, near Chickasaw Park, African American neighborhoods flourished. When housing barriers that caused segregation began to be dismantled in the 1960s the first African Americans moved into the northern sections of the Shawnee neighborhood along Northwestern Parkway.

The APE includes most of the Shawnee Golf Course, an USACE earthen levee, a portion of the Shawnee neighborhood along the Northwestern Parkway corridor, a portion of the Portland neighborhood on the north side of Rudd Avenue, and a large portion of river frontage that contains the McAlpine Locks and Dam Visitors Area, the Portland Wharf Park, and the Sherman Minton approach bridges. Northwestern Parkway and the Portland Historic District are both listed on the NRHP. And, as stated previously, the golf course and the earthen levee have been previously determined NRHP eligible. Though the McAlpine Locks and Dam Visitors Center is within the APE, the engineering system itself lies outside of the APE, and thus it will not be evaluated in this report. Since the Sherman Minton Bridge is part of the Interstate system, it has previously been determined not eligible for the NRHP under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005. For photographs of the APE, refer to Appendix G.

The following section will evaluate the NRHP eligibility, or continued eligibility, of the Shawnee Golf Course and Northwestern Parkway as part of the Olmsted Park System of Louisville HD, the earthen levee, the Shawnee residential neighborhood along the Northwestern Parkway corridor, and the Portland HD.

NRHP Evaluations

Project architectural historians surveyed the project area and utilized public property records to identify all properties that are 50 years old or older. A field survey was completed to identify and photograph the properties meeting the age criteria by Metric Environmental staff. Project architectural historians took photographs of individual properties, as well as representative viewscape and streetscape photographs. Based on research and field review, properties were documented individually or in groups (i.e., districts).

Following completion of the field survey, all identified properties in the PA were evaluated for NRHP eligibility, using established professional criteria and considerations set forth in the National Park Service's *National Register Bulletin 15: How to Apply the National Register Criteria for Evaluation*.¹⁰ According to the NRHP's Eligibility Guidelines, for a property to be eligible for listing on the NRHP it must meet two requirements: it must be significant under one or more of the NRHP criteria and it must also possess integrity. The NRHP recognizes seven aspects or qualities that define integrity: location, design, setting, materials, workmanship, feeling and association. To retain integrity a property should possess most of

¹⁰ Available at http://www.nps.gov/nr/publications/bulletins/nrb15/nrb15_2.htm.

these aspects since a property's integrity is the key component for conveying its significance. To understand a property's significance, it is important to know why, where and when the property is significant. A property's most important aspects of integrity should best convey its area(s) of significance. Thus, establishing a property's significance is the first necessity of evaluating a property's integrity. The steps in assessing integrity of a property are:

- Defining the essential physical features necessary for representing its significance.
- Determining whether the essential physical features are visible enough to convey their significance.
- Determining whether the property should be compared with similar properties.
- Determining, based upon its significance, which aspects of a property's integrity are particularly vital for being nominated.

Once a property is found to possess integrity, it must also satisfy at least one of the four following criteria, known as the Criteria for Evaluation:

- A. The property is associated with events that have made a significant contribution to the broad patterns of our history; or
- B. The property is associated with the lives of persons significant in our past; or
- C. The property embodies the distinctive characteristics of a type, period, or method of construction, or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components may lack individual distinction; or
- D. The property may have yielded or may be likely to yield information important in prehistory or history.

To determine a property's significance, it must be evaluated within its historic context. A property must be shown through scholarly research to have been extant at the time of its significance and to have played an important role within the theme(s) of its significance; mere association with the time period, historic event or themes, or important person is not enough to establish eligibility under Criterion A or B. Likewise, it is not sufficient for a property to be eligible under Criterion C based upon its rarity or because it was designed by a noted architect; it must also demonstrate significance.

Candace Hudziak evaluated the potential of each resource to meet one or more of the National Register Criteria for Evaluation and conducted an appropriate level of research to determine the NRHP eligibility of each resource.



Photo 1. Photo of the rear elevation of the Shawnee Golf Course's Club House facing southwest

**Olmsted Park System of Louisville Historic District
Cherokee, Iroquois, and Shawnee Parks; Algonquin, Eastern, Northwestern, Southwestern and
Southern Parkways; and Shawnee Golf Course
NPS File No. 82002715
JFL 271 (Northwestern Parkway) and JFWS 306 (Shawnee Park)**

Description: The Olmsted Park System of Louisville Historic District (HD) was listed on the NRHP on May 17, 1982. In his lifetime Frederick Law Olmsted, Sr. created five park systems, and Louisville was his last. Between 1891 and 1935 Olmsted, and later his sons, were responsible for the design and creation of eighteen parks and six parkways throughout the city. The HD's boundary is largely defined by Olmsted's plan, which he presented to the Louisville Parks Commission in 1891.

The nomination's boundary includes Shawnee Park and Northwestern Parkway. Shawnee is in the city's west end next to the Ohio River, and it includes 182 acres of grassy lawns, promenades, boat launches and beaches that were meant to take advantage of the river's scenic beauty. Northwestern Parkway was a 120-foot-wide roadway that Olmsted's design connected it to Eastern Parkway, and from there to Cherokee Park, but that section was not built. Instead, Northwestern Parkway traverses around the tip of western Louisville near the Ohio River, where it is cut off in places due to later interstate and floodwall construction, but it officially terminates near the 14th Street Bridge, running a total length of approximately 3.5 miles.

The 18-hole Shawnee Golf Course was added later to the park's northeastern boundary in 1927, and its inclusion to the park fit neatly with Olmsted's vision for Shawnee as being a recreational-based park. The golf course encompasses roughly 150 acres along the Ohio River waterfront, part of which is located below

the Sherman Minton Bridge. It features a c.1927 one-story clubhouse on a raised basement clad with rustic cut stone with Romanesque-style openings in an arcade across all elevations with a gable-on-hip terra cotta roof.

Significance: The Olmstead Park System of Louisville HD is significant for its association with Fredrick Law Olmsted, a recognized master of landscape architecture throughout the country. It marks the city's first attempt at urban planning, and it reflects the popular City Beautiful movement of the time that emphasized the importance of integrating nature and beauty into urban settings. Though added later to the park, the Shawnee Park Golf Course contributes to Olmsted's vision for Shawnee Park's purpose of providing open space for athletic recreational pursuits. Over time its presence and the architectural contribution of the club house has added value to the surroundings' setting and for this reason the golf course should be considered as a contributing resource to the overall HD.

Recommendation: The Olmsted Park System of Louisville HD is recommended to remain NRHP eligible under Criterion A, for its association with the history of community planning in Louisville, as well as under Criterion C for its outstanding landscape architectural design and its association with master landscape architect Fredrick Law Olmsted, Sr. Due to their significance and intact integrity, the Shawnee Golf Course and Northwestern Parkway are considered contributing elements to this HD. Refer to Appendix F for the proposed NRHP boundary map that includes the Shawnee Golf Course.



Photo 2. View of levee facing southwest from Shawnee Golf Course

Earthen Levee

**Located west of Interstate 64 between the Shawnee Golf Course and Northwestern Parkway
Part of the proposed Louisville Reach Metro Flood Protection System HD**

Description: After passage of the Flood Control Act of 1944, which tasked the US Army Corps of Engineers with building more flood control barriers, the Louisville Division of the USACE undertook construction of a systematic flood protection in Louisville in 1948 that included eight miles of earthen levees, three miles of concrete floodwalls, and thirteen pumping plants to protect the city from the magnitude of the Great Flood of 1937. Construction was completed in 1956, but modifications and additions have continued since, and today the Louisville system of walls, levees, and pump stations stretches over 29 miles along the river front, making it one of the largest in the country. The earthen levee within the APE is approximately 30 feet in height and is approximately 1,400 feet long from its starting point on the west side of the Sherman Minton Bridge’s approach bridge west to the entrance of Shawnee Golf Course.

Significance: The levee has been previously determined NRHP eligible as part of the proposed Louisville Reach of the Louisville Metro Flood Protection System HD, significant under Criterion A for its association with flood control and community planning in Louisville. Its period of significance is recommended to be 1947 to 1956, when the earliest portions of the flood system were implemented by the USACE.

Recommendation: The levee is recommended to remain a contributing feature to this proposed NRHP HD due to its significance as one of the first of the city’s levees to be constructed, as well as its intact condition and, thus, retention of integrity. Refer to Appendix F for a proposed NRHP boundary map of this property.



Photo 3. View of houses on the south side of Northwestern Parkway just northeast of Bank Street

**Shawnee Residential Historic District
Northwestern Parkway between Bank Street and 38th Street**

Description: The proposed Shawnee Residential HD consists of approximately 80 residential buildings (approximately 57 of which are within the APE) on both sides of Northwestern Parkway that date from the late 1800s to the mid-1900s. The buildings at the western end of the district were the earliest constructed along this corridor. The homes share similar setbacks from the street ranging from 50 to 80 feet, and the neighborhood is shaded with mature trees located on most properties as well as along the street. Northwestern Parkway gracefully curves through the neighborhood, an intentional choice made by Fredrick Law Olmsted, Sr. when he designed the road as part of his park system for Louisville. No sidewalks are present on either side of the roadway except for the last block between N 39th and 38th streets. The houses on the south side of the street sit on a rise throughout most of the corridor. The west end closest to Shawnee Park includes the neighborhood's grander houses. Through this area are homes built in the popular styles of the late-1800s and early 1900s, such as Colonial Revival and Craftsman, that featured higher-quality materials and possessed larger lots. East of 42nd Street the homes are placed closer together on smaller lots, and they display more vernacular architectural styles such as bungalows, English cottages, and even some ranches. However, most of these more middle-class houses share similarities with the rest of the corridor, including shaded lots and setbacks along the curvilinear street. Sprinkled in the eastern end of the corridor are a few houses that pre-date the implementation of Olmsted's park system that began in the 1890s, and these are typified by vernacular construction, smaller size, and closer proximity to the street. The construction of Interstate 64 effectively divided this neighborhood from the Portland neighborhood to the east, but an overpass allows Northwestern Parkway to continue beneath.

Northwestern Parkway was listed in the NRHP in May 1982 as part of the Olmsted Park System of Louisville Historic District (HD).

Significance: The proposed Shawnee Residential HD is significant for its association with Louisville's earliest suburban development on the west end of the city in the 1890s, due in large part to the importance of nearby streetcar transportation. The proximity to dependable public transit stimulated residential development away from the city's downtown into outlying areas that had previously been farmland. With the annexation of Shawnee Park and its environs in the 1890s, residential plats were developed on both sides of Northwestern Parkway for those seeking suburban living. The lots were large, with grassy, shaded lawns and houses that did not crowd the street but had a generous set back from it. This was particularly true for houses along the south side of Northwestern Parkway, which sat on higher ground that offered more privacy and views of the Ohio River. Its exclusivity attracted wealthier buyers, as evidenced by the high-styled designs built there, which were primarily Colonial Revival and Craftsman. However, even the more modest houses in the district display fine details as well, and include numerous well-preserved bungalows, English cottages, Cape Cods, and ranch styles. According to the National Park Services' *National Register Bulletin: Historic Residential Suburbs, Guidelines for Evaluation and Documentation for National Register of Historic Places*, most historic suburbs are eligible at the local level for their contribution to a community's growth and development, as well as to the broader trends that shaped suburbanization in the United States. The Shawnee Residential HD is distinguished by its association with transportation improvements in the city, its association Louisville's suburban community planning, and its collection of residential architecture distinctive of its period and method of construction.

Recommendation: The proposed Shawnee Residential HD retains a high degree of its historic integrity and it meets the 50-year age requirement for NRHP listing. The neighborhood's development is directly associated with the introduction of a streetcar line in the 1890s that connected it to downtown Louisville, a distance of approximately four miles. Built to allow visitors an easy transit to Shawnee Park, the streetcar also enabled people to use it to quickly commute to work downtown. Platted in the 1890s, the Shawnee Residential HD was one of Louisville's earliest suburban additions in an otherwise undeveloped part of town. Additionally, this residential area along the Northwestern Parkway corridor reflects popular architectural housing styles of its day, such as Queen Anne, Colonial Revival, Tudor Revival, and Craftsman. Some of these properties embody the distinctive characteristics of these high styles to the extent that they could be considered individually eligible for NRHP listing. The mostly intact overall plan, architectural design and landscape are what make this neighborhood distinctive and noteworthy. For these reasons, the proposed HD meets the registration requirements for listing under Criterion A for community development, and under Criterion C for its outstanding collection of architecture. Its recommended period of significance is 1890 to 1960. Refer to Appendix E for a properties table of the properties within this proposed HD, and to Appendix F for a proposed NRHP boundary map.



Photo 4. View along Rudd Avenue within the Portland Historic District

Portland Historic District

Bound by Bank Street and Pflanz Avenue on the south, North 37th and 36th Streets on the west, Rudd Avenue on the north, and North 33rd Street on the east

NPS File No. 80001615

JFWP, multiple sites

Description: The Portland Historic District was listed on the NRHP on February 21, 1980. The district includes what is extant from the original settlement of Portland founded in the early 1800s, which consists mainly of vernacular residential properties but also includes a handful of religious and commercial buildings, as well as a cemetery. Most of the residences are built in vernacular styles popular in the 19th and early 20th centuries, including modest shotgun, upright-and-wings, and gable-front structures. The finest collection of homes in the district are located along Northwestern Parkway, originally known as 3rd Street, and includes Greek Revival, Italianate, and Tudor Revival styles. Frequent flooding over the years, combined with the construction of flood walls and Interstate 64 have greatly reshaped this neighborhood over the years. Today Rudd Avenue is the northern boundary, but historically the neighborhood extended to the riverfront, where most of its commercial buildings and its wharf were located.

Significance: Portland is as old as the City of Louisville itself, and upon its establishment in the early 1800s it quickly became a thriving town based on river trade. Though it was annexed by Louisville in 1852, Portland continued to maintain a distinct working-class neighborhood character throughout its history. The district is significant for its collection of buildings that date to c.1820 that reflect a wide variety of architectural types and styles, as well as for its contribution to the area's economic growth that relied heavily on river commerce.

Recommendation: The Portland HD is recommended to remain NRHP eligible under Criterion A, for its association with the history of river commerce on the Ohio, as well as under Criterion C for its collection of vernacular and high style buildings that date back to c.1820 through the 1920s. Please refer to Appendix F for the NRHP boundary map for this district.

Discussion of Effects

Section 106 requires federal agencies to determine whether an undertaking has the potential to have an effect, either directly or indirectly, upon historic properties. An “effect” is defined by Section 106 regulations as “an alteration to the characteristics of a historic property qualifying it for inclusion in or eligibility for the National Register.” The degree to which a historic property is diminished by an undertaking is used to measure its effect, which can be “no historic properties affected,” “no adverse effect,” or “adverse effect.” The regulation’s criteria of “adverse effect” are defined in 36 CFR 800.5(a), and states:

(1) Criteria of adverse effect. An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. Consideration shall be given to all qualifying characteristics of a historic property, including those that may have been identified subsequent to the original evaluation of the property's eligibility for the National Register. Adverse effects may include reasonably foreseeable effects caused by the undertaking that may occur later in time, be farther removed in distance or be cumulative.

Below are examples of adverse effects given in 36 CFR 800.5(a)(2):

- (i) Physical destruction of or damage to all or part of the property;
- (ii) Alteration of a property, including the restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR Part 68) and applicable guidelines;
- (iii) Removal of the property from its historic location;
- (iv) Change of the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance;
- (v) Introduction of visual, atmospheric, or audible elements that diminish the integrity of the property’s significant historic features;
- (vi) Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance...”
- (vii) Transfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property’s historic significance.

The discussion below considers the effects of this undertaking, using the examples given in 36 CFR 800.5(a)(2), on the historic resources within the APE: Shawnee Golf Course and Northwestern Parkway as part of the Olmsted Park System of Louisville HD, the earthen levee, the Shawnee residential neighborhood along the Northwestern Parkway corridor, and the Portland HD.

**Shawnee Golf Course and Northwestern Parkway
Contributing resources to the Olmsted Park System of Louisville HD**

There are no anticipated physical changes occurring to the Shawnee Golf Course as part of this undertaking, and no temporary or permanent right-of-way will be acquired. Construction traffic will utilize Northwestern Parkway at certain times during construction. The most frequent usage periods would occur during construction of the levee access road and its removal after construction. Normal day-to-day usage of Northwestern Parkway is anticipated to be infrequent and to include water trucks making passes for dust control, occasional pickup trucks entering and exiting the site, concrete mixing trucks when pier cap patching occurs, and low-boy trailers hauling specialized equipment. Kokosing will document existing roadway conditions via video camera before construction begins and will compare and make patches as needed after the project is completed.

The criteria of Adverse Effect, as defined and described in 36 CFR 800.5(a)(1) and in 36 CFR 800.5(a)(2)(i) through (v), will now be evaluated for the Shawnee Golf Course and Northwestern Parkway:

Per 36 CFR 800.5(a)(2)(i), the undertaking will not cause “physical destruction of or damage to all or part of the property.” To ensure that Northwestern Parkway returns to its pre-construction state, Kokosing intends to document the roadway’s existing condition before construction begins, and then make patch repairs afterwards where needed.

Per 36 CFR 800.5(a)(2)(ii), there will be no “restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines.”

Per 36 CFR 800.5(a)(2)(iii), the property will not be removed from its historic location.

Per 36 CFR 800.5(a)(2)(iv), the project would not change the “the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance.” The project would not change the way the properties are currently used, nor would it change any physical features within its setting that are contributing features to their historic significance.

Per 36 CFR 800.5(a)(2)(v), there will not be an “introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The proposed work will be audible and visible from both properties during construction, but those impacts would be temporary in nature. The area already experiences significant vibratory and auditory traffic noise due to the proximity of Interstate 64.

Per 36 CFR 800.5(a)(2)(vi), there will be no neglect or deterioration of the property.

Per 36 CFR 800.5(a)(2)(vii), there will be no “transfer, lease, or sale of the property out of Federal ownership or control.”

Based upon the temporary and minor impacts anticipated for both resources within the Olmsted Park System of Louisville’s HD, the undertaking will have **no adverse effect** upon the properties.

Earthen Levee

Part of the proposed Louisville Reach Metro Flood Protection System HD

The proposed access road to be built on top of the levee would be constructed of temporary gravel aggregate over geotextile fabric, and it would measure 12 feet wide, one foot thick, and 1,440 feet long. Construction of the access road and aggregate pads under the approach spans would require approximately 20 loads of stone per day for four days, one Cat D6 bulldozer to strip top soil to a depth of six to eight inches and spread stone for four days, one 72-inch smooth drum roller to compact stone for four days, and an occasional pickup truck in and out of the site during the levee road access construction process. The stripped topsoil will be stored and stabilized for the duration of the project.

After the access road is constructed, normal day-to-day operations in the project area would include water trucks making passes as needed for dust control, occasional pickup trucks entering and exiting the site, one to two concrete trucks with light loads (two to three cubic yards) utilizing the access road for pier cap patching, and an occasional low-boy trailer will be needed to bring equipment in and out for KCC and North Star (anticipated to be an infrequent occurrence after the initial mobilization).

During hydro-demolition activities one additional water truck will utilize the access road emptying frac tanks from hydro-demo operation site to be hauled offsite, and occasional pickup trucks will enter and leave the site.

During removal of the access road, aggregate pads, and subsequent stabilization one track-hoe loading dump truck would be required for four days, approximately 20 dump truck loads would occur per day for four days, one Cat D6 bulldozer would be used for two days redressing the levee, one hydro seeder would be needed for one day to stabilize and plant grass seed on the levee once the work is complete, and occasional pickup trucks will be entering and leaving the site.

Total temporary right-of-way needed for the access road is approximately 6.77 acres. Upon completion of construction activities, the aggregate and geotextile fabric would be removed, and the topsoil that had been placed in storage would be replaced. The topsoil would be graded and seeded to restore it to pre-construction conditions. This will include seeding the topsoil. No permanent alterations or impacts to the levee are anticipated. A water truck will be on site to manage dust control, and will be used on an as-needed basis.

The criteria of Adverse Effect, as defined and described in 36 CFR 800.5(a)(1) and in 36 CFR 800.5(a)(2)(i) through (vii), will now be evaluated for the earthen levee wall:

Per 36 CFR 800.5(a)(2)(i), the undertaking will not cause “physical destruction of or damage to all or part of the property.” All proposed changes to the levee are temporary and no permanent physical changes will remain after the project is completed.

Per 36 CFR 800.5(a)(2)(ii), there will be no “restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines.”

Per 36 CFR 800.5(a)(2)(iii), the property will not be removed from its historic location.

Per 36 CFR 800.5(a)(2)(iv), the project would not change the “the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance.” The project would not change the way the properties are currently used, nor would it change any physical features within its setting that are contributing features to their historic significance.

Per 36 CFR 800.5(a)(2)(v), there will not be an “introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The proposed work will introduce audible and visible elements during construction, but those impacts would be temporary in nature. The property would be returned to its pre-construction condition after the project is complete, and no permanent visual or auditory changes are anticipated to occur.

Per 36 CFR 800.5(a)(2)(vi), there will be no neglect or deterioration of the property.

Per 36 CFR 800.5(a)(2)(vii), there will be no “transfer, lease, or sale of the property out of Federal ownership or control.”

Based upon the temporary and minor impacts anticipated for the earthen levee, the undertaking will have **no adverse effect** upon the property.

Shawnee Residential Historic District

Construction traffic will utilize Northwestern Parkway at certain times during construction. Kokosing will document existing roadway conditions via video camera before construction begins and will compare and make patches as needed after the project is completed. No temporary or permanent right-of-way will be needed from any of the properties within this HD.

The criteria of Adverse Effect, as defined and described in 36 CFR 800.5(a)(1) and in 36 CFR 800.5(a)(2)(i) through (v), will now be evaluated for the Shawnee Residential Historic District:

Per 36 CFR 800.5(a)(2)(i), the undertaking will not cause “physical destruction of or damage to all or part of the property.”

Per 36 CFR 800.5(a)(2)(ii), there will be no “restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines.”

Per 36 CFR 800.5(a)(2)(iii), the property will not be removed from its historic location.

Per 36 CFR 800.5(a)(2)(iv), the project would not change the “the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance.” The project would not change the way the properties are currently used, nor would it change any physical features within its setting that are contributing features to their historic significance.

Per 36 CFR 800.5(a)(2)(v), there will not be an “introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The proposed work will be audible and visible from both properties during construction, but those impacts would be temporary in nature. The area already experiences significant vibratory and auditory traffic noise due to the proximity of Interstate 64.

Per 36 CFR 800.5(a)(2)(vi), there will be no neglect or deterioration of the property.

Per 36 CFR 800.5(a)(2)(vii), there will be no “transfer, lease, or sale of the property out of Federal ownership or control.”

Based upon the temporary and minor impacts anticipated for the Shawnee Residential HD, the undertaking will have **no adverse effect** upon the properties.

Portland Historic District

No temporary or permanent right-of-way will be needed from any of the properties within this HD, and no physical impacts to the HD are anticipated to occur.

The criteria of Adverse Effect, as defined and described in 36 CFR 800.5(a)(1) and in 36 CFR 800.5(a)(2)(i) through (v), will now be evaluated for the Portland Historic District:

Per 36 CFR 800.5(a)(2)(i), the undertaking will not cause “physical destruction of or damage to all or part of the property.”

Per 36 CFR 800.5(a)(2)(ii), there will be no “restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines.”

Per 36 CFR 800.5(a)(2)(iii), the property will not be removed from its historic location.

Per 36 CFR 800.5(a)(2)(iv), the project would not change the “the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance.” The project would not change the way the properties are currently used, nor would it change any physical features within its setting that are contributing features to their historic significance.

Per 36 CFR 800.5(a)(2)(v), there will not be an “introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The proposed work to the approach bridges may be audible and visible to some areas within the HD during construction, but those

impacts would be temporary in nature. The HD is not anticipated to experience construction traffic, as the defined construction routes are located west of Interstate 64. The area already experiences significant vibratory and auditory traffic noise due to the proximity of two interstates. And lastly, traffic counts in the area are not expected to increase because of the project.

Per 36 CFR 800.5(a)(2)(vi), there will be no neglect or deterioration of the property.

Per 36 CFR 800.5(a)(2)(vii), there will be no “transfer, lease, or sale of the property out of Federal ownership or control.”

Based upon the temporary and minor impacts anticipated for the Portland HD, the undertaking will have **no adverse effect** upon the properties.

Conclusion

In accordance with Section 106 of the National Historic Preservation Act of 1966, as amended (16 U.S.C. 470f), the project historian has provided an assessment of this project’s potential adverse effect upon the Shawnee Golf Course and Northwestern Parkway as part of the Olmsted Park System of Louisville HD, the earthen levee, the Shawnee residential neighborhood along the Northwestern Parkway corridor, and the Portland HD, all historic properties located within the proposed APE.

At this time, we now respectfully put forward the recommendation for this undertaking’s effect will result in “No Adverse Effect” to historic properties per 36 CFR 800.5(a)(1) and in 36 CFR 800.5(a)(2)(i) through (vii). At this time, we are now seeking concurrence from the KHC with these findings.

For questions concerning this report you may contact Candy Hudziak of Metric Environmental, LLC, at candaceh@metricenv.com or at 317-443-4123.

Sincerely,



Candace Hudziak, M.A.
Architectural Historian
Metric Environmental, LLC

Appendices:

- A. Project Location Maps*
- B. Project Plans (including the Proposed Levee Access Road and MOT)*
- C. Section 106 Coordination and Correspondence*
- D. Expanded (and Final) Area of Potential Effects Map*
- E. Properties Table of the Proposed Shawnee Residential HD*
- F. NRHP Boundary Maps for Historic Properties in the APE*
- G. Photographs and Key Map of the APE*
- H. Bibliography*

Appendix B, Project Plans, were intentionally omitted from this document. Please refer to Appendix A in the Additional Information document.

APPENDIX A: PROJECT LOCATION MAPS



- APE
- Project Footprint
- Proposed Levee Road

APE Details on a portion of the 1992
 New Albany, IN-KY topographic quadrangle
 Sherman Minton Bridge
 Louisville, Jefferson County, Kentucky
 Metric Project No. 20-0201
 Map Date: 07/08/2021

All locations approximate
 Source: Imagery Date 2012 Aerial



1 inch = 1,300 feet





- APE
- Project Footprint
- Proposed Levee Road
- Portion of the Olmsted Park System Historic District
- Portland Historic District
- Proposed Shawnee Residential HD Boundary
- Proposed Shawnee Golf Course NRHP Boundary
- Proposed Earthen Levee NRHP Boundary

APE Details on an aerial photograph
 Sherman Minton Bridge
 Louisville, Jefferson County, Kentucky
 Metric Project No. 20-0201
 Map Date: 07/08/2021

All locations approximate
 Source: Imagery Date 2012 Aerial



1 inch = 1,400 feet



APPENDIX C: SECTION 106 COORDINATION AND CORRESPONDENCE



MATTHEW G. BEVIN
GOVERNOR

**TOURISM, ARTS AND HERITAGE CABINET
KENTUCKY HERITAGE COUNCIL
THE STATE HISTORIC PRESERVATION OFFICE**

REGINA STIVERS
DEPUTY SECRETARY

DON PARKINSON
SECRETARY

410 HIGH STREET
FRANKFORT, KENTUCKY 40601
PHONE (502) 564-7005
FAX (502) 564-5820
www.heritage.ky.gov

CRAIG A. POTTS
EXECUTIVE DIRECTOR
& STATE HISTORIC
PRESERVATION OFFICER

December 20, 2018

Ms. Wendy L. Vachet
Indiana Department of Transportation
100 North Senate Avenue
Indianapolis, Indiana 46204

Re: *Primary Des. No. 1702255 (Multiple Des. Nos.) Sherman Minton Renewal*

Dear Ms. Vachet,

Thank you for our telephone conversation earlier today. As we discussed, the Kentucky State Historic Preservation Office does not feel that the Section 106 process is far enough along at this time to make any comments regarding adverse effects. At this time, we have no comment pertaining to archaeological concerns. We appreciate the early coordination information that was provided and look forward to continued consultation with you, the Kentucky Transportation Cabinet, and the Federal Highway Administration as the planning for this proposed project moves forward.

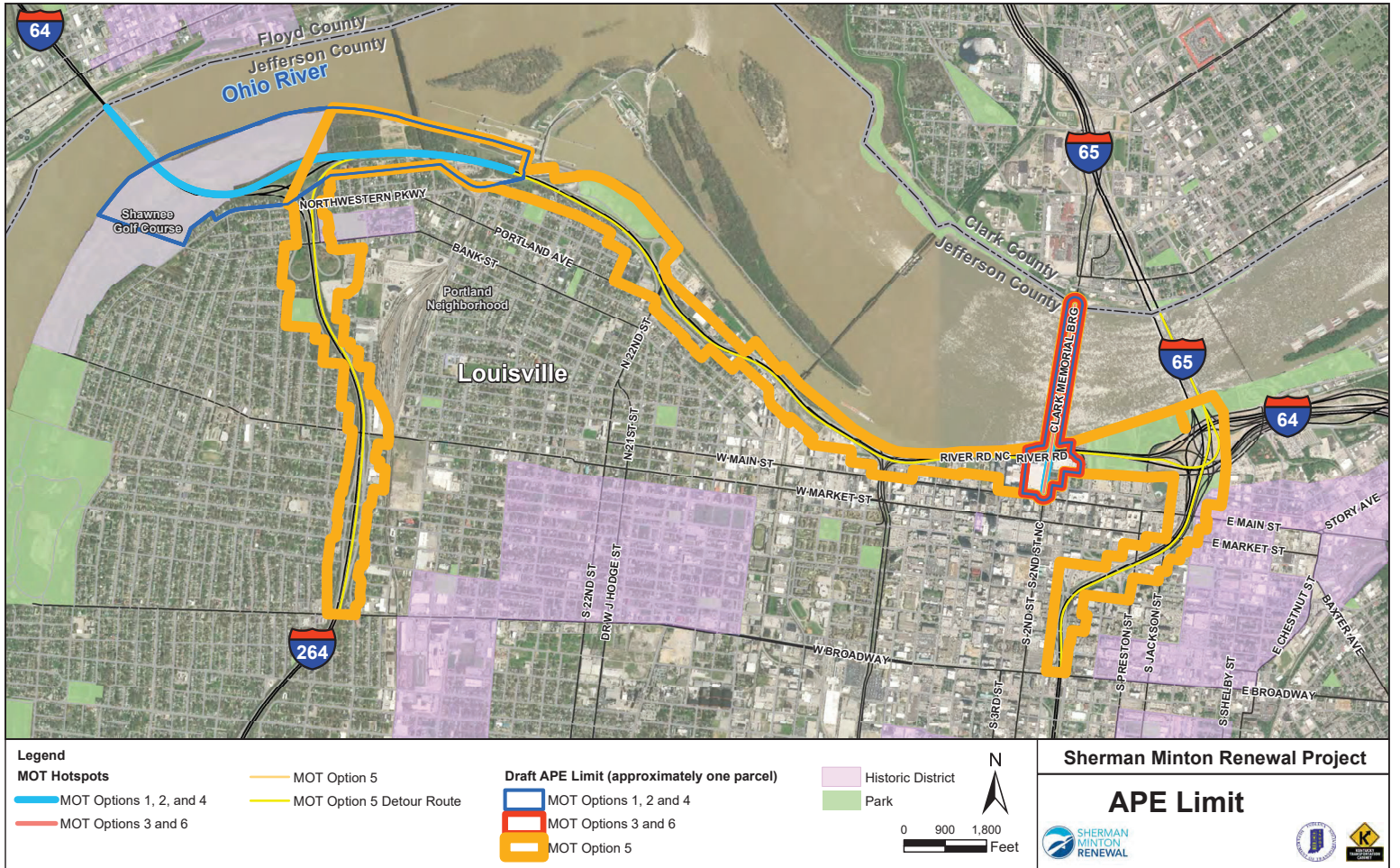
We understand that the listed Portland Historic District and the listed Shawnee Park are within the Area of Potential Effect (APE) for this project. We also understand that project team is trying to stick to the existing Right of Way (ROW) to minimize the impacts to all resources historic and environmental. At present we have no specific comments relating to the historic resources as our office does not have plans showing where the disturb limits are or how the secondary impacts will affect the historic resources. Our office finds the half-mile APE to be appropriate for the above-ground resources and looks forward to continuing communication regarding the project.

Should you have any questions pertaining to archaeological concerns, please feel free to contact Nicole Konkol of my staff at nicole.konkol@ky.gov. Christina Sabol may be contacted at christina.sabol@ky.gov with any issues pertaining to above ground resources in Kentucky.

Sincerely,

Craig A. Potts,
Executive Director and
State Historic Preservation Officer

CP KHC # 53037
cc: Susan Neumeyer (KYTC); SMRP@mbakerintl.com





COMMONWEALTH OF KENTUCKY
TRANSPORTATION CABINET
transportation.ky.gov

Andy Beshear
GOVERNOR

Jim Gray
SECRETARY

March 2, 2020

Greg Fischer, Mayor
527 W. Jefferson Street, 4th Floor
Louisville, KY 40202

RE: Invitation to Become a Consulting Party
Project: Sherman Minton Renewal Project
Jefferson County, KY
KYTC Item No. 5-10027

Dear Mayor Fischer:

The Kentucky Transportation Cabinet (KYTC) is initiating studies for the Sherman Minton Renewal Project. The Sherman Minton Bridge is one of two Interstate crossings of the Ohio River in the Louisville metro area. The Sherman Minton Bridge carries approximately 90,000 vehicles per day on I-64 over the Ohio River between the City of New Albany, Indiana and the area of West Louisville in Kentucky and is a critical part of the existing Interstate System facilitating daily regional traffic. The purpose of this project is to rehabilitate the deteriorating Sherman Minton Bridge and the associated Indiana and Kentucky approach bridges with the goal of extending the service life of this Interstate crossing of the Ohio River by up to 30 years.

The Sherman Minton Renewal Project includes coordination of needed maintenance activities along I-64 from I-265 in Southern Indiana to the I-264 interchange in Kentucky. The project may include bridge deck replacements and bridge deck overlays, structural repairs, replacement lighting, drainage components, bridge painting, Hot Mix Asphalt (HMA) overlay with Americans with Disabilities Act (ADA) ramps reconstruction, and ramp fractionalization. The project does not include Right-of-Way (ROW) acquisition, increase in capacity (additional lanes), or add bicycle/pedestrian appurtenances to this portion of I-64.

As part of its project review, and in accordance with Section 106 of the National Historic Preservation Act (Section 106), KYTC will be identifying historic properties in the project area and evaluating potential impacts to these resources. A summary of Section 106 is provided in *A Citizen's Guide to Section 106 Review* located on the webpage of the Advisory Council on Historic Preservation at <https://www.achp.gov/digital-library-section-106-landing/citizens-guide-section-106-review>.

Section 106 establishes a framework for consultation that affords interested organizations and citizens the opportunity to participate in the consultation process as Consulting Parties. As a Consulting Party, you or your organization will be afforded the opportunity to provide input on: 1) the identification of historic properties; 2) the effects of the project on those properties; and, 3) if adverse effects are determined to result, potential measures to mitigate for the adverse effects.

Consulting Parties must express their interest in participating in the consultation and be approved by the federal agency involved with the project, in this case the Federal Highway Administration. The application process has been streamlined and can be completed on-line at <https://transportation.ky.gov/EnvironmentalAnalysis/Pages/Consulting-Party-Projects.aspx> by selecting project number 05-0063 and 05-0064. If internet accessibility is an obstacle, you may contact Jonna Wallace at the KYTC Division of Environmental Analysis (502.564.7250) for alternative means of submitting your request.

AN EQUAL OPPORTUNITY EMPLOYER M/F/D

We hope that you will consider assisting in our efforts to minimize the impacts of the project on historic properties. If you have any questions, please contact me at Danny.Peake@KY.gov or 502.564.7250[MKU1].

Sincerely,

A handwritten signature in blue ink that reads "Danny Peake". The signature is fluid and cursive, with the first name "Danny" and last name "Peake" clearly distinguishable.

Danny Peake, Director
Division of Environmental Analysis
Kentucky Transportation Cabinet

EC:

Tim Foreman, Branch Manager Project Coordination Section and UST/HAZ Materials KYTC-DEA
Donna Hardin, Environmental Coordinator, KYTC-DEA, District 5
Amanda Abner, Historic Preservation Program Coordinator, KYTC-DEA, District 5
Daniel Davis, Administrative Branch Manager, Cultural Resources Section, KYTC-DEA
Wendy Vachet, Regional Environmental Manager, Michael Baker International, Inc.
Timothy Zinn, Senior Architectural Historian, Michael Baker International, Inc.



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Andy Beshear
GOVERNOR

Jim Gray
SECRETARY

March 2, 2020

Layla George, President and CEO
Olmstead Parks Conservancy
1299 Trevilian Way
Louisville, KY 40213

RE: Invitation to Become a Consulting Party
Project: Sherman Minton Renewal Project
Jefferson County, KY
KYTC Item No. 5-10027

Dear Ms. George:

The Kentucky Transportation Cabinet (KYTC) is initiating studies for the Sherman Minton Renewal Project. The Sherman Minton Bridge is one of two Interstate crossings of the Ohio River in the Louisville metro area. The Sherman Minton Bridge carries approximately 90,000 vehicles per day on I-64 over the Ohio River between the City of New Albany, Indiana and the area of West Louisville in Kentucky and is a critical part of the existing Interstate System facilitating daily regional traffic. The purpose of this project is to rehabilitate the deteriorating Sherman Minton Bridge and the associated Indiana and Kentucky approach bridges with the goal of extending the service life of this Interstate crossing of the Ohio River by up to 30 years.

The Sherman Minton Renewal Project includes coordination of needed maintenance activities along I-64 from I-265 in Southern Indiana to the I-264 interchange in Kentucky. The project may include bridge deck replacements and bridge deck overlays, structural repairs, replacement lighting, drainage components, bridge painting, Hot Mix Asphalt (HMA) overlay with Americans with Disabilities Act (ADA) ramps reconstruction, and ramp fractionalization. The project does not include Right-of-Way (ROW) acquisition, increase in capacity (additional lanes), or add bicycle/pedestrian appurtenances to this portion of I-64.

As part of its project review, and in accordance with Section 106 of the National Historic Preservation Act (Section 106), KYTC will be identifying historic properties in the project area and evaluating potential impacts to these resources. A summary of Section 106 is provided in *A Citizen's Guide to Section 106 Review* located on the webpage of the Advisory Council on Historic Preservation at <https://www.achp.gov/digital-library-section-106-landing/citizens-guide-section-106-review>.

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Consulting Parties must express their interest in participating in the consultation and be approved by the federal agency involved with the project, in this case the Federal Highway Administration. The application process has been streamlined and can be completed on-line at

<https://transportation.ky.gov/EnvironmentalAnalysis/Pages/Consulting-Party-Projects.aspx> by selecting project number 05-0063 and 05-0064. If internet accessibility is an obstacle, you may contact Jonna Wallace at the KYTC Division of Environmental Analysis (502.564.7250) for alternative means of submitting your request.

We hope that you will consider assisting in our efforts to minimize the impacts of the project on historic properties. If you have any questions, please contact me at Danny.Peake@KY.gov or 502.564.7250(MK11).

Sincerely,



Danny Peake, Director
Division of Environmental Analysis
Kentucky Transportation Cabinet

EC:

Tim Foreman, Branch Manager Project Coordination Section and UST/HAZ Materials KYTC-DEA
Donna Hardin, Environmental Coordinator, KYTC-DEA, District 5
Amanda Abner, Historic Preservation Program Coordinator, KYTC-DEA, District 5
Daniel Davis, Administrative Branch Manager, Cultural Resources Section, KYTC-DEA
Wendy Vachet, Regional Environmental Manager, Michael Baker International, Inc.
Timothy Zinn, Senior Architectural Historian, Michael Baker International, Inc.



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Andy Beshear
GOVERNOR

Jim Gray
SECRETARY

March 2, 2020

Jay Gulicke, Chair
Olmstead Parks Conservancy
1299 Trevilian Way
Louisville, KY 40213

RE: Invitation to Become a Consulting Party
Project: Sherman Minton Renewal Project
Jefferson County, KY
KYTC Item No. 5-10027

Dear Mr. Gulicke:

The Kentucky Transportation Cabinet (KYTC) is initiating studies for the Sherman Minton Renewal Project. The Sherman Minton Bridge is one of two Interstate crossings of the Ohio River in the Louisville metro area. The Sherman Minton Bridge carries approximately 90,000 vehicles per day on I-64 over the Ohio River between the City of New Albany, Indiana and the area of West Louisville in Kentucky and is a critical part of the existing Interstate System facilitating daily regional traffic. The purpose of this project is to rehabilitate the deteriorating Sherman Minton Bridge and the associated Indiana and Kentucky approach bridges with the goal of extending the service life of this Interstate crossing of the Ohio River by up to 30 years.

The Sherman Minton Renewal Project includes coordination of needed maintenance activities along I-64 from I-265 in Southern Indiana to the I-264 interchange in Kentucky. The project may include bridge deck replacements and bridge deck overlays, structural repairs, replacement lighting, drainage components, bridge painting, Hot Mix Asphalt (HMA) overlay with Americans with Disabilities Act (ADA) ramps reconstruction, and ramp fractionalization. The project does not include Right-of-Way (ROW) acquisition, increase in capacity (additional lanes), or add bicycle/pedestrian appurtenances to this portion of I-64.

As part of its project review, and in accordance with Section 106 of the National Historic Preservation Act (Section 106), KYTC will be identifying historic properties in the project area and evaluating potential impacts to these resources. A summary of Section 106 is provided in *A Citizen's Guide to Section 106 Review* located on the webpage of the Advisory Council on Historic Preservation at <https://www.achp.gov/digital-library-section-106-landing/citizens-guide-section-106-review>.

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Consulting Parties must express their interest in participating in the consultation and be approved by the federal agency involved with the project, in this case the Federal Highway Administration. The application process has been streamlined and can be completed on-line at

<https://transportation.ky.gov/EnvironmentalAnalysis/Pages/Consulting-Party-Projects.aspx> by selecting project number 05-0063 and 05-0064. If internet accessibility is an obstacle, you may contact Jonna Wallace at the KYTC Division of Environmental Analysis (502.564.7250) for alternative means of submitting your request.

We hope that you will consider assisting in our efforts to minimize the impacts of the project on historic properties. If you have any questions, please contact me at Danny.Peake@KY.gov or 502.564.7250(MKU1).

Sincerely,

A handwritten signature in blue ink that reads "Danny Peake". The signature is fluid and cursive, with the first name "Danny" and the last name "Peake" clearly distinguishable.

Danny Peake, Director
Division of Environmental Analysis
Kentucky Transportation Cabinet

EC:

Tim Foreman, Branch Manager Project Coordination Section and UST/HAZ Materials KYTC-DEA
Donna Hardin, Environmental Coordinator, KYTC-DEA, District 5
Amanda Abner, Historic Preservation Program Coordinator, KYTC-DEA, District 5
Daniel Davis, Administrative Branch Manager, Cultural Resources Section, KYTC-DEA
Wendy Vachet, Regional Environmental Manager, Michael Baker International, Inc.
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Andy Beshear
GOVERNOR

Jim Gray
SECRETARY

March 2, 2020

Donna Purvis, Councilwoman
Louisville Metro Council District 5
601 W. Jefferson Street
Louisville, KY 40202

RE: Invitation to Become a Consulting Party
Project: Sherman Minton Renewal Project
Jefferson County, KY
KYTC Item No. 05-10027

Dear Ms. Purvis:

The Kentucky Transportation Cabinet (KYTC) is initiating studies for the Sherman Minton Renewal Project. The Sherman Minton Bridge is one of two Interstate crossings of the Ohio River in the Louisville metro area. The Sherman Minton Bridge carries approximately 90,000 vehicles per day on I-64 over the Ohio River between the City of New Albany, Indiana and the area of West Louisville in Kentucky and is a critical part of the existing Interstate System facilitating daily regional traffic. The purpose of this project is to rehabilitate the deteriorating Sherman Minton Bridge and the associated Indiana and Kentucky approach bridges with the goal of extending the service life of this Interstate crossing of the Ohio River by up to 30 years.

The Sherman Minton Renewal Project includes coordination of needed maintenance activities along I-64 from I-265 in Southern Indiana to the I-264 interchange in Kentucky. The project may include bridge deck replacements and bridge deck overlays, structural repairs, replacement lighting, drainage components, bridge painting, Hot Mix Asphalt (HMA) overlay with Americans with Disabilities Act (ADA) ramps reconstruction, and ramp fractionalization. The project does not include Right-of-Way (ROW) acquisition, increase in capacity (additional lanes), or add bicycle/pedestrian appurtenances to this portion of I-64.

As part of its project review, and in accordance with Section 106 of the National Historic Preservation Act (Section 106), KYTC will be identifying historic properties in the project area and evaluating potential impacts to these resources. A summary of Section 106 is provided in *A Citizen's Guide to Section 106 Review* located on the webpage of the Advisory Council on Historic Preservation at <https://www.achp.gov/digital-library-section-106-landing/citizens-guide-section-106-review>.

Section 106 establishes a framework for consultation that affords interested organizations and citizens the opportunity to participate in the consultation process as Consulting Parties. As a Consulting Party, you or your organization will be afforded the opportunity to provide input on: 1) the identification of historic properties; 2) the effects of the project on those properties; and, 3) if adverse effects are determined to result, potential measures to mitigate for the adverse effects.

Consulting Parties must express their interest in participating in the consultation and be approved by the federal agency involved with the project, in this case the Federal Highway Administration. The application process has been streamlined and can be completed on-line at <https://transportation.ky.gov/EnvironmentalAnalysis/Pages/Consulting-Party-Projects.aspx> by selecting project

number 05-0063 and 05-0064. If internet accessibility is an obstacle, you may contact Jonna Wallace at the KYTC Division of Environmental Analysis (502.564.7250) for alternative means of submitting your request.

We hope that you will consider assisting in our efforts to minimize the impacts of the project on historic properties. If you have any questions, please contact me at Danny.Peake@KY.gov or 502.564.7250[MKU1].

Sincerely,



Danny Peake, Director
Division of Environmental Analysis
Kentucky Transportation Cabinet

EC:

Tim Foreman, Branch Manager Project Coordination Section and UST/HAZ Materials KYTC-DEA
Donna Hardin, Environmental Coordinator, KYTC-DEA, District 5
Amanda Abner, Historic Preservation Program Coordinator, KYTC-DEA, District 5
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Timothy Zinn, Senior Architectural Historian, Michael Baker International, Inc.



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Andy Beshear
GOVERNOR

Jim Gray
SECRETARY

March 26, 2020

Mr. Craig Potts, Executive Director
Kentucky Heritage Council and
State Historic Preservation Officer
410 High Street
Frankfort, KY 40601

SUBJECT: Eligibility and Effects Determination for Above Ground Resources
Sherman Minton Bridge Rehabilitation Project, I-64 over the Ohio River
KYTC Item No. 5-10027.00
Jefferson County, Kentucky and Floyd County, Indiana

Dear Mr. Potts:

Attached please find information pertaining to the proposed Sherman Minton Bridge Rehabilitation Project (SMRP). Your office previously reviewed and approved a proposed Area of Potential Effect that presented a “worst case scenario” for potential impacts and included six alternatives for maintenance of traffic (MOT) due to partial and full bridge closure. Four MOT alternatives are being carried forward at this point. Until a design-build team (DBT) is selected to complete the project, the final preferred alternative will not be selected. For this reason KYTC requests that your office review the attached determinations of effect for each of the four viable alternatives. The effects from the alternatives no longer under consideration are included as well.

It is the determination of KYTC on behalf of FHWA that the Sherman Minton Rehabilitation Project as proposed will have **No Adverse Effect** on the Shawnee Golf Course or Shawnee Park or Northwestern Parkway (JFL 271 / JFWS 306), listed on the National Register as part of the Olmstead Park System. The Kentucky bridge approach spans above the golf course. Staging areas in or adjacent to the park will be subject to additional review once they are identified.

The National Register listed Clark Memorial Bridge (Louisville Municipal Bridge and Pylons, JFCD 217) is the only historic site that could be directly affected by the diverted traffic; however, the bridge should not be harmed by added vehicles. Additional analysis of traffic impacts during the SMRP may be necessary to avoid impacts to the Clark Memorial Bridge, which currently has an ADT of 14,800 trips. The bridge is currently posted for loads between 20 and 40 tons depending on the number of axles. Based on information available now the project should have **No Adverse Effect** on this bridge.

The project as proposed does not have the potential to adversely affect any other historic sites within the APE. The preferred alternative as selected by the DBT will be presented to SHPO to determine if any additional consultation is warranted. In order to keep this important project moving forward we request that you provide a conditional determination of **No Adverse Effect** based on our commitment to consult further with you once the DBT contractor selects an alternative.

Because of the project’s timeline we request a response as soon as possible. As always, we appreciate your input and consultation. If you have any questions, please contact Amanda Abner of my staff at Amanda.abner@ky.gov.

Very truly yours,

Daniel R. Peake, Director
Division of Environmental Analysis

DRP/aba

Cc: T. Foreman, A. Abner, central file, reading file

SHERMAN MINTON BRIDGE REHABILITATION PROJECT DETERMINATION OF EFFECTS TO ABOVE GROUND RESOURCES

Project Description- Provided by Michael Baker International, Inc.

The Sherman Minton Renewal Project, hereinafter referred to as “the Project,” is located at the Interstate I-64 and US 150 Sherman Minton Bridge crossing of the Ohio River in Floyd County, New Albany, Indiana, and in Jefferson County, Louisville, Kentucky. The Project is centered at 38.278665°N, -85.822237°E, in Sections 21, 22, 27, and 28, Township 27N, Range 8E, of the New Albany, Indiana United States Geographic Survey (USGS) topographic quadrangle. Land adjacent to the project consists of maintained grassy state right-of-way as well as commercial and residential properties.

The Project is the rehabilitation of the Sherman Minton Bridge and the approach bridges in Indiana and Kentucky. The Project includes coordination of needed maintenance activities along I-64 from I-265 in Southern Indiana to the I-264 interchange in Kentucky. The Project may include bridgedeck replacements and bridge deck overlays, structural repairs, replacement lighting, drainage components, bridge painting, Hot Mix Asphalt (HMA) overlay with Americans with Disabilities Act (ADA) ramps reconstruction, and ramp fractionalization. The Project does not include Right-of-Way (ROW) acquisition, increase in capacity (additional lanes), or add bicycle/pedestrian appurtenances to this portion of I-64.

REVISED RANGE OF ALTERNATIVES

The Project will impact interstate and local travel and local communities; however, the effects are temporary and will cease upon construction completion. Options that retained continuous travel in both directions across the Sherman Minton Bridge had lower induced traffic diversions (with related travel times, travel costs, and congestion) and fewer disruptions for local community cohesion and access.

ALTERNATIVES CARRIED INTO THE NEPA DOCUMENT AS THE PREFERRED – Design Build Team (DBT) can select any one of these

MOT 1 – Two lanes open, both decks/directions (existing EB-eastbound and WB-westbound decks.) Consists of three phases; travel lanes shift for each phase; 1-lane closure for both EB and WB during all phases, 2-lanes maintained for both EB and WB during all phases, maintains east/west river crossing, no access changes required, and traffic lanes adjacent to construction work zone. MOT 1 has the fewest/lowest temporary changes from existing conditions for access, east/west crossings, diverted traffic, congestion, user cost, driver expectation, community and EJ impacts, and the most public support. However, MOT 1 also has the highest duration and estimated project cost, and reduced safety with traffic adjacent to the work zone.

MOT 2 – One lane open, both decks/directions (EB and WB decks) Consists of two phases; travel lanes shift for each phase, 2-lane closures for both EB and WB during all phases, 1-lane maintained for both EB and WB during all phases, Kentucky crossover for I-64 WB merge with I-264, maintains east/west river crossing, no access changes required, and traffic lanes adjacent to construction work zone. MOT 2 has the fewer/lower temporary changes from existing conditions for access, east/west crossings, diverted traffic, travel time, congestion, estimated project cost, user cost,

driver expectation, community and EJ impacts, shorter duration, and more favored public comments. However, MOT 2 also has reduced safety with traffic adjacent to the work zone.

MOT 4 – Reversible center lane (AM-EB / PM-WB) and one-way EB/WB lanes open on one deck

Consists of two phases; Phase 1 - lower deck closed / Phase 2 - upper deck closed, traffic lanes separated from construction work zone, AM (midnight to noon) – two (2) EB lanes and one (1) WB lane open during all phases, PM (midnight to noon) – one (1) EB lanes and two (2) WB lanes open during all phases, minimal center lane transition with moveable barrier, maintains east/west river crossing, access changes required, and a Kentucky crossover for I-64 WB merge with I-264. MOT 4 has lower temporary changes from existing conditions for access, diverted traffic, travel time, congestion, user cost, driver expectation, separated traffic and work zone, community and EJ impacts, and the favorable public support. The one-deck MOT options (3, 4, and 6) have similar duration, estimated project cost, and separate traffic from the work zone, however, MOT 4 retains open east/west crossings, the movable barrier can be operated during open traffic conditions (no temporary closure), and with some uncertainty for driver expectation.

Short-Term MOT 5 – Short-term closure of the bridge and all six-lanes

Closes east/west river crossing, consists of one phase, access changes required, all traffic diverted, detour route: I-265 to I-65, and concurrent access ramp modifications. MOT5 (Full Duration Closure) has the most/highest temporary changes from existing conditions for access, east/west crossings, diverted traffic, travel time, congestion, user cost, driver expectation, community and EJ impacts, and little to no public support. Short-term use of MOT 5 may be considered based upon constructability requirements, as warranted. May be considered, based upon constructability requirements or as warranted to keep the Sherman Minton Bridge safely in-service during construction.

NOT BEING CARRIED FORWARD

MOT 3 – Alternating three one-way lanes (AM-EB / PM-WB) open on one deck

Consists of two phases; Phase 1 - upper deck closed / Phase 2 - lower deck closed, AM (midnight to noon) – three (3) EB lanes open; WB closed; during all phases, PM (midnight to noon) – three (3) EB lanes closed; WB open; during all phases, two (2) estimated 90-minute closures per day to switch directions (AM/EB - PM/WB) and gates at each end, a Kentucky crossover for I-64 WB merge with I-264, limits east/west river crossing, access changes required; detour route: I-265 to I-65, and traffic lanes separated from construction work zone. MOT 3 has more/higher temporary changes from existing conditions for access, diverted traffic, travel time, congestion, estimated project cost, user cost, community and EJ impacts, less favorable public support, and closes half of the east/west crossings daily during both phases and the highest uncertainty for driver expectation (two direction changes per day with transition closures (one during Noon/lunch-time increased activity)).

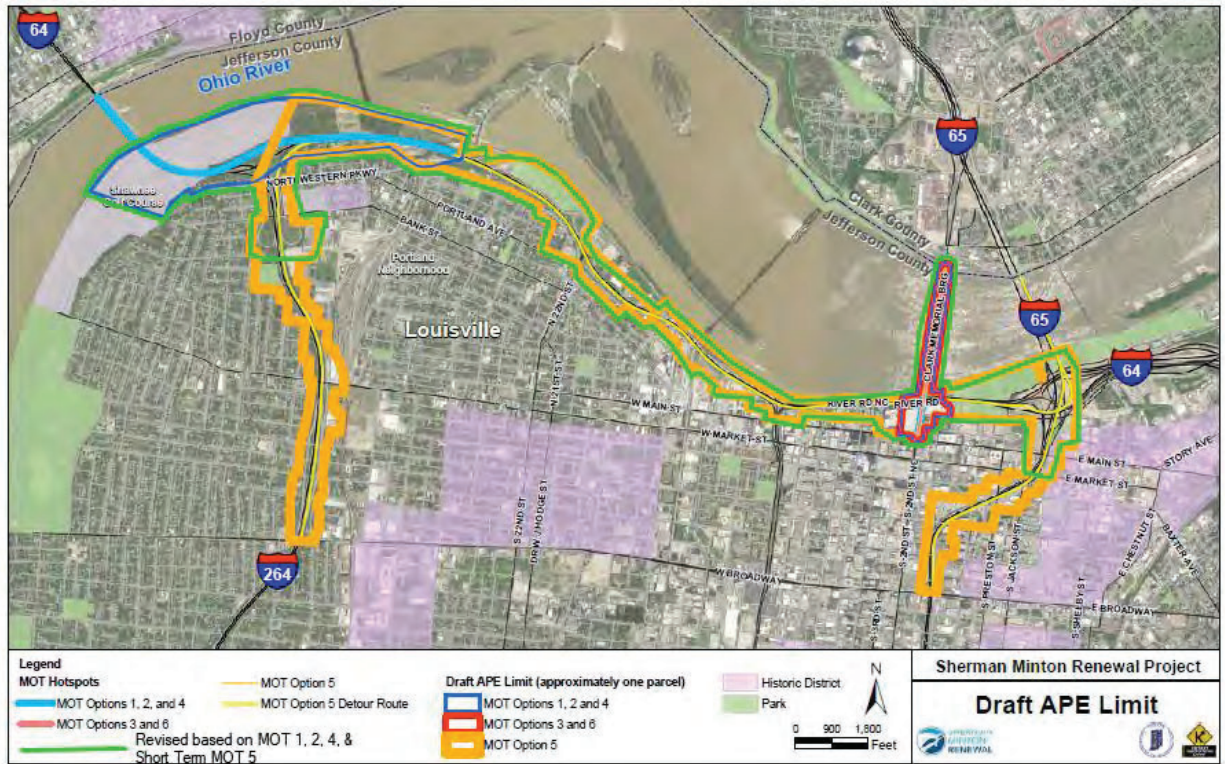
MOT 5 – Full Duration Closure of all six lanes and both decks

Closes east/west river crossing, consists of one phase, access changes required, all traffic diverted, detour route: I-265 to I-65, and concurrent access ramp modifications. MOT5 (Full Duration Closure) has the most/highest temporary changes from existing conditions for access, east/west crossings, diverted traffic, travel time, congestion, user cost, driver expectation, community and EJ impacts, and little to no public support. Short-term use of MOT 5 may be considered based upon constructability requirements, as warranted.

MOT 6 – One Direction/Phase three one-way lanes (WB-Phase 1/EB-Phase 2) open on one deck
Limits east/west river crossing, consists of two phases; Phase 1-lower deck closed / Phase 2-upper deck closed, only allows travel in one direction per phase, diversion of all traffic in the return direction, separate construction work zone, access changes required, and I-265 to I-65 detour route. MOT 6 has the second highest temporary changes from existing conditions for access, diverted traffic, travel time, congestion, user cost, driver expectation, community and EJ impacts, little public support, and closes half of the east/west crossings during both phases.

REVISED AREA OF POTENTIAL EFFECT

This graphic shows the revised APE in green overlaid on the original “worst case” APE, which accounted for long term, full bridge closure. As a long term closure seems unlikely at this time, the APE has been revised to focus on movement primarily between the two interstate crossings. The Clark Memorial Bridge (Louisville Municipal Bridge and Pylons, JFCD 217) is still included as it provides for movement across the river without tolls.



INVENTORY OF HISTORIC SITES-

The following tables were provided by Timothy Zinn of Michael Baker International, Inc.

National Historic Landmarks in APE

| Site # Sub # | Historic Name Est. Year | Location | Property Type & Historic Use | Status |
|-----------------|---|---|-----------------------------------|-------------------------------------|
| JFCD 218 | Idlewild / Belle of Louisville [1914] | 3700 Upper River Road, Carrie Gaulbert Cox Park, Louisville, KY | Object: Steamboat | NRHP #72000535 (1972) NHL (1989) |
| JFCD 252 | U.S. Life Saving Station #10 (Mayor Andrew Broaddus) [1929] | 4 th Street at River Road, Louisville, KY | Object: Lifesaving Station | NRHP #89001446 (1989) NHL (1989) |
| JFWP 182 | U. S. Marine Hospital [1837] | 2215 Portland Avenue, Louisville, KY | Building Health Care: Hospital | NRHP #97001265 (1997) NHL (1997) |

National Register Listed Sites in APE

| Site # Sub # | Historic Name Est. Year | Location | Property Type & Historic Use | Status |
|-----------------|--|--|--|---------------------------|
| JFCD 10 | Hamilton Brothers Warehouse/ Burwinkle- Hendershot Building | 125-127 W Main Street, Louisville, KY | Building Commerce/Trade: Warehouse | NRHP # 08000007 (2008) |
| JFCD 11 | Trade Mart Building (L & N Railroad Office) | 129-131 W. Main Street, Louisville, KY | Building Commerce/ Trade: Office | NRHP # 73000811 (1973) |
| JFCD 217 | Louisville Municipal Bridge, Pylons, and Administration Building | 2 nd Street at Main Street, Louisville, KY | Structure, Object, and Building Transportation | NRHP #84001578 (1984) |
| JFCD 246 | Old Presbyterian Theological Seminary | 109 E. Broadway Street, Louisville, KY | Building Religion: Seminary Education: College | NRHP # 78001362 (1978) |
| JFCH 701 | Tyler-Muldoon House | 132 E. Gary Street, Louisville, KY | Building Domestic: Single Dwelling | NRHP #77000629 (1977) |

| Site # Sub # | Historic Name Est. Year | Location | Property Type & Historic Use | Status |
|-----------------------------|--|---|--|---------------------------|
| JFL 271 & JFWS 306 | Northwestern Parkway (Olmstead Park System) | Shawnee Park, Louisville, KY | Site Roadway | NRHP # 82002715 (1982) |
| JFWP 134 | New Enterprise Tobacco Warehouse | 921-925 W. Main Street, Louisville, KY | Building Commerce/Trade: Warehouse | NRHP # 80001611 (1980) |
| JFWP 137 | Tobacco Realty Company Building | 118-126 N. 10 th Street, Louisville, KY | Building Commerce/Trade: Warehouse | NRHP #83002742 (1983) |
| JFWP 178 | James F. Irvin House | 2910 North Western Parkway, Louisville, KY | Building: Domestic: Single Dwelling | NRHP #83002688 (1983) |

National Register Districts in or partially within APE

| <i>NRHP-Listed Historic Districts</i> | | | | |
|---------------------------------------|---|--|----------|--|
| | Butchertown Historic District | | District | NRHP # 76000900 (1976) |
| | Marlow-Place Bungalows Historic District | | District | NRHP # 83002699 (1983) |
| | Old Portland Residential Historic District | | District | NRHP # 80001615 (1980) |
| | Phoenix Hill Historic District | | District | NRHP # 83002721 (1983) |
| | West Main Street Historic District / and West Main Street Historic District, Expanded | | District | NRHP # 80001615 (1980) NRHP # 80004594 (1980) |
| | Whiskey Row Historic District | | District | NRHP # 80004594 (1980) |

Contributing elements within National Register Districts in APE-

| Site # Sub # | Historic Name Est. Year | Location | Property Type & Historic Use | Status |
|-----------------|----------------------------|--|------------------------------------|--|
| JFWP 404 | House | 816 Cedar Grove Court Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 405 | House | 3425 Rudd Avenue Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 406 | House | 3421 Rudd Avenue Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 407 | House | 3419 Rudd Avenue Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 408 | House | 3417 Rudd Avenue Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 409 | House | 4311 Rudd Avenue Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 410 | House | 3323 Rudd Avenue Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 411 | House | 3321 Rudd Avenue Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 413 | House | 815 N. 34 th Street Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 415 | House | 3321 Rudd Avenue Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 416 | House | 3317 Rudd Avenue Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 417 | House | 3315 Rudd Avenue Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 418 | House | 3305 Rudd Avenue Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 419 | House | 810 N. 33 rd Street Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 420 | House | 815 N. 33 rd Street Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |

| | | | | |
|-------------|--------------------|--|----------|--|
| JFWP 421 | House | 812 N. 33 rd Street Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 422 | House | 816 N. 33 rd Street Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 432 | House | 3427 Rudd Avenue Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 433 | House | 805 Cedar Grove Terrace Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 434 | House | 807 Cedar Grove Terrace Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 436 | House | 541 N. 37 th Street Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 452 | Portland School | 3410 Northwestern Parkway Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 453 | Church of Our Lady | 3519 Rudd Avenue Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |
| JFWP 467 | Parochial School | Louisville, KY | Building | Contributes to NRHP District <i>Old Portland Residential District</i> |

| <i>Phoenix Hill Historic District</i> | | | | |
|---------------------------------------|---|---|----------|---|
| JFCH 1209 | Commercial Building | 505-507 E. Market Street Louisville, KY | Building | Contributes to NRHP District <i>Phoenix Hill Historic District</i> |
| JFCH 1211 | Commercial Building | 509-511 E. Market Louisville, KY | Building | Contributes to NRHP District <i>Phoenix Hill Historic District</i> |
| JFCH 1212 | Industrial Building (Baer Fabrics) | 515-523 E. Market Street Louisville, KY | Building | Contributes to NRHP District <i>Phoenix Hill Historic District</i> |
| JFCH 1216 | Commercial Building | 133 S. Jackson Street Louisville, KY | Building | Contributes to NRHP District <i>Phoenix Hill Historic District</i> |
| JFCH 1217 | American Elevator and Machine Company Warehouse and Shop | 117-129 S. Jackson Street Louisville, KY | Building | Contributes to NRHP District <i>Phoenix Hill Historic District</i> |
| JFCH 1249 | Albert Hess Furniture Store | 552 Market Street Louisville, KY | Building | Contributes to NRHP District <i>Phoenix Hill Historic District</i> |
| JFCH 1250 | Commercial Buildings | 607-609 E Market Street Louisville, KY | Building | Contributes to NRHP District <i>Phoenix Hill Historic District</i> |
| JFCH 3 | American Machine Co. / Vermont American Machine Co. | 500-510 E. Main Street and 101 S. Jackson Street Louisville, KY | Building | Contributes to NRHP District <i>Phoenix Hill Historic District</i> |
| JFCH 440 | Billy Goat Strut Building | 600 E. Main Street Louisville, KY | Building | Contributes to NRHP District <i>Phoenix Hill Historic District</i> |

| <i>West Main Street Historic District</i> | | | | |
|---|-------------------------------|--|--|---|
| JFCD 39 | Fort Nelson Building | 801 W. Main Street Louisville, KY | Building | Contributes to NRHP District <i>West Main Street Historic District</i> |
| JFCD 40 | Kentucky Warehouse | 803 W. Main Street Louisville, KY | Building Commerce/ Trade: Warehouse | Contributes to NRHP District <i>West Main Street Historic District</i> |
| JFCD 41 | Brandeis and Sanders Building | 809 W. Main Street Louisville, KY | Building | Contributes to NRHP District <i>West Main Street Historic District</i> |
| JFCD 42 | Todd-Donigan Iron Building | 811 W. Main Street Louisville, KY | Building | Contributes to NRHP District <i>West Main Street Historic District</i> |
| JFCD 43 | Todd-Dunigan Iron Building | 813 W. Main Street Louisville, KY | Building | Contributes to NRHP District <i>West Main Street Historic District</i> |
| JFCD 44 | Tapp-Leathers & Company | 815 W. Main Street Louisville, KY | Building | Contributes to NRHP District <i>West Main Street Historic District</i> |
| JFCD 45 | Ingalls & Co. Store | 819 W. Main Street Louisville, KY | Building | Contributes to NRHP District <i>West Main Street Historic District</i> |
| JFCD 46 | J W Morrill &Co. Building | 821 W. Main Street Louisville, KY | Building | Contributes to NRHP District <i>West Main Street Historic District</i> |
| JFCD 47 | Dr John Bull's Store | 823 W. Main Street Louisville, KY | Building | Contributes to NRHP District <i>West Main Street Historic District</i> |
| JFCD 48 | Commercial Building | 827-833 W. Main Street Louisville, KY | Building | Contributes to NRHP District <i>West Main Street Historic District</i> |

Sites previously determined eligible in APE-

| Site # Sub # | Historic Name Est. Year | Location | Property Type & Historic Use | Status |
|-----------------|--|---|--|---|
| JFWP 327 | Pennsylvania Railroad Bridge | Ohio River at 14 th Street Louisville, KY | Structure Transportation: Bridge | NRHP Eligible (Meets Criteria) |
| JFWP 528 | Peaslee-Gaulbert Paint Manufacturing Plant Complex [1900- 1924] | 226 N. 15 th Street Louisville, KY | Building | NRHP Eligible (Suggested NRHP Group) |

Previously surveyed demolished sites in APE-

| Site # Sub # | Historic Name Est. Year | Location | Property Type & Historic Use | Status |
|-----------------|--|--|--|--|
| JFWP 174 | Schweitzer Pharmacy | 2000 Portland Avenue Louisville, KY | Building | Demolished |
| JFWP 174-001 | Garage (Demolished) | 2000 Portland Avenue Louisville, KY | Building Domestic: Garage | Demolished |
| JFWP 135 | Commercial Building [1875-1899] | 927 W. Main Street Louisville, KY | Building | Demolished |
| JFWP 136 | Commercial Building | 929 W. Main Street Louisville, KY | Building | Demolished |
| JFCB 2 | House [1875-1899] | 613 E. Washington Street Louisville, KY | Building Domestic: Single Dwelling | Demolished <i>Butchertown Historic District</i> |
| JFWP 468 | Illinois Central Railroad Freight Depot [1875-1899] | N. side Rowan Street between 10 th and 12 th streets Louisville, KY | Building Transportation: Warehouse | Demolished |
| JFL 64 | House 1875-1899 | 2040 Portland Avenue Louisville, KY | Building Domestic: House | Demolished |

Previously surveyed sites in APE with undetermined eligibility-

| Site # Sub # | Historic Name Est. Year | Location | Property Type & Historic Use | Status |
|-----------------|---|---|--|---|
| JF 1031 | McAlpine Locks and Dam / Dam 41 Ohio River | Off Northwestern Parkway, Louisville, KY | Structure | Undetermined |
| JFCB 722 | John F. Kennedy Memorial Bridge | Ohio River Between Louisville, KY and Jeffersonville, IN | Structure Bridge | Undetermined |
| JFCD 228 | University of Louisville Dental School | 129 E. Broadway Street Louisville, KY | Building | Undetermined |
| JFCH 530 | Children's Hospital | 610 S. Floyd Street Louisville, KY | Building Health Care: Hospital | Undetermined |
| JFCH 531 | Commercial Building | 238 S. Preston Street Louisville, KY | Building | Undetermined |
| JFL 501 | Louisville Floodwall | Along the Riverfront Louisville, KY | Structure | Undetermined |
| JFWP 287 | House | 1801 Portland Avenue Louisville, KY | Building Domestic: Single Dwelling | Undetermined |
| JFWP 332 | Kentucky and Indiana Terminal Railroad Bridge | Ohio River at 32 nd Street Louisville, KY | Structure Transportation: Bridge | Undetermined |
| JFWP 525 | Commercial Warehouse (No Report Received) | Off Northwestern Parkway between Louisville & Portland Canal & I-64 Louisville, KY | Building | Undetermined |
| JFWP 534 | House | 3231 Rudd Avenue Louisville, KY | Building | Undetermined |
| JFWP 535 | House | 3229 Rudd Avenue Louisville, KY | Building | Undetermined |
| JFWP 536 | House | 3225 Rudd Avenue Louisville, KY | Building | Undetermined |
| JFWP 537 | House | 3223 Rudd Avenue Louisville, KY | Building | Undetermined |
| JFWP 538 | House | 3219 Rudd Avenue Louisville, KY | Building | Undetermined |
| JFWP 539 | House | 3217 Rudd Avenue Louisville, KY | Building | Undetermined |
| JFCD 254 | Waterside Station Louisville Gas & Electric Company | 227 W. Washington Street Louisville, KY | Building | Undetermined <i>Appears Demolished</i> |
| JFCH 534 | Commercial Building | 217-219 S. Preston Street Louisville, KY | Building | Undetermined <i>Appears Demolished</i> |

Sites with historic preservation easements within APE-

| Site # Sub # | Historic Name Est. Year | Location | Property Type & Historic Use | Status |
|-----------------|---------------------------------------|--|---|---|
| JFCB 634 | Grocer's Ice and Cold Storage Company | 601-615 E Main Street Louisville, KY | Building Commerce/ Trade: Warehouse | Contributes to NRHP District <i>Butchertown Historic District Easement</i> |
| JFCD 246 | Old Presbyterian Theological Seminary | 109 E. Broadway Street Louisville, Ky | Building Religion: Seminary | Individually Listed on the NRHP <i>Easement</i> |
| JFWP 182 | U. S. Marine Hospital | 2215 Portland Avenue Louisville, KY | Building Health Care: Hospital | National Historic Landmark |

Project Impacts and Determination of Effects-

The following explanation of effects was provided by Timothy Zinn of Michael Baker International, Inc.

The proposed Sherman Minton Renewal Project poses no direct impacts to any of the identified historic resources. The project involves the rehabilitation of the Sherman Minton Bridge, within direct impacts associated with diverted traffic, temporary staging areas, and impermanent crossover routes. In this way, the project has the *potential for historic properties to be affected* [36CFR 800.4(D)(2)]. In Kentucky, the crossover areas and staging areas will likely be situated at the base of the Sherman Minton approach bridges, in the Shawnee Golf Course. The Shawnee Golf Course is part of the Olmstead Park System/ Northwestern Parkway NRHP-listed historic resource and has the largest potential for impact of all the identified resources within the APE. However, the installation of temporary crossover areas and staging areas does not adversely affect the historic resource. In addition to the bridge itself, the APE includes the directed detour route for MOT Option 5. While other MOT options will also result in displaced traffic, Option 5 diverts all the 90,000 daily trips away from the Sherman Minton Bridge onto other bridges. The suggested detour route in Louisville follows I-64 to the other local bridges, which will carry most of the vehicle movement. Several other east/west roadways are also available to alleviate traffic congestion. In this way, heavy traffic is not anticipated to back-up roadways in Louisville as a result of Option 5, much less Options 1, 2, 3, 4, and 6. The street network in West Louisville is an arterial grid that generally operates under capacity. The main north-south roadways are 22nd Street and 9th Street to the east. The main east-west arterials are Portland Street and Bank Street to the north and Main Street (2 lanes EB and WB), Market Street (1 lane EB and WB), Muhammad Ali Boulevard (2 lanes WB), Chestnut Street (2 lanes EB), and Broadway (2 lanes EB and WB) to the south. These arterials provide sufficient lanes to allow east-west travel with excess capacity to spare. All the MOT options show a very modest increase in traffic considering the number of arterial routes included. The largest increases in traffic occur in MOT 4 where there are restrictions to ramps within the I-64/I-264 interchange. The minimal increases in per-lane peak-hour traffic onto an already under capacity network is not likely to cause any noteworthy congestion in West Louisville. Even the highest increase experienced in MOT 4 would only be on the order of about one additional vehicle per minute. The indirect effects associated with the Sherman Minton Renewal Project will have No Adverse Effect on historic properties.

Summary of effects for each alternative-

MOT Alternatives 1, 2, 4, and Short Term 5 are moving forward for consideration by the DBT.

Tables provided by Timothy Zinn of Michael Baker International, Inc.

| MOT | Proposed Action | Nearby Historic Resources | Historic Status | Finding of Effect |
|-------------------------------------|---|---|------------------------------|--|
| MOT OPTION 1 Single Lane Closure | <p>Action: This option will maintain two lanes of traffic (and close four lanes of traffic) throughout the entire bridge rehabilitation. The open and closed lanes will alternate in three phases. All ramps will remain open.</p> <p>Traffic Impacts: This option is the least impactful to regional travel. Only 7,400 daily trips (out of 90,000) will be diverted to other bridges.</p> | JFL 271 / JFWS 306 Olmstead Park System/ Northwestern Parkway | NRHP # 82002715 (1982) | <p><input type="checkbox"/> (i) Physical destruction of or damage to all or part of the property; <i>this option will not result in physical destruction or damage to the Olmstead Park System. Further, the golf course is not a contributing feature of the resource.</i></p> <p><input type="checkbox"/> (ii) Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines; <i>N/A</i></p> <p><input type="checkbox"/> (iii) Removal of the property from its historic location; <i>N/A</i></p> <p><input type="checkbox"/> (iv) Change of the character of the property's use or of physical features within the property's setting that contribute to its historic significance; <i>N/A</i></p> <p><input type="checkbox"/> (v) Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features; <i>the introduction of visual and audible elements (part of the staging areas) will be temporary and will not diminish the Olmstead Park System's integrity. The introduction of any diverted traffic throughout the APE will not be significant. The minimal increases in per-lane peak-hour traffic in Louisville onto an already under capacity network is not likely to cause any noteworthy congestion in West Louisville. No historic resources will be directly or indirectly affected by this increase.</i></p> <p><input type="checkbox"/> (vi) Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization; <i>N/A</i></p> <p><input type="checkbox"/> (vii) Transfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance. <i>N/A</i></p> <p>FINDING: NO ADVERSE EFFECT</p> |

| MOT | Proposed Action | Nearby Historic Resources | Historic Status | Finding of Effect |
|----------------------------------|---|---|------------------------------|--|
| MOT OPTION 2 Two Lane Closure | <p>Action: This option is similar to Option 1, except only requires only two phases of construction instead of three. To accomplish this, temporary crossovers are required to shift traffic. All ramps will remain open.</p> <p>Traffic Impacts: This option is moderately impactful to regional travel. Approximately 33,400 daily trips (out of 90,000) will be diverted to other bridges.</p> | JFL 271 / JFWS 306 Olmstead Park System/ Northwestern Parkway | NRHP # 82002715 (1982) | <p><input type="checkbox"/> (i) Physical destruction of or damage to all or part of the property; <i>this option will not result in physical destruction or damage to the Olmstead Park System. Further, the golf course is not a contributing feature of the resource.</i></p> <p><input type="checkbox"/> (ii) Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines; <i>N/A</i></p> <p><input type="checkbox"/> (iii) Removal of the property from its historic location; <i>N/A</i></p> <p><input type="checkbox"/> (iv) Change of the character of the property's use or of physical features within the property's setting that contribute to its historic significance; <i>N/A</i></p> <p><input type="checkbox"/> (v) Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features; <i>the introduction of visual and audible elements (part of the staging areas) will be temporary and will not diminish the Olmstead Park System's integrity. The introduction of any diverted traffic throughout the APE will not be significant. The minimal increases in per-lane peak-hour traffic in Louisville onto an already under capacity network is not likely to cause any noteworthy congestion in West Louisville. No historic resources will be directly or indirectly affected by this increase.</i></p> <p><input type="checkbox"/> (vi) Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization; <i>N/A</i></p> <p><input type="checkbox"/> (vii) Transfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance. <i>N/A</i></p> <p>FINDING: NO ADVERSE EFFECT</p> |

| MOT | Proposed Action | Nearby Historic Resources | Historic Status | Finding of Effect |
|---|--|---|------------------------------|---|
| MOT OPTION 3 One Directional Closure (Full Closure of One Deck) | <p>Action: This option keeps three lines of traffic open in the direction of the peak flow during the peak hours, on one level of the bridge at a time. Temporary crossovers shift traffic in either direction. Some ramps will be closed.</p> <p>Traffic Impacts: This option is moderately impactful to regional travel. Approximately 40,600 daily trips (out of 90,000) will be diverted to other bridges.</p> | JFL 271 / JFWS 306 Olmstead Park System/ Northwestern Parkway | NRHP # 82002715 (1982) | <p><input checked="" type="checkbox"/> (i) Physical destruction of or damage to all or part of the property; <i>this option will not result in physical destruction or damage to the Olmstead Park System. Further, the golf course is not a contributing feature of the resource.</i></p> <p><input type="checkbox"/> (ii) Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines; <i>N/A</i></p> <p><input type="checkbox"/> (iii) Removal of the property from its historic location; <i>N/A</i></p> <p><input type="checkbox"/> (iv) Change of the character of the property's use or of physical features within the property's setting that contribute to its historic significance; <i>N/A</i></p> <p><input type="checkbox"/> (v) Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features; <i>the introduction of visual and audible elements (part of the staging areas) will be temporary and will not diminish the Olmstead Park System's integrity. The introduction of any diverted traffic throughout the APE will not be significant. The minimal increases in per-lane peak-hour traffic in Louisville onto an already under capacity network is not likely to cause any noteworthy congestion in West Louisville. No historic resources will be directly or indirectly affected by this increase.</i></p> <p><input type="checkbox"/> (vi) Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization; <i>N/A</i></p> <p><input type="checkbox"/> (vii) Transfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance. <i>N/A</i></p> <p>FINDING: NO ADVERSE EFFECT</p> |

| MOT | Proposed Action | Nearby Historic Resources | Historic Status | Finding of Effect |
|---|---|---|------------------------|---|
| MOT OPTION 4 Movable Barrier Option (Full Closure of One Deck) | <p>Action: This option keeps two lines of traffic open in the direction of the peak flow during the peak hours, and one counter flow lane in the opposite direction during those peak hours. Temporary crossovers shift traffic in either direction. Multiple ramps will be closed.</p> <p>Traffic Impacts: This option is moderately impactful to regional travel. Approximately 19,700 daily trips (out of 90,000) will be diverted to other bridges.</p> | JFL 271 / JFWS 306 Olmstead Park System/ Northwestern Parkway | NRHP # 82002715 (1982) | <ul style="list-style-type: none"> <input type="checkbox"/> (i) Physical destruction of or damage to all or part of the property; <i>this option will not result in physical destruction or damage to the Olmstead Park System. Further, the golf course is not a contributing feature of the resource.</i> <input type="checkbox"/> (ii) Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines; <i>N/A</i> <input type="checkbox"/> (iii) Removal of the property from its historic location; <i>N/A</i> <input type="checkbox"/> (iv) Change of the character of the property's use or of physical features within the property's setting that contribute to its historic significance; <i>N/A</i> <input type="checkbox"/> (v) Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features; <i>the introduction of visual and audible elements (part of the staging areas) will be temporary and will not diminish the Olmstead Park System's integrity. The introduction of any diverted traffic throughout the APE will not be significant. The minimal increases in per-lane peak-hour traffic in Louisville onto an already under capacity network is not likely to cause any noteworthy congestion in West Louisville. No historic resources will be directly or indirectly affected by this increase.</i> <input type="checkbox"/> (vi) Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization; <i>N/A</i> <input type="checkbox"/> (vii) Transfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance. <i>N/A</i> <p>FINDING: NO ADVERSE EFFECT</p> |

| MOT | Proposed Action | Nearby Historic Resources | Historic Status | Finding of Effect |
|--|--|---|------------------------|---|
| MOT OPTION 5 Full closure (Both Decks Closed) | <p>Action: This option closes the bridge to all traffic, thereby decreasing the overall construction duration. Detour routes add traffic to the other Ohio River bridges, particularly the Clarke Memorial Bridge (carrying US Highway 31).</p> <p>Traffic Impacts: This option is significantly impactful to regional travel. All 90,000 daily trips will be diverted to other bridges.</p> | JFL 271 / JFWS 306 Olmstead Park System/ Northwestern Parkway | NRHP # 82002715 (1982) | <ul style="list-style-type: none"> <input type="checkbox"/> (i) Physical destruction of or damage to all or part of the property; <i>this option will not result in physical destruction or damage to the Olmstead Park System. Further, the golf course is not a contributing feature of the resource.</i> <input type="checkbox"/> (ii) Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines; <i>N/A</i> <input type="checkbox"/> (iii) Removal of the property from its historic location; <i>N/A</i> <input type="checkbox"/> (iv) Change of the character of the property's use or of physical features within the property's setting that contribute to its historic significance; <i>N/A</i> <input type="checkbox"/> (v) Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features; <i>the introduction of visual and audible elements (part of the staging areas) will be temporary and will not diminish the Olmstead Park System's integrity. The introduction of any diverted traffic throughout the APE will not be significant. The minimal increases in per-lane peak-hour traffic in Louisville onto an already under capacity network is not likely to cause any noteworthy congestion in West Louisville. No historic resources will be directly or indirectly affected by this increase.</i> <input type="checkbox"/> (vi) Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization; <i>N/A</i> <input type="checkbox"/> (vii) Transfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance. <i>N/A</i> <p>FINDING: NO ADVERSE EFFECT</p> |

| MOT | Proposed Action | Nearby Historic Resources | Historic Status | Finding of Effect |
|---|--|---|------------------------------|--|
| MOT OPTION 6 One Directional Closure (Full Closure of One Deck/One Direction) | <p>Action: This option is similar to Option 3 in that each deck will be closed during two phases of construction, while three lanes of traffic will remain open. One direction of traffic will be completely shut down during each phase of this option, and the opposite direction of traffic will be diverted.</p> <p>Traffic Impacts: This option is moderately impactful to regional travel. Approximately 46,600 daily trips (out of 90,000) will be diverted to other bridges.</p> | JFL 271 / JFWS 306 Olmstead Park System/ Northwestern Parkway | NRHP # 82002715 (1982) | <ul style="list-style-type: none"> <input type="checkbox"/> (i) Physical destruction of or damage to all or part of the property; <i>this option will not result in physical destruction or damage to the Olmstead Park System. Further, the golf course is not a contributing feature of the resource.</i> <input type="checkbox"/> (ii) Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines; <i>N/A</i> <input type="checkbox"/> (iii) Removal of the property from its historic location; <i>N/A</i> <input type="checkbox"/> (iv) Change of the character of the property's use or of physical features within the property's setting that contribute to its historic significance; <i>N/A</i> <input type="checkbox"/> (v) Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features; <i>the introduction of visual and audible elements (part of the staging areas) will be temporary and will not diminish the Olmstead Park System's integrity. The introduction of any diverted traffic throughout the APE will not be significant. The minimal increases in per-lane peak-hour traffic in Louisville onto an already under capacity network is not likely to cause any noteworthy congestion in West Louisville. No historic resources will be directly or indirectly affected by this increase.</i> <input type="checkbox"/> (vi) Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization; <i>N/A</i> <input type="checkbox"/> (vii) Transfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance. <i>N/A</i> <p>FINDING: NO ADVERSE EFFECT</p> |

Conclusion

It is the determination of KYTC on behalf of FHWA that the Sherman Minton Rehabilitation Project as proposed will have **No Adverse Effect** on the Shawnee Golf Course or Shawnee Park or Northwestern Parkway, all listed on the National Register as part of the Olmstead Park System. Staging areas will be subject to additional review once they are identified. The preferred alternative selected by the DBT will be presented to SHPO to determine if any additional consultation is warranted. The project as proposed does not have the potential to adversely affect any other historic sites within the APE.

The Clark Memorial Bridge (Louisville Municipal Bridge and Pylons) is the only historic site that could be directly affected by the increased traffic; however, the bridge should not be harmed by added capacity. Additional analysis of traffic impacts during the Sherman Minton project may be necessary to avoid impacts to the Clark Memorial Bridge, which currently has an ADT of 14,800 trips. The bridge is currently posted for loads between 20 and 40 tons depending on the number of axles. Based on information available now the project should have **No Adverse Effect** on this bridge. The SHPO will have the opportunity for additional consultation regarding impacts once the DBT selects a preferred MOT alternative.



ANDY BESHEAR
GOVERNOR

**TOURISM, ARTS AND HERITAGE CABINET
KENTUCKY HERITAGE COUNCIL
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CRAIG A. POTTS
EXECUTIVE DIRECTOR
& STATE HISTORIC
PRESERVATION OFFICER

MIKE BERRY
SECRETARY

03/30/2020

Mr. Daniel R. Peake, Director
Division of Environmental Analysis
Kentucky Transportation Cabinet
200 Mero Street,
Frankfort, KY 40622

**Subject: Re: Sherman Minton Eligibility and Effects Determination
Jefferson County, KY and Floyd County, IN
KYTC Item No. 5-10027**

Dear Mr. Peake:

Thank you for your submittal of an eligibility and effects determination for the above-reference project. We understand that due to situations outside of KYTC's control, the timeline for Sherman Minton has accelerated rapidly and that a response is requested from our office. Given the timeline, our office will accept an alternative Section 106 route. Your office proposes a No Adverse Effect finding for the project. Our office can provide a **conditional No Adverse Effect concurrence contingent on the following:**

- 1) Since this project is a design-build contract, once an alternative has been chosen KYTC shall provide SHPO with the chosen alternative and final effects recommendation.
- 2) Once the alternative and APE has been chosen, KYTC shall identify and reach out to the appropriate consulting parties based upon the chosen APE.

We look forward to additional consultation on this project. If you have any questions, please contact Gabrielle Fernandez of my staff at (502) 892-3623.

Sincerely,

Craig Potts
Executive Director and
State Historic Preservation Officer

CP: GF, KHC #57519
cc. Amanda Abner (DEA)



Wood Environment & Infrastructure Solutions, Inc.
11003 Bluegrass Parkway, Suite 690
Louisville, Kentucky 40299
USA

T: 502-267-0700

www.woodplc.com

26 March 2020

Ms. Susan Neumeyer
Archaeologist Coordinator
Kentucky Transportation Cabinet
Division of Environmental Analysis
200 Mero Street
Frankfort, Kentucky 40622

Re: Management Summary, Phase I Archaeological Survey of Approximately 2.5 Acres for Proposed maintenance of the I-64 Sherman Minton Bridge, Three Indiana Approach Bridges, and One Kentucky Approach Bridge (Joint Project with INDOT), Jefferson County, Kentucky. (KYTC Item No. 5-10027.00) (KY OSA registration No.: FY20-10684)

Dear Ms. Neumeyer:

On 23 and 24 March 2020, Wood Environment & Infrastructure Solutions, Inc. (Wood) conducted a Phase I archaeological survey of approximately 2.5 acres for proposed maintenance of the I-64 Sherman Minton Bridge and three Indiana approach bridges and one Kentucky approach bridge in Jefferson County, Kentucky (KYTC Item No. 5-10027.00) (**Figure 1**). The survey was undertaken at the request of the Kentucky Transportation Cabinet (KYTC) to ensure compliance with Section 106 of the National Historic Preservation Act (NHPA) (Public Law 89-665; 54 U.S.C. 300101 *et seq.*). The project Area of Potential Effect (APE) for the survey consists of an approximately 2.5 acres (ac) (1.01 hectares [ha]) area beneath the Kentucky approach to the Sherman Minton Bridge within the Shawnee Golf Course.

Two newly recorded archaeological sites (FS-1 and FS-2) were identified during this survey. Site FS-1 is a mid- to late nineteenth century artifact scatter (**Figure 2**). The scatter consisted of two brick fragments and one piece of domestic stoneware recovered from a single shovel test in disturbed deposits. Site FS-2 is a small prehistoric lithic scatter of undetermined cultural affiliation. A total of seven pieces of lithic debitage was recovered from two shovel tests in the upper 40 cm of soils. No features or significant artifact concentrations were identified at either site FS-1 or FS-2. Given the paucity of artifacts found, with FS-1 artifacts in disturbed context, and lack of observed feature, sites FS-1 and FS-2 are recommended as not eligible for the NRHP and no further archaeological investigations are recommended.

Description of Area of Potential Effect (APE)

The 2.5 ac APE falls beneath a 0.15 mile (mi) [250 meter (m)] stretch of I-64 directly beneath the Kentucky approach to the Sherman Minton Bridge, in Louisville, Jefferson County, Kentucky. Ground cover in the APE consisted of short to tall patchy grass areas on the level floodplain of the Ohio River (**Figures 3 and 4**).

Field Methods

Field methods consisted of pedestrian survey and visual inspection of the entire APE and systematic shovel testing. No areas within the APE contained greater than 15 percent slope and as per KYTC guidance, fieldwork

consisted of a single transect with shovel tests excavated at 20-m (65 ft) (Susan Neumeyer, personal communication, March 20, 2020). Shovel test probes (STP) measured 30 cm (12 in) in diameter and were excavated to a depth of 1 m (3.3 ft), water table, gravel fill impasse, or compact soil impasse, whichever was encountered first. The project area resides within the floodplain of the Ohio River and contains the potential for deeper soils with cultural deposits. Deeper soils were examined through the excavation of auger probes into the base of selected STPs. Auger probes were conducted at no more than 50 m (164 ft) intervals by hand using a 3 in (7.6 cm) bucket auger and excavated to a depth of 2 m (6.6 ft). All shovel test and auger fill were screened through 0.25-in (6.35-mm) hardware cloth, and artifacts were bagged and labeled with appropriate provenience information. When an archaeological site was encountered, delineation shovel tests were excavated at no more than 10 m intervals within the APE. STP and auger locations were mapped with a handheld GPS instrument.

Summary of Current Findings

A total of 28 STPs and six bucket augers were excavated within the APE, that includes a single transect of 12 STPs and 16 site delineation STPs (**Figure 2**). Background research revealed that no archaeological sites have been recorded within or directly adjacent to the APE. However, the APE is located within the Shawnee Park golf course, a contributing element of the National Register of Historic Places (NRHP) listed Olmstead Park system (NRHP #82002715). Additionally, site 15Jf418, the Portland Proper Archaeological Site and Portland Wharf Park, is located approximately 350 m east of the APE. Review of historic topographic maps and aerial images do not show any historic structures within or directly adjacent to the APE, but the 1858 Atlas of Jefferson County, Kentucky does show a structure just northeast of the APE.

Site FS-1: Site FS-1 is historic artifact scatter located in a drainage area between an earthen floodwall to the south and the golf course to the north (**Figure 5**). The site encompasses an area of 0.02 ac (0.008 ha) and is covered by grasses limiting surface visibility (**Figure 6**). Site FS-1 consists of a single positive shovel test containing one historic ceramic and two brick fragments. Preliminary analysis conducted on the historic ceramic recovered at FS-1 classifies it as domestic stoneware with Albany and Salt glaze decoration that dates from 1830 to 1925 (Raycraft and Raycraft 1990). Shovel testing revealed a disturbed, heavily mottled soil profile that extended to a maximum depth of 50 cm below surface and underlain by dense gravel fill (**Figure 7**). These artifacts are most likely associated with a structure documented on the 1858 Atlas of Jefferson County, Kentucky (**Figure 8**). However, no intact structural remnants or other features related to an occupation, such as a midden or cellar, were observed. In addition to the mottled soils and gravel fill, visible disturbances to the site include the construction of a floodwall, landscaping for the golf course, and construction of the existing approach to the Sherman Minton Bridge.

Overall, FS-1 yielded a low density historic artifact scatter, with no intact features or significant artifact concentrations noted. While the materials are likely associated with the mid- to late nineteenth century occupation depicted nearby on historic maps, the heavy disturbance of the area and recovery of all artifacts in disturbed soils suggest site FS-1 is not likely to contain significant information regarding historic occupations in Kentucky.

Site FS-2: Site FS-2 is a prehistoric lithic scatter of undetermined cultural affiliation (**Figure 9**). The site, encompassing an area of 0.04 ac (0.016 ha), is located on a mild slope leading to the Ohio River and covered by short patchy grass with limited surface visibility (**Figure 10**). Artifacts recovered at the site include seven pieces of lithic debitage from two shovel tests. All artifacts were recovered from the top 40 cm below surface and no features were identified or diagnostic materials recovered. Disturbances observed at the site included the existing approach to the Sherman Minton Bridge and a pile of asphalt debris along the eastern end of the site (**Figure 11**). Due to the location of the asphalt pile within the APE, along the eastern edge of the site, the site could not be fully delineated. No shovel tests were completed outside the APE; however, shovel tests excavated along the edge of the asphalt pile yielded no artifacts and suggested the site was unlikely to extend outside the APE.

Overall, FS-2 yielded a low density prehistoric lithic scatter of undetermined cultural affiliation with no intact features or significant artifact concentrations noted. Disturbances at the site included a pile of asphalt debris and

the existing Sherman Minton Bridge approach. While the location of the asphalt debris limited delineation testing, shovel tests excavated along the edge of this asphalt suggests the site is unlikely to extend outside the current APE. Site FS-2 is not likely to provide significant information regarding prehistoric occupations in Kentucky. Therefore, Wood recommends FS-2 as not eligible for the NRHP and no further work is recommended.

As a result of this survey Wood recommends that the proposed activities will not adversely impact significant archaeological resources within the APE, and no further archaeological investigations within the project APE are recommended.

If you have any questions concerning the results of the field investigation and the information provided in this interim management summary, please contact Tim Reynolds at tim.reynolds@woodplc.com / 502-541-1228 or Hank McKelway at henry.mckelway@woodplc.com / 859-566-3721.

Sincerely,



Timothy Reynolds, BA
Staff Archaeologist
Field Director

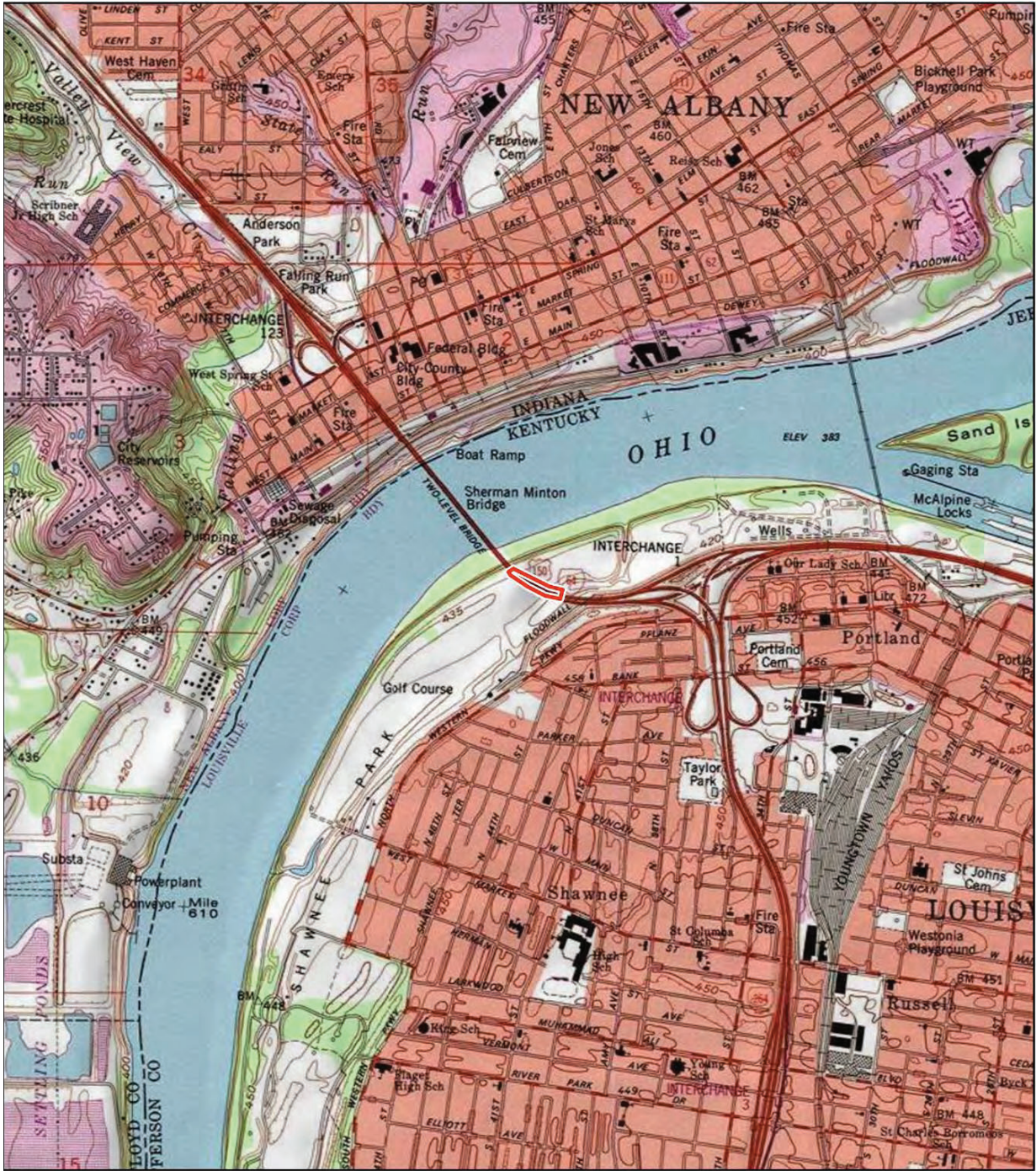


Hank McKelway, PhD, RPA
Cultural Resources Program Manager
Project Manager

TSR

References Cited

Raycraft D. and C. Raycraft
1990 *Collectors Guide to Country Stoneware and Pottery (Second Series)*. Collector Cooks, Paducah.



| | | | | |
|---|--|--|-------------------------------|-------------------------|
| | | MAP BY: tim.reynolds | LEGEND: APE | TIME: 10:44:54 AM |
| | | CHK'D BY: MW | | DATE: 3/16/2020 |
| Wood Environment & Infrastructure Solutions, Inc. 690 Commonwealth Center 11003 Bluegrass Parkway Louisville, KY 40299 | | | DATUM: North American 1983 | PROJECT #: 7361XXXXX |
| | | PROJECTION: NAD 1983 StatePlane Kentucky North FIPS 1601 | SCALE: 1 in=2,000 ft | |

T:\KYTC\ShermanMinton\ShermanMinton.mxd

Imagery: USGS 24k Quad - New Albany, IN (1992)

Figure 1. APE depicted on 1992 USGS 7.5' New Albany, IN topographic quadrangle.

March 27, 2020

Mr. Craig Potts, Executive Director and State Historic Preservation Officer
Kentucky Heritage Council
The Barstow House
410 High Street
Frankfort, KY 40601

SUBJECT: Request for Conditional Concurrence with No Historic Properties Affected based on attached Management Summary for Phase I Archaeological Survey of Approximately 2.5 Acres for the Proposed Maintenance of the Sherman Minton Bridge Carrying I-64 over the Ohio River in Louisville, Jefferson County, Kentucky
Jefferson County
KYTC Item No. 5-10027.00

Dear Mr. Potts,

The proposed maintenance project of the Sherman Minton Bridge carrying I-64 over the Ohio River in Louisville will affect a portion of the Shawnee Golf Course (within the National Register-listed Shawnee Park) below the approaches to the bridge. This area, encompassing approximately 2.5 acres, was subjected to an archaeological survey by Wood Environmental Solutions, who provided the attached Management Summary. Two small archaeological sites were identified during this survey. The sites have not received OSA numbers yet and are identified in the Management Summary as FS-1 and FS-2.

FS-1 is a very small historic artifact assemblage consisting of two brick fragments and a single ceramic sherd dating between 1830 and 1925. The three artifacts were recovered from a single shovel test. The artifacts may be related to a structure that appears on an 1858 Atlas of Jefferson County. The shovel test exhibited evidence of prior disturbance as the soil was heavily mottled throughout its profile. The shovel test terminated at a layer of dense gravel fill. Visual evidence of disturbance in the area include a floodwall, golf course landscaping, and the piers for the Sherman Minton Bridge approaches.

FS-2 consisted of seven lithic flakes recovered from two separate shovel tests. The site is located on a mile slope leading to the Ohio River. There is an asphalt pile that prohibited defining the site boundary in one direction, but no additional materials were located in or near the asphalt pile.

Six bucket augers were excavated. These did not reveal evidence of deeply buried intact archaeological deposits or buried soils.

Wood is not recommending additional work for these two sites. KYTC concurs that based on limited artifact assemblages and existing disturbances in the area, no additional archaeological

investigation is warranted for FS-1 and FS-2. KYTC, on behalf of FHWA, recommends a finding of No Historic Properties Affected for archaeological resources.

Because this project is on an extremely tight schedule, KYTC requests your Conditional Concurrence with the finding of No Historic Properties Affected for FS-1 and FS-2. The conditions of your concurrence would be:

1. OSA site numbers will be requested for FS-1 and FS-2 by Wood;
2. An acceptable Phase I report will be submitted in accordance with the project contract; and
3. Any areas the construction contractor requires outside the area surveyed and beyond the areas identified in the October 2019 Area of Potential Effect submitted by Michael Baker, Inc., will be subjected to review and archaeological survey, if required. It has been confirmed with the KYTC project team that there will be measures in the construction contract ensuring this condition.

This letter is initially being submitted electronically without signature due to the coronavirus and the demands of teleworking. A signed, hard copy will follow in a timely manner. Please respond to this digital copy as soon as possible.

Sincerely,

Daniel R. Peake, Director
Division of Environmental Analysis

DRP/msn
Enclosure

c: Tim Foreman, FHWA; Eric Rothermel, Archaeology Files, Reading Files



ANDY BESHEAR
GOVERNOR

**TOURISM, ARTS AND HERITAGE CABINET
KENTUCKY HERITAGE COUNCIL
THE STATE HISTORIC PRESERVATION OFFICE**

MIKE BERRY
SECRETARY

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CRAIG A. POTTS
EXECUTIVE DIRECTOR
& STATE HISTORIC
PRESERVATION OFFICER

March 27, 2020

Mr. Daniel R. Peake, Director
Division of Environmental Analysis
Kentucky Transportation Cabinet
200 Mero Street
Frankfort, Kentucky 40601

Re: *Management Summary for Phase I Archaeological Survey of Approximately 2.5 Acres for the Proposed Maintenance of the Sherman Minton Bridge Carrying I-64 over the Ohio River in Louisville, Jefferson County, Kentucky (KYTC Item # 5-10027)*

Dear Mr. Peake,

Thank you for one copy of the above-referenced *Management Summary*. We understand this investigation covered an area of approximately 2.5 acres proposed for use in the maintenance of the I-64 Sherman Minton bridge approach. Survey methods included pedestrian survey, shovel test excavation, and deep testing. Two archaeological sites were identified within the Area of Potential Effect (APE) and were assigned temporary field identification numbers FS-1 and FS-2.

FS-1 was documented as a mid-to-late 19th century historic artifact scatter. FS-2 was documented as a prehistoric lithic scatter of indeterminate temporal affiliation. The authors recommend these sites be considered ineligible for listing on the National Register of Historic Places (NRHP.) KYTC, on behalf of FHWA, concurs with these findings.

We concur with these findings, conditional upon the following stipulations:

1. OSA site numbers will be requested for FS-1 and FS-2 by Wood;
2. Three bound copies of an acceptable Phase I report be submitted to our office for review and comment no later than June 30, 2020; and
3. Should there be any additions or modifications to the APE, this office will be consulted and additional archaeological survey may be required.

Please feel free to contact Nicole Konkol of my staff at nicole.konkol@ky.gov with any questions or concerns.

Sincerely,



Craig A. Potts,
Executive Director and
State Historic Preservation Officer

KHC # 57310
CP nk
cc: Susan Neumeyer (KYTC)



COMMONWEALTH OF KENTUCKY
TRANSPORTATION CABINET
transportation.ky.gov

Andy Beshear
GOVERNOR

Jim Gray
SECRETARY

July 15, 2020

Mr. Craig Potts, Executive Director and State Historic Preservation Officer
Kentucky Heritage Council
The Barstow House
410 High Street
Frankfort, KY 40601

SUBJECT: Phase I Archaeological Survey of Approximately 2.5 Acres for Proposed Maintenance of the Sherman Minton Bridge, Three Indiana Approach Bridges, and One Kentucky Approach Bridge (Joint Project with INDOT)
By Tim Reynolds and Forest Kelly, Wood Environment Infrastructure & Solutions, July 13, 2020
Jefferson County
KYTC Item No. 5-10027.00

Dear Mr. Potts:

Attached please find an electronic copy of the revised subject report and the Kentucky Transportation Cabinet's (KYTC) letter to the consultant requesting the revision and providing them with our combined agency comments on the initial report submission. The requested changes have been made in the revision.

If you have questions, please contact Susan Neumeyer of my staff at susan.neumeyer@ky.gov.

Sincerely,

A handwritten signature in black ink, appearing to read "Dan Peake".

Daniel R. Peake, Director
Division of Environmental Analysis

DRP/sn
Attachment

Ec: T. Foreman
D-5: Donna Hardin
FHWA: Eric Rothermel
Michael Baker: Wendy Vachet
Reading File
Archaeology File



COMMONWEALTH OF KENTUCKY
TRANSPORTATION CABINET

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GOVERNOR

Jim Gray
SECRETARY

June 22, 2020

Henry McKelway, Ph.D.
Midwest Cultural Resource Manager
Wood Environment and Infrastructure Solutions, Inc.
2456 Fortune Drive, Suite 100
Lexington, Kentucky 40509

Subject: Request for Revision: *Phase I Archaeological Survey of Approximately 2.5 Acres for Proposed Maintenance of the Sherman Minton Bridge, Three Indiana Approach Bridges, and One Kentucky Approach Bridge (Joint Project with INDOT)*
By Tim Reynolds and Forest Kelly, Wood Environment & Infrastructure Solutions, Inc.,
May 11, 2020
Jefferson County
KYTC Item No. 5-10027.00

Dear Dr. McKelway:

The Kentucky Transportation Cabinet (KYTC) and the Kentucky State Historic Preservation Office (SHPO) have conducted a concurrent agency review of the subject report. The agencies have determined that the submitted report is deficient and a revised report is required. Please revise the report in accordance with the enclosed comments and submit seven hard copies and one digital copy.

If you have any questions, please contact Ms. Susan Neumeyer at susan.neumeyer@ky.gov.

Sincerely,

A handwritten signature in black ink, appearing to read "Daniel R. Peake", followed by the word "for".

Daniel R. Peake, Director
Division of Environmental Analysis

DRP/msn
Attachment

c: Tim Foreman
D-5: Donna Hardin (w/a)
FHWA: Eric Rothermel (w/a)
INDOT: Ron Heustis (w/a)
Michael Baker: Wendy Vachet (w/a)
Reading Files (w/a)
Archaeology Files (w/a)

KYTC Item No. 5-10027.00
Combined Agency Comments – Request for Report Revision

Prepared by Susan Neumeyer, Archaeologist Coordinator
KYTC - DEA
June 22, 2020

Reynolds, Tim and Forest Kelly

5/11/20 *Phase I Archaeological Survey of Approximately 2.5 Acres for Proposed Maintenance of the Sherman Minton Bridge, Three Indiana Approach Bridges, and One Kentucky Approach Bridge (Joint Project with INDOT).* Wood Environment & Infrastructures Solutions, Inc.

1. The authors recommend No Adverse Effect for a site that they are recommending as not eligible for inclusion in the National Register of Historic Places (NRHP). When a site is not eligible for the NRHP, a No Adverse Effect recommendation is not appropriate. It leads the future reader to wonder if a historic property is present. A recommendation of “No Historic Properties Affected” is appropriate when no sites are present or when sites that are being recommended as not eligible are present, or when (potentially) eligible sites are present but will not be affected by the project, as outlined in the 36 CFR 800 regulations excerpts below:
 - a. When no eligible (i.e., significant) sites are present, the correct determination of effect is “No Historic Properties are Affected”.
 - b. 36 CFR §800.16 defines “Historic Properties” as: “any prehistoric or historic district, site, building, structure, or object **included in, or eligible for inclusion in,** the National Register of Historic Places maintained by the Secretary of the Interior.” [emphasis added]
 - c. 36 CFR §800.4(d)(1) directs a “**No historic properties affected**” finding is appropriate when ...the agency official finds that either there are no historic properties present or there are historic properties present but the undertaking will have no effect upon them as defined in §800.16(i).
 - i. Again, there may be no properties present at all, or
 - ii. There may be old properties or sites, but they are not eligible for inclusion in the NRHP and they MAY or MAY NOT be AFFECTED by the project, or
 - iii. There may be historic properties (eligible) present, but they will not be affected by the project.
2. Be sure that the recommendations for the sites refer throughout the report (Abstract, Introduction, Conclusion, elsewhere) to only the portion of the sites within the Area of Potential Effect and not to any portion of each site that has not been investigated.
3. Check for typos: examples – “hectare” omitted from Page 1, section 1.1; “Olmstead” is misspelled throughout report; “whorf” is misspelled on page 21, “Kelber” is misspelled, etc.
4. Page 4 15Jf952 – inconsistency in site description here and with that of 15Jf951.
5. Page 22 – delete the statement that an Addendum report will be submitted once the OSA files re-open. Neither agency wants a second round of report review. Replace this statement with one that simply says the OSA files were closed during coronavirus and describe the files that were available. SHPO guidance during this time has noted their assistance upon request to access files and provide information, if necessary.



ANDY BESHEAR
GOVERNOR

TOURISM, ARTS AND HERITAGE CABINET
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THE STATE HISTORIC PRESERVATION OFFICE

MICHAEL E. BERRY
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CRAIG A. POTTS
EXECUTIVE DIRECTOR &
STATE HISTORIC
PRESERVATION OFFICER

July 27, 2020

Mr. Daniel R. Peake, Director
Division of Environmental Analysis
Kentucky Transportation Cabinet
200 Mero Street
Frankfort, Kentucky 40601

Re: *Phase I Archaeological Survey of Approximately 2.5 Acres for Proposed Maintenance of the Sherman Minton Bridge, Three Indiana Approach Bridges, and One Kentucky Approach Bridge (Joint Project with INDOT) By Tim Reynolds and Forest Kelly, Wood Environment Infrastructure & Solutions, July 13, 2020 Jefferson County KYTC Item No. 5-10027.00 (Revised)*

Dear Mr. Peake,

Thank you for your recent digital submission of the above referenced revised report. We understand that this survey was conducted for an Area of Potential Effect (APE) measuring approximately 2.5 acres proposed for maintenance of the Sherman Minton bridge. Archaeological survey methods included pedestrian survey, shovel test excavation, and bucket augering. Two archaeological sites were identified during this survey, Site 15Jf951 and 15Jf952. After concurrent review with your staff no additional work is recommended. We agree that no additional revisions to the report are required.

Site 15Jf951 is a mid-to-late nineteenth century artifact scatter that KYTC finds ineligible for listing on the National Register of Historic Places (NRHP). Site 15Jf952 is a prehistoric lithic scatter that extends beyond this APE. KYTC finds the portion of Site 15Jf952 that falls within the APE to be ineligible for listing on the NRHP, while the remainder of the site is considered unassessed. We concur with these findings.

KYTC presents a finding of *No Historic Properties Affected* for this undertaking. We concur with that determination and accept this report without revision. We look forward to receipt of three bound paper copies of the report at your earliest convenience.

Sincerely,

Craig A. Potts,
Executive Director and
State Historic Preservation Officer

KHC # 58799
CP/nk
cc: S. Neumeier (KYTC); George Crothers (OSA)

From: [Candace Hudziak](#)
To: savannah.darr@louisvilleky.gov
Subject: Olmsted Park System of Louisville question
Date: Tuesday, January 26, 2021 12:35:31 PM

Hi Savannah,

I also left you a voicemail moments ago about this question, so feel free to call or reply to this email, whichever is better for you.

I'm trying to locate the exact NRHP boundary for Shawnee Park within the Olmsted Park System HD that was listed in 1982. In some instances I find that the golf course that runs under the Sherman Minton Bridge is included in the boundary, and in other cases it's not. The NR application I found online only includes UTM points but no map. I don't know where to go to definitively determine if the golf course is part of the HD or not. It's possible that the original HD's boundary was widened later to include the golf course, but I didn't find anything about that on the NR database site.

It's also possible that this golf course is locally designated, or was identified as historic in a statewide survey, but I don't know where I could access that information. I'm basically just trying to find out if this golf course has been identified as a historic resource in some official capacity, and if so, I need that information to be based on a source citation that is suitable for academic research (so not information found in wikipedia, in other words). Can you help me?

Thank you!

Candy Hudziak

Senior Project Manager/Architectural Historian
Metric Environmental, LLC Certified DBE/MBE/SBE Company
Phone: 317.400.1633
Mobile: 317.443.4123
Email: candaceh@metricenv.com

From: [Stevens, Karen \(Heritage Council\)](#)
To: [Candace Hudziak](#); [Darr, Savannah](#)
Cc: [Thompson, Lisa M \(Heritage Council\)](#); [Rinehart, Candice A \(Heritage Council\)](#)
Subject: RE: Olmsted Park System of Louisville question
Date: Thursday, January 28, 2021 12:26:30 PM

Hi Candy,

This is a tough one. It looks like in our GIS I have the Golf Course included, BUT based on the map I was able to find (two maps with two different borders actually) it is not. (Here is the nomination that includes the two maps and acreages for Shawnee Park-- <https://catalog.archives.gov/id/123850704>) I usually make sure when I map Large NR properties in our GIS that I use a map as the basis. Since all of our NR files are currently off being digitized, I unfortunately can't go pull the file we have to see what map I might have used. It may be that the source I had was incorrect, or that I just mapped the Large NR based on the boundary for the park on the topo.

What other sources have the golf course?

We do have a survey form that might mention the golf course, but most of the resources in that survey were in the southern portion of the park. Candi R., can you see if we have JFWS-306 and scan that in?

Karen Stevens

From: Candace Hudziak <candaceh@metricenv.com>
Sent: Wednesday, January 27, 2021 9:53 AM
To: Darr, Savannah <Savannah.Darr@louisvilleky.gov>
Cc: Stevens, Karen (Heritage Council) <karen.stevens@ky.gov>; Thompson, Lisa M (Heritage Council) <lisam.thompson@ky.gov>; Rinehart, Candice A (Heritage Council) <candicea.rinehart@ky.gov>
Subject: Re: Olmsted Park System of Louisville question

Thank you for your help, Savannah!

Candy Hudziak
Senior Project Manager/Architectural Historian
Metric Environmental, LLC Certified DBE/MBE/SBE Company
Phone: 317.400.1633
Mobile: 317.443.4123
Email: candaceh@metricenv.com

From: Darr, Savannah <Savannah.Darr@louisvilleky.gov>
Sent: Wednesday, January 27, 2021 9:37 AM
To: Candace Hudziak <candaceh@metricenv.com>

From: [Thompson, Lisa M \(Heritage Council\)](#)
To: [Darr, Savannah](#); [Candace Hudziak](#)
Cc: [Stevens, Karen \(Heritage Council\)](#); [Rinehart, Candice A \(Heritage Council\)](#)
Subject: RE: Olmsted Park System of Louisville question
Date: Thursday, January 28, 2021 12: 11 PM
ttac me 1 : [image001.png](#)

Thanks Savannah,

Dear Candy,

Below is a link the nomination found on the Archives website. It does include a map with boundary. The verbal boundary description does reference Block 8-K, Lot 1-12. Not sure if you can look that up. Interestingly, I also tried to match the size of the park as depicted in the NR and it comes in at 277 acres not the 181.5 acres noted on the NR.

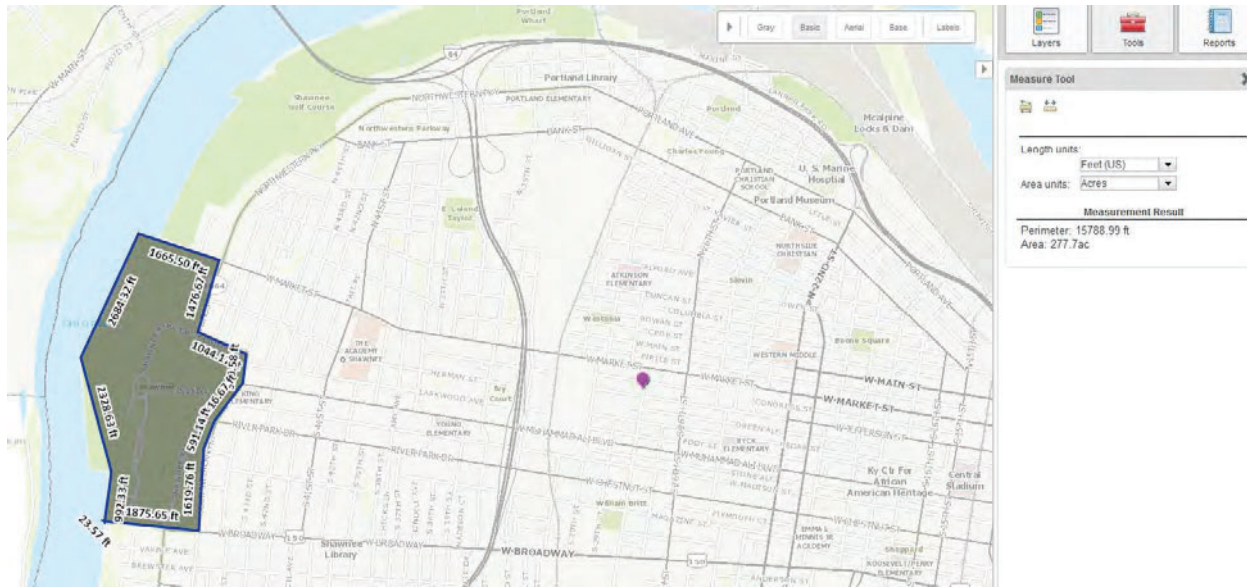
I did note the Parkway is adjacent to the extended golf course and the Parkways, also listed, include a 120 foot right of way.

<https://catalog.archives.gov/id/123850704>

I would appreciate knowing where you find a map that includes more of golf course.

Thanks,

Lisa



LISA MULLINS THOMPSON
National Register Coordinator
P 502.892.3609

KENTUCKY HERITAGE COUNCIL
410 High Street Frankfort, Kentucky 40601

To our constituents, please be advised the KHC Historic Resource Library is now open for consultants wishing to conduct background research and site checks. Consultants can make appointments to visit our office in two time slots a day on Mondays, Wednesdays, and Fridays: 9 a.m. to 12 p.m. and 1:30 p.m. to 4:30 p.m. We ask that you [please refer to this memo for information](#) and follow all protocols outlined there and posted at our facility. Consultants who require this service may also continue to utilize the electronic records review portal at <https://secure.kentucky.gov/formservices/Heritage/SitelD>. The rest of the office remains open on a limited basis. Staff continue to telecommute or alternate days in the office and are not available for face-to-face meetings or site visits. We continue to recommend that when possible, *environmental review reports, tax credit applications and supporting materials, National Register correspondence, or other documents that require hard-copy submissions be mailed or sent by delivery service* to the Kentucky Heritage Council, 410 High Street, Frankfort, KY 40601, so that staff may follow up with you by phone.

From: Darr, Savannah <Savannah.Darr@louisvilleky.gov>
Sent: Wednesday, January 27, 2021 9:38 AM
To: Candace Hudziak <candaceh@metricenv.com>
Cc: Stevens, Karen (Heritage Council) <karen.stevens@ky.gov>; Thompson, Lisa M (Heritage Council) <lisam.thompson@ky.gov>; Rinehart, Candice A (Heritage Council) <candicea.rinehart@ky.gov>
Subject: RE: Olmsted Park System of Louisville question

Hi Candy,

I can tell you is that the golf course is not locally designated. However, I cannot answer your questions about the HD or any survey efforts. I have copied Lisa Thompson, the National Register Coordinator, Candi Rinehart, the Survey Coordinator, and Karen Stevens, the Database Coordinator, with the Kentucky Heritage Council. These ladies can best help answer your questions.

Thanks,

Savannah Darr
Planning & Design Coordinator
Planning & Design Services
444 South Fifth Street, Suite 300
Louisville, KY 40202
(502) 574-5705

From: Candace Hudziak <candaceh@metricenv.com>
Sent: Tuesday, January 26, 2021 12:36 PM
To: Darr, Savannah <Savannah.Darr@louisvilleky.gov>
Subject: Olmsted Park System of Louisville question

CAUTION: This email came from outside of Louisville Metro. Do not click links or open attachments unless you recognize the sender and know the content is safe

Hi Savannah,

I also left you a voicemail moments ago about this question, so feel free to call or reply to this email, whichever is better for you.

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From: [Thompson, Lisa M \(Heritage Council\)](mailto:Thompson_Lisa_M@Heritage_Council)
To: [Candace Hudziak Darr Savannah](mailto:Candace_Hudziak_Darr_Savannah)
Cc: [Stevens, Karen \(Heritage Council\)](mailto:Stevens_Karen@Heritage_Council); [Rinehart, Candice A \(Heritage Council\)](mailto:Rinehart_Candice_A@Heritage_Council)
Subject: RE: Olmsted Park System of Louisville question
Date: Thursday, January 28, 2021 2:03:14 PM
Attachments: [image001.png](#)

Dear Candy,

If you could send us the 2020 correspondence that would be greatly appreciated.

Thanks,

Lisa

LISA MULLINS THOMPSON
National Register Coordinator
P 502.892.3609

KENTUCKY HERITAGE COUNCIL
410 High Street Frankfort, Kentucky 40601

To our constituents, please be advised the KHC Historic Resource Library is now open for consultants wishing to conduct background research and site checks. Consultants can make appointments to visit our office in two time slots a day on Mondays, Wednesdays, and Fridays: 9 a.m. to 12 p.m. and 1:30 p.m. to 4:30 p.m. We ask that you [please refer to this memo for information](#) and follow all protocols outlined there and posted at our facility. Consultants who require this service may also continue to utilize the electronic records review portal at <https://secure.kentucky.gov/formservices/Heritage/SiteID>. The rest of the office remains open on a limited basis. Staff continue to telecommute or alternate days in the office and are not available for face-to-face meetings or site visits. We continue to recommend that when possible, [environmental review reports](#), [tax credit applications](#) and [supporting materials](#), [National Register correspondence](#), or other documents that require hard-copy submissions **be mailed or sent by delivery service** to the Kentucky Heritage Council, 410 High Street, Frankfort, KY 40601, so that staff may follow up with you by phone.

From: Candace Hudziak <candaceh@metricenv.com>
Sent: Thursday, January 28, 2021 1:14 PM
To: Thompson, Lisa M (Heritage Council) <lisam.thompson@ky.gov>; Darr, Savannah <Savannah.Darr@louisvilleky.gov>
Cc: Stevens, Karen (Heritage Council) <karen.stevens@ky.gov>; Rinehart, Candice A (Heritage Council) <candicea.rinehart@ky.gov>
Subject: Re: Olmsted Park System of Louisville question

This is very helpful, thank you! Lisa, would the map you provided be considered the "definitive" boundary map? Also, is it possible the HD was added on to at a later date to include the golf course?

So, a bit of background here as to why I'm asking you all about this: I'm working on a Section 106 project for the Sherman Minton Bridge, and in early 2020 a previous consultant for the project stated this golf course was part of Shawnee Park's HD boundary, which as you know is part of the larger Olmsted Park System HD. The Kentucky Heritage Council did not dispute the consultant's inclusion of the golf course as part of the HD, and their office ended up concurring with a No Adverse Effect finding (KYTC Item No. 5-10027).

Beyond this consultant and concurrence by the KHC, I cannot verify anywhere that this golf course is actually part of the HD. However, the KHC has set a precedent here and I don't know if or how it can be challenged.

I can send you copies of the correspondence I have if you'd like.

Thanks again to all of you for your help, it is much appreciated.

Candy Hudziak
Senior Project Manager/Architectural Historian
Metric Environmental, LLC Certified DBE/MBE/SBE Company
Phone: 317.400.1633
Mobile: 317.443.4123
Email: candaceh@metricenv.com

From: Thompson, Lisa M (Heritage Council) <lisam.thompson@ky.gov>
Sent: Thursday, January 28, 2021 12:45 PM
To: Darr, Savannah <Savannah.Darr@louisvilleky.gov>; Candace Hudziak <candaceh@metricenv.com>
Cc: Stevens, Karen (Heritage Council) <karen.stevens@ky.gov>; Rinehart, Candice A (Heritage Council) <candicea.rinehart@ky.gov>
Subject: RE: Olmsted Park System of Louisville question

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I did note the Parkway is adjacent to the extended golf course and the Parkways, also listed, include a 120 foot right of way.

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I would appreciate knowing where you find a map that includes more of golf course.

Thanks,

Lisa

From: [Thompson, Lisa M \(Heritage Council\)](#)
To: [Candace Hudziak](#); [Darr, Savannah](#); [Laracuente, Isolas \(Heritage Council\)](#)
Cc: [Stevens, Karen \(Heritage Council\)](#); [Rinehart, Candice A \(Heritage Council\)](#)
Subject: RE: Olmsted Park System of Louisville question
Date: Friday, January 2, 2021 11:2:0 AM
Attachments: [lmsa001.png](#)
[Screen Shot 2021-01-2 at 11:01 AM.png](#)

Dear Candy,

The boundary of Shawnee Park is the listed boundary as presented on Map #1 (page 37/66). The link again of the NR is below. I have also attached a screen shot of the area in question.

If you have any further questions regarding the project or the correspondence, you can refer them to Nick, copied above.

<https://catalog.archives.gov/id/123850704>

Be well.

Lisa

LISA MULLINS THOMPSON
National Register Coordinator

Kentucky Heritage Council
Tel 502.892.3609

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From: Candace Hudziak <candaceh@metricenv.com>

Sent: Thursday, January 28, 2021 2:44 PM

To: Thompson, Lisa M (Heritage Council) <lisam.thompson@ky.gov>; Darr, Savannah <Savannah.Darr@louisvilleky.gov>

Cc: Stevens, Karen (Heritage Council) <karen.stevens@ky.gov>; Rinehart, Candice A (Heritage Council) <candicea.rinehart@ky.gov>

Subject: RE: Olmsted Park System of Louisville question

****CAUTION** PDF attachments may contain links to malicious sites. Please contact the COT Service Desk ServiceCorrespondence@ky.gov for any assistance.**

I've attached a map produced by the previous consultant, showing the golf course as an HD. I've also attached the KY SHPO's concurrence to the project. Unfortunately that's all I have for correspondence related to above ground investigations.

Candy Hudziak

Senior Project Manager/Architectural Historian

Metric Environmental, LLC

Certified DBE/MBE/SBE Company

Phone: 317.400.1633

Mobile: 317.443.4123

Email: candaceh@metricenv.com

From: Thompson, Lisa M (Heritage Council) <lisam.thompson@ky.gov>

Sent: Thursday, January 28, 2021 2:02 PM

To: Candace Hudziak <candaceh@metricenv.com>; Darr, Savannah <Savannah.Darr@louisvilleky.gov>

Cc: Stevens, Karen (Heritage Council) <karen.stevens@ky.gov>; Rinehart, Candice A (Heritage Council) <candicea.rinehart@ky.gov>

Subject: RE: Olmsted Park System of Louisville question

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Sent: Thursday, January 28, 2021 1:14 PM

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Cc: Stevens, Karen (Heritage Council) <karen.stevens@ky.gov>; Rinehart, Candice A (Heritage Council) <candicea.rinehart@ky.gov>

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Beyond this consultant and concurrence by the KHC, I cannot verify anywhere that this golf course is actually part of the HD. However, the KHC has set a precedent here and I don't know if or how it can be challenged.

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Thanks again to all of you for your help, it is much appreciated.

Candy Hudziak

Senior Project Manager/Architectural Historian

Metric Environmental, LLC Certified DBE/MBE/SBE Company

Phone: 317.400.1633

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Email: candaceh@metricenv.com

From: Thompson, Lisa M (Heritage Council) <lisam.thompson@ky.gov>

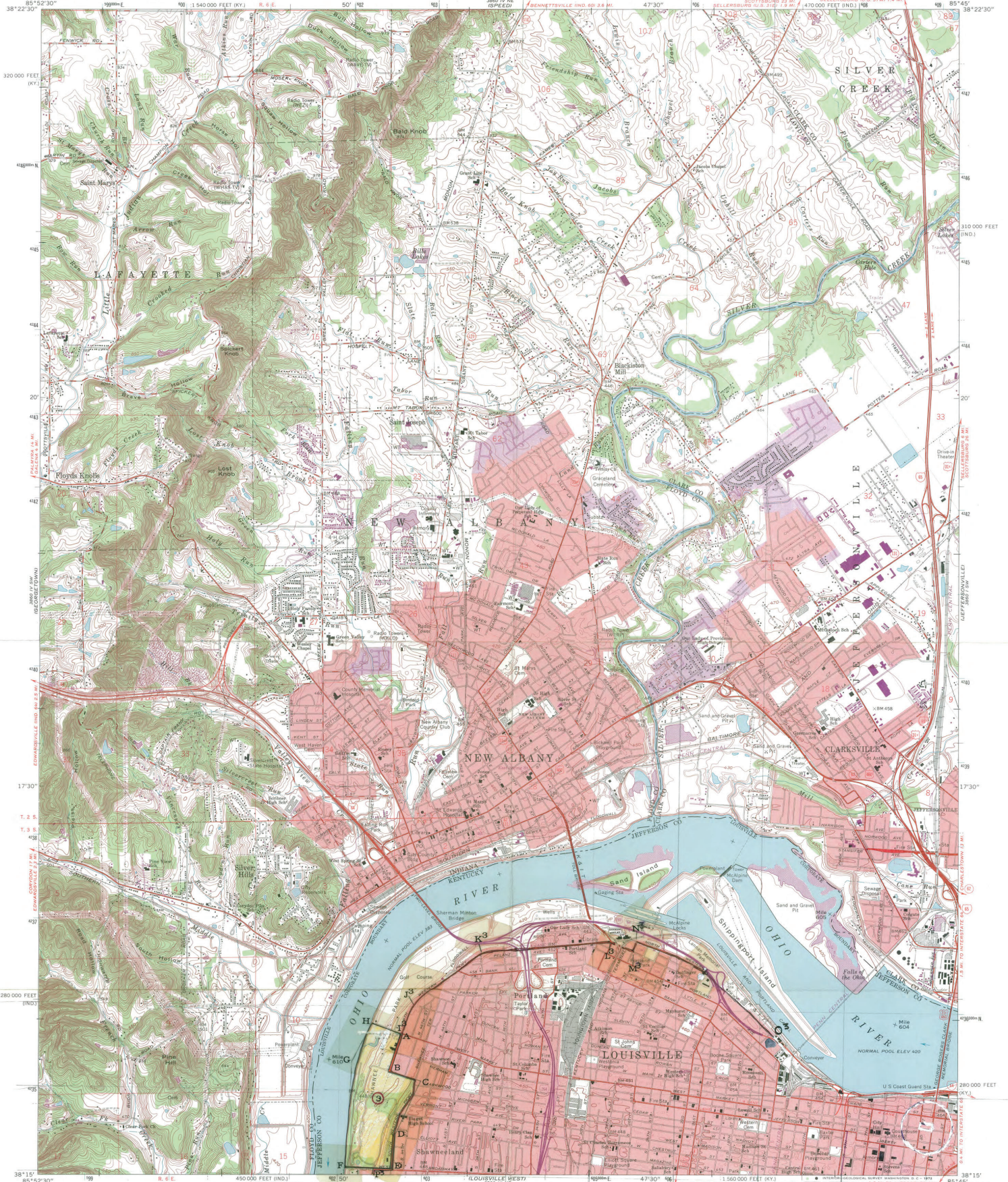
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Cc: Stevens, Karen (Heritage Council) <karen.stevens@ky.gov>; Rinehart, Candice A (Heritage Council) <candicea.rinehart@ky.gov>

Subject: RE: Olmsted Park System of Louisville question

Thanks Savannah,



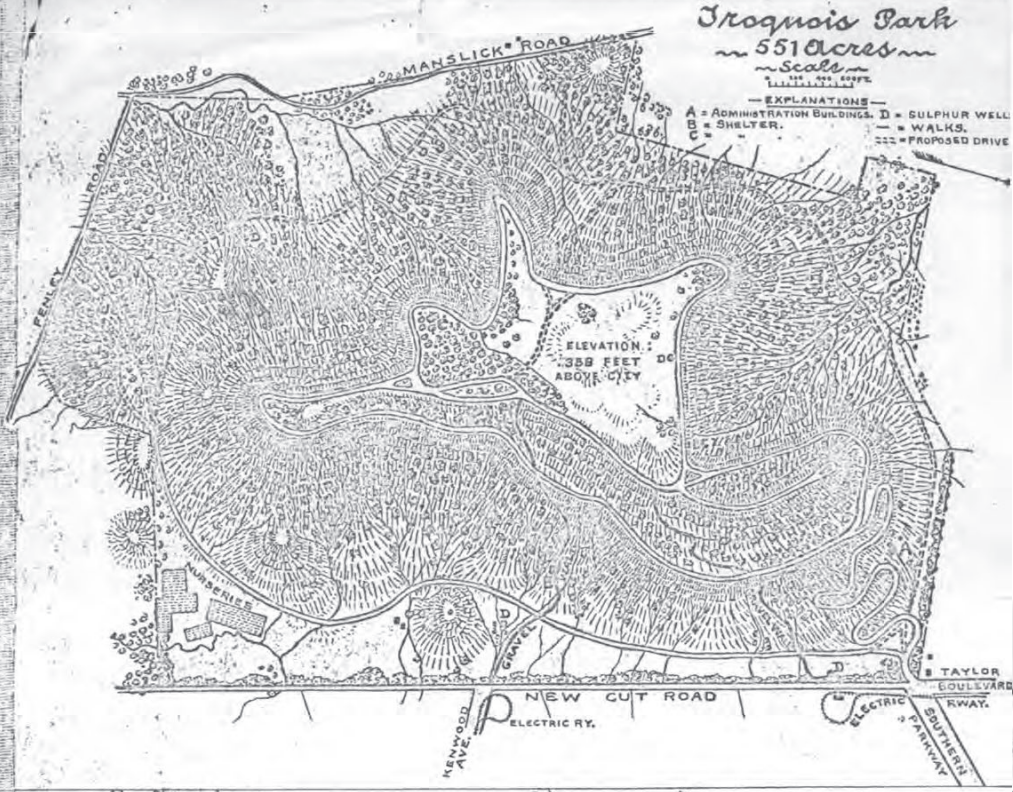
Mapped, edited, and published by the Geological Survey in cooperation with Indiana Department of Natural Resources and Kentucky Geological Survey. Revised in cooperation with Kentucky Geological Survey.
Control by USGS, USCGS, USACE, and City of Louisville.
Topography in Indiana by plane-table surveys 1937-1938. Planimetry in Kentucky by photogrammetric methods from aerial photographs taken 1949. Topography by plane-table surveys 1950. Entire map revised 1965.
Polyconic projection. 1927 North American datum. 10,000-foot grid based on Indiana coordinate system, east zone, and Kentucky coordinate system, north zone. 1000-meter Universal Transverse Mercator grid ticks, zone 16, shown in blue.
The state boundary as shown represents the approximate position of the low water line as determined from U. S. Corps of Engineers Ohio River charts, surveyed 1911-1912, and supplementary information.
Red tint indicates areas in which only landmark buildings are shown.
Dotted land lines established by private survey.

Olmsted Park System of Louisville
Jefferson County, Kentucky
New Albany Quadrangle
Indiana-Kentucky
Scale 1:24000
1965
Photorevised 1971
MAP #1

SCALE 1:24000
CONTOUR INTERVAL 10 FEET
DOTTED LINES REPRESENT FOOT CONTOURS
DATUM IS MEAN SEA LEVEL
THIS MAP COMPLIES WITH NATIONAL MAP ACCURACY STANDARDS
PREPARED BY U. S. GEOLOGICAL SURVEY, WASHINGTON, D. C. 20242
DIVISION OF NATURAL RESOURCES, INDIANAPOLIS, INDIANA 46204
JACKSONVILLE GEOLOGICAL SURVEY, LEXINGTON, KENTUCKY 40506
U. S. DEPARTMENT OF COMMERCE, FRANKFORD, KENTUCKY 40601
A FOLDER DESCRIBING TOPOGRAPHIC MAPS AND SYMBOLS IS AVAILABLE ON REQUEST

ROAD CLASSIFICATION
Heavy-duty ——— Light-duty ———
Medium-duty ——— Unimproved dirt ———
Interstate Route — U. S. Route — State Route —
NEW ALBANY, IND.-KY.
SECTION NEW ALBANY 19 QUADRANGLE
1:24,000 — 406547-4
1965
PHOTOREVISED 1971
AMS 3840 IV SE-SERIES V881

RECEIVED
JAN 15 1967
NATIONAL REGISTER



Approaches and Drives
 TO THE
Parks of Louisville, Ky.
 JULY 1, 1894.

IROQUOIS PARK:
 To the south of the City, $\frac{3}{4}$ miles from Court-House; 607 acres, including the Southern Parkway; reached by two electric car lines—on Fourth Street, via Seventh, and Third Street routes—and by the Southern Parkway. The Southern Parkway is $\frac{1}{2}$ miles long, 150 feet wide, and extends from P Street to Iroquois Park.

CHEROKEE PARK:
 To the east of the City, $\frac{1}{2}$ miles from Court-House; 306 acres; reached by driving through East or New Broadway, and by Green Street and Broadway Electric Cars.

SHAWNEE PARK:
 To the west of the City, on the Ohio River, $\frac{3}{4}$ miles from the Court House; 167 acres; reached by driving via West Broadway, Chestnut, Madison, or Market Streets, and by Market Street Cars through Fountain Ferry Park.



May 18, 2021

[Transmitted via SharePoint]

Kokosing Construction Company, Inc.
Vince Martini,
Senior Project Manager
6235 Westerville Road
Westerville OH, 43081
vpm@kokosing.biz

**Re: Sherman Minton Corridor Project – Public-Private Agreement
CORR-0010 – Kentucky Revised Area of Potential Effect Limits**

Dear Mr. Martini:

Reference is made to the Public-Private Agreement dated as of March 2, 2021 (as amended, the “PPA”), by and between the Indiana Finance Authority (“IFA”) and Kokosing Construction Company, Inc. (“Design-Build Contractor”). Capitalized terms not defined in this letter have the meanings given in the PPA.

The archaeological review for the proposed levee access road has been completed and approved by KYTC and the Kentucky State Historic Preservation Office. This approval is issued based on the plans provided May 11, 2021. If the footprint of the project changes or new areas are proposed that have not been previously reviewed, a new Section 106 review for archaeology shall be required.

The conceptual design has been cleared from an archaeological perspective. IFA notes that the Design-Build Contractor shall be required to incorporate this design and these materials into the revised environmental documentation, needed to complete the Additional Information to the approved Categorical Exclusion, prior to construction.

Nothing contained herein shall modify, release or waive any provisions or requirements set forth in the PPA, all of which shall apply and remain in full force and effect.

Sincerely,



Daniel Corbin
Authorized Representative
Indiana Finance Authority

Attachment: KYTC Archaeological Investigation Form

Cc: Mr. Kevin Ohl, KCC Vice President
Mr. Brant Mercer, KCC

Mr. Dan Droesch, KCC
Mr. Dan Schweiger, KCC
Mr. Gary Obert, KCC
Mr. Oliver Bluestone, KCC
Mr. Dan Morris, Jacobs
Mr. Jeff Kokal, Jacobs
Mr. Ron Heustis, INDOT DPM
Mr. Paul Boone, INDOT DPM
Mr. Royce Meredith, KYTC PM
Ms. Mary Jo Hamman, MBI PM
Mr. Dav Kessinger, MBI DPM

KYTC Item No: 5-10027

County: Jefferson

Route: N/A

KYTC Archaeological Investigation Form

Project Description: Sherman Minton Bridge Project: the Design Build contractor has proposed an access road from a golf course path off of the intersection of Bank Street and Northwestern Parkway and then along the existing levee within Shawnee Park east to the bridge access.

USGS Quad Name: New Albany

USGS Date: 1978

Coordinates (Project center point) LAT: 38.2738 LONG: -85.8170

Project Type listed in Attachment 1 (in Section 106 Programmatic Agreement)?

Yes (list project activity types) _____

No (Continue)

Project Type listed in Attachment 2 (in Section 106 Handbook)?

Yes (list project activity types) #11 bridge rehabilitation _____

Are all new or existing ROW areas previously disturbed?

Yes (Describe disturbance or basis for conclusion. Attach photos or maps):

The area proposed for the access road is on paved road, an artificial levee, and an area confirmed as disturbed by Wood in their Phase I survey.

| | |
|---|--------------|
| No Historic Properties Affected | |
| As Determined By: | |
| Susan Neumeyer | May 13, 2021 |
| KYTC Representative | Date |
| <i>[Signature]</i> | 5-17-2021 |
| SHPO Representative | Date |
| <i>(Concurrence is assumed if no response is received within 30 days)</i> | |
| Attachments | |
| <input checked="" type="checkbox"/> Project Plans (show date on plans) | |
| <input checked="" type="checkbox"/> Photos | |
| <input checked="" type="checkbox"/> Mapping | |
| <input type="checkbox"/> Other: _____ | |
| <input checked="" type="checkbox"/> Copy EPM | |
| <input checked="" type="checkbox"/> Copy DEC | |
| <input type="checkbox"/> Copy DEA Archaeologist | |
| <input type="checkbox"/> Copy SHPO | |

If the project plans change then additional archaeological survey may be required. If human remains are discovered or a previously unidentified archaeological site is encountered, work must cease and the KYTC Division of Environmental Analysis be notified immediately.

The Sherman Minton Bridge project proposes to extend the life of the bridge for another 30 years. In 2020, a Phase I archaeological survey was conducted by Wood (Reynolds and Kelly 2020) of the proposed archaeological Area of Potential Effect.

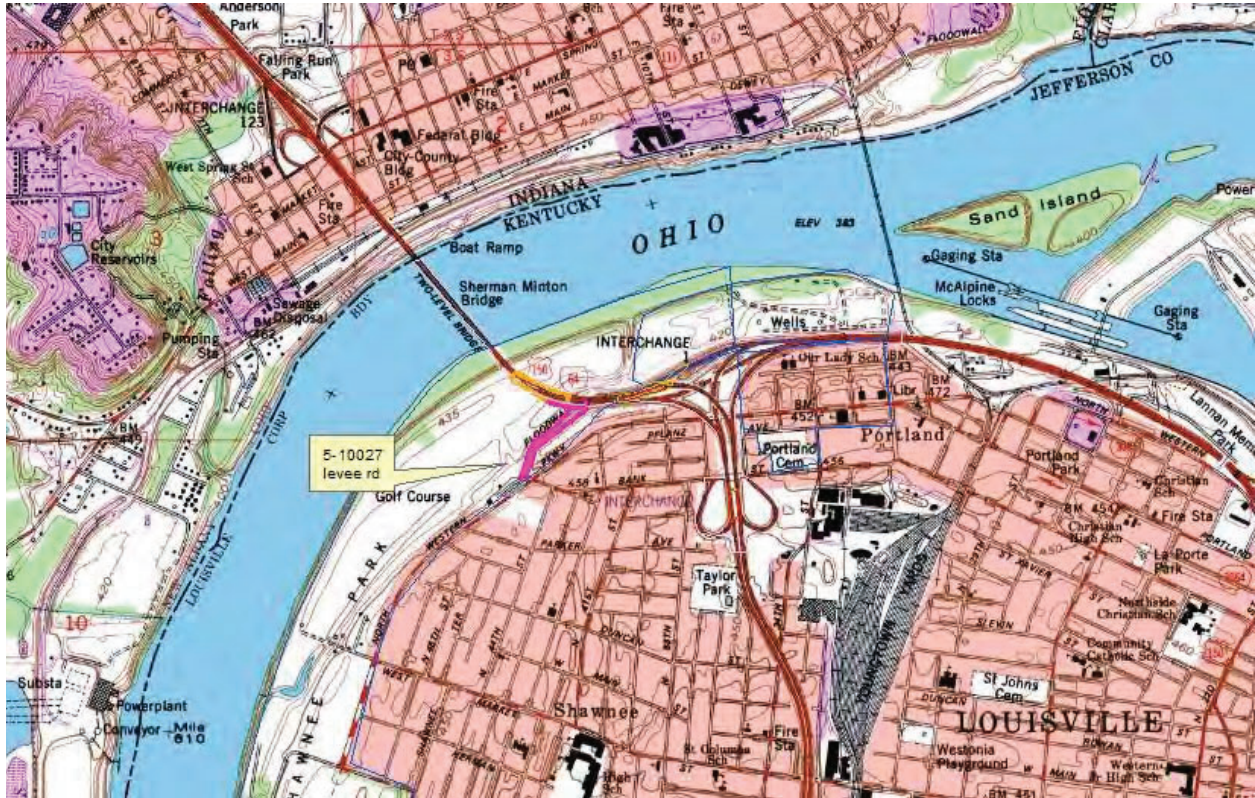
Subsequently, the project was let for construction to Kokosing, the construction contractor. Kokosing now requires a road in Shawnee Golf Course to provide access to approximately six dozen trucks over 800 days (approximately 3 years) to the Kentucky approach bridges. The proposed area of this road was not in the previously reviewed APE and is the subject of this document.

The truck traffic would enter the Shawnee Golf Course from the intersection of the Northwestern Parkway at Bank Street. There is a golf course road at this location that will allow access from Bank Street to the levee. A gravel road will be constructed along the top of the levee to the approach bridges.

The existing golf course road is paved and is a disturbed area. The levee is constructed of artificial fill and has no archaeological potential. The small area beneath the approach bridges that will be affected by the road was included in the Reynolds and Kelly 2020 survey and has no significant archaeological resources.

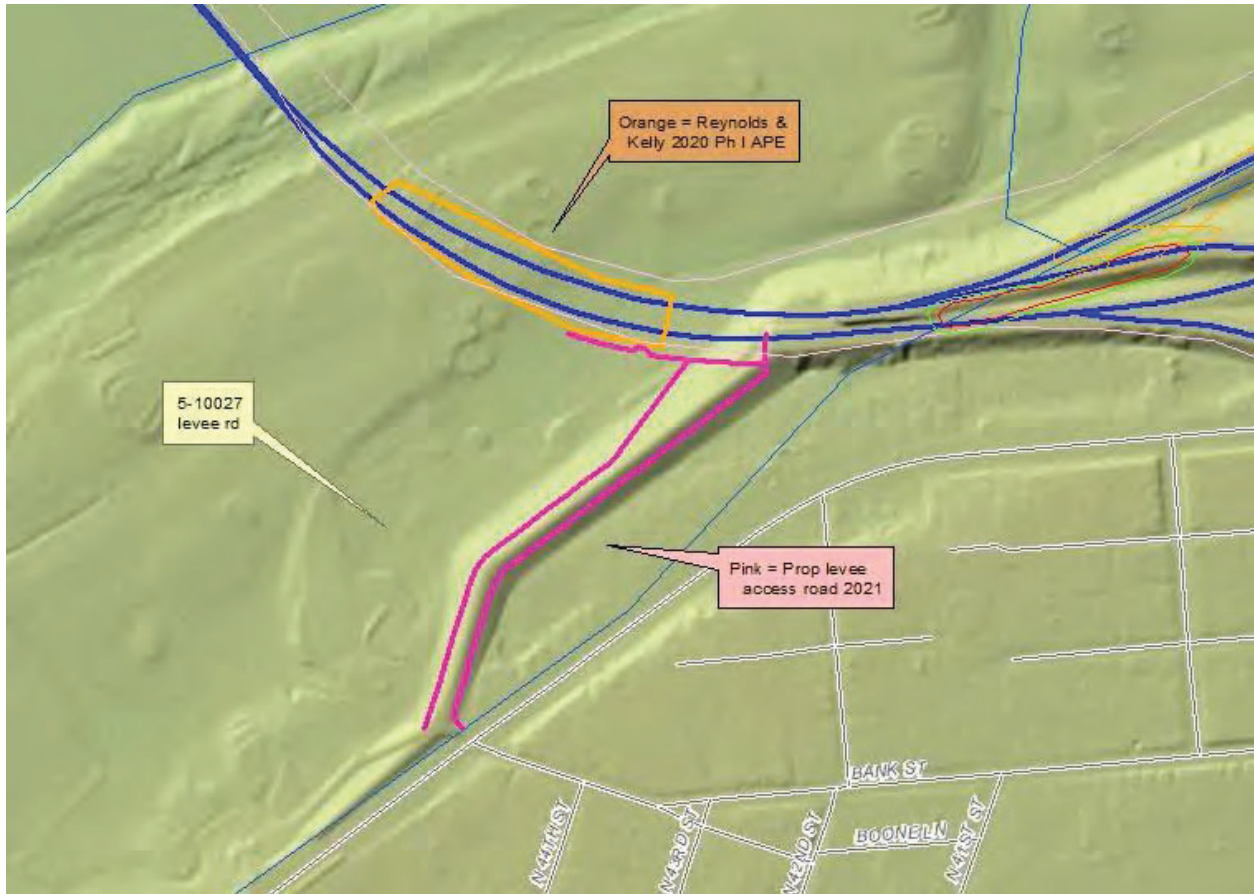
No new ROW will be obtained for this gravel road.

No archaeological investigations are warranted for the proposed gravel road atop the artificial levee, as defined in the plans provided on May 11, 2021.





2019 aerial



LiDAR imagery showing proposed levee road corridor

The following images are taken from Google Street View:



On Northwestern Parkway looking west at the golf course road. The levee is to the right of the road, center of photo (behind the utility pole).



On the golf course road looking north toward along the top of the levee, the proposed corridor for the access road.



ANDY BESHEAR
GOVERNOR

TOURISM, ARTS AND HERITAGE CABINET
KENTUCKY HERITAGE COUNCIL
THE STATE HISTORIC PRESERVATION OFFICE

MICHAEL E. BERRY
SECRETARY

JACQUELINE COLEMAN
LT. GOVERNOR

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FRANKFORT, KENTUCKY 40601
(502) 564-7005
www.heritage.ky.gov

CRAIG A. POTTS
EXECUTIVE DIRECTOR &
STATE HISTORIC
PRESERVATION OFFICER

06/03/2021

Danny Peake, Director
Division of Environmental Analysis
Kentucky Transportation Cabinet
200 Mero Street,
Frankfort, KY 40622

RE: Sherman Minton/I-64 Bridge Project Revised APE
Louisville, Jefferson County, Kentucky
KYTC Item No. 5-10027

Dear Mr. Peake:

Thank you for your submittal of a revised cultural historic APE for the above-referenced undertaking. Our office initially reviewed this undertaking in 2019 and 2020, providing a conditional No Adverse Effect concurrence to meet project demands. While our office is concurring with the revised APE proposed for this undertaking, we strongly suggest avoidance/minimization to avoid the NRHP-eligible or listed resources within the proposed APE. Furthermore, by pursuing this avoidance, the proposed revised APE may need to change in the future, and thus there may be the need to re-consult on the APE in the future, while failure to avoid/minimize effects may result in adverse effects to one or more historic resources.

We look forward to additional consultation on this project. Should you have any questions, please contact Gabrielle Fernandez of my staff at Gabrielle.Fernandez@ky.gov.

Sincerely,

Craig A. Potts,
Executive Director and
State Historic Preservation Officer

CP: gf, KHC # 61807
cc. Amanda Abner, DEA

June 22, 2021

Kokosing Construction Company, Inc.
Kevin Ohl, PE, DBIA
Vice President, Engineering and Alternative Delivery
6235 Westerville Road
Westerville OH, 43081
kao@kokosing.biz

**Re: Sherman Minton Corridor Project – Public Private Agreement
IFA to Kokosing Construction Company
CORR-0016 – Revised APE Correspondence from KY SHPO**

Dear Mr. Ohl:

Reference is made to the Public-Private Agreement dated as of March 2, 2021 (as amended, the “PPA”), by and between the Indiana Finance Authority (“IFA”) and Kokosing Construction Company, Inc. (“Design-Build Contractor”). Capitalized terms not defined in this letter have the meanings given in the PPA.

Please see attached correspondence from Kentucky State Historic Preservation Officer regarding the revised cultural historic Area of Potential Effect (APE). Additional coordination is scheduled to take place at an on-site meeting on June 29, 2021 at 10:00 a.m. ET.

Nothing contained herein shall modify, release or waive any provisions or requirements set forth in the PPA, all of which shall apply and remain in full force and effect.

Sincerely,

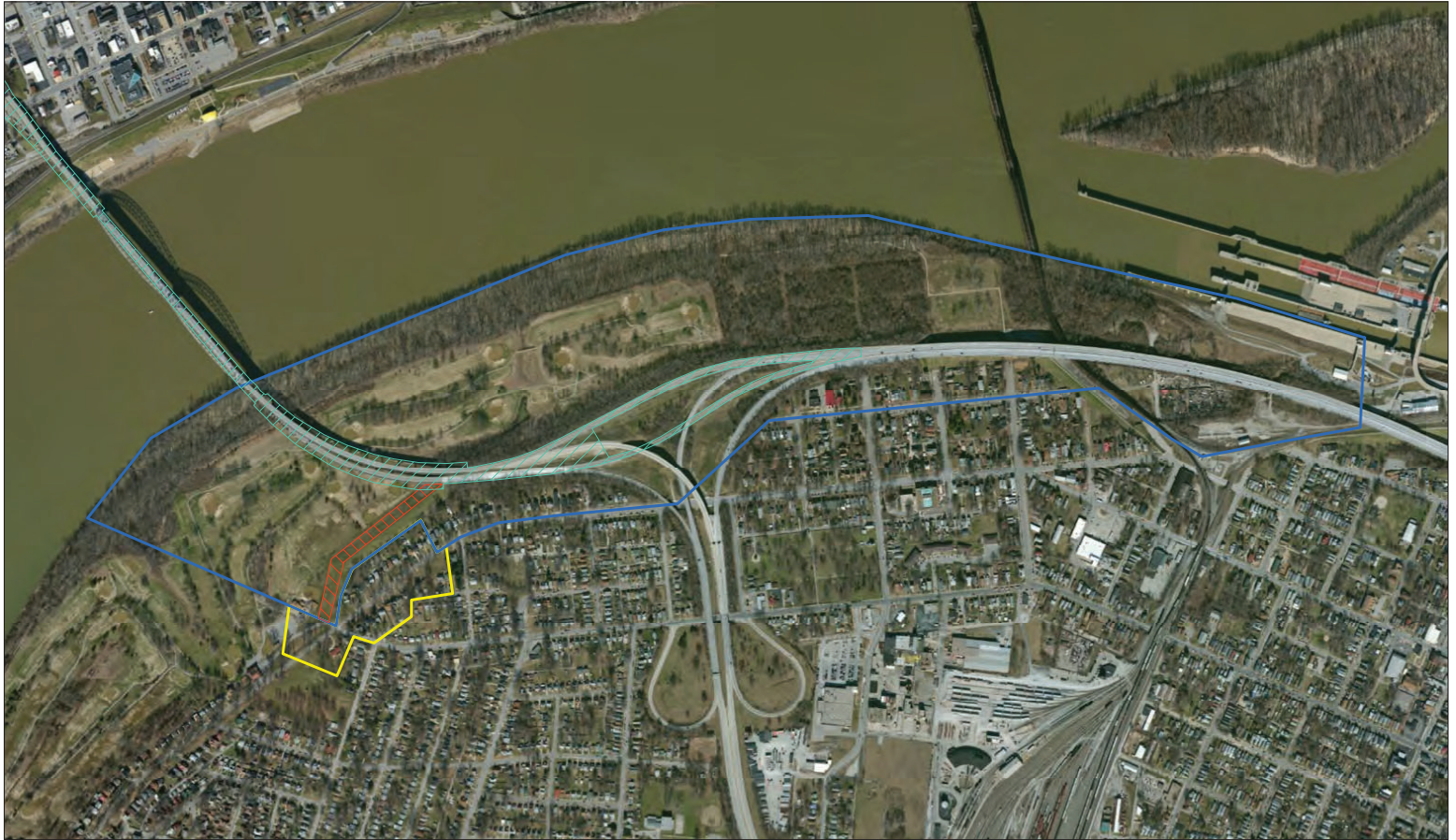


Daniel Corbin
Authorized Representative
Indiana Finance Authority

Cc: Mr. Vince Martini, KCC Project Manager
Mr. Brant Mercer, KCC
Mr. Dan Droesch, KCC
Mr. Dan Schweiger, KCC
Mr. Gary Obert, KCC
Mr. Oliver Bluestone, KCC
Mr. Dan Morris, Jacobs
Mr. Jeff Kokal, Jacobs
Mr. Ron Heustis, INDOT DPM
Mr. Paul Boone, INDOT DPM
Mr. Royce Meredith, KYTC PM

Ms. Mary Jo Hamman, MBI PM
Mr. Dav Kessinger, MBI DPM

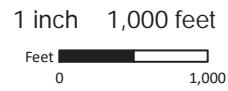
Attachment: Sherman Minton/I-64 Bridge Project Revised APE



- Project Footprint
- original AP
- Proposed Levee Road
- Revised AP for Historic Properties

APE Details on an aerial photograph
 Sherman Minton Bridge
 Louisville, Jefferson County, Kentucky
 Metric Project No. 20-0201
 Map Date: 05/21/2021

All locations approximate
 Source: Imagery Date 2012 Aerial



APPENDIX D: EXPANDED (AND FINAL) AREA OF POTENTIAL EFFECTS MAP







- AP
- Project Footprint
- Portion of the Imsted Park System Historic District
- Portland Historic District
- Proposed Shawnee of Course NRHP Boundary
- Proposed Levee Road
- Proposed Shawnee Residential HD Boundary
- Proposed Arthen Levee NRHP Boundary

APE Details on an aerial photograph
 Sherman Minton Bridge
 Louisville, Jefferson County, Kentucky
 Metric Project No. 20-0201
 Map Date: 07/08/2021

All locations approximate
 Source: Imagery Date 2012 Aerial



APPENDIX E: PROPERTIES TABLE OF THE PROPOSED SHAWNEE RESIDENTIAL HD

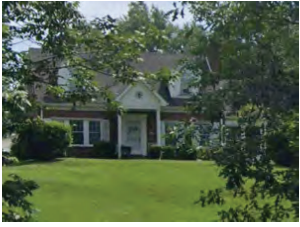



| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|---------------------------------|------|---------------------------------|------|---|
| 1 |  | AL-001 | House | 553 Northwestern Parkway | 1960 | Ranch | C | One story, L-footprint, gabled roof with eave overhang, brick veneer, wood 6/1 and 1/1 sash windows with simple wood trim, brick sills, and decorative shutters, single bay small gabled porch simple paired support posts, single door under porch with storm door, single bay gabled two car garage |
| 2 |  | AL-002 | House | 555 Northwestern Parkway | 1957 | Ranch | C | One story, irregular-footprint, hipped roof with eave overhang and oversized stone chimney, limestone veneer, wood 2/2 and 1/1 sash windows with simple wood trim, single door with storm door |
| 3 |  | AL-003 | House | 4257 Northwestern Parkway | 1941 | Colonial Revival Cape Cod | C | One and a half story, rectangular-footprint, side gable roof with eave overhang, two gabled dormers, and brick chimney with chimney pots, brick veneer, wood 8/8 and 6/6 sash windows with simple wood trim and shutters, single door with Doric pediment and pilasters and storm door, enclosed side porch |
| 4 |  | AL-004 | House | 4255 Northwestern Parkway | 1951 | Minimal Traditional | C | One and a half story, rectangular-footprint, gabled roof with eave overhang, limestone veneer, vinyl 1/1 sash windows with simple wood trim, single door and enclosed 2/3 front porch, rear addition |

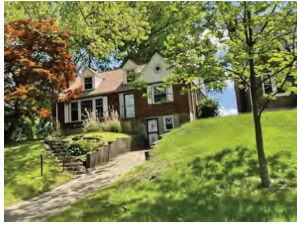


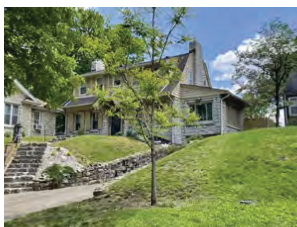
| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|---------------------------------|------|-------------------------|------|---|
| 5 |  | AL-005 | House | 4253 Northwestern Parkway | 1954 | Ranch | C | One story, rectangular-footprint, hipped roof with eave overhang, limestone veneer, 1/1 vinyl sash windows with simple wood trim, single door and enclosed 2/3 front porch |
| 6 |  | AL-006 | House | 4247 Northwestern Parkway | 1953 | Ranch | C | One story, rectangular-footprint, hipped roof with wide eave overhang, limestone veneer, wood 1/1 and 4/1 wood sash windows with simple wood trim and limestone sills, single center door, single car basement garage, concrete foundation |
| 7 |  | AL-007 | House | 4245 Northwestern Parkway | 1954 | American small house | C | One and a half story, rectangular footprint, side gabled roof with eave overhang, limestone veneer, wood 2/2 wood sash windows with simple wood trim and limestone sills, single off-center door, concrete basement and foundation, detached 2 car garage |
| 8 |  | AL-008 | House | 4243 Northwestern Parkway | 1956 | American small house | C | One story, rectangular-footprint, hipped roof with eave overhang, limestone veneer, 1/1 and fixed vinyl sash windows with simple wood trim and limestone sills, single off-center door, concrete foundation |


| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|---------------------------------|------|-------------------------|--|--|
| 9 |  | AL-009 | House | 4241 Northwestern Parkway | 1952 | American small house | C | One and a half story, rectangular-footprint, gabled roof with eave overhang, brick veneer, ½ front porch, concrete foundation |
| 10 |  | AL-010 | House | 4239 Northwestern Parkway | 1930 | Craftsman Bungalow | C | One and a half story, rectangular-footprint, gable roof with extended eave overhang, gable dormer and oversized end chimney, brick veneer, 5/1 and 3/1 wood sash and 8 paned casement windows with simple wood trim, single off-center door and storm door, ½ front inset porch with brick support and rail, concrete foundation |
| 11 |  | AL-011 | House | 4237 Northwestern Parkway | 1930 | Craftsman Bungalow | C | One and a half story, rectangular-footprint, gable roof with extended eave overhang, gabled dormer and oversized end chimney, brick veneer, 3/1 wood sash windows with simple wood trim, full front enclosed porch with brick supports and rail, concrete foundation |
| 12 |  | AL-012 | House | 4235 Northwestern Parkway | 1914 | Craftsman Bungalow | C; individually NRHP eligible | One and a half story, rectangular-footprint, gable roof with extended eave overhang, hipped dormer and oversized end chimney, sandstone veneer, 9 and 11 paned fixed and 9/1 wood sash windows with simple wood trim, center bay front hipped porch with rustic concrete block supports |

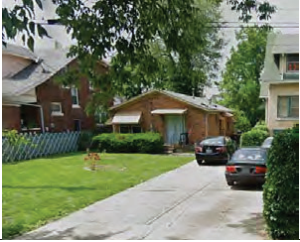



| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|---------------------------------|------|------------------------------------|------|---|
| 13 |  | AL-013 | House | 4231 Northwestern Parkway | 1913 | Colonial Revival Double Pile | C | Two story rectangular-footprint, hipped roof with wide eave overhang and chimney, clapboard siding, wood 5/1 sash windows with simple wood trim and bracketed pediment and decorative shutters, single door with transom and storm door, single bay hipped porch with simple Doric supports and 2/3 deck |
| 14 |  | AL-014 | House | 4229 Northwestern Parkway | 1926 | Craftsman | C | Two story, irregular-footprint, clipped gable roof with extended eave overhang, brick veneer, 6/6 and 4/4 vinyl sash windows with simple wood trim, 2/3 front porch with brick supports and rail, concrete foundation |
| 15 |  | AL-015 | House | 4227 Northwestern Parkway | 1926 | Dutch Colonial Revival | C | One and a half story, rectangular-footprint, side gambrel roof with extended eave overhang and shed dormer, brick veneer and vinyl siding, 6/6 and 1/1 vinyl sash windows with simple wood trim, front gabled stoop with front replacement door and storm door, concrete foundation |
| 16 |  | AL-016 | House | 4225 Northwestern Parkway | 1925 | American Foursquare | C | Two and a half story rectangular-footprint, hipped roof with wide eave overhang, gabled dormer and 2 chimneys, clapboard siding, wood 5/1 sash windows with simple wood trim and decorative shutters, single door with 4 paned sidelights and storm door, full front hipped porch with simple Doric stuccoed supports and concrete deck and rail deck |

| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|---------------------------------|------|------------------------------|------|--|
| 17 |  | AL-017 | House | 4223 Northwestern Parkway | 1928 | Dutch Colonial Revival | C | One and a half story, rectangular-footprint, side gambrel roof with extended eave overhang and shed dormer, clapboard siding, 6/1 wood sash windows with simple wood trim, full front porch with Ionic supports and rustic concrete block rail, off center front door and storm door, concrete foundation, detached two car garage |
| 18 |  | AL-018 | House | 4221 Northwestern Parkway | 1928 | Craftsman Bungalow | C | One and a half story, rectangular-footprint, gable roof with extended eave overhang and gabled dormer, clapboard siding, 20/1 and 1/1 wood sash windows with simple wood trim and decorative shutters, single center door and storm door, full front overhanging eave porch with rustic concrete block supports and rail, rustic concrete block foundation |
| 19 |  | AL-019 | House | 4219 Northwestern Parkway | 1927 | Craftsman Bungalow | C | One and a half story, rectangular-footprint, gable roof with extended eave overhang and gabled dormer, brick veneer, 5/1 and 1/1 wood windows with simple wood trim, single center door with storm door and sidelights, full front overhanging eave porch with brick supports and rail, concrete foundation |
| 20 |  | AL-020 | House | 4222 Northwestern Parkway | 1964 | Ranch | NC | One story, irregular-footprint, hipped roof with eave overhang, brick veneer, wood 1/1 windows with simple wood trim, brick sills, two bay hipped enclosed porch |

| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|---------------------------------|------|------------------------------|--|---|
| 21 |  | AL-021 | House | 4224 Northwestern Parkway | 1945 | Cape Cod | C; individually NRHP eligible | One and a half story, rectangular-footprint, side gable roof with eave overhang, and two gabled dormers, and brick end chimney, brick veneer, wood 6/1 sash windows with simple wood trim and decorative shutters, single center door with Doric pediment and pilasters and storm door, center bay gabled front stoop with simple Doric supports, enclosed side porch |
| 22 |  | AL-022 | House | 4230 Northwestern Parkway | 1964 | Ranch | NC | One story, L-footprint, gabled roof with eave overhang, brick veneer, wood 6/6 and 4/4 sash windows with simple wood trim, brick sills, and decorative shutters, single bay overhanging eave stoop with a simple support post, single door under stoop with storm door |
| 23 |  | AL-023 | House | 4232 Northwestern Parkway | 1925 | Dutch Colonial Revival | C | One and a half story, rectangular-footprint, gambrel roof with extended eave overhang and shed dormer, clapboard siding, 1/1 and 6/6 vinyl sash, sash windows with simple wood trim, single off-center door with transom and sidelights, 3/4 front gabled enclosed porch, concrete foundation |
| 24 |  | AL-024 | House | 4236 Northwestern Parkway | 1925 | Colonial Revival | C; individually NRHP eligible | Two story, rectangular-footprint, side gable roof with eave overhang, cornice returns, and oversized brick end chimney, brick veneer, wood 8/8 and 6/6 sash windows with simple wood trim and shutters, single center door with transom, sidelights, and storm door, gabled bracketed stoop |



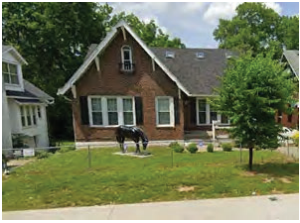
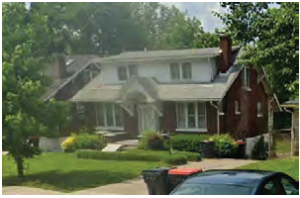
| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|---------------------------------|------|---------------------------------|--|--|
| 25 |  | AL-025 | House | 4238 Northwestern Parkway | 1938 | Colonial Revival Cape Cod | C | One and a half story, L-footprint, side gable roof with two gabled dormers, brick veneer, wood 8/8 and 6/6 sash windows with simple wood trim and decorative shutters, single door with sidelights and storm door, concrete basement with added front basement door, concrete foundation |
| 26 |  | AL-026 | House | 4242 Northwestern Parkway | 1926 | English cottage | C | One and a half story, irregular-footprint, gabled roof with oversized brick end chimney and gabled dormer, brick veneer, vinyl 3/1 sash windows with simple wood trim, single door with stoop and steel railing, concrete basement with single bay garage |
| 27 |  | AL-027 | House | 4246 Northwestern Parkway | 1915 | American small house | C | One story, rectangular-footprint, hipped roof with eave overhang, limestone veneer, 1/1 vinyl sash windows with simple wood trim, single round top door and bracketed stoop |
| 28 |  | AL-028 | House | 4252 Northwestern Parkway | 1915 | Dutch Colonial Revival | C; individually NRHP eligible | One and a half story, rectangular-footprint, gambrel roof with extended eave overhang, two stone end chimneys and shed dormer, limestone veneer and clapboard siding, 1/1 wood sash windows with simple wood trim and shutters, single off-center door with sidelights with gabled bracketed stoop, basement with single bay garage and stone retaining wall |

| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|---------------------------------|------|-------------------------|------|--|
| 29 |  | AL-029 | House | 4260 Northwestern Parkway | 1949 | American small house | C | One and a half story, rectangular footprint, side gabled roof with eave overhang, brick veneer, wood 8/8 and 6/6 wood sash windows with simple wood trim, off single center door with a storm door, 2/3 front gabled porch with simple post supports and concrete deck, side entry stoop |
| 30 |  | AL-030 | House | 4266 Northwestern Parkway | 1913 | Gable-front | C | One and a half story, rectangular-footprint, gable front roof with eave overhang and hipped dormer, brick veneer, 1/1 vinyl sash windows with simple wood trim, single door, full front hipped porch with simple post and brick supports and wood rail, concrete foundation |
| 31 |  | AL-031 | House | 4270 Northwestern Parkway | 1915 | Stick/ Gable-front | C | One and a half story, rectangular-footprint, gable front roof with extended eave overhang, clapboard siding, 1/1 wood sash windows with simple wood trim, paired front doors, 1/2 front gabled porch with concrete deck, simple post supports and rail, concrete foundation |
| 32 |  | AL-032 | House | 4278 Northwestern Parkway | 1905 | Cross-gable | C | One and a half story, X-footprint, gabled roof with extended eave overhang and two chimneys, clapboard siding, 1/1 wood sash windows with simple wood trim, two off center front doors, gable end front hipped porch and front stoop with Doric supports with rustic concrete block bases, rear porch and hipped dormer, concrete foundation |




| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|--------------------------------|------|----------------------|--|---|
| 33 |  | AL-033 | House | 4217 Northwestern Parkway | 1972 | Gable-front | NC | One story, rectangular-footprint, front gable roof with eave overhang, brick veneer, 1/1 wood sash windows with simple wood trim and brick sills, off center front sliding door with awning stoop |
| 34 |  | AL-034 | House | 4215 Northwestern Parkway | 1925 | Craftsman Bungalow | C; individually NRHP eligible | One and a half story, rectangular-footprint, side gabled roof with eave overhang, white brick veneer, wood 8/1 and 1/1 sash windows with simple wood trim, two bay gabled porch with brick supports and rail, single door under porch with storm door, single car basement garage |
| 35 |  | AL-035 | House | 4205-4207 Northwestern Parkway | 1951 | American small house | C | One story, rectangular-footprint, gable front roof with eave overhang, weatherboard siding, wood fixed and 1/1 sash windows with simple wood trim, single off center front door with storm door, 1/2 bay gabled front porch with concrete deck and simple post supports, attached carport |
| 36 |  | AL-036 | House | 4203 Northwestern Parkway | 1951 | American small house | C | One half story, rectangular-footprint, front gable roof with eave overhang, clapboard siding, wood 2/2 sash windows with simple wood trim and decorative shutters, single door and storm door, 2 bay awning porch with steel supports, concrete foundation |


| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|---------------------------------|------|------------------------|--|--|
| 37 |  | AL-037 | House | 4201 Northwestern Parkway | 1951 | Gable-front | C | One half story, rectangular-footprint, front gable roof with eave overhang, clapboard siding, wood 2/2 sash windows with simple wood trim single off center door and storm door, 2 bay gabled porch with simple post supports, concrete foundation |
| 38 |  | AL-038 | House | 4005 Northwestern Parkway | 1925 | Craftsman Bungalow | C | One and a half story, rectangular-footprint, side gable roof with eave overhang and shed dormer, clapboard siding, 1/1 vinyl sash windows with simple wood trim, single door with storm door and full front porch with brick supports and rail, concrete foundation |
| 39 |  | AL-039 | House | 4001 Northwestern Parkway | 1924 | Craftsman Bungalow | C; individually NRHP eligible | One and a half story, rectangular-footprint, side gable roof with eave overhang, gabled dormer, and brick chimney with chimney pot, clapboard siding, 3/1 wood sash windows with simple wood trim, single entry door with sidelights, transom, and storm door and full front porch with brick supports and rail, concrete foundation |
| 40 |  | AL-040 | House | 3953 Northwestern Parkway | 1921 | California bungalow | C | One and a half story, rectangular footprint, front gabled roof with eave overhang, clapboard siding, 1/1 wood sash windows with simple wood trim, single entry door with sidelights and storm door, full front gabled porch with brick supports and rail, concrete foundation |

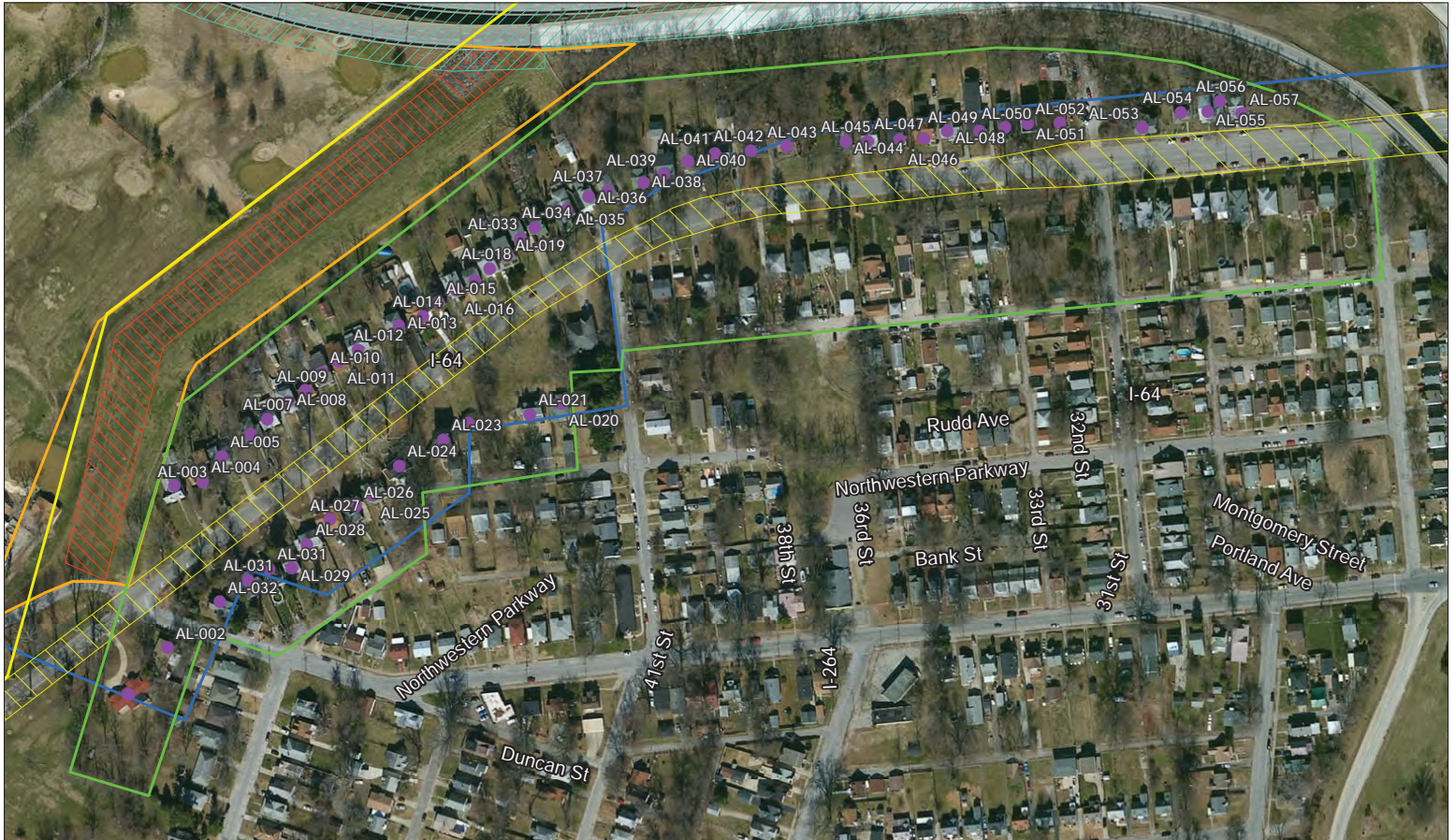
| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|---------------------------------|------|------------------------------|------|---|
| 41 |  | AL-041 | House | 3941 Northwestern Parkway | 1922 | Dormer- front bungalow | C | One and a half story, rectangular-footprint, side gable roof with eave overhang, and gabled dormer, stucco veneer, 4/1 wood sash windows with simple wood trim, single center door in a full front enclosed porch with stuccoed supports and rail, concrete foundation |
| 42 |  | AL-042 | House | 3939 Northwestern Parkway | 1955 | Ranch | C | One story, L-footprint, Gabled hip roof with eave overhang, clapboard siding, off center front door and storm door, 1/1 vinyl sash windows, detached concrete block garage |
| 43 |  | AL-043 | House | 3937 Northwestern Parkway | 1955 | Ranch | C | One story, L-footprint, Gabled hip roof with eave overhang, clapboard siding, off center front door and screen door, 3 paned and 4 paned sash windows |
| 44 |  | AL-044 | House | 3933 Northwestern Parkway | 1920 | Gable-front | NC | Two story, rectangular-footprint, gable front roof with eave overhang, clapboard siding, 1/1 vinyl sash windows with simple wood trim, center front replacement door, full front porch with stuccoed supports and rail, concrete foundation, detached one car concrete block garage |

| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|---------------------------------|------|-----------------------|--|--|
| 45 |  | AL-045 | House | 3931 Northwestern Parkway | 1922 | Craftsman Bungalow | C | One and a half story, rectangular-footprint, side gable roof with eave overhang, and gabled dormer, brick veneer, 4/1 and 3/1 wood sash windows with simple wood trim, single center door in a full front porch with brick supports and rail |
| 46 |  | AL-046 | House | 3929 Northwestern Parkway | 1923 | Gable-front | C | One and a half story, rectangular-footprint, front gable roof with eave overhang, shed dormers and brackets, and gabled dormer, stucco veneer, 4/1 and 3/1 wood windows with simple wood trim, single center front door in a 2-bay gabled front porch with stuccoed supports and full front deck and rail |
| 47 |  | AL-047 | House | 3927 Northwestern Parkway | 1930 | English cottage | C | One and a half story, L-footprint, gable roof with eave overhang with brackets and large end brick chimney, brick veneer, 6/1 and 4/1 vinyl sash windows with simple wood trim and shutters, single door with a 1/2 front deck with brick rail |
| 48 |  | AL-048 | House | 3925 Northwestern Parkway | 1924 | Cape Cod | C; individually NRHP eligible | One and a half story, rectangular-footprint, side gable roof with eave overhang, brackets, brick end chimney, and shed dormer, brick veneer, 3/1 wood sash windows with simple wood trim, single center door and storm door with transom and sidelights and a gabled bracketed stoop and concrete deck with brick rails, concrete foundation |

| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|---------------------------------|--------|------------------------|------|--|
| 49 |  | AL-049 | House | 3923 Northwestern Parkway | 1963 | Ranch | NC | One story, rectangular-footprint, hipped roof with eave overhang, limestone veneer, wood 6/6 sash windows with simple wood trim, single door with storm door and 2/3 front porch with steel supports and rail |
| 50 |  | AL-050 | House | 3909 Northwestern Parkway | 1940 | Gable-front | C | One and a half story, rectangular footprint, front gabled roof with eave overhang, clapboard siding, wood 1/1 wood sash windows with simple wood trim, single off center front entry door storm door, 2/3 front hipped porch with steel supports and rail, concrete foundation |
| 51 |  | AL-051 | House | 3907 Northwestern Parkway | 1930 | American foursquare | C | Two story, rectangular footprint, end gabled roof with eave overhang, brick veneer, wood 3/1 wood sash windows with simple wood trim, single front replacement door with sidelights, concrete basement and foundation, second story deck and door with sidelights |
| 52 |  | AL-052 | House | 3901 Northwestern Parkway | c.1900 | Gable-front | C | One story, rectangular-footprint, gable front roof with eave overhang, clapboard siding, 1/1 sash and fixed vinyl windows with simple wood trim, two front doors, Gable end front porch with hipped front porch with simple Doric supports and concrete block bases and rail, side flat-roof porch with Doric supports and concrete block supports and rails, large rear addition, concrete foundation |

| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|---------------------------------|--------|------------------------------|------|--|
| 53 |  | AL-053 | House | 3831 Northwestern Parkway | c.1880 | Central passage | C | One story, rectangular-footprint, side gable roof with end chimney, brick walls, 1/1 vinyl windows, center front door with transom |
| 54 |  | AL-054 | House | 3825 Northwestern Parkway | c.1900 | Vernacular | NC | Two story, rectangular-footprint, hipped roof with eave overhang, board and batten siding, 1/1 vinyl sash windows with simple wood trim, single off-center door, 2/3 front porch with simple spindle supports and rail, rear addition |
| 55 |  | AL-055 | House | 3821 Northwestern Parkway | c.1920 | Dormer- front bungalow | C | One and a half story, rectangular-footprint, side gable roof with eave overhang, and gabled dormer, weatherboard siding, 1/1 wood sash windows with simple wood trim, single front entry door with storm door and sidelights with a full front porch with steel supports |
| 56 |  | AI-056 | House | 3819 Northwestern Parkway | c.1900 | Gable-front | C | One and a half story, rectangular-footprint, front gable roof with eave overhang and brick chimney, weatherboard siding, fixed and 1/1 wood sash windows with simple wood trim, full front porch with rustic concrete block supports and rail, rear addition |

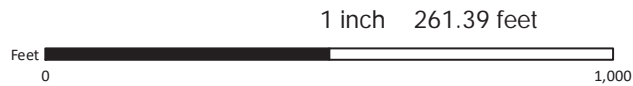
| No. | Photo | Site ID | Type | Location | Date | Style | C/NC | Notes |
|-----|---|---------|-------|---------------------------------|------|-----------------------|------|---|
| 57 |  | AI-057 | House | 3817 Northwestern Parkway | 1935 | Craftsman Bungalow | C | <p>One and a half story, rectangular-footprint, side gable roof with eave overhang with brackets and a gabled dormer, weatherboard siding, 1/1 wood sash windows with simple wood trim, single front entry door with storm door and sidelights, full front hipped porch with rustic concrete block supports, rustic concrete block foundation</p> |



- Proposed Shawnee Residential HD Boundary
- AP
- Portion of the Imsted Park System Historic District
- Proposed Shawnee Golf Course NRHP Boundary
- Architectural Location
- Project Footprint
- Portland Historic District
- Proposed Levee Road
- Proposed Levee NRHP Boundary

Shawnee Residential HD on an aerial photograph
 Sherman Minton Bridge
 Louisville, Jefferson County, Kentucky
 Metric Project No. 20-0201
 Map Date: 07/09/2021

All locations approximate
 Source: Imagery Date 2012 Aerial



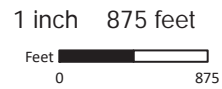
APPENDIX F: NRHP BOUNDARY MAPS FOR HISTORIC PROPERTIES IN THE APE



- AP
- Project Footprint
- Proposed Levee Road
- Portion of the Imsted Park System Historic District
- Portland Historic District
- Proposed Shawnee Golf Course NRHP Boundary

Proposed Shawnee Golf Course NRHP boundary on an aerial photograph
 Sherman Minton Bridge
 Louisville, Jefferson County, Kentucky
 Metric Project No. 20-0201
 Map Date: 07/08/2021

All locations approximate
 Source: Imagery Date 2012 Aerial

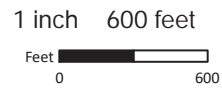




- AP
- Project Footprint
- Proposed Levee Road
- Portion of the Imsted Park System Historic District
- Portland Historic District
- Proposed Earthen Levee NRHP Boundary

Proposed Earthen Levee NRHP boundary
 on an aerial photograph
 Sherman Minton Bridge
 Louisville, Jefferson County, Kentucky
 Metric Project No. 20-0201
 Map Date: 07/08/2021

All locations approximate
 Source: Imagery Date 2012 Aerial

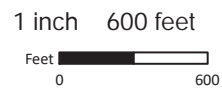




- AP
- Project Footprint
- Proposed Levee Road
- Portion of the Imsted Park System Historic District
- Portland Historic District
- Proposed Shawnee Residential HD Boundary

Proposed Shawnee Residential Historic District
 NRHP boundary on an aerial photograph
 Sherman Minton Bridge
 Louisville, Jefferson County, Kentucky
 Metric Project No. 20-0201
 Map Date: 07/08/2021

All locations approximate
 Source: Imagery Date 2012 Aerial

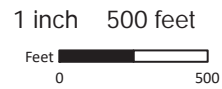




- AP
- Portion of the Instead Park System Historic District
- Project Footprint
- Portland Historic District

Proposed Shawnee Residential Historic District
 NRHP boundary on an aerial photograph
 Sherman Minton Bridge
 Louisville, Jefferson County, Kentucky
 Metric Project No. 20-0201
 Map Date: 07/08/2021

All locations approximate
 Source: Imagery Date 2012 Aerial



APPENDIX G: PHOTOGRAPHS AND KEY MAPS OF THE APE



1. View of Shawnee Golf Course entrance facing southeast toward Northwestern Parkway



2. View on top of levee facing southwest



3. View on top of levee facing northeast toward approach bridges



4. View from top of levee north toward approach bridges



5. View from base of levee facing northeast



6. View of ranch houses on the north side of Northwestern Parkway south of Bank Street



7. View along the north side of Northwestern Parkway facing northeast



8. View along the north side of Northwestern Parkway facing northeast



9. View of Northwestern Parkway's curvilinear route facing west



10. View of the north side of Northwestern Parkway facing northwest



11. View of Northwestern Parkway with interstate overpasses



12. Facing west along Rudd Avenue where it terminates due to the interstate



13. Good Shepherd Church on the north side of Rudd Avenue facing northeast from N 36th Street



14. View of the north side of Rudd Avenue east of Cedar Grove Terrace



15. View of the north side of Rudd Avenue facing west from N 34th Street



16. View of the north side of Rudd Avenue facing north, east of N 34th Street



17. View of the north side of Rudd Avenue facing northeast mid-block between N 34th and N 33rd streets



18. View of the north side of Rudd Avenue facing northeast from N 33rd Street



19. View of the McAlpine Locks and Dam Visitors Center facing north



20. View facing south from N 26th Street near the Louisville Riverwalk



- AP
- Portion of the Imsted Park System Historic District
- Photograph Location
- Portland Historic District
- Proposed Levee Road

Photograph locations on an aerial photograph
 Sherman Minton Bridge
 Louisville, Jefferson County, Kentucky
 Metric Project No. 20-0201
 Map Date: 07/08/2021

All locations approximate
 Source: Imagery Date 2012 Aerial
 Photographs taken by Metric Staff on 5/7/21 and 6/29/21



1 inch = 1,000 feet
 Feet 01.00



APPENDIX H: BIBLIOGRAPHY

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**MEMORANDUM OF UNDERSTANDING
AMONG FEDERAL HIGHWAY ADMINISTRATION,
KENTUCKY STATE HISTORIC PRESERVATION OFFICER AND
KENTUCKY TRANSPORTATION CABINET**

Regarding Section 4(f) of the U.S. Department of Transportation (USDOT) Act of 1966 and notification requirements of intent to make *de minimis* determinations for historic resources.

Whereas, as detailed in 23 CFR 774 and FHWA's Section 4(f) Policy Paper, there are increased flexibilities with respect to minor transportation project impacts to properties that are protected under Section 4(f), including historic properties; and

Whereas, the Kentucky Transportation Cabinet (KYTC) have been delegated the authority, on behalf of the Federal Highway Administration (FHWA) and in conjunction with the Section 106 Programmatic Agreement for historic resources, to evaluate project impacts to historic properties and make determinations that a project will have No Adverse Effects or that there are No Historic Properties Affected; and

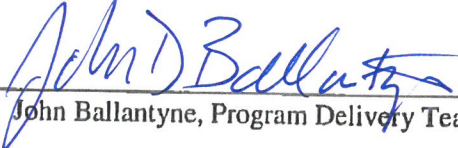
Whereas, the Kentucky State Historic Preservation Officer (SHPO) concurs in writing on the effect determination in accordance with Section 106 of the National Historic Preservation Act (NHPA); and

Whereas, as detailed in 23 CFR 774, the FHWA may make *de minimis* 4(f) impact determinations for those historic properties for which the SHPO concurs with a finding that the project will have No Adverse Effects or that there are No Historic Properties Affected;

Now, therefore, the FHWA is hereby notifying the SHPO of FHWA's intent to determine Section 4(f) *de minimis* use(s) (when applicable) for those projects in which the SHPO has previously concurred with a finding that the project will have No Adverse Effects or that there are No Historic Properties Affected and that this agreement satisfies the notification requirements specified in 23 CFR 774. Should the SHPO prefer to conduct project-specific Section 4(f) *de minimis* consultation, the SHPO shall provide a request, in writing, to FHWA providing reasoning why a project-specific consultation is appropriate. If the SHPO does not concur or request project-specific consultation within the specified times defined in accordance with Section 106 of NHPA, then FHWA will assume SHPO concurrence and make a Section 4(f) *de minimis* determination.

SIGNATORIES

FEDERAL HIGHWAY ADMINISTRATION, KENTUCKY DIVISION OFFICE

BY:  3/5/2018
John Ballantyne, Program Delivery Team Leader

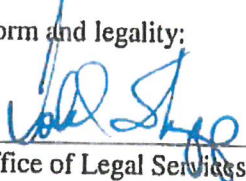
KENTUCKY HERITAGE COUNCIL

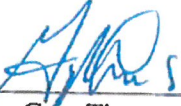
BY: 
Craig Potts, State Historic Preservation Officer

INVITED SIGNATORIES:

KENTUCKY TRANSPORTATION CABINET

Approved as to form and legality:

BY: 
KYTC Office of Legal Services

BY: 
Greg Thomas, Secretary, Kentucky Transportation Cabinet

APPENDIX D: Water Resources

WATERS DELINEATION REPORT

Sherman Minton Corridor Project

DES. NO. 1702255

CITY OF LOUISVILLE, JEFFERSON COUNTY, KENTUCKY

Prepared for:

Kentucky Transportation Cabinet

March 31, 2021

Prepared by:



Complex Environment. Creative Solutions.

6971 Hillsdale Court
Indianapolis, IN 46256
Telephone: 317.400.1633
www.metricenv.com

WATERS DELINEATION REPORT
Sherman Minton Corridor Project
Des. No. 1702255
City of Louisville, Jefferson County, Kentucky
Prepared By: Cory Shumate
March 31, 2021

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Exhibits

Exhibit 1 – Location Map

Exhibit 2 – USGS Topographic Map

Exhibit 3 – National Wetlands Inventory and National Hydrography Dataset Flowline Map

Exhibit 4 – NRCS Soil Survey and FEMA Flood Insurance Rate Map

Exhibit 5 – Waters Delineation and Photo Location Map

Appendices

Appendix A – Wetland Determination Data Sheets

Appendix B – Site Photographs

1.0 INTRODUCTION

Metric Environmental, LLC (Metric) was contracted to perform a determination of the presence of Waters of the United States (U.S.) and/or Waters of the State within the project study limits (PSL) of the proposed bridge rehabilitation project solely associated with a newly added temporary access area. The proposed project is located in the City of Louisville, Jefferson County, Kentucky as shown on **Exhibit 1**. The site investigation, conducted by Cory Shumate and Samantha Wickizer on March 19, 2021, found no wetlands and streams located within the PSL.

2.0 PROJECT DESCRIPTION

The purpose of this project (KYTC Item ID 5-64, INDOT Lead Des. No. 1702255) is to address the deterioration of structural elements of the Sherman Minton Bridge (Bridge No. I64-123-04691 D, Des. No. 1702255), the associated Indiana westbound approach (I64-123-02294 CWBL, Des. No. 1702257), Indiana eastbound approach (I64-123-02294 CEBL, Des. No. 1702258), Kentucky approach (056B00161N, Des. No. 1702260, 1702254), and select associated side streets (Elm St., Spring St., and 5th St., Des. No. 1701215) with the goal of extending the service life of the I-64 crossing over the Ohio River up to 30 years. Additional Des. No. associated with this project include 1592187, 1702259, and 1900579. The project is located on the I-64 Interstate and US 50 over the Ohio River connecting New Albany, Floyd County, Indiana and Louisville, Jefferson County, Kentucky, in the Indiana New Albany Quadrangle, Sections 21, 22, 27, and 28, Township 27 N and Range 8 E. A previous waters delineation report, prepared by Kaskaskia Engineering Group, LLC and dated September 16, 2019, was prepared for these areas.

Metric specifically performed the determination of the Waters of the U.S. and/or State for additional area needed for a temporary access road on a levee located approximately 1,800 feet southeast of the Sherman Minton Bridge. A location map showing the project location is provided as **Exhibit 1** and a USGS West Louisville, Kentucky Quadrangle Topographic Map is provided as **Exhibit 2**. The project is located in the 051401010904 12-Digit Hydrologic Unit Code (HUC) watershed.

2.1 Purpose

The objective of this investigation is to identify and delineate the Waters of the U.S. and Waters of the State, including wetlands, streams, and ponds, located within the proposed project study limits. This report identifies the Waters of the U.S. as defined by the U.S. Army Corps of Engineers (USACE) regulations and guidance documents, as well as Waters of the State and wetlands as defined by the State of Kentucky rules and regulations.

2.2 Regulatory Definitions

2.2.1 Waters of the U.S.

The definition of Waters of the U.S. includes Traditional Navigable Waters (TNWs) of the U.S. and adjacent wetlands, non-navigable tributaries to TNWs, and wetlands that directly abut such tributaries (Department of the Army, Corps of Engineers and Environmental Protection agency, 2015). The USACE has jurisdiction over all navigable Waters of the U.S. under the Rivers and Harbors Act of 1899. The USACE also regulates the placement of dredged or fill materials into Waters of the U.S. under Section 404 of the federal Clean Water Act (CWA). Section 404 of the CWA defines the landward limit for non-tidal waters as the Ordinary High Water Mark (OHWM). When adjacent wetlands are present, the limit of jurisdiction extends to the limit of the wetland. Depositing dredge or fill materials into wetlands or other Waters of the U.S. requires written permission through the USACE Section 404 permit process.

2.2.2 Waters of the State

Waters of the State are defined as surface and underground waterbodies, which exist wholly in the State (Kentucky Energy & Environment Cabinet, 2016). Private ponds, reservoirs, or facilities built for reduction of pollutants prior to discharge are not included in this definition. In Kentucky, two government agencies have jurisdiction over Waters of the State: Kentucky Energy & Environment Cabinet and the USACE. Kentucky Energy & Environment Cabinet is responsible for maintaining, protecting, and improving the physical, chemical, and biological integrity of Kentucky's waters. Kentucky Energy & Environment Cabinet administers the Section 401 Water Quality Certification (WQC) Program and draws its authority from the federal CWA and Kentucky's Water Quality Standards. Any person who wishes to place fill materials, excavate or dredge, or mechanically clear within a wetland, lake, river, stream, or other Waters of the State, must first apply for a CWA Section 404 permit through USACE and a Section 401 WQC permit through Kentucky Energy & Environment Cabinet. If a Waters of the State is determined to be non-jurisdictional by the USACE, these waters can be regulated by Kentucky Energy & Environment Cabinet under the 401 certification if the area is one acre in size or more.

2.2.3 Wetlands

Wetlands are a category of Waters of the U.S. for which a specific identification methodology has been developed. Wetlands are identified using three criteria: hydrophytic vegetation, hydric soils, and wetland hydrology. Isolated wetlands are those waters no longer subject to regulation under the CWA. Areas defined as wetlands are delineated under the *Corps of Engineers Wetlands Delineation Manual (Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Eastern Mountains and Piedmont Region (Version 2.0), April 2012)*. For the purposes of expediting the review of the wetlands identified for this project, all wetlands are assumed to be jurisdictional Waters of the U.S. and subject to regulation by the USACE.

3.0 BACKGROUND INFORMATION – EXISTING MAPS

The initial steps in the wetland determination process include a review of documents that provide information on areas where wetlands have been previously identified or that possess a high likelihood of containing wetlands. Several sources of information were consulted to help identify potential jurisdictional areas within the survey boundaries. These resources included:

- U.S. Geological Survey (USGS) 7.5 Minute Topographic Map (West Louisville, IN Quadrangle, 2020)
- U.S. Department of Agriculture (USDA), National Resources Conservation Service (NRCS), Soil Survey Maps (Jefferson County, Kentucky)
- U.S. Fish and Wildlife Services (USFWS) National Wetlands Inventory (NWI) Map
- U.S. Federal Emergency Management Agency (FEMA), Flood Insurance Rate Map (FIRM)

A review of the USGS topographic map allowed for interpretation of slopes and the identification of potential Waters of the U.S. within the survey boundary.

Published soil surveys for Jefferson County in Kentucky were reviewed to identify listed hydric soils and/or potential inclusions of hydric soils. Identified areas containing hydric soils were evaluated against other data collected to identify potential wetland areas. The county soils survey maps were developed from

actual field investigations. However, they address only one of the three required wetland criteria (hydric soils) and may reflect historical conditions rather than current site conditions. The resolution of soil maps limits their accuracy as well. The mapping units are often generalized based on topography and many mapping units contain inclusions of other soil types for up to 15% of the area of the unit.

The NWI maps were developed to identify probable wetland areas and are mapped on USGS 7.5-minute quadrangle maps. The NWI maps were prepared from high-altitude photography and in most cases were not field checked. There are several limitations to the quality of this data. Therefore, the NWI maps should not be used as a sole determination to identify potential wetlands.

FIRM maps were developed to identify areas subject to flood hazards. These maps identify areas located within a flood zone, which may contain wetlands.

3.1 USGS 7.5-minute Topographic Map

Geographically, the PSL is located in the City of Louisville, Jefferson County, Kentucky. **Exhibit 2** includes the USGS West Louisville, Kentucky Quadrangle Topographic Maps. The flow regime of field-identified streams was verified using the topographic maps, with perennial streams verified as solid blue lines on the map, intermittent streams verified as dashed blue lines on the map, and ephemeral streams verified where no blue lines were present on the map. During a review of the USGS topographic maps, no streams were identified.

3.2 USFWS National Wetlands Inventory (NWI) Map

The NWI map of the area included in **Exhibit 3** was retrieved from the USFWS NWI website (USFWS, 2016). No mapped NWI polygons are located within the PSL of the proposed access area. The nearest mapped NWI polygon is located approximately 1,400 feet (ft.) northwest of the PSL and was associated with the riparian area of the Ohio River. The NWI map is provided as **Exhibit 3**.

3.3 USDA-NRCS Soil Survey Maps

The NRCS Web Soil Survey (USDA-NRCS, 2019) soil map and soil data for Jefferson County was compared against the National and State of Kentucky Hydric Soils lists in order to assess the location of hydric soils. The soil map is provided as **Exhibit 4**. **Table 1** identifies the soil unit symbol, map unit name, and hydric soil rating. Four nationally listed hydric soil units were identified within the PSL: Urban land-Haplic Udarents-Newark complex, 0 to 6 percent slopes, rarely flooded (UaeB), Urban land-Udorthents complex, 0 to 12 percent slopes, rarely flooded (UaiC), Urban land-Udorthents complex, smoothed, 0 to 50 percent slopes (UakF), and Urban land-Alfic Udarents complex, loamy substratum, 0 to 12 percent slopes (UbC).

Table 1: Soil Map Unit Legend – Soil Map Units within Project Study Limits

| Map Unit symbol | Map Unit Name | Hydric Rating (%) |
|-----------------|--|-------------------|
| UaeB | Urban land-Haplic Udarents-Newark complex, 0 to 6 percent slopes, rarely flooded | Not Hydric (0) |
| UaiC | Urban land-Udorthents complex, 0 to 12 percent slopes, rarely flooded | Not Hydric (0) |
| UakF | Urban land-Udorthents complex, smoothed, 0 to 50 percent slopes | Not Hydric (0) |
| UbC | Urban land-Alfic Udarents complex, loamy substratum, 0 to 12 percent slopes | Not Hydric (0) |

Source: USDA-NRCS Web Soil Survey 2019, National Hydric Soils List

3.4 Flood Insurance Rate Map (FIRM)

The FIRM map of the area, **Exhibit 4**, was retrieved from the FEMA website (FEMA, 2018). One mapped floodplain is located within the PSL. This floodplain was identified as Zone AE, an area subject to inundation by the 1 percent annual chance of flood. This floodplain was associated with Ohio River.

4.0 WETLAND AND STREAM DELINEATION METHODOLOGY

For the purpose of identifying wetlands regulated under Section 404 and 401 of the CWA, wetland determinations were made using the three criteria of assessment approach defined in the *1987 U.S. Army Corps of Engineers' Wetland Delineation Manual* and the *Eastern Mountains Piedmont Regional Supplement "Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Eastern Mountains Piedmont Region (Version 2.0)"*. According to the procedure described in the manual, areas that reflect a predominance of hydrophytic vegetation, hydric soils, and wetland hydrology are considered wetlands.

Streams were identified based on the presence of an OHWM as defined in 33 CFR 328.3(3) as the "line on the shore established by the fluctuations of water and indicated by physical characteristics such as a clear, natural line impressed on the bank, shelving, changes in the character of soil, destruction of terrestrial vegetation, the presence of litter and debris, or other appropriate means that consider the characteristics of the surrounding areas." If identified, streams would be assessed using the rapid bioassessment protocols (RBP) assessments described in the Kentucky Energy and Environment Cabinet Division of *Water's Methods for Assessing Habitat in Wadeable Waters (Revision No. 1)(March 1, 2011)* to determine overall stream aquatic habitat quality.

A reconnaissance (waters delineation) was conducted to determine the general topography, plant communities, soils, and hydrology present within the survey boundary. Areas identified as either Waters of the U.S. or Waters of the State were delineated and mapped using a Trimble R1 GNSS Receiver handheld GPS unit with submeter accuracy.

5.0 RESULTS

The field reconnaissance was conducted on March 19, 2021 by Cory Shumate and Samantha Wickizer of Metric. The site was investigated for evidence of hydrophytic vegetation, hydric soil, and wetland hydrology, with sampling points (SP) being dug in areas suspected of being wetlands as shown on **Exhibit 5**. Data was recorded on wetland determination data sheets from the USACE Eastern Mountains and Piedmont Regional Supplement and are included in **Appendix A**. Any streams identified within the PSL were to be evaluated using the HHEI and QHEI stream assessments. No streams were identified within the PSL. A photograph location map is provided as **Exhibit 5** and site photographs are provided in **Appendix B**. The photographs are visual documentation of site conditions at the time of the inspection and are intended to provide representative visual examples of the features found on the site.

No streams or wetlands were identified within the PSL. The PSL consisted of the levee, a golf course (Shawnee Golf Course), and Interstate 64 road right-of-way (ROW). Upland areas where sampling points were not taken were determined to be upland due to upward sloping topography and/or dominance of upland vegetation. Dominant vegetation along the levee included tall false rye grass (*Schedonorus arundinaceus*, FACU), purple deadnettle (*Lamium purpureum*, UPL), curly dock (*Rumex crispus*, FAC), and red clover (*Trifolium pratense*, FACU). A sloped forested area in the northeast section of the PSL was dominated by northern red oak (*Quercus rubra*, FACU), box elder (*Acer negundo*, FAC), black walnut

(*Juglans nigra*, FACU), amur honeysuckle (*Lonicera maackii*, UPL), English ivy (*Hedera helix*, FACU), and winter creeper (*Euonymus fortunei*, UPL).

5.1 Streams

No streams were identified within the PSL during the site investigation.

5.2 Wetlands

No wetlands were identified within the PSL during the site investigation.

5.3 Upland Sampling Points

One upland sampling point was taken in the PSL in an area that was suspected of being wetlands due to its location within the 100-year (Q100) floodplain of the Ohio River. This sampling point did not qualify as a wetland as it did not meet all three wetland criteria. **Table 2** lists the sampling point that was taken but was not associated with wetlands. A description of this upland sampling point is provided below.

Table 2: Upland Sampling Point Data Summary Table

| Plot # | Photo Points | Lat/Long | Hydrophytic Vegetation | Hydric Soils | Wetland Hydrology | Within a Wetland |
|--------|--------------|----------------------|------------------------|--------------|-------------------|------------------|
| SP-1 | 32-34 | 38.27479, -85.816159 | No | Yes | No | No |

Upland Sampling Point 1 (SP-1)

SP-1 was located on a hillslope within the Q100 of the Ohio River. The dominant vegetation at this sampling point was green ash (*Fraxinus pennsylvanica*, FACW) and northern red oak (*Quercus rubra*, FACU) in the sapling/shrub stratum and tall false rye grass (*Schedonorus arundinaceus*, FACU) in the herb stratum. This did not pass the criteria for hydrophytic vegetation due to a dominance test of 33 percent and a prevalence index of 3.87. To a depth of 16 inches (in.), the soils in the test pit were a silt loam. A restrictive layer of riprap at 16 inches prevented further excavation despite multiple attempts. From 0 to 8 in., the soil exhibited a matrix color of 10YR 4/2 (98 percent) with 10YR 4/3 (2 percent) faint redox concentrations in the matrix. From 8 to 16 in., the soils in the test pit exhibited a matrix color of 10YR 4/2 (85 percent) with 10YR 4/1 (10 percent) faint redox depletions and 10YR 4/4 (5 percent) distinct redox concentrations. This met the hydric soil indicator of depleted matrix (F3). No primary or secondary indicators of wetland hydrology were observed. Since only one of the three required criteria were met, this area did not qualify as a wetland.

6.0 CONCLUSION

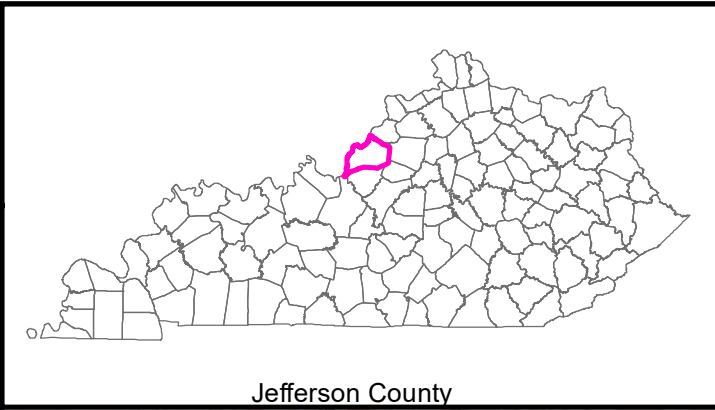
We have performed a waters delineation for the proposed Sherman Minton Renewal project. No Waters of the U.S. or Waters of the State were observed within the PSL. If the project area changes to areas that could potentially impact wetlands and Waters of the U.S. or State, additional delineation might be required. Every effort should be taken to avoid and minimize impacts to the wetlands and waterways. If any wetlands or streams will be impacted by this project, permits will be required by the USACE and Kentucky Energy and Environment Cabinet. The final determination of jurisdictional waters is ultimately made by the USACE. This report is our best judgment based on the guidelines set forth by USACE.

7.0 REFERENCES

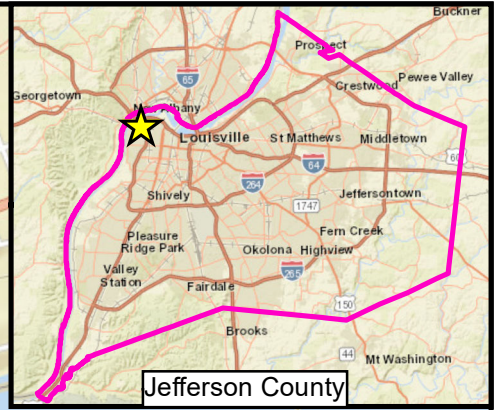
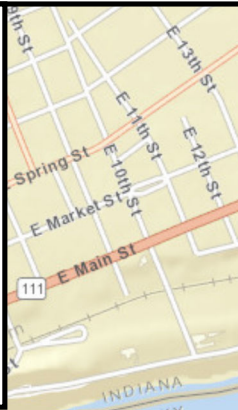
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EXHIBITS



Jefferson County



Jefferson County




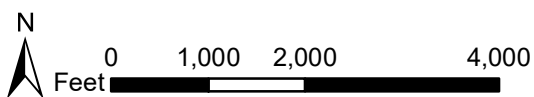
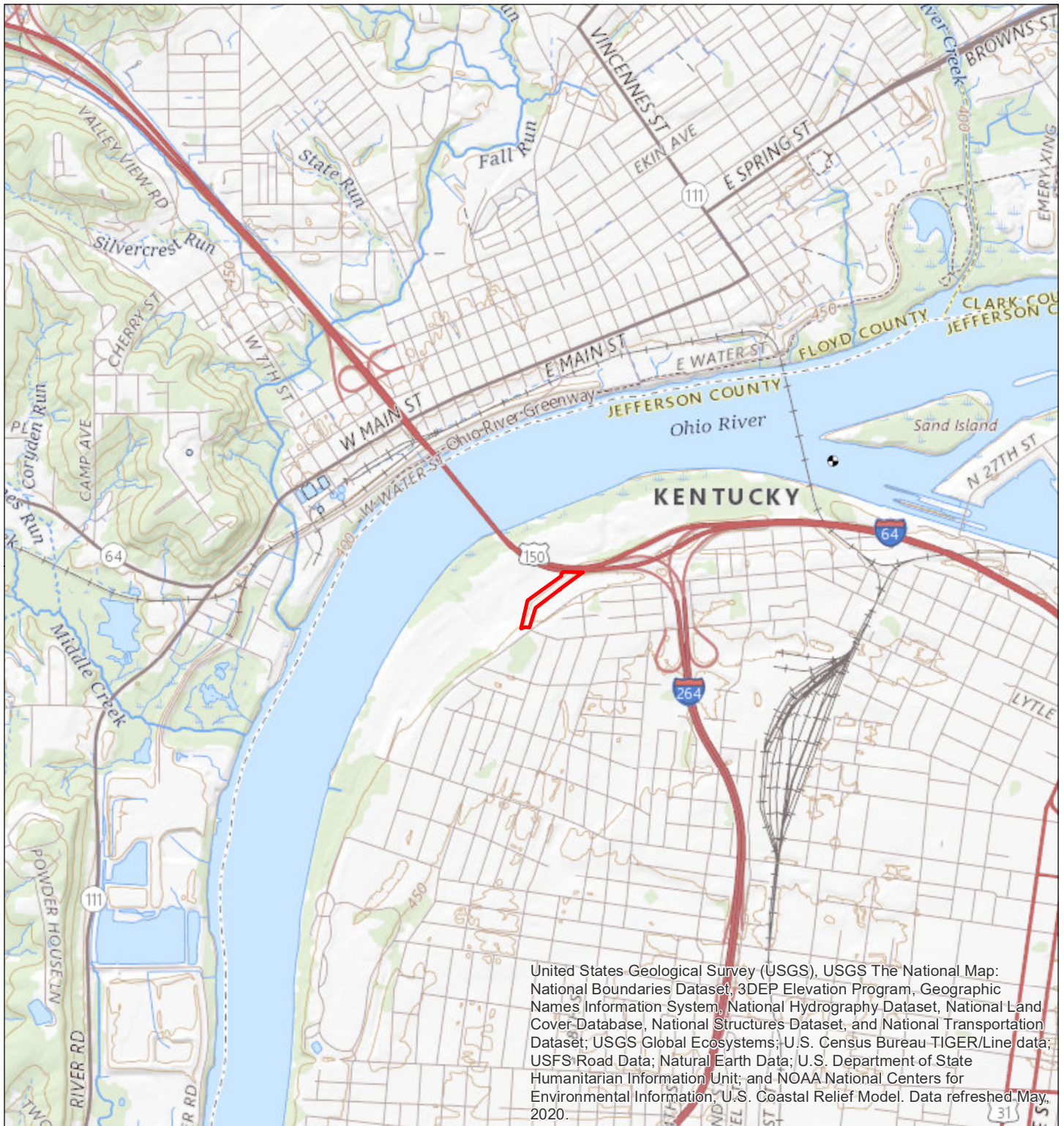
 Project Study Limits (PSL) for Temporary Access Area

Exhibit 1 - Location Map
 Sherman Minton Corridor Project
 City of Louisville, Jefferson County, Kentucky
 Des. No. 1702255
 Metric Project No. 20-0201
 Map Date: 3/23/2021
 Map Author: Cory Shumate

All locations approximate
 2018 Basemap
 Latitude: 38.27357 Longitude: -85.81697



Exh. 1



United States Geological Survey (USGS), USGS The National Map: National Boundaries Dataset, 3DEP Elevation Program, Geographic Names Information System, National Hydrography Dataset, National Land Cover Database, National Structures Dataset, and National Transportation Dataset; USGS Global Ecosystems; U.S. Census Bureau TIGER/Line data; USFS Road Data; Natural Earth Data; U.S. Department of State Humanitarian Information Unit; and NOAA National Centers for Environmental Information; U.S. Coastal Relief Model. Data refreshed May, 2020.


 Project Study Limits (PSL) for Temporary Access Area

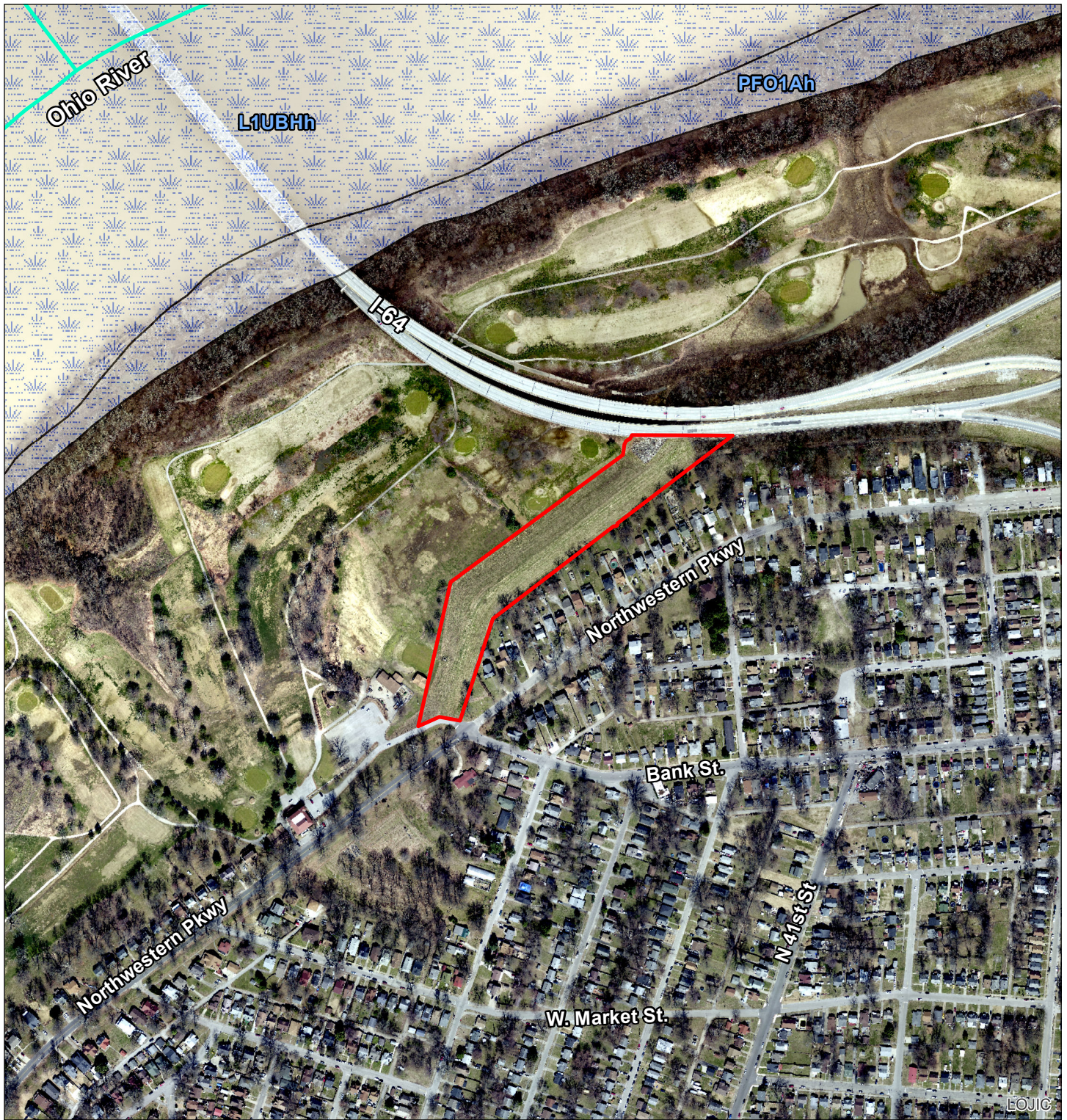
Exhibit 2A - USGS Topographic Map - Small Scale
 West Louisville, IN 7.5 minute Quadrangle
 Sherman Minton Corridor Project
 City of Louisville, Jefferson County, Kentucky
 Des. No. 1702255
 Metric Project No. 20-0201
 Map Date: 3/23/2021
 Map Author: Cory Shumate

All locations approximate

Source: United States Geological Survey (2020)



Exh. 2A



Project Study Limits (PSL) for Temporary Access Area

— NHD Flowline

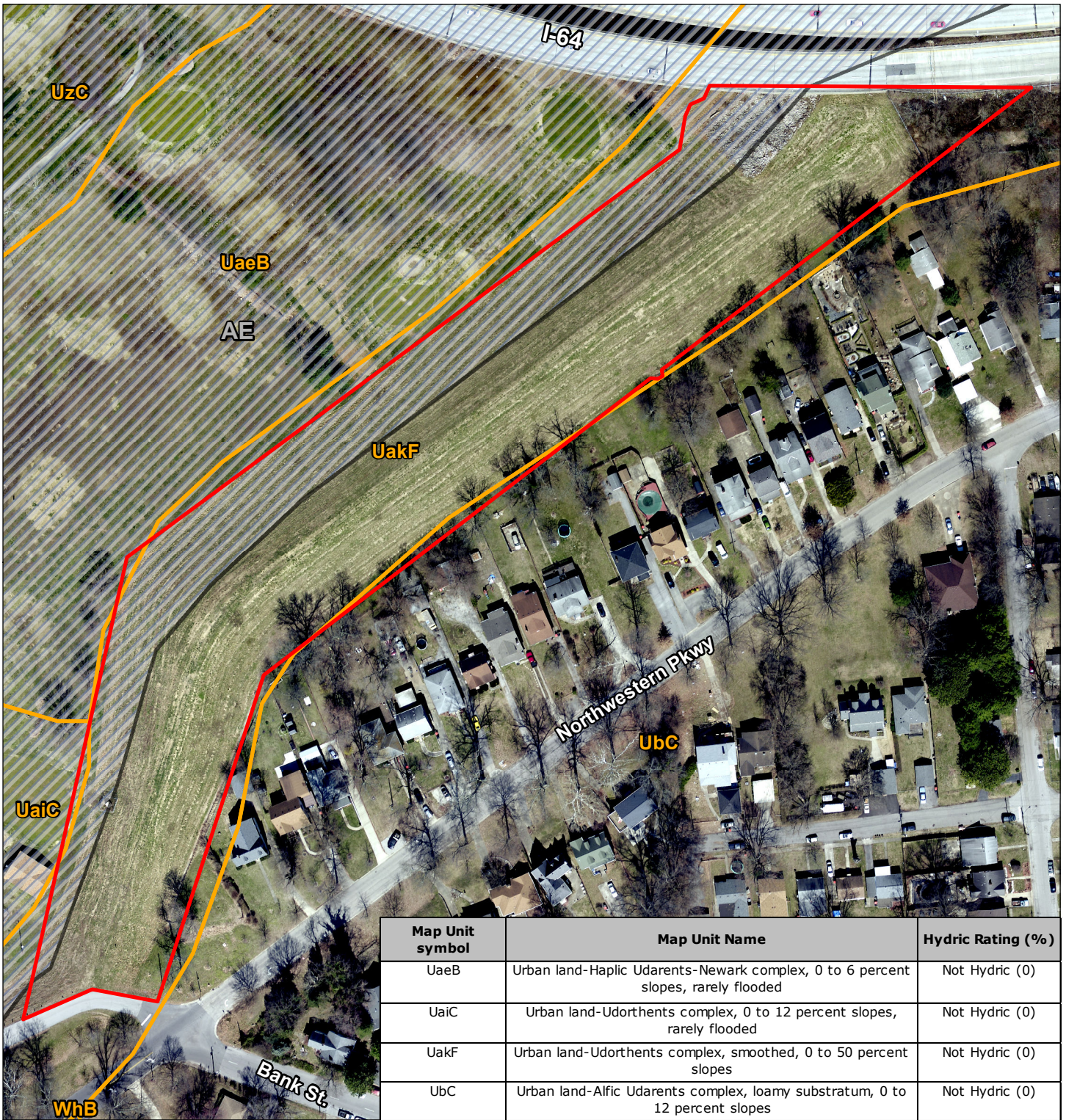
 NWI Wetlands

Exhibit 3 - NWI Wetland and NHD Flowline Map
 Sherman Minton Corridor Project
 City of Louisville, Jefferson County, Kentucky
 Des. No. 1702255
 Metric Project No. 3/23/2021
 Map Date: 3/23/2021
 Map Author: Cory Shumate

All locations approximate
 Source: Louisville/Jefferson County
 Information Consortium (LOJIC)(2019)



Exh. 3



| Map Unit symbol | Map Unit Name | Hydric Rating (%) |
|-----------------|--|-------------------|
| UaeB | Urban land-Haplic Udarents-Newark complex, 0 to 6 percent slopes, rarely flooded | Not Hydric (0) |
| UaiC | Urban land-Udorthents complex, 0 to 12 percent slopes, rarely flooded | Not Hydric (0) |
| UakF | Urban land-Udorthents complex, smoothed, 0 to 50 percent slopes | Not Hydric (0) |
| UbC | Urban land-Alfic Udarents complex, loamy substratum, 0 to 12 percent slopes | Not Hydric (0) |

- Project Study Limits (PSL) for Temporary Access Area
- NRCS Soil Survey
- FEMA Floodplain - Zone AE - 1% Annual Chance

Exhibit 4 - NRCS Soil Survey and FEMA Flood Insurance Rate Map (FIRM)
 Sherman Minton Renewal Corridor
 City of Louisville, Jefferson County, Kentucky
 Des. No. 1702255
 Metric Project No. 3/23/2021
 Map Date: 3/23/2021
 Map Author: Cory Shumate

All locations approximate
 Source: Louisville/Jefferson County Information Consortium (LOJIC)(2019)

N
 0 100 200 400
 Feet



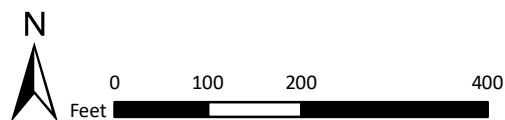
Exh. 4



- Project Study Limits (PSL) for Temporary Access Area
- Culvert (CV)
- Stream
- Sampling Point (SP)
- Roadside Ditch (RSD)
- Wetland

Exhibit 5 - Waters Delineation & Photo Location Map
 Sherman Minton Corridor Project
 City of Louisville, Jefferson County, Kentucky
 Des. No. 1702255
 Metric Project No. 20-0201
 Map Date: 3/23/2021
 Map Author: Cory Shumate

All locations approximate
 Source: Louisville/Jefferson County
 Information Consortium (LOJIC)(2019)



Exh. 5

APPENDIX A

Wetland Determination Data Sheets

WETLAND DETERMINATION DATA FORM -- Eastern Mountains and Piedmont Region

Project/Site: Des. No. 1702255 - Sherman Minton Corridor Project City/County: Louisville / Jefferson Sampling Date: 3/19/2021
 Applicant/Owner: Kentucky Transportation Cabinet State: KY Sampling Point: SP-1
 Investigator(s): Cory Shumate & Samantha Wickizer Section, Township, Range: N/A
 Landform (hillslope, terrace, etc.): Hillslope Local relief (concave, convex, none): Convex Slope (%): 1
 Subregion (LRR or MLRA): LRR Lat: 38.27479 Long: -85.816159 Datum: NAD83
 Soil Map Unit Name: Urban land-Udorthents complex, loamy substratum, 0 to 12 percent slopes - Not Hydric NWI classification: None

Are climatic / hydrologic conditions on the site typical for this time of year? Yes No (If no, explain in Remarks.)
 Are Vegetation No, Soil No, or Hydrology No significantly disturbed? Are "Normal Circumstances" present? Yes No
 Are Vegetation No, Soil No, or Hydrology No naturally problematic? (If needed, explain any answers in Remarks.)

SUMMARY OF FINDINGS -- Attach site map showing sampling point locations, transects, important features, etc.

| | | | | | |
|---------------------------------|---|--|--|------------------------------|--|
| Hydrophytic Vegetation Present? | Yes <input type="checkbox"/> | No <input checked="" type="checkbox"/> | Is the Sampled Area within a Wetland? | Yes <input type="checkbox"/> | No <input checked="" type="checkbox"/> |
| Hydric Soil Present? | Yes <input checked="" type="checkbox"/> | No <input type="checkbox"/> | | | |
| Wetland Hydrology Present? | Yes <input type="checkbox"/> | No <input checked="" type="checkbox"/> | | | |

Remarks:
 Upland Sampling Point 1 (SP-1). SP-1 was taken due to its location within the Q100 of the Ohio River.

HYDROLOGY

| Wetland Hydrology Indicators: | |
|---|---|
| Primary Indicators (minimum of one is required: check all that apply) | Secondary Indicators (minimum of two required) |
| <input type="checkbox"/> Surface Water (A1) | <input type="checkbox"/> True Aquatic Plants (B14) |
| <input type="checkbox"/> High Water Table (A2) | <input type="checkbox"/> Hydrogen Sulfide Odor (C1) |
| <input type="checkbox"/> Saturation (A3) | <input type="checkbox"/> Oxidized Rhizospheres on Living Roots (C3) |
| <input type="checkbox"/> Water Marks (B1) | <input type="checkbox"/> Presence of Reduced Iron (C4) |
| <input type="checkbox"/> Sediment Deposits (B2) | <input type="checkbox"/> Recent Iron Reduction in Tilled Soils (C6) |
| <input type="checkbox"/> Drift Deposits (B3) | <input type="checkbox"/> Thin Muck Surface (C7) |
| <input type="checkbox"/> Algal Mat or Crust (B4) | <input type="checkbox"/> Surface Soil Cracks (B6) |
| <input type="checkbox"/> Iron Deposits (B5) | <input type="checkbox"/> Sparsely Vegetated Concave Surface (B8) |
| <input type="checkbox"/> Inundation Visible on Aerial Imagery (B7) | <input type="checkbox"/> Drainage Patterns (B10) |
| <input type="checkbox"/> Water-Stained Leaves (B9) | <input type="checkbox"/> Moss Trim Lines (B16) |
| <input type="checkbox"/> Aquatic Fauna (B13) | <input type="checkbox"/> Dry-Season Water Table (C2) |
| | <input type="checkbox"/> Crayfish Burrows (C8) |
| | <input type="checkbox"/> Saturation Visible on Aerial Imagery (C9) |
| | <input type="checkbox"/> Stunted or Stressed Plants (D1) |
| | <input type="checkbox"/> Geomorphic Position (D2) |
| | <input type="checkbox"/> Shallow Aquitard (D3) |
| | <input type="checkbox"/> Microtopographic Relief (D4) |
| | <input type="checkbox"/> FAC-Neutral Test (D5) |

| | |
|--|---|
| Field Observations: | Wetland Hydrology Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> |
| Surface Water Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____ | |
| Water Table Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____ | |
| Saturation Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____ (includes capillary fringe) | |

Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available:

Remarks:

VEGETATION -- Use scientific names of plants.

Sampling Point: **SP-1**

| | Absolute % Cover | Dominant Species? | Indicator Status | |
|--|------------------|--------------------------------|------------------|---|
| Tree Stratum (Plot size: <u>30' radius</u>) | | | | |
| 1. _____ | _____ | _____ | _____ | Dominance Test worksheet: Number of Dominant Species That Are OBL, FACW, or FAC: <u>1</u> (A) Total Number of Dominant Species Across All Strata: <u>3</u> (B) Percent of Dominant Species That Are OBL, FACW, or FAC: <u>33%</u> (A/B) |
| 2. _____ | _____ | _____ | _____ | |
| 3. _____ | _____ | _____ | _____ | |
| 4. _____ | _____ | _____ | _____ | |
| 5. _____ | _____ | _____ | _____ | |
| 6. _____ | _____ | _____ | _____ | |
| 7. _____ | _____ | _____ | _____ | |
| 50% of total cover: <u>0%</u> | | 20% of total cover: <u>0%</u> | | |
| Sapling/Shrub Stratum (Plot size: <u>15' radius</u>) | | | | |
| 1. <i>Fraxinus pennsylvanica</i> | 5% | Yes | FACW | Prevalence Index worksheet: Total % Cover of: _____ Multiply by: _____ That Are OBL, FACW, or FAC: _____ A/B OBL species _____ x1 = _____ FACW species <u>5%</u> x2 = <u>0.1</u> FAC species <u>5%</u> x3 = <u>0.15</u> FACU species <u>107%</u> x4 = <u>4.28</u> UPL species _____ x5 = _____ Column Totals: <u>1.17</u> (A) <u>4.53</u> (B) Prevalence Index = B/A = <u>3.87</u> |
| 2. <i>Quercus rubra</i> | 2% | Yes | FACU | |
| 3. _____ | _____ | _____ | _____ | |
| 4. _____ | _____ | _____ | _____ | |
| 5. _____ | _____ | _____ | _____ | |
| 50% of total cover: <u>4%</u> | | 20% of total cover: <u>1%</u> | | |
| Herb Stratum (Plot size: <u>5' radius</u>) | | | | |
| 1. <i>Schedonorus arundinaceus</i> | 95% | Yes | FACU | Hydrophytic Vegetation Indicators: <input type="checkbox"/> 1-Rapid Test for Hydrophytic Vegetation <input type="checkbox"/> 2-Dominance Test is >50% <input type="checkbox"/> 3-Prevalence Index is ≤3.0 ¹ <input type="checkbox"/> 4-Morphological Adaptations ¹ (Provide supporting data in Remarks or on a separate sheet) <input type="checkbox"/> Problematic Hydrophytic Vegetation ¹ (Explain) ¹ Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic. |
| 2. <i>Glechoma hederacea</i> | 10% | No | FACU | |
| 3. <i>Rumex crispus</i> | 5% | No | FAC | |
| 4. _____ | _____ | _____ | _____ | |
| 5. _____ | _____ | _____ | _____ | |
| 6. _____ | _____ | _____ | _____ | |
| 7. _____ | _____ | _____ | _____ | |
| 8. _____ | _____ | _____ | _____ | |
| 9. _____ | _____ | _____ | _____ | |
| 10. _____ | _____ | _____ | _____ | |
| 11. _____ | _____ | _____ | _____ | |
| 50% of total cover: <u>55%</u> | | 20% of total cover: <u>22%</u> | | |
| Woody Vine Stratum (Plot size: <u>30' radius</u>) | | | | |
| 1. _____ | _____ | _____ | _____ | Hydrophytic Vegetation Present? Yes <u> </u> No <u>X</u> |
| 2. _____ | _____ | _____ | _____ | |
| 3. _____ | _____ | _____ | _____ | |
| 4. _____ | _____ | _____ | _____ | |
| 5. _____ | _____ | _____ | _____ | |
| 6. _____ | _____ | _____ | _____ | |
| 50% of total cover: <u>0%</u> | | 20% of total cover: <u>0%</u> | | |

Remarks: (Include photo numbers here or on a separate sheet.)

APPENDIX E:
Red Flag and Hazardous Materials

Susan Castle

Subject: FW: RFI Addendum Question - Des 1702255 - Sherman Minton Bridge Renewal Project - I-64 over Ohio River - Floyd County

From: Foheybreting, Nicole K <NFoheyBreting@indot.IN.gov>
Sent: Wednesday, June 16, 2021 3:23 PM
To: Colin Keith <colink@metricenv.com>
Cc: Susan Castle <susanc@metricenv.com>; samantha2@metricenv.com
Subject: RE: RFI Addendum Question - Des 1702255 - Sherman Minton Bridge Renewal Project - I-64 over Ohio River - Floyd County

Greetings Colin –

Thank you for the email. It does not appear as though an RFI Addendum is warranted.

Thank you,
Nicole

Nicole Fohey-Breting

Site Assessment & Management (SAM) Specialist

100 North Senate Avenue N758-ES

Indianapolis, Indiana 46204

Office: (317) 416-7084

Email: NFoheyBreting@indot.in.gov

Office Hours: 8 to 4 PM



The Site Assessment and Management (SAM) Manual can be found at <https://www.in.gov/indot/4170.htm>

Be sure to refer to the updated information in the SAM Manual for document preparation and submission.

From: Colin Keith <colink@metricenv.com>
Sent: Friday, June 04, 2021 1:41 PM
To: INDOT esd.sam <esd.sam@indot.IN.gov>
Cc: Susan Castle <Susanc@metricenv.com>; Samantha Wickizer <samanthaw@metricenv.com>
Subject: RFI Addendum Question - Des 1702255 - Sherman Minton Bridge Renewal Project - I-64 over Ohio River - Floyd County

****** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ******

Marlene and Nicole – The original RFI for this project was signed by Nicole on April 1, 2019; as it is more than a year old, we conducted a review of the findings to evaluate any changes. I made new RFI graphics and compared the features to the 2019 RFI, as outlined below. Both the 2019 RFI and the new maps are attached for your reference. The project area

has not changed significantly since the 2019 RFI, apart from an addition on the Kentucky side that is beyond the scope of this addendum review.

- Infrastructure: no new features within the 0.5 mile radius.
- Water Resources: minor changes to NWI line and wetland feature counts within the 0.5 mile radius, but no new resources within or near the project area. The Waters of the US Report for this project has been completed.
- Mining and Mineral Resources: no change – still no features within the 0.5 mile radius.
- Hazardous material Concerns: minor change to feature counts within the 0.5 mile radius (Brownfields sites (8 instead of 9), Institutional Control sites (7 instead of 5), NPDES facilities (4 instead of 3), and NPDES pipes (5 instead of 0)). No impact or additional coordination would be expected (none of the new listings are near the project area).

Based on the reviewed information, we do not believe an addendum to the 2019 RFI would be necessary. Would you concur?



Colin Keith
Project Scientist

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M 317.413.9090

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Certified DBE/MBE/SBE Company
Indiana | Kentucky | Ohio | West Virginia

APPENDIX F:
Section 4(f) and 6(f)

Sherman Minton Corridor Project

Section 106 & 4(f) & 6(f) on-site Meeting Summary

Location: Shawnee Golf Course, Project Corridor
Date: June 29, 2021
Time: 10:00 AM – 12:00 PM EST

Attendance

KYTC: Royce Meredith, Dave Harmon, Amanda Abner, and Connor Ouellette, Tyler Reynolds;
KY SHPO: Craig Potts and Gabrielle Fernandez; Kokosing: Brad Young; Metric Environmental: Samantha Wickizer, Beth Hillen, and Candace Hudziak; Metro Parks: Nathan Maiwald and Chris Gayheart

Meeting Summary

The purpose of this meeting was to have an on-site discussion between KYTC, KY SHPO and the Design Build Team in order to determine the path forward in obtaining Section 106, Section 4(f) and Section 6(f) approvals prior to commencing with construction activities of the Sherman Minton Corridor Project. Contact details for all attendees is attached at the end of this document

Section 106 Concerns

1. KY SHPO outlined that the entire floodwall is in the process of being determined as an eligible historic resource.
 - a. Levee access now needs to be included in the Section 106 piece of the NEPA Additional Information (AI) document.
 - i. Metric and Kokosing outlined to KY SHPO the plans for accessing the project via Northwestern Parkway (which is also an eligible historic resource) and the plans to construct the levee access road.
 1. The levee access road will be approximately 12 ft. wide to provide access beneath the KY approach structures of the Sherman Minton bridge in order to preform structural rehabilitation.
 2. Stripping of approximately 8" of topsoil for storage, temporary stabilization, and replacement upon completion of the project.
 3. Upon removal of topsoil, geotextile fabric shall be installed to provide a barrier between stone application and the levee soil
 4. Approximately 12" of temporary aggregate consisting of 4" INDOT Type 9 Coarse Aggregate over 8" of INDOT Type 5 Coarse Aggregate shall be installed to provide for heavy machinery access along the levee and into the project corridor.

5. Additional fill material shall be utilized to facilitate the extension of the access road from the top of the levee down the slope and into the project corridor.
 6. Upon completion of the project all temporary fill materials shall be removed, the topsoil will be replaced, and the area will be permanently stabilized with seed and mulch.
- ii. Throughout construction watering trucks shall be used to minimize the potential for dust generation.
 - iii. A construction traffic summary is to be provided within the Section 106 report outlining the type of equipment that will utilize the levee access as well as potential volume of traffic per day associated with the following phases of construction activities:
 1. Construction of the initial levee access road and aggregate pads beneath the approach structures
 2. Normal daily operations
 3. Hydro demolition activities
 4. Removal of the access road and stabilization
2. Northwestern Parkway
- a. Construction traffic shall utilize Northwestern Parkway between I -264 and the Shawnee Golf Course to access the project.
 - b. A pre-construction evaluation of the roadway including photo documentation is to be included in the Section 106 report provided to the KY SHPO.
 - c. Following completion of the project, Northwestern Parkway shall be restored to pre-existing condition as documented in the pre-construction evaluation.
3. Shawnee Park Golf Course
- a. The project has taken every path to minimize the potential impacts to the golf course.
 - b. The levee access road will have a temporary impact along the southern corner that abuts I-64 and the levee where the access road drops into the project corridor, but the area will be restored as described above for the levee access road.
 - c. There are two cart paths that pass under the approach spans between holes 1 and 2, and between holes 7 and 8, crossing each other under the approach span within the highway ROW. The cart paths shall be maintained throughout construction by merging before entering the work zone, and a single path will pass under the approach span in a protective tunnel.
 - d. Upon completion of construction, the cart paths shall be restored to their pre-construction condition.
4. In regards to completion of the Section 106 documentation, the Design Build team is waiting on direction from KYTC-Amanda Abner as to how the documentation and finding of effect can be submitted in the most efficient manner possible.
 5. In addition, KYTC is to provide the Design Build team with documentation relating to previous coordination with Section 106 consulting parties, if it was conducted.

Section 4(f) and 6(f) Concerns

1. The Louisville Loop/ Louisville Riverwalk is a Section 4(f) park resource.
 - a. Metro Parks outlined that the trail has been closed to the public since 2009 and shall remain closed throughout the construction project.
 - b. KYTC indicated that 4(f) impact for this resource could be processed as temporary use.
2. The Shawnee Park Golf Course is a Section 4(f) and Section 6(f) park resource.
 - a. As outlined above the impacts to the golf course shall be minimal and shall not disrupt recreational activities.
 - b. KYTC indicated that the 4(f) impact for this resource could be processed as temporary occupancy, and no use from the 6(f) standpoint.

| <u>Name</u> | <u>Company</u> | <u>Phone</u> | <u>Email</u> |
|---------------------|----------------|--------------|--|
| Royce Meredith | KYTC | 502-330-7345 | royce.meredith@ky.gov |
| Dave Harmon | KYTC | 502-577-2800 | Dave.Harmon@ky.gov |
| Amanda Abner | KYTC | 502-782-5006 | Amanda.Abner@ky.gov |
| Tyler Reynolds | KYTC | 502-564-7250 | Tyler.reynolds@ky.gov |
| Connor Ouellette | KYTC | 859-967-8221 | Connor.ouellette@ky.gov |
| Craig Potts | KY SHPO | 502-330-8362 | Craig.potts@ky.gov |
| Gabrielle Fernandez | KY SHPO | | Gabrielle.fernandez@ky.gov |
| Brad Young | Kokosing | 614-989-2043 | gby@kokosing.biz |
| Samantha Wickizer | Metric | 317-608-2798 | samanthaw@metricenv.com |
| Beth Hillen | Metric | 317-617-0296 | bethh@metricenv.com |
| Candace Hudziak | Metric | 317-782-5006 | candaceh@metricenv.com |
| Nathan Maiwald | Metro Parks | 502-287-9167 | Nathan.maiwald@louisvilleky.gov |
| Chris Gayheart | Metro Parks | 502-681-4779 | Christian.gayheart@louisvilleky.gov |



U.S. Department
of Transportation
**Federal Highway
Administration**

Kentucky Division

August 2, 2021

330 West Broadway
Frankfort, KY 40601
PH (502) 223-6720
FAX (502) 223-6735
<http://www.fhwa.gov/kydiv>

In Reply Refer To:
HDA-KY

Mr. Jason T. Canuel
Assistant Director
Louisville Parks and Recreation Department
P.O. Box 37280
Louisville, KY 40233

Subject: Temporary Section 4(f) Use Determination for Shawnee Park Golf Course
Sherman Minton Bridge Rehabilitation Project
Jefferson County, Kentucky
KYTC Item No. 5-10027

Dear Mr. Canuel:

We have reviewed information provided by the Kentucky Transportation Cabinet (KYTC) and Indiana Department of Transportation (INDOT) regarding the project's proposed temporary occupancy, to reroute certain golf cart paths under the Sherman Minton Bridge on the Shawnee Park Golf Course, and minor impacts to the Louisville Riverwalk/Louisville Loop Trail.

The temporary impacts to the golf cart paths will occur between holes 1 and 2, and between holes 7 and 8, which cross under a bridge approach span within the existing KYTC Highway right-of-way. Impacts will occur to approximately 5,500 square feet (0.13 ac) of cart path. Golfers access to and use of the cart paths will be maintained during construction. The cart paths will merge before entering the work zone. A single gravel cart path will pass through a temporary tunnel to protect golfers from the overhead bridge rehabilitation work. The cart paths will diverge to their respective holes on the other side of the work zone. All changes to the cart path configuration will occur within the existing highway right-of-way. Access to the golf course will be maintained at all times that the course is in operation. The temporary impacts will occur between August 2021 and June 2023.

Temporary impacts will also occur to the Louisville Riverwalk/ Louisville Loop Trail, which also passes under a bridge approach span within the existing KYTC highway right-of-way. The Louisville Riverwalk is not currently maintained at this location. It has been closed to public use at since 2010, with barricades across the trail. Access to the closed trail will not change during the construction. Impacts totaling approximately 640 square feet will occur between August 2021 and June 2023.

We are requesting your concurrence with our determination that the project meets the Section 4(f) temporary occupancy exemption under 23 CFR 774.13(d) because:

- (1) The duration is temporary, less than the time needed for construction of the project, and there is no change in the ownership of the land;
- (2) the scope of the work is minor, in both nature and the magnitude, and the changes to the Section 4(f) properties (cart path and trail) are minimal;
- (3) There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis; and
- (4) The land being used will be fully restored. The property will be returned to a condition which is at least as good as that which existed prior to the project.

Please concur with our determination of temporary occupancy. Please sign on the concurrence line below and return it to me via email. Please contact me at (502) 223-6747 or via email at John.Ballantyne@dot.gov at your earliest convenience if you have any questions.

Sincerely,

JOHN D
BALLANTYNE

Digitally signed by JOHN
D BALLANTYNE
Date: 2021.08.02
12:00:24 -04'00'

John D. Ballantyne
System Performance Team Leader

Sent via email: Jason.Canuel@Louisvilleky.gov

cc: Mr. Dana Kasler, Director (Dana.Kasler@Louisvilleky.gov)
Mr. Dave Harmon, KYTC (Dave.Harmon@ky.gov)

Jason T.
Canuel

Digitally signed by Jason
T. Canuel
Date: 2021.08.02
12:20:37 -04'00'

Concurrence by: _____ Date: _____
Louisville Parks and Recreation Department



City of New Albany, Indiana
Shane L. Gibson, Corporate Counsel

August 10, 2021

Daniel Corbin
Project Manager
Indiana Department of Transportation
100 N. Senate Ave., N758-MPD
Indianapolis, IN 46204

RE: Sherman Minton Corridor Project
INDOT Des. No. 1702255 (Lead Des.)
New Albany, Floyd County, Indiana
City of Louisville, Jefferson County, Kentucky

Dear Mr. Corbin:

The City of New Albany understands that the rehabilitation of the Sherman Minton Bridge and approach structures will require minor and temporary impacts to the Levee Trail and Ohio River Greenway Trail, publicly owned recreational resources meeting the applicability requirements of Section 4(f) of the U.S. Department of Transportation [DOT] Act of 1966. The Levee Trail and Ohio River Greenway Trail are maintained as public recreational resources by the City of New Albany.

A section of the Levee Trail and Ohio River Greenway Trail is located within the highway right-of-way (ROW), and will require the closure of the trails for equipment staging, for a portion of the construction period. The trails currently end under the highway, within the highway ROW. The portion of both trails within the ROW, approximately the last 70 feet of the trails, will be closed from July 2021 until June 2023. The full duration of construction is from July 2021 to August 2023. There will be no change of ownership and no permanent changes will occur to the trail facilities.

We understand examples of *de minimis* impact to a Section 4(f) land include right-of-entry, project construction, a temporary easement, or other short-term arrangement involving a Section 4(f) property. A Section 4(f) *de minimis* impact finding is applicable under 23 CFR 774.3 (b) when the Administration (in this case Federal Highway Administration [FHWA]) determines that the use of the property, including any measure(s) to minimize harm (such as any avoidance, minimization, mitigation, or enhancement measures) committed to by the applicant (in this case


the Indiana Department of Transportation [INDOT]), will have a *de minimis* impact, as defined in § 774.17 on the property. That definition for parks, recreational areas, and wildlife refuges is as follows:

(2) For parks, recreation areas, and wildlife and waterfowl refuges, a *de minimis* impact is one that will not adversely affect the features, attributes, or activities qualifying the property for protection under Section 4(f).

In accordance with 23 CFR 774.5(b)(2), a public notice was placed in the New Albany News and Tribune on July 13, 2021, with a 14-day comment period ending on July 26, 2021. No comments were received from the public.

The City of New Albany understands that the *de minimis* uses of the Section 4(f) resources are necessary and is of the opinion that such work will not permanently adversely affect the activities, features, or attributes of the recreational resources.

The City of New Albany concurs with the FHWA Section 4(f) *de minimis* finding for these resources, for this project.

 8/10/21
Concurrence By: Date:

City of New Albany

**Proof of
Publication**

**STATE OF INDIANA
COUNTY OF FLOYD - SS**

Theresa Wheatbrook says on oath that she is the
bookkeeper of NEWS AND TRIBUNE and in
the employ of the publisher of

NEWS AND TRIBUNE,

a daily newspaper of general circulation printed and
published in the city of New Albany, Floyd County
State of Indiana, and further says that the annexed
advertisement was published in said paper for
#(1) time(s) to-wit: In issue of said NEWS AND TRIBUNE
Dated: 07-13 2021

(X) Theresa Wheatbrook

**STATE OF INDIANA
COUNTY OF FLOYD**

Subscribed and sworn to before me this

13TH day of July 2021

(X) Joann Galligan
Joann Galligan



Notary Public, Floyd County, Indiana
(My Commission Expires August 27, 2022)
Commission Number 655965

**Publication
Fee \$ 357.46**

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PUBLIC NOTICE

In compliance with Section 4(f) of the U.S. Department of Transportation Act of 1966 (49 USC 303), hereafter referred to as "Section 4(f)" of 23 CFR 774.5(b)(2), the public is being notified through this notice that the Indiana Department of Transportation (INDOT) will request the Federal Highway Administration (FHWA) to make a de minimis (or minor) impact finding in accordance with Section 4(f) regarding the temporary effects of the Levee Trail and the Ohio River Greenway Trail, within the Sherman Minton Corridor Project (SMCP). The project location is the I-64 Interstate and US 150 (I-64) over the Ohio River, the Sherman Minton Bridge, connecting New Albany, Indiana and the west side of Louisville, Kentucky. Public notice and opportunity for public review and comment on the finding is required.

Project elements include bridge deck replacements and bridge deck overlays, structural repairs, replacement lighting, bridge painting, local streets Hot Mix Asphalt (HMA) overlay, Americans with Disabilities Act (ADA) ramp reconstruction, and interstate ramp frictionalization. SMCP is a joint effort between INDOT and the Kentucky Transportation Cabinet (KYTC). INDOT is leading SMCP in close collaboration with KYTC.

Access beneath the Indiana approach bridges will be needed for painting equipment staging. Access will be attained via local streets. A temporary coarse aggregate pad approximately 12" in depth over geotextile fabric will be installed beneath the approach structures within the right-of-way (ROW) adjacent to the Ohio River resulting in 536 cubic yards of stone over 0.55 acre of temporary fill within the Ohio River Floodway. The temporarily impacted area will be restored using Seed Mix, Type Floodplain, per INDOT Standard Specifications. There will be no tree removal as part of the project. No permanent impacts shall occur in association with this work.

The proposed work would involve sections of the Levee Trail and the Ohio River Greenway Trail, located within the highway ROW, and will require the closure of the trails for a portion of the construction period. The trails currently end under the highway, within the highway ROW. The portions of the trails within the ROW, approximately the last 70 feet of the trails, will be closed from July 2021 to June 2023. The full duration of construction is from July 2021 to August 2023. There will be no change of ownership and no permanent changes will occur to the trail facilities. Although the proposed construction activities are considered a "use" as defined by Section 4(f) of the U.S. Department of Transportation Act (1966), there would be no negative impact on the activities, features and attributes that qualify the recreation areas for protection under Section 4(f).

Indiana Department of Transportation has coordinated with the City of New Albany, Parks and Recreation, officials with jurisdiction over the Levee Trail and the Ohio River Greenway Trail. The public is requested to provide comments on this finding. Information on this proposed project is available by contacting Daniel Corbin, at the following:

Indiana Department of Transportation
100 N. Senate Ave., N758-MPD
Indianapolis, IN 46204
(317)914-4977
E-Mail: dcorbin@indot.in.gov

Comments must be submitted, in writing, to Daniel Corbin, Indiana Department of Transportation during the 14-day comment period. The comment deadline is July 26, 2021. Comments will be discussed with the officials with jurisdiction before concurrence is requested and will become part of the environmental document. hspaxlp

Susan Castle

Subject: FW: Sherman Minton Bridge Rehab

From: Harmon, Dave L (KYTC) <Dave.Harmon@ky.gov>

Sent: Thursday, August 5, 2021 9:34 AM

To: Samantha Wickizer <samanthaw@metricenv.com>; Meredith, Royce J (KYTC) <Royce.Meredith@ky.gov>; Rothermel, Eric (FHWA) <eric.rothermel@dot.gov>; Harmon, Dave L (KYTC) <Dave.Harmon@ky.gov>

Cc: Corbin, Daniel <DCorbin@indot.IN.gov>; Mary Jo Hamman <mhamman@mbakerintl.com>; Vince Epps <vincee@metricenv.com>; Luella Beth Hillen <bethh@metricenv.com>; Susan Castle <susanc@metricenv.com>; vpm@kokosing.biz; Oliver Bluestone <obluestone@kokosing.biz>; Kokal, Jeff <Jeff.Kokal@jacobs.com>; Morris, Daniel <Daniel.Morris@jacobs.com>

Subject: RE: Sherman Minton Bridge Rehab

Yes. I don't see the need to get a signed letter.

David L. Harmon P.G.
Assistant Director
Division of Environmental Analysis
Office – 502-782-5016
Cell – 502-517-2800

From: Samantha Wickizer <samanthaw@metricenv.com>

Sent: Thursday, August 5, 2021 9:23 AM

To: Harmon, Dave L (KYTC) <Dave.Harmon@ky.gov>; Meredith, Royce J (KYTC) <Royce.Meredith@ky.gov>; Rothermel, Eric (FHWA) <eric.rothermel@dot.gov>

Cc: Corbin, Daniel <DCorbin@indot.IN.gov>; Mary Jo Hamman <mhamman@mbakerintl.com>; Vince Epps <vincee@metricenv.com>; Luella Beth Hillen <bethh@metricenv.com>; Susan Castle <susanc@metricenv.com>; vpm@kokosing.biz; Oliver Bluestone <obluestone@kokosing.biz>; Kokal, Jeff <Jeff.Kokal@jacobs.com>; Morris, Daniel <Daniel.Morris@jacobs.com>

Subject: RE: Sherman Minton Bridge Rehab

Good morning Dave,

Following up to this discussion in our call. The email concurrence below will suffice for the KY 6(f) resource task within the NEPA AI in lieu of an official letter with signature?

Thank you,



Samantha Wickizer, CESSWI

Project Manager
M 317.608.2798

6958 Hillside Court
Indianapolis, IN 46250
www.metricenv.com

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From: Harmon, Dave L (KYTC) <Dave.Harmon@ky.gov>

Sent: Tuesday, July 20, 2021 1:00 PM

To: Meredith, Royce J (KYTC) <Royce.Meredith@ky.gov>; Rothermel, Eric (FHWA) <eric.rothermel@dot.gov>; Samantha Wickizer <samanthaw@metricenv.com>; Harmon, Dave L (KYTC) <Dave.Harmon@ky.gov>

Subject: FW: Sherman Minton Bridge Rehab

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Looks like we have DLG concurrence.

David L. Harmon P.G.
Assistant Director
Division of Environmental Analysis
Office – 502-782-5016
Cell – 502-517-2800

From: Hill, Jessica M (DLG) <JessicaM.Hill@ky.gov>

Sent: Tuesday, July 20, 2021 12:57 PM

To: Harmon, Dave L (KYTC) <Dave.Harmon@ky.gov>; Rothermel, Eric (FHWA) <eric.rothermel@dot.gov>

Subject: RE: Sherman Minton Bridge Rehab

Oh, yes! By your description, there will be no conversion. The LWCF Manual states:

Any property so acquired and/or developed shall not be wholly or partly converted to other than public outdoor recreation uses without the approval of NPS pursuant to the LWCF Act.

Since there will be no property loss to the Shawnee Golf Course due to the construction of the access road, we do not have to seek approval from the NPS. If anything changes and the road infringes on the golf course's boundary, please let me know.

Jessica Hill
Federal Program Specialist
Land and Water Conservation Fund
Recreational Trails Program
Department for Local Government
Office: 502-564-2198
Cell: 859-230-0249

I am currently working remotely. Until further notice, email and cell are the best methods to reach me.

From: Harmon, Dave L (KYTC) <Dave.Harmon@ky.gov>

Sent: Tuesday, July 20, 2021 12:38 PM

To: Hill, Jessica M (DLG) <JessicaM.Hill@ky.gov>; Rothermel, Eric (FHWA) <eric.rothermel@dot.gov>; Harmon, Dave L (KYTC) <Dave.Harmon@ky.gov>

Subject: RE: Sherman Minton Bridge Rehab

Good Afternoon Jessica,

Have you had a chance to look this over? Thanks!

David L. Harmon P.G.

Assistant Director

Division of Environmental Analysis

Office – 502-782-5016

Cell – 502-517-2800

From: Harmon, Dave L (KYTC)

Sent: Thursday, July 15, 2021 1:09 PM

To: Hill, Jessica M (DLG) <JessicaM.Hill@ky.gov>; Rothermel, Eric (FHWA) <eric.rothermel@dot.gov>; Harmon, Dave L (KYTC) <Dave.Harmon@ky.gov>

Subject: RE: Sherman Minton Bridge Rehab

Hi Jessica,

I had to confirm to make sure that we won't be encroaching on the golf course property, even if it would be temporary.. It looks like we will be on the levee (owned/operated by the Corps and MSD) then directly onto KYTC ROW, so no impact or use of the golf course property. I hope this makes sense. Feel free to give me a call if you need any clarification. Thanks!

David L. Harmon P.G.

Assistant Director

Division of Environmental Analysis

Office – 502-782-5016

Cell – 502-517-2800

From: Hill, Jessica M (DLG) <JessicaM.Hill@ky.gov>

Sent: Thursday, July 15, 2021 10:08 AM

To: Harmon, Dave L (KYTC) <Dave.Harmon@ky.gov>; Rothermel, Eric (FHWA) <eric.rothermel@dot.gov>

Subject: RE: Sherman Minton Bridge Rehab

Good Morning Dave,

Per Section 6(f) of the LWCF Act, the park developed with LWCF funds becomes protected, not the project area. Please note that even though the grant was specifically for the construction of a maintenance building, the entire golf course is protected under Section 6(f) provisions. However, if the access road does not encroach on the golf course's property resulting in a loss of acreage, there will be no need for conversion.

Please let me know if you have any questions. Thanks!

Jessica Hill

Federal Program Specialist
Land and Water Conservation Fund
Recreational Trails Program
Department for Local Government
Office: 502-564-2198
Cell: 859-230-0249

I am currently working remotely. Until further notice, email and cell are the best methods to reach me.

From: Harmon, Dave L (KYTC) <Dave.Harmon@ky.gov>

Sent: Wednesday, July 14, 2021 10:56 AM

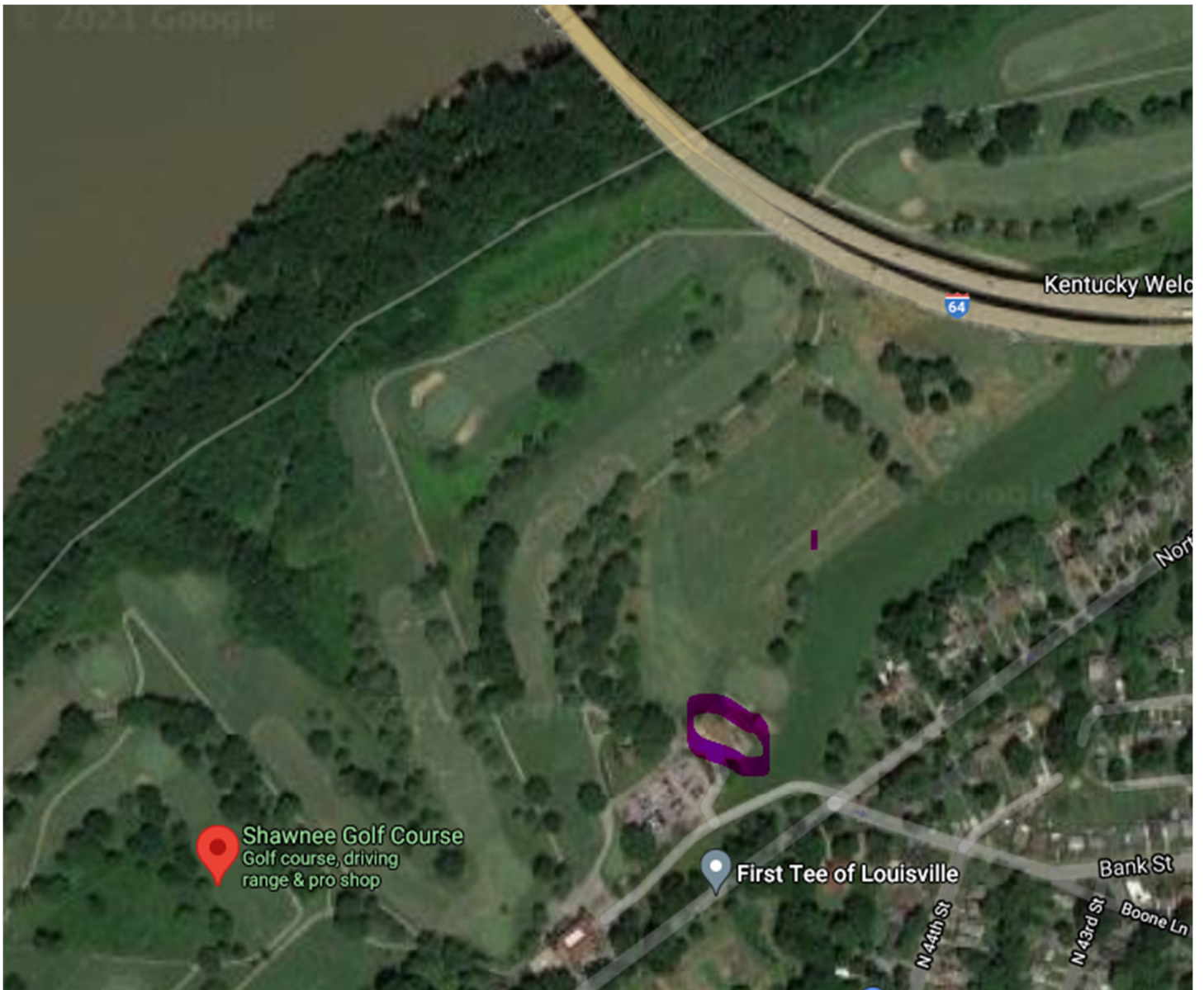
To: Hill, Jessica M (DLG) <JessicaM.Hill@ky.gov>; Rothermel, Eric (FHWA) <eric.rothermel@dot.gov>; Harmon, Dave L (KYTC) <Dave.Harmon@ky.gov>

Subject: Sherman Minton Bridge Rehab

Good Morning Jessica,

We are getting ready to move forward with our Sherman Minton Bridge Rehab project. We have identified one 6(f) funded resource on the Golf Course and it's a maintenance building that we will not be touching. We will be accessing our Right of Way by crossing the levee and driving off of it below the bridge. We have sufficient ROW below the bridge that we should be able to stay off the golf course. Based on our temporary impacts and the 6(f) resources, we think that there will be no conversion. What are your thoughts?

Golf Course and Maintenance Building



Close up of the maintenance building.



Access along the top of the levee to below the bridge.



David L. Harmon P.G.
Assistant Director
Division of Environmental Analysis
Office – 502-782-5016
Cell – 502-517-2800

APPENDIX G:
Approved CE Document

Indiana Department of Transportation

County Floyd County, IN Jefferson County, KY Route I-64 Des. No. Primary Des. No. 1702255


FHWA-Indiana Environmental Document CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM GENERAL PROJECT INFORMATION


| | |
|-------------------------------------|--|
| Road No./County: | I-64 / Floyd County, Indiana - Jefferson County, Kentucky |
| Designation Number: | INDOT Primary Des. No. 1702255, Additional Des. Nos. 1702260, 1702254, 1592187, 1702257, 1702258, 1702259, 1701215, & 1900579. KYTC Item ID 5-64. |
| Project Description/Termini: | I-64 Sherman Minton Bridge Rehabilitation and associated approaches. The project is located at the Interstate I-64 and US 150 Sherman Minton Bridge crossing of the Ohio River in Floyd County, New Albany, Indiana, and in Jefferson County, Louisville, Kentucky. The lead project termini extend from the I-64 / I-264 interchange in Louisville Kentucky 3.5 miles to the northwest to the I-64 / I-265 interchange in New Albany Indiana. |

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

| | |
|----------|---|
| | Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager) |
| | Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services Division) |
| X | Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA |
| | Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA |

Note: For documents prepared by or for Environmental Services Division, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

| | | | |
|----------------------------|---|--|------------------|
| Approval <u>N/A</u> | |  <small>Digitally signed by Brandon Miller Date: 2020.10.05 16:56:02 -04'00'</small> | <u>10/5/2020</u> |
| ESM Signature | Date | INDOT ES Signature | Date |
| |  | <u>10/5/2020</u> | |
| | KYTC DEA Signature | Date | |
| | <u>Erica Tait</u> | <u>10/7/2020</u> | |
| | <small>Digitally signed by Erica Tait Date: 2020.10.07 16:14:16 -04'00'</small> | Date | |
| | FHWA Signature | Date | |

| | | | |
|---------------------------------------|------|--|------------------|
| Release for Public Involvement | |  | <u>6/23/2020</u> |
| ESM Initials | Date | ES Initials | Date |
| | | <u>REB</u> | |

| | | |
|--|--|----------------|
| Certification of Public Involvement |  | <u>10/1/20</u> |
| | Office of Public Involvement | Date |

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

| | |
|--|------------------------|
| INDOT ES/District Env. Reviewer Signature:  | Date: <u>10-5-2020</u> |
|--|------------------------|

Name and Organization of CE/EA Preparer: Mary Pusti, Michael Baker International Inc.

This is page 1 of 54 Project name: I-64 Bridge Rehabilitation Sherman Minton Renewal Project Date: October 2, 2020

Indiana Department of Transportation

County Floyd County, IN Jefferson County, KY Route I-64 Des. No. Primary Des. No. 1702255

Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

| | | |
|---|-------------------------------------|-------------------------------------|
| Does the project have a historic bridge processed under the Historic Bridges PA*? | Yes | No |
| | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| If No, then: | | |
| Opportunity for a Public Hearing Required? | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks:

Notice of Entry Letters
Indiana: Notice of Entry Letters were mailed to potentially affected property owners near the project area on September 13, 2018 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of entry letter for both Indiana and Kentucky is included in Appendix G, page 1.

Kentucky: Notice of Entry Letters were mailed to potentially affected property owners near the project area on September 13, 2018 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter for both Indiana and Kentucky is included in Appendix G, page 1.

Section 106
Indiana: This project qualifies under the Minor Projects Programmatic Agreement (MPPA), therefore does not require additional public involvement under Section 106 in Indiana.

Kentucky: The Kentucky Heritage Council, which is the Kentucky State Historic Preservation Office (SHPO), requirements of Section 106 in Kentucky are satisfied through the public involvement process of this project.

Public Involvement Comments and Responses
 A Project-specific Public Involvement Plan (PIP) was developed that identifies various communication and outreach tools to facilitate project communication, outreach and engagement for the project and is included with the public involvement documentation for the project (Appendix G, page 4). General project communication and outreach for both Indiana and Kentucky, are supported through the use of a project website, social media, traditional media outlets, fact sheets, use of comment cards, newsletters, small group and public information meetings, and a feedback survey:

- Project Website – via <http://shermanmintonrenewal.com>, was designed to constantly update the public on project developments, upcoming events, answers to their questions and to share their feedback; the website has accumulated over approximately 10,442 visitors.
- Social media – via the Sherman Minton Facebook Page and Twitter account, [@ShermanRenewal](#), has recorded over 5,251 engagements (public shares, likes, and comments via a social media post or page), and approximately 97,606 social media views. Local media has covered approximately 120 stories of the project, including an “In Conversation” Radio Call in Program with local radio station WFPL on August 2, 2019.
- Comment Cards – for the project were handed out at outreach events; to date, approximately 170 public comments have been received via the website or completed comment cards available at public open house and information meeting.
- Project Survey – was developed and posted on the website from July 2019 thru November 2019 to elicit feedback on potential Maintenance of Traffic (MOT) Options; there were approximately 3,006 survey responses.
- Community Advisory Committee (CAC) – established to facilitate communication about regional economic and community considerations for both sides of the Sherman Minton Bridge (greater New Albany and west

Indiana Department of Transportation

County Floyd County, IN Jefferson County, KY Route I-64 Des. No. Primary Des. No. 1702255

Louisville). The CAC is comprised of business, community and municipal representatives from both sides of the Ohio River. Four (4) CAC meetings for the project have been held; September 19, 2018, November 29, 2018, July 16, 2019 and February 6, 2020.

- Environmental Justice (EJ) Technical Advisory Committees (TACs) – due to the distinct characteristics between greater New Albany and west Louisville, two (2) separate EJ TACs were developed to facilitate communication and outreach to these specific populations. Four (4) sets of EJ TAC meetings have been held: the New Albany EJ TAC met on September 18, 2018, December 4, 2018, July 17, 2019, and February 11, 2020 while the west Louisville EJ TAC met on September 18, 2018, November 29, 2018, July 18, 2019, and February 6, 2020.
- Public Official Briefings – offered opportunities for elected officials and their operational constituencies to provide the project team with feedback on important public service considerations; two (2) rounds of public officials’ briefings were included during the project planning phase. Louisville Metro, City of New Albany, Floyd County, Harrison County, Jefferson County and Clark County officials met on September 10, 2018 and July 8, 2019. An additional public officials meeting will be held in the final stages of public involvement.
- Public Open House/Information Meetings – facilitate general communication with the broader public both formally through presentations and more informally through individual conversations with project team members at information stations; to date, two (2) rounds of Public Open Houses were held in both New Albany and west Louisville. Public comments from the Open Houses were facilitated through the project website and comments cards available during meetings. New Albany Public Open Houses were held October 2, 2018 and July 25, 2019. Louisville Public Open Houses were held October 4, 2018 and July 23, 2019.
- Small Group Stakeholder Meetings – allowed for technical interaction between the project team and key stakeholder groups that focused on specific concerns or constituencies; to date, individual meetings/briefings have been held with the following:
 - One Southern Indiana (OSI) met November 7, 2018
 - Develop New Albany met August 21, 2019
 - Greater Louisville Inc (GLI) Transportation Committee met September 24, 2019
 - Transit Authority of River City (TARC), met September 5, 2019 and February 4, 2020
 - One West Community Conversation met on September 5, 2019
 - First Responders Meeting with local emergency service providers took place on August 22, 2019
 - West Jefferson County Community Task Force met March 19, 2019 and August 20, 2019
- Additional Public Involvement – In accordance with the Indiana Department of Transportation (INDOT) and the Kentucky Transportation Cabinet (KYTC) Virtual Public Involvement Procedures, the environmental document was available between July 1, 2020, and August 16, 2020, for public review and comment. Project information materials, including a description of the Preferred Alternative for Maintenance of Traffic (MOT) during construction, project brochure and comment cards were also available to facilitate public understanding of the project and to encourage feedback. Both the environmental document and additional public information materials were available in five public repository locations, on the project website, <http://shermanmintonrenewal.com/> and were offered upon request. A presentation with audio and a project video was available on the project website and via social media channels. Follow-up communications were conducted with the project’s Community Advisory Committee and both Environmental Justice Technical Committees to encourage the dissemination of project information and to request feedback. A public hearing is not required due to the level of environmental documentation; however, the opportunity to request a public hearing by members of the public was advertised during the public review period. Advertisement of the public review period occurred through social media, the project website and local newspaper listings. *The Courier Journal* published a public notice on July 3, 2020 and on July 10, 2020 (Appendix G, page 7). *The New Albany Tribune* published a public notice on July 7, 2020 and July 14, 2020 (Appendix G, page 10). *The Louisville Defender* published a public notice July 2, 2020 and on July 9, 2020 (Appendix G, page 12). No requests for a public hearing were received during the 45-day public review period of July 1, 2020, and August 16, 2020. During the public review and comment period over 350 comment cards and brochures were distributed, and the

This is page 3 of 54 Project name: I-64 Bridge Rehabilitation Sherman Minton Renewal Project Date: October 2, 2020

Indiana Department of Transportation

County Floyd County, IN Jefferson County, KY Route I-64 Des. No. Primary Des. No. 1702255

project website had 1,160 views and 13,000 social media impressions were recorded. Social media impressions are defined as are the number of times content is displayed over one social media or multiple social media sources. The public provided a total of 25 comments through a variety of means and methods including the project website (7 comments received), e-mail (8 comments received), telephone hotline (2 comments received), and via comment cards (5 comments received). Appendix G, page 25 identifies comments collected during the public review period. All public involvement related materials, including comments received on the environmental document and corresponding responses are provided in Appendix G.

Applicable public involvement input summaries are included in the Community Impact Assessment (CIA) and EJ Analysis Technical Report (Appendix I, page 18) and the public inquires identified on Appendix G, page 110.

Public Controversy on Environmental Grounds Yes No
 Will the project involve substantial controversy concerning community and/or natural resource impacts?

Remarks: The project was identified early as having potential controversial community impacts to local residences, business and EJ populations. These impacts are based upon required temporary changes in access to and across the Sherman Minton Bridge during construction, all temporary changes of access are considered in the MOT options, and the potential EJ populations within the study:

- The Sherman Minton Bridge has been in service since 1962 and is the non-tolled option of the two Interstate crossing of the Ohio River in the Louisville metro area. The double-decker Sherman Minton Bridge carries approximately 90,000 vehicles per day as part of interstate (I-64) and daily regional (US 150) travel between the City of New Albany, Indiana and the area of west Louisville in Kentucky.
- An emergency closure of the Sherman Minton Bridge was announced and implemented on September 9, 2011 through February 17, 2012. The 2011 closure and related traffic diversions, congestion, uncertainty, and lengthy travel delays for the five months that followed left a lasting impression for many local officials, individuals, communities, and businesses.
- Potential EJ communities were identified early within the project study area and as utilizing the Sherman Minton Bridge with input from the Kentuckiana Regional Planning & Development Agency (KIPDA) regional model.
- Public outreach with local officials, community representatives, and public comments indicated that while there is broad community support for rehabilitating the Sherman Minton Bridge, the temporary impact of traffic diversion, congestion, and heavy trucks on the regional network and local streets, increased travel times and costs (and tolls), and loss of cohesion associated with the project were substantial concerns both regionally and locally. EJ populations were particularly sensitive to potential access and mobility impacts

Based upon early coordination meetings with regulatory agencies and public outreach, at this time, there is no controversy concerning impacts to natural resources.

Consideration to the public and local EJ communities was included in the development of MOT sections, due to the importance of community cohesion between New Albany, IN, and Louisville Kentucky. Minimization measures were part of the project development evaluations detailed in the remarks of the MOT Section, page 18, Community Impact Section, page 41, and EJ Section, page 45 of this document. CAC and EJ constituents along with the general public have been updated as MOT has evolved throughout the project via public meetings, the project website, and social media platforms.

The Preferred Alternative MOT was developed to meet constructability requirements, reflect public involvement input, and to reduce temporary MOT impacts for those that rely on the Sherman Minton Bridge in both Indiana and Kentucky; which should lower potential public controversy for the proposed action. The completion of proposed action will also provide long-term benefits with increased certainty and an extended service life for those that rely on the Sherman Minton Bridge in both Indiana and Kentucky. Furthermore, during the latest public involvement activities the public has not currently expressed concern with the Preferred Alternative MOT.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Indiana Department of Transportation (INDOT) & Kentucky Transportation Cabinet (KYTC) INDOT District: Seymour
 Local Name of the Facility: Interstate I-64 and US 150 Sherman Minton Bridge crossing of the Ohio River KYTC District: District 5

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Funding Source (mark all that apply): Federal State Local Other*

*If other is selected, please identify the funding source: _____

PURPOSE AND NEED:

Describe the transportation problem that the project will address. The solution to the traffic problem should NOT be discussed in this section. (Refer to the CE Manual, Section IV.B.2. Purpose and Need)

Need - The need for the project is due to the deteriorating structural condition of the existing Sherman Minton Bridge over the Ohio River, the deteriorating associated Indiana and Kentucky approaches, and deteriorating pavement of select associated side streets.

Structural elements deficiencies, depicted in Appendix B, page 7, were identified in the October 18, 2017 INDOT Bridge Assessment Report, the November 2019 INDOT Bridge Inspection Reports, and the November 2019 KYTC Bridge Inspection Report and are summarized below. Condition ratings are out of 1 to 9 (poor to good).

Des. Nos. 1702255 & 1592187 (Bridge No. I64-123-04691 D; Sherman Minton Bridge)

- **Bridge Decks** – Approximately 60 years old and were part of the original construction, the bridge decks have internal and external cracking observed at the underside of deck joints. Spalling was identified on limited areas on the upper deck, along the piers, abutments, copings and curbs. High amount of chloride exposure primarily from road salts was identified at the level of the reinforcing steel; 2018 INDOT Deck Condition Assessment confirmed the presence of chlorides as the primary deterioration mechanism. Corrosion identified on reinforcement steel leads to a weakening of the bridge deck and localized spalling and delamination of the concrete. This corrosion eventually leads to potholes. The bridge decks received a rating of 5/fair condition rating on the November 2019 INDOT inspection.
- **Arch and Truss Members** - Members exhibit areas of paint failure throughout the structure, leaving steel elements unprotected, with minor to moderate section loss observed. The arch and truss members received a rating of 5/fair condition rating on the November 2019 INDOT inspection.
- **Cable Hangers** – Are comprised of many individual wires bundled together in one cable, with a minimum of 130 individual wires in each cable. These Cable Hangers and the connectors exhibit surface corrosion due to inadequate protection from natural elements. In addition, one cable was observed to be swelling due to internal corrosion. Most cables exhibit 1 to 3 displaced wires along the length of the cables; and several cables connections exhibit moderate to heavy corrosion or pack rust of the connecting elements. The cable hangers received a rating of 5/fair condition rating on the November 2019 INDOT inspection.
- **Floorbeam Frames** – Carry all the load/weight from the deck out to the supporting hanger cables. Significant widespread paint failure was observed. The paint failure has left the frames vulnerable to corrosion. Corrosion varies from surface corrosion to heavy section loss with some areas approaching 50% of the original thickness. This heavy reduction in the size due to corrosion results in a reduction of the overall load carrying capacity of individual members. No overall load capacity of the bridge is affected at this time. The floorbeam frames received a rating of 5/fair condition rating on the November 2019 INDOT inspection.
- **Steel Stringer** – In place to support the deck between the floor beam frames. Widespread paint failure was observed. The paint failure has left the Steel Stringers vulnerable to corrosion. Corrosion varies from surface corrosion to heavy section loss. Cracking is observed in some stringer ends as a result of section loss due to corrosion and out of plane distortion. The steel stringer received a rating of 5/fair condition rating on the November 2019 INDOT inspection.
- **Substructure (concrete piers and foundations)** – Support of the overlying substructure elements. Limited concrete cracking was identified along the piers and abutments. Some spalling was identified on limited areas along the piers, and abutments. The substructure received a rating of 6/fair condition rating on the November 2019 INDOT inspection.

Des. Nos. 1702254 & 1702260 (Bridge #056B00161N; KY Approach)

- **Bridge Deck and Superstructure elements** – Evidence of internal and external cracking and corrosion was identified. Minor amounts of section loss were present. The bridge deck and superstructure received a rating of 5/fair condition rating on the November 2019 KYTC inspection.
- **Substructure** – Concrete cracking along the piers and abutments and some spalling has been identified. The substructure received a rating of 5/fair condition rating on the November 2019 KYTC inspection.
- **Paint** – Major deterioration of paint was evident giving the structure a poor condition rating. The paint failure has allowed

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exposure to the super structure elements, which increases corrosion on the superstructure. The bridge paint received a rating of 4/poor condition rating on the November 2019 KYTC inspection.

Des. No. 1702257 (Bridge #I64-123-02294 CWBL; I-64 WB over SR 111/Main Street, RR IN Approach WB)

- *Bridge Deck and Superstructure elements* – Evidence of minor internal and external cracking and corrosion. The bridge deck received a rating of 6/fair condition rating and the superstructure received a rating of 5/fair condition rating on the November 2019 INDOT inspection.
- *Substructure* - Minor spalling has been identified. The substructure received a rating of 6/fair condition rating on the November 2019 INDOT inspection.

Des. No. 1702258 (Bridge #I64-123-02294 CEBL; I-64 EB over SR 111/Main Street., RR IN Approach EB)

- *Bridge Deck and Superstructure elements* – Evidence of internal and external cracking and corrosion identified. Minor amounts of section loss were present. The bridge deck received a rating of 6/fair condition rating and the superstructure received a rating of 6/good on the November 2019 INDOT inspection.
- *Substructure* – Minor cracking along the abutments. The superstructure received a rating of 6/good on the November 2019 INDOT inspection.

Des. No. 1702259 (Bridge #I64-123-02294 JCEB; I-64 EB over SR 111/Main Street., RR IN Approach EB)

- *Bridge Deck and Superstructure elements* – Evidence of external cracking and corrosion identified. Minor amounts of section loss were present. The bridge deck and superstructure received a rating of 6/fair condition rating on the November 2019 INDOT inspection.
- *Substructure* – Minor cracking along the abutments. The substructure received a rating of 6/good condition rating on the November 2019 INDOT inspection.

Des. No. 1900579 (Bridge #I64-123-04690 BEBL; I-64 EB over Market Street 0.11 W of SR 111)

- *Paint* – Minor deterioration of paint was evident. The paint failure has allowed exposure to the super structure elements, which could lead to an increase of corrosion on the superstructure. The bridge paint received a rating of 5/fair condition rating on the November 2019 INDOT inspection.

Deterioration was also identified on side streets:

Des. No. 1701215 (Old SR 62 [Elm Street] from I-64 Exit Ramp to State Street)

- *Elm, Spring, and 5th Streets* - Pavement deterioration and non-compliant American Disability Association (ADA) standard curb ramps are identified along Elm, Spring, and 5th Streets of New Albany, IN.

Purpose - The purpose of the Project is to address the deterioration of structural elements listed below; of the Sherman Minton Bridge, the associated Indiana and Kentucky approaches, and select associated side streets with the goal of extending the service life of the I-64 Interstate crossing over the Ohio River up to 30 years.

Des. Nos. 1702255 & 1592187 (Bridge No. I64-123-04691 D; Sherman Minton Bridge)

- Address the deterioration of the Bridge Decks, Arch and Truss Members, Cable Hangers, Floor Frames, Steel Stringer and Substructure; and to protect the structure for future use.

Des. Nos. 1702254 & 1702260 (Bridge #056B00161N; KY Approach)

- Address the deterioration of the Bridge Decks, Superstructure elements and Substructure; and to protect the structure for future use.

Des. No. 1702257 (Bridge #I64-123-02294 CWBL; I-64 WB over SR 111/Main Street, RR IN Approach WB)

- Address the deterioration of the Bridge Decks, Superstructure elements and Substructure.

Des. No. 1702258 (Bridge #I64-123-02294 CEBL; I-64 EB over SR 111/Main Street., RR IN Approach EB)

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- Address the deterioration of the Bridge Decks, Superstructure elements and Substructure.
Des. No. 1702259 (Bridge #164-123-02294 JCEB; I-64 EB over SR 111/Main Street., RR IN Approach EB)
 - Address the deterioration of the Bridge Decks, Superstructure elements and Substructure.
Des. No. 1900579 (Bridge #164-123-04690 BEBL; I-64 EB over Market Street 0.11 W of SR 111)
 - Protect the Superstructure elements for future use.
- Purpose identified on side streets:
Des. No. 1701215 (Old SR 62 [Elm Street] from I-64 Exit Ramp to State Street)
- Address the deterioration of the pavement and provide ADA compliant standard curb ramps along Elm, Spring, and 5th Streets in New Albany, IN.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Floyd, IN & Jefferson, KY Municipality: New Albany, Indiana and Louisville, Kentucky

Limits of Proposed Work: Elm Street from 2nd Street to State Street in Indiana, Spring Street from State Street to 5th Street to Main Street in Indiana, upper and lower decks of the Sherman Minton Bridge to the Kentucky approach crossovers.

Total Work Length: 1.8 Mile(s) Total Work Area: 17.3 Acre(s) (This includes work area measured on both decks of the Sherman Minton Bridge)

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required?

| | |
|--|-------------------------------------|
| Yes ¹ | No |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Date: <input style="width: 100px;" type="text"/> | |

¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

In the remarks box below, describe existing conditions, provide in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

Location
The project location is the I-64 Interstate and US 150 (I-64) over the Ohio River connecting New Albany, Floyd County, Indiana and Louisville, Jefferson County, Kentucky (Appendix B, page 1). The project is centered at 38.278665°N, -85.822237°E, the location of the Sherman Minton Bridge, in the Indiana New Albany Quadrangle, Sections 21, 22, 27, and 28, Township 27N, and Range 8E. The Sherman Minton Bridge connects New Albany, Indiana and the west side of Louisville, Kentucky, which is often referred to as west Louisville throughout this document.

The Sherman Minton Renewal Project (SMRP) logical termini are the system to system I-64/I-264 interchange to the south in Kentucky and the I-64/I-265 interchange to the north in Indiana and have independent utility with the surrounding interstate network. Local side streets, Elm Street, Spring Street, and 5th Street (also referred to as West Elm Street, West Spring Street and West 5th Street), are located within the City of New Albany, IN near the Sherman Minton Bridge. In the past when there have been closures of the Sherman Minton Bridge, traffic diverts onto these local side streets. The side streets were identified as needing improvements including ADA compliant standard curb ramps. Due to these side streets close proximity to the bridge repairs, it was identified by INDOT and recommended to be included in the same contract as the Sherman Minton Bridge. The logical termini for the side street improvements is Elm Street from the I-64 exit ramp and Spring Street from State Street to 5th Street to Main Street (Appendix B, page 3).

Since the project is located within two states, the project has different identifiers for each state. Indiana identifies the project using INDOT Designation numbers (Des. Nos.) and Kentucky identifies the project using Item IDs. There are nine INDOT Des. Nos., 1702255, 1592187, 1702260, 1702254, 1702257, 1702258, 1702259, 1701215, and 1900579, associated with this project and covered in this CE documentation. The lead Des. No. is 1702255. The Kentucky Item ID is 5-64 for the Kentucky approaches, also identified

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under INDOT Des. Nos. 1702260 & 1702254. The INDOT Des. Nos. will be used as the main reference throughout this document.

Specific project locations separated by each INDOT Des. No. and Bridge No. are included in Table 1 and are depicted in graphics in Appendix B.

Table 1. Sherman Minton Renewal Project: Project Location and Termini

| Des. No. | Bridge No. | Description | Location |
|--|--|--|---|
| 1702255 & 1592187 | I64-123-04691 D | Sherman Minton Bridge | Construction extends from 0.32 miles south of the Elm Street south on ramp to 0.45 miles from the I-264 south off ramp MOT elements extend from the I-64 / I-264 interchange 3.5 miles to the northwest to the I-64 / I-265 interchange in New Albany Indiana; and EB I-265 exit ramp to SB I-65 |
| 1702260 & 1702254 (KY Item ID 5-64) | 056B00161N | KY Approaches | Extend 0.45 miles from the I-264 south off ramp to 0.11 miles from the I-264 off ramp |
| 1702257 | I64-123-02294 CWBL | I-64 WB over SR 111/Main Street, RR (IN Approach WB) | 0.32 miles south of the Elm Street North off Ramp to 0.17 miles south of the Elm Street North off Ramp |
| 1702258 | I64-123-02294 CEBL | I-64 EB over SR 111/Main Street (IN Approach EB) | 0.19 miles south of the Elm Street South on ramp to 0.23 miles south of the Elm Street South on Ramp |
| 1702259 | I64-123-02294 JCEB | I-64 EB over Southern RR (IN Approach EB over RR) | 0.26 miles south of the Elm Street South on ramp to 0.32 miles south of the Elm Street South on Ramp |
| 1701215 | n/a Broken into 3 roadway segments. | Old SR 62 (Elm Street) from I-64 Exit Ramp to State Street and Spring Street | 0.10 mile of Elm Street from the northbound I-64 exit ramp to 170 ft west of State Street; 0.36 mile of Spring Street from West 5th Street to State Street; 0.19 mile of West 5th Street from the southbound I-64 exit ramp to 65 ft north of SR 111/Main Street; |
| 1900579 | I64-123-04690 BEBL | I-64 EB over Market Street 0.11 W of SR 111 | 0.09 miles south of the Elm Street South on Ramp to 0.13 miles south of the Elm Street South on Ramp |

Existing Conditions

The Sherman Minton Bridge has been in service since 1962 and is the non-tolled option of the two Interstate crossings of the Ohio River in the Louisville metro area. The land use in the vicinity of the project is primarily metropolitan and industrial. The project area spans over the Ohio River and contains other water resources identified in this document: part III, section A and in Appendix F. EJ communities and historic districts are identified in the surrounding area of the project. A public recreation area and Ohio River Greenway Trail are located adjacent to the project in New Albany, IN. A public park, Shawnee Park, which includes a public golf course Shawnee Golf Course, and a public trail, Louisville Loop, are adjacent to the project in west Louisville, Kentucky. The double-decker Sherman Minton Bridge carries approximately 90,000 vehicles per day as part of interstate (I-64) and daily regional (US 150) travel between the City of New Albany, Indiana and the area of west Louisville in Kentucky. Other Ohio River crossings in the region include the US 31 Clark Memorial Bridge (non-tolled) in downtown Louisville, the I-65 Kennedy and Lincoln Bridges (tolled), and the SR 265 Lewis and Clark Bridge (tolled) east of Louisville.

Because of the age and condition of these structures, the frequency of both planned and unplanned (urgent) repair projects to keep the Sherman Minton Bridge safely in service have been increasing. For example, the 2011 5-month emergency closure of the Sherman Minton Bridge for repair of cracks in structural members, the 2013 bridge deck expansion, joint replacement and steel repairs, the 2017 unplanned urgent repair project to strengthen steel floor system elements due to corrosion and section loss and the 2018 unplanned urgent repair project occurred to repair holes and deterioration identified in the bridge decks. Currently the October 18, 2017 INDOT Bridge Assessment Report, and the November 2019 INDOT Inspection Reports, and the November 2019 KYTC Bridge Inspection Report identify the following in a brief summary:

Des. Nos. 1702255 & 1592187 (Bridge No. I64-123-04691 D; Sherman Minton Bridge)

- Bridge decks have evidence of cracking, spalling, road salt damage, and corrosion.

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- Arch and Truss Members exhibit widespread paint failure throughout the structure, leaving the structure unprotected and susceptible to corrosion.
- Cable Hangers and the connectors exhibit heavy surface corrosion, minor internal corrosion, and displaced wires.
- Floorbeam Frames and Steel Stringers exhibit widespread paint failure, heavy corrosion, and section loss.
- Substructure has evidence of minor cracking and spalling along the piers, and abutments.

Des. Nos. 1702254 & 1702260 (Bridge #056B00161N; KY Approach)

- Bridge Deck and Superstructure elements have evidence of cracking, corrosion and minor amounts of section loss were present along with major paint deterioration.
- Substructure has evidence of cracking, spalling and paint deterioration.

Des. No. 1702257 (Bridge #I64-123-02294 CWBL; I-64 WB over SR 111/Main Street, RR IN Approach WB)

- Bridge Deck and Superstructure elements have evidence of cracking and corrosion.
- Substructure has evidence of spalling.

Des. No. 1702258 (Bridge #I64-123-02294 CEBL; I-64 EB over SR 111/Main Street, RR IN Approach EB)

- Bridge Deck and Superstructure elements have evidence of cracking, corrosion and minor amount of section loss.
- Substructure has minor cracking along the abutments.

Des. No. 1702259 (Bridge #I64-123-02294 JCEB; I-64 EB over SR 111/Main Street., RR IN Approach EB)

- Bridge Deck and Superstructure elements have evidence of cracking, corrosion and minor amounts of section loss.
- Substructure has Minor cracking along the abutments.

Des. No. 1900579 (Bridge #I64-123-04690 BEBL; I-64 EB over Market Street 0.11 W of SR 111)

- Superstructure and substructure exhibit deterioration of paint.

Des. No. 1701215 (Old SR 62 [Elm Street] from I-64 Exit Ramp to State Street)

- Elm, Spring, and 5th Streets have evidence of pavement deterioration and non-compliant ADA standard curb ramps at intersections identified along Elm, Spring, and 5th Streets.

Preferred Alternative

The SMRP is the rehabilitation of the Sherman Minton Bridge and related approaches in Indiana and Kentucky. The goal of SMRP is to address the deteriorating structural condition of the existing bridges to extend the service life of the bridges up to 30 years. Project elements include bridge deck replacements and bridge deck overlays, structural repairs, replacement lighting, bridge painting, local streets Hot Mix Asphalt (HMA) overlay and ADA ramp reconstruction, and interstate ramp frictionalization. SMRP is a joint effort between INDOT and KYTC. INDOT is leading SMRP in close collaboration with key staff from KYTC. In efforts of avoiding repetition in this document the structural repairs, lighting replacements, bridge drain components, and structural elements included on this project are further detailed in the remarks of the Design Criteria Section, page 13 of this document.

Since construction zones and temporary lane/ramp closures are required for construction on the Sherman Minton Bridge structure, additional maintenance work is included as part of SMRP: deck overlay on I-64 westbound and eastbound over SR 111/Main Street and I-64 eastbound over Southern Railroad, painting of the I-64 EB bridge over Market Street, and local street work including HMA overlay, preventative maintenance, and ADA curb ramp reconstruction on Elm Street, Spring Street, and 5th Street near the I-64 ramps in New Albany, IN. The SMRP construction area is from the I-64 exit ramp to State Street in Indiana to the I-64/I-264 interchange in Kentucky, for a total of approximately 1.5 miles. Appendix B, page 3 depicts the geographic location of the various individual elements of SMRP. Appendix B, page 26-110 includes the draft plan sets per structure. Table 2 below provides additional details for the proposed improvements that are anticipated to be completed as part of SMRP.

Table 2. Sherman Minton Renewal Project: Individual Project Elements

| Des. No. | Bridge No. | Description | Work Type |
|----------|------------|-------------|-----------|
|----------|------------|-------------|-----------|

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| | | | |
|------------------------------|-----------------------|--|--|
| 1702255 | I64-123-04691 D | Sherman Minton Bridge | Bridge Deck Replacement, Structural Repairs and Substructure Repairs |
| 1592187 | I64-123-04691 D | Sherman Minton Bridge | Bridge Painting |
| 1702260 (KY Item ID 5-64) | 056B00161N | KY Approach | Bridge Deck Replacement, Substructure Repairs, and Bridge Painting |
| 1702254 (KY Item ID 5-64) | 056B00161N | KY Approach | Bridge Deck Replacement, Substructure Repairs, and Bridge Painting |
| 1702257 | I64-123-02294 CWBL | I-64 WB over SR 111/Main Street, RR (IN Approach WB) | Bridge Deck Overlay and Bridge Painting |
| 1702258 | I64-123-02294 CEBL | I-64 EB over SR 111/Main Street (IN Approach EB) | Bridge Deck Overlay and Bridge Painting |
| 1702259 | I64-123-02294 JCEB | I-64 EB over Southern RR (IN Approach EB over RR) | Bridge Deck Overlay and Bridge Painting |
| 1701215 | n/a | Old SR 62 (Elm Street) from I-64 Exit Ramp to State Street and Spring Street | HMA Overlay, Preventative Maintenance, and ADA Curb Ramp |
| 1900579 | I64-123-04690 BEBL | I-64 EB over Market Street 0.11 W of SR 111 | Bridge Painting |

The roadway project spans three segments of side streets: Elm Street from I-64 exit ramp to 0.04 miles west of State Street; 5th Street from 0.02 miles north of SR 111 to Spring Street; and Spring Street from W. 5th Street to 0.02 miles West of State Street in the City of New Albany, IN. The HMA resurface includes 1.5-inch mill and HMA overlay of the existing pavement (Appendix B, page 85). In addition to the HMA overlay activities, traffic signal loops will be reestablished at the west approach of Elm Street and Scribner Drive and the east approach of Spring Street and Scribner Drive. A total of eleven (11) intersections will have ADA curb ramp work completed in order to meet ADA-compliant standards. The intersections where ADA curb ramp work will occur are provided in Table 3 below:

Table 3. Sherman Minton Renewal Project: Intersections with ADA Curb Ramp Work

| Intersections: | | Quadrants: |
|----------------|--|-----------------------|
| 1 | West 5 th Street & SR 111/Main Street | All four corners |
| 2 | West 5 th Street & Market Street | All four corners |
| 3 | West 5 th Street & Spring Street | NW, SW and SE corners |
| 4 | West 4 th Street & Spring Street | SW and SE corners |
| 5 | Washington Place & Spring Street | SW and SE corners |
| 6 | Scribner Drive & Spring Street | All four corners |
| 7 | West 1 st Street & Spring Street | All four corners |
| 8 | State Street & Spring Street | All four corners |
| 9 | Scribner Drive & Elm Street | NE and SE corners |
| 10 | West 1 st Street & Elm Street | All four corners |
| 11 | State Street & Elm Street | All four corners |

Six MOT options have been analyzed and considered for this project. The six MOT options are further detailed on page 18 in the MOT section of this document and included as exhibits in Appendix B, pages 10-18. Minimization measures were part of project development, evaluations, and a combination of MOT options to reduced impacts. While the MOT 1 option had the most positive public involvement comments, fewest changes from existing travel patterns and local access, lowest induced traffic diversions and related environmental impacts; project constructability requirements were not entirely met. The Preferred Alternative MOT combined the predominant use of MOT 1, limited off-peak use of MOT-2 (*allowance for about 180 nights per construction year*), and short-term use of MOT-5. Detour routes are listed below and included in Appendix B, pages 19-26.

Indiana

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- I-64 through traffic will be detoured via I-265 to I-65 paired Kennedy and Lincoln bridges through Louisville and the I-265 (SR 265) Lewis and Clark Bridge to the east of Louisville.
- Local access to New Albany IN would follow the same detour route or remain on the local roadway network; State Street will be the detour route during closure of the Spring Street access ramps in New Albany IN.

Kentucky

- Both I-264 and I-64 through traffic will be detoured to either the I-65 paired Kennedy and Lincoln bridges through Louisville or the I-265 (SR 265) Lewis and Clark Bridge to the east of Louisville.
- Local access to west Louisville KY would follow the same detour route or remain on the local arterial network.

The completion of proposed action will also provide long-term benefits with increased certainty and an extended service life for those that rely on the Sherman Minton Bridge in both Indiana and Kentucky. The Sherman Minton Bridge is the most heavily travelled crossing of the Ohio River in Louisville, and as such, many people are affected by increasingly frequent repairs and associated lane closures. A portion of the traffic that uses this bridge is commercial and lane closures can cause delays due to queuing at the bridge or detouring around the bridge. The rehabilitation of the Bridge maximizes use of existing structures, while extending their service life and meets the purpose and need.

No permanent or temporary ROW is anticipated for this project. No impacts to natural resources are anticipated.

Implementation

This project will be implemented as a Design-Build Best Value contract. INDOT and KYTC will select the Design-Build Contractor team based upon evaluation including but not limited to factors such as: contractor approach, cost, time and impacts minimization of environmental and public.

The Design-Build Contractor will be responsible for compliance with all approved NEPA Documents. While no impacts are anticipated, this is dependent on the MOT option and construction methods proposed by the Design-Build Contractor; further resource analysis and permits may be required. All permits, associated documentation, and coordination will be the responsibility of the Design-Build Contractor. Any applicable recommendations made by resource agencies during initial coordination are included in the Environmental Commitments section of this CE document on page 51.

OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

No Build Alternative: The “No-Build” is an avoidance alternative that would have no work on the existing Sherman Minton Bridge and approach structures, no immediate expenditure of federal funds, and would result in no environmental impact. Without rehabilitation, the existing Sherman Minton Bridge and approach structures would continue to deteriorate, require more frequent inspections, maintenance work, additional closures over time to keep the bridge functioning safely, and increase community impact. Eventually, without rehabilitation, it will become too costly to maintain safe travel conditions and result in the permanent closure of the bridge. As the most heavily travelled crossing of the Ohio River in the Louisville area, the increasingly frequent repairs and associated lane closures have negatively affected local traffic patterns and adjacent communities. Permanent closure would disrupt regional traffic patterns for thousands of interstate and local users and affect local EJ populations. This alternative would not meet the purpose and need of the project, therefore; this alternative has been dismissed.

Replacement Alternative: Replacement of the existing double-decker bridge would have new environmental impacts; expanded evaluation, justification, and permitting requirements; require new Right of Way (ROW) and potential relocations; a longer timeframe to address deteriorating conditions, and significantly higher financial costs. Replacement was not carried forward by INDOT or FHWA as an alternative in the project’s planning stage therefore, replacement has not been carried over in the project scope.

The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

- It would not correct existing capacity deficiencies;
 - It would not correct existing safety hazards;
 - It would not correct the existing roadway geometric deficiencies;
 - It would not correct existing deteriorated conditions and maintenance problems; or
 - It would result in serious impacts to the motoring public and general welfare of the economy.
- Other (Describe)

| |
|----------|
| |
| |
| |
| X |
| X |
| |

Indiana Department of Transportation

County Floyd County, IN Jefferson County, KY Route I-64 Des. No. Primary Des. No. 1702255

ROADWAY CHARACTER:

I-64

Functional Classification: Interstate
 Current ADT: 79,541 VPD (2019) Design Year ADT: 98,025 VPD (2040)
 Design Hour Volume (DHV): 7,402 Truck Percentage (%) 10
 Designed Speed (mph): 55 Legal Speed (mph): 55

| | Existing | | Proposed |
|------------------|--------------|-----|--------------|
| Number of Lanes: | 3 NB, 3 SB | | 3 NB, 3 SB |
| Type of Lanes: | Travel Lanes | | Travel Lanes |
| Pavement Width: | 88 | ft. | 88 |
| Shoulder Width: | 4 min | ft. | 4 min |
| Median Width: | N/A | ft. | N/A |
| Sidewalk Width: | N/A | ft. | N/A |

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway

Spring Street

Functional Classification: Minor Arterial
 Current ADT: 18,817 VPD (2018) Design Year ADT: 23,421 VPD (2040)
 Design Hour Volume (DHV): 1,882 Truck Percentage (%) 2
 Designed Speed (mph): 25 Legal Speed (mph): 25

| | Existing | | Proposed |
|------------------|---------------------------------|-----|---------------------------------|
| Number of Lanes: | 5 | | 5 |
| Type of Lanes: | 3 travel lanes, 2 parking lanes | | 3 travel lanes, 2 parking lanes |
| Pavement Width: | 52 | ft. | 52 |
| Shoulder Width: | N/A | ft. | N/A |
| Median Width: | N/A | ft. | N/A |
| Sidewalk Width: | 6 min | ft. | 6 min |

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

Elm Street

Functional Classification: Minor Arterial
 Current ADT: 23,329 VPD (2018) Design Year ADT: 23,347 VPD (2040)
 Design Hour Volume (DHV): 2,332 Truck Percentage (%) 2
 Designed Speed (mph): 25 Legal Speed (mph): 25

| | Existing | | Proposed |
|------------------|----------------|-----|----------------|
| Number of Lanes: | 3 | | 3 |
| Type of Lanes: | 3 travel lanes | | 3 travel lanes |
| Pavement Width: | 52 | ft. | 52 |
| Shoulder Width: | N/A | ft. | N/A |
| Median Width: | N/A | ft. | N/A |
| Sidewalk Width: | 4 min | ft. | 4 min |

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

Indiana Department of Transportation

County Floyd County, IN Jefferson County, KY Route I-64 Des. No. Primary Des. No. 1702255

| |
|-------------------------------------|
| DESIGN CRITERIA FOR BRIDGES: |
|-------------------------------------|

Structure/NBI Number(s): 5 Structures, NBI number listed below Sufficiency Rating: 9-1, INDOT Bridge Inspection Report
 (Rating, Source of Information)

(2019 INDOT Bridge Inspection Report)

| Bridge (Inspection Date) | Component | Condition Rating | | | | | | | | |
|---|----------------|------------------|---|---|------|---|------|---|---|---|
| | | Good | | | Fair | | Poor | | | |
| | | 9 | 8 | 7 | 6 | 5 | 4 | 3 | 2 | 1 |
| <u>I64-123-04691 D</u> Sherman Minton Bridge (Nov 2019) | Deck | | | | | ✓ | | | | |
| | Superstructure | | | | | ✓ | | | | |
| | Substructure | | | | ✓ | | | | | |
| | Paint | | | | | | ✓ | | | |
| <u>I64-123-02294 CWBL</u> I-64 WB over SR 111/Main Street, RR IN Approach WB (Nov 2019) | Deck | | | | ✓ | | | | | |
| | Superstructure | | | | | ✓ | | | | |
| | Substructure | | | | | ✓ | | | | |
| | Paint | | | ✓ | | | | | | |
| <u>I64-123-02294 CEBL</u> I-64 EB over SR 111/Main Street IN Approach EB (Nov 2019) | Deck | | | | ✓ | | | | | |
| | Superstructure | | | ✓ | | | | | | |
| | Substructure | | | | ✓ | | | | | |
| | Paint | | ✓ | | | | | | | |
| <u>I64-123-02294 JCEB</u> I-64 EB over Southern RR IN Approach EB (Nov 2019) | Deck | | | | ✓ | | | | | |
| | Superstructure | | | | ✓ | | | | | |
| | Substructure | | | ✓ | | | | | | |
| | Paint | | | | ✓ | | | | | |
| <u>056B00161N</u> KY Approach (Sept 2019) | Deck | | | | | ✓ | | | | |
| | Superstructure | | | | | ✓ | | | | |
| | Substructure | | | | | ✓ | | | | |
| | Paint | | | | | | ✓ | | | |
| <u>I64-123-04690 BEBL</u> I-64 EB over Market Street (Nov 2019) | Deck | | | ✓ | | | | | | |
| | Superstructure | | | | ✓ | | | | | |
| | Substructure | | | ✓ | | | | | | |
| | Paint | | | | | ✓ | | | | |

| | |
|-----|--|
| 9-7 | Excellent to Good condition with none to some minor problems noted. |
| 6-5 | Satisfactory to fair condition, all primary structural elements found but minor deterioration, section loss is present |
| 4-1 | Poor to "imminent" failure condition, Advanced to Major deterioration to primary structure elements, Fatigue cracks may be present, or scour could be damaging to support. |
| 0 | Failed condition the bridge is out-of-service. |

Source: INDOT and KYTC Bridge Inspection Reports

Indiana Department of Transportation

County Floyd County, IN Jefferson County, KY Route I-64 Des. No. Primary Des. No. 1702255

| I64-123-04691 D | Existing | Proposed |
|---------------------------|--|--|
| Bridge Type: | Steel through arch, Steel through/deck truss | Steel through arch, Steel through/deck truss |
| Number of Spans: | 5 spans | 5 spans |
| Weight Restrictions: | Open (no restrictions) | Open (no restrictions) |
| Height Restrictions: | 17.58 (from SI&A) ft. | 17.58 (from SI&A) ft. |
| Curb to Curb Width: | 42 ft. | 42 ft. |
| Outside to Outside Width: | 45 ft. | 45 ft. |
| Shoulder Width: | 3 (both sides) ft. | 3 (both sides) ft. |
| Length of Channel Work: | N/A | N/A |

Remarks: Structure No. I64-123-04691 D
 This is the lead structure number for the Sherman Minton Bridge. The Sherman Minton bridge is a double-deck bridge carrying I-64 traffic over the Ohio River, connecting New Albany, Indiana and Louisville, Kentucky. The proposed work type for this structure is a bridge deck replacement, replacement of lighting components, structural repairs, and cleaning and painting of all steel (Appendix B, page 29).

The bridge deck replacement work includes demolition of the existing bridge decks, cleaning the top flanges of the stringers, installing shear stud connectors, and casting a new reinforced concrete bridge deck with new expansion joints. The proposed bridge decks will feature bridge railings, longitudinal grooving, snow-plowable raised pavement markers, barrier delineators, and new pavement markings. The proposed decks will be surface sealed.

The structural repair work includes repair or replacement of selected structural steel members. Specifically, the following members included in the work are:

- The hanger cable assemblies will be replaced.
- Stringer ends will be repaired.
- Floorbeams and floorbeam frames will be repaired.
- The inspection access system including the catwalk, fall-arrest cables, and associated hardware will be repaired or replaced as needed to restore the safe working capacity of these systems. In addition, the inspection access system will be enhanced through the addition of inspection access ladders from the tops of the piers to the upper chords of the arches, and a safety cable will be added to the top chord of the arches.
- Deteriorated bolts and rivets will be replaced with new bolts throughout the bridge.
- Grinding and bolted repairs will be performed at floorbeam frames to reduce the fatigue sensitivity of certain steel details.

The cleaning and painting work include removal of the existing paint system and application of a new paint system on all steel members of the bridge, above the tops of the substructures. The metal drill shavings at the bottom of the arch ties will be addressed during the cleaning of the steel in these areas.

Substructure repair work will be performed on the pier cap at Pier 6. Epoxy injection grouting repair will be performed at a crack in the pier cap. This work is to be completed under Des. No. 1702255 and 1592187.

Will the structure be rehabilitated or replaced as part of the project? Yes No N/A

| I64-123-02294 CWBL | Existing | Proposed |
|---------------------------|---|---|
| Bridge Type: | Steel multi-girder | Steel multi-girder |
| Number of Spans: | 9 spans | 9 spans |
| Weight Restrictions: | Open (no restrictions) | Open (no restrictions) |
| Height Restrictions: | 99.99 (from SI&A means no restrictions) ft. | 99.99 (from SI&A means no restrictions) ft. |
| Curb to Curb Width: | 42 ft. | 42 ft. |
| Outside to Outside Width: | 45 ft. | 45 ft. |
| Shoulder Width: | 3 (both sides) ft. | 3 (both sides) ft. |
| Length of Channel Work: | N/A | N/A |

Remarks: Structure Nos. I64-123-02294 CWBL
 This structure is the I-64 westbound lane (WBL) Indiana approach over SR 111/Main Street connecting to the Sherman Minton Bridge. The proposed work type for this structure is bridge deck overlay, replacement of lighting

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components, structural repairs, and partial bridge cleaning and painting (Appendix B, page 43).

The bridge deck overlay work includes milling the existing bridge deck to remove concrete cover and unsound concrete, deck and barrier concrete repairs, replacement of bridge deck expansion joints, and casting a new latex modified concrete overlay. The proposed work also includes installation of snow-plowable raised pavement markers, barrier delineators, and new pavement markings. The existing reinforced concrete approach slabs will be demolished, and new reinforced concrete approach slabs will be constructed.

The structural repair work includes repair or replacement of selected structural steel members. Specifically, the following members included in the work are:

- The bearings will be replaced.
- Girder ends will be repaired.

The partial cleaning and painting work include local repairs of the existing paint system and painting of areas disturbed by steel repair work. This work is to be completed under Des. No. 1702257.

Will the structure be rehabilitated or replaced as part of the project? **Yes** **No** **N/A**

| | Existing | Proposed |
|---------------------------|---|---|
| Bridge Type: | Steel continuous multi-girder | Steel continuous multi-girder |
| Number of Spans: | 3 spans | 3 spans |
| Weight Restrictions: | Open (no restrictions) | Open (no restrictions) |
| Height Restrictions: | 99.99 (from SI&A means no restrictions) ft. | 99.99 (from SI&A means no restrictions) ft. |
| Curb to Curb Width: | 51'-1 3/4" to 58'-7 1/2" | 51'-1 3/4" to 58'-7 1/2" |
| Outside to Outside Width: | 54'-1 3/4" to 61'-7 1/2" | 54'-1 3/4" to 61'-7 1/2" |
| Shoulder Width: | Varies (left 3'-6" to 6', right 4'-7 1/2") | Varies (left 3'-6" to 6', right 4'-7 1/2") |
| Length of Channel Work: | N/A | N/A |

Remarks: Structure Nos. I64-123-02294 CEBL
 This structure is the I-64 eastbound lane (EBL) Indiana approach over SR 111/Main Street connecting to the Sherman Minton Bridge. The proposed work type for this structure is deck overlay, structural repairs and painting (Appendix B, page 36).

The bridge deck overlay work includes milling the existing bridge deck to remove concrete cover and unsound concrete, deck and barrier concrete repairs, replacement of bridge deck expansion joints, and casting a new latex modified concrete overlay. The proposed work also includes installation of snow-plowable raised pavement markers, barrier delineators, and new pavement markings. The existing reinforced concrete approach slabs will be demolished, and new reinforced concrete approach slabs will be constructed.

The structural repair work includes repair or replacement of selected structural steel members. Specifically, the following members included in the work are:

- The bearings will be replaced.
- Girder ends will be repaired.

The partial cleaning and painting work include local repairs of the existing paint system and painting of areas disturbed by steel repair work. This work is to be completed under Des. No. 1702258.

Will the structure be rehabilitated or replaced as part of the project? **Yes** **No** **N/A**

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| I64-123-04690 BEBL | Existing | | Proposed | |
|---------------------------|---|-----|---|-----|
| Bridge Type: | Steel multi girder | | Steel multi girder | |
| Number of Spans: | 3 | | 3 | |
| Weight Restrictions: | Open (no restrictions) | | Open (no restrictions) | |
| Height Restrictions: | 99.99 (from SI&A means no restrictions) | ft. | 99.99 (from SI&A means no restrictions) | ft. |
| Curb to Curb Width: | 59.0 | ft. | 59.0 | ft. |
| Outside to Outside Width: | 61.6 | ft. | 61.6 | ft. |
| Shoulder Width: | 6 | ft. | 6 | ft. |
| Length of Channel Work: | N/A | | N/A | |

Remarks: I64-123-04690 BEBL
 This structure is the I-64 eastbound lane (EBL) over Market Street 0.11 W of SR 111. This structure has a fair condition rating for the deck, superstructure, and substructure. The proposed work type for this structure is bridge painting (Appendix B, page 77). This work is to be completed under Des. No. 1900579.

Yes
 No
 N/A

Will the structure be rehabilitated or replaced as part of the project?

| I64-123-02294 JCEB | Existing | | Proposed | |
|---------------------------|------------------------|-----|------------------------|-----|
| Bridge Type: | Steel multi-girder | | Steel multi-girder | |
| Number of Spans: | 4 spans | | 4 spans | |
| Weight Restrictions: | Open (no restrictions) | | Open (no restrictions) | |
| Height Restrictions: | 15.05 (from SI&A) | ft. | 15.05 (from SI&A) | ft. |
| Curb to Curb Width: | 42 | ft. | 42 | ft. |
| Outside to Outside Width: | 45 | ft. | 45 | ft. |
| Shoulder Width: | 3 (both sides) | ft. | 3 (both sides) | ft. |
| Length of Channel Work: | N/A | | N/A | |

Remarks: Structure Nos. I64-123-02294 JCEB
 This structure is the I-64 eastbound lane (EBL) Indiana approach over Southern RR connecting to the Sherman Minton Bridge. The proposed work type for this structure is bridge deck overlay, replacement of lighting components, structural repairs, and partial bridge cleaning and painting (Appendix B, page 81).

The bridge deck overlay work includes milling the existing bridge deck to remove concrete cover and unsound concrete, deck and barrier concrete repairs, replacement of bridge deck expansion joints, and casting a new latex modified concrete overlay. The proposed work also includes installation of snow-plowable raised pavement markers, barrier delineators, and new pavement markings. The existing reinforced concrete approach slabs will be demolished, and new reinforced concrete approach slabs will be constructed.

The structural repair work includes repair or replacement of selected structural steel members. Specifically, the following members included in the work are:

- The bearings will be replaced.
- Girder ends will be repaired.

The partial cleaning and painting work include local repairs of the existing paint system and painting of areas disturbed by steel repair work. This work is to be completed under Des. No. 1702259.

Yes
 No
 N/A

Will the structure be rehabilitated or replaced as part of the project?

| 056B00161N | Existing | | Proposed | |
|----------------------|--------------------------|-----|--------------------------|-----|
| Bridge Type: | Steel multi girder | | Steel multi girder | |
| Number of Spans: | 27 spans (14 WB + 13 EB) | | 27 spans (14 WB + 13 EB) | |
| Weight Restrictions: | Open (no restrictions) | | Open (no restrictions) | |
| Height Restrictions: | 16'-3" | | 16'-3" | |
| Curb to Curb Width: | 42 | ft. | 42 | ft. |

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| | | | | |
|---------------------------|----------------|-----|----------------|-----|
| Outside to Outside Width: | 48'-3" | ft. | 48'-3" | ft. |
| Shoulder Width: | 3 (both sides) | | 3 (both sides) | |
| Length of Channel Work: | N/A | | N/A | |

Describe bridges and structures; provide specific location information for small structures.

Remarks: Structure No. 056B00161N

This structure is the I-64 eastbound lane (EBL) and westbound lane (WBL) Kentucky approach connecting to the Sherman Minton Bridge. The proposed work type for this structure is a bridge deck replacement and structural repairs (Appendix B, page 52).

The bridge deck replacement work includes demolition of the existing bridge decks, cleaning the top flanges of the girders, installing shear stud connectors, and casting a new reinforced concrete bridge deck with new expansion joints. The proposed bridge decks will feature bridge railings, longitudinal grooving, snow-plowable raised pavement markers, barrier delineators, and new pavement markings. The proposed decks will be surface sealed. The existing reinforced concrete approach slabs will be demolished, and new reinforced concrete approach slabs will be constructed.

The structural repair work includes repair or replacement of selected structural steel members. Specifically, the following members included in the work are:

- Repair of cracks in steel members.
- The lateral restraints will be repaired or replaced.
- Deteriorated bolts and rivets will be replaced with new bolts throughout the bridge.

The cleaning and painting work include removal of the existing paint system and application of a new paint system on all steel members of the bridge, above the tops of the substructures. This work will be completed under Des. Nos. 1702254 and 1702260, and KY Item ID 5-64.

Substructure repair work will be performed on the piers supporting this bridge. Proposed pier cap work includes:

- Concrete cover will be removed, reinforcement will be repaired as needed, deeper concrete repairs will be performed as needed, passive cathodic protection will be installed, and new cover concrete will be cast to restore the original surface of the pier caps. Pier caps beneath expansion joints will be surface sealed.

Columns, from bottom of pier caps down to existing ground line:

- These surfaces will receive substructure concrete repairs as needed. Reinforcement repair will be performed as needed, and passive cathodic protection will be installed in new concrete in the repaired areas.

| | | | |
|---|-------------------------------------|--------------------------|--------------------------|
| | Yes | No | N/A |
| Will the structure be rehabilitated or replaced as part of the project? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

| | Yes | No |
|---|-------------------------------------|-------------------------------------|
| Is a temporary bridge proposed? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Is a temporary roadway proposed? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Will the project involve the use of a detour or require a ramp closure? (describe in remarks) | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Provisions will be made for access by local traffic and so posted. | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Provisions will be made for through-traffic dependent businesses. | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Provisions will be made to accommodate any local special events or festivals. | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Will the proposed MOT substantially change the environmental consequences of the action? | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Is there substantial controversy associated with the proposed method for MOT? | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

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Remarks: MOT with temporary construction access for work on the bridge and both the Indiana and Kentucky sides of the Ohio River will be required throughout the duration of SMRP. Construction zone speed limit, travel lane, and access reductions and/or closures will reduce traffic on the Sherman Minton Bridge as well as divert traffic along detour routes and the local roadway network. A summary of the full traffic analysis is identified in Appendix I, page 11. Six MOT options were developed and evaluated during SMRP design engineering, traffic modeling, Community Impact Assessment (CIA) and EJ Analysis (in Section G – Community Impacts of this document (page 43) and Appendix I, page 90); MOT graphics are identified in Appendix B, page 10.

MOT 1 – Two lanes open, both decks (existing EB-eastbound and WB-westbound decks)
One (1) EB and one (1) WB lane will be closed for construction. Two (2) EB and two (2) WB travel lanes will remain open but will shift location on the bridge during each of the three (3) construction phases; existing access ramps will remain open.

MOT 2 – One lane open, both decks (EB and WB decks)
Two (2) EB and two (2) WB lanes will be closed for construction. One (1) EB and one (1) WB travel lanes will remain open but will shift location on the bridge during two (2) construction phases; existing access ramps will remain open. A temporary Kentucky crossover lane for to I-64 WB merge with I-264 will be required.

MOT 3 – Alternating three one-way lanes (AM-EB / PM-WB) open on one deck
One (1) bridge deck with three (3) lanes will be closed for construction during two (2) construction phases. The remaining bridge deck will be open with all three (3) travel lanes open in one direction only (EB 1AM to Noon), all travel will be closed during a 1-hour transition before reopening for travel in the opposite direction (WB 1PM to Midnight) Access ramp closures and a Kentucky crossover for I-64 WB merge with I-264 will be required.

MOT 4 – Reversible center lane (AM-EB / PM-WB) and one-way EB/WB lanes open on one deck
One (1) bridge deck with three (3) lanes will be closed for construction during two (2) construction phases. The remaining bridge deck will be open with one (1) EB and one (1) WB travel lanes, and movable barrier system that will change a (1) center lane from one-way EB (Midnight to Noon) to the opposite direction one-way WB (Noon to Midnight) each day. Access ramp closures and a Kentucky crossover for I-64 WB merge with I-264 will be required.

MOT 5 – Full Duration Closure of all six lanes and both decks
Both bridge decks, all six (6) lanes, and associated access ramps will be closed for construction; all traffic will be diverted to detour routes.

MOT 6 – One Direction/Phase three one-way lanes (WB-Phase 1/EB-Phase 2) open on one deck
One (1) bridge deck with three (3) lanes will be closed for construction during two (2) construction phases. During Phase 1, the remaining bridge deck will be open with all three (3) travel lanes open for only EB flow and all WB traffic will be diverted to detour routes. In Phase 2, the remaining bridge deck will be open with all three (3) travel lanes open for only WB flow and all EB traffic will be diverted to detour routes. Access ramp closures would be required.

As detailed in the Community Impact and EJ Analysis (in Section G - Community Impacts, page 43 of this document and Appendix I, page 90), public involvement, community profiles, and EJ population and demographics were evaluated for induced traffic diversions and related travel times, travel costs, congestion, and cross-river connectivity impacts. Quality of Life Factors (Air Quality and Noise Impacts) and Safety Factors (driver expectancy, emergency incident response access, and work zone safety) had similar and/or minimal temporary changes for the overall traveling public and local communities.

Minimization measures were part of project development, evaluations, and a combination of MOT options to reduced impacts. While the MOT 1 option had the most positive public involvement comments, fewest changes from existing travel patterns and local access, lowest induced traffic diversions and related environmental impacts; project constructability requirements were not entirely met. The Preferred Alternative MOT combined the predominant use of MOT 1, limited off-peak use of MOT-2 (*allowance for about 180 nights per construction year*), and short-term use of MOT-5. Detour routes are listed below and included in Appendix B, pages 19-26.

Preferred Alternative MOT –

- One (1) EB and one (1) WB lane will be closed throughout construction.

This is page 18 of 54 Project name: I-64 Bridge Rehabilitation Sherman Minton Renewal Project Date: October 2, 2020

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- Open travel lanes will shift location on the Sherman Minton Bridge during construction.
- Two (2) EB and two (2) WB travel lanes will remain open for cross-river traffic and existing access ramps will remain open except for the following allowances:
 - 180 nights per construction year during which two (2) EB and two (2) WB lanes and associated access ramps will be closed each night approximately from 9 pm to 4 am and 10 pm to 5 am, respectively. Cross-river traffic will be maintained with one (1) EB and one (1) WB travel lane open and a temporary crossover lane in Kentucky for I-64 WB to merge with I-264.
 - Short-term closure of the Sherman Minton Bridge will be allowed for one (1) nine (9) consecutive day period and up to three (3) weekend closures during each construction year; excluding holidays and community events detailed below. During the short-term bridge closure, all I-64 (US 150) cross-river traffic will be diverted to detour routes.
- Provisions are included for local traffic access and through-traffic dependent businesses by retaining existing access ramps in Indiana and Kentucky; through-traffic dependent businesses by maintaining cross-river travel lanes in both directions; public notification, signage according to MOT, and posting requirements during construction; and detour routes that remain within the interstate system to alternate local river crossings (Appendix B, page 19):

Indiana

- I-64 through traffic will be detoured via I-265 and the I-65 paired Kennedy/Lincoln bridges
- Local access to New Albany would follow the same detour route or remain on the local roadway network; State Street will be the detour route during Spring Street access ramp closures.

Kentucky

- Both I-264 and I-64 through traffic will be detoured via the I-65 paired Kennedy/Lincoln bridges
- Local access to west Louisville would follow the same detour route or remain on the local arterial network

- Minimization measures included additional accommodation for local special events and festivals with the exclusion of bridge closure work during the following:

New Year's Day - If New Year's Day falls on a Sunday, work shall be suspended from noon December 31 until sunrise January 3. or if New Year's Day falls on a Monday through Saturday, work shall be suspended from noon December 31 until sunrise January 2.

Good Friday - Work shall be suspended from noon on Good Friday until sunrise Monday.

Memorial Day - Work shall be suspended from noon the Friday before Memorial Day until sunrise Tuesday, the day after Memorial Day.

Independence Day - If Independence Day falls on a:

Sunday - work shall be suspended from noon Friday, July 2, until sunrise Tuesday, July 6.

Monday - work shall be suspended from noon Friday, July 1, until sunrise Tuesday, July 5.

Tuesday - work shall be suspended from noon Friday, June 30, until sunrise Wednesday, July 5.

Wednesday - work shall be suspended from sunset on Tuesday, July 3, until sunrise Thursday, July 5.

Thursday - work shall be suspended from noon Wednesday, July 3, until sunrise Monday, July 8.

Friday - work shall be suspended from noon Thursday, July 3, until sunrise Monday, July 7.

Saturday - work shall be suspended from noon Thursday, July 2, until sunrise Monday, July 6.

Labor Day - Work shall be suspended from noon the Friday before Labor Day until sunrise Tuesday, the day after Labor Day.

Thanksgiving Day - Work shall be suspended from noon the Wednesday before Thanksgiving Day until sunrise the Monday after Thanksgiving Day.

Christmas Day - Work shall be suspended from noon December 24 until sunrise December 27.

Thunder Over Louisville - Work suspended from Midnight Friday till 6:00 a.m. Sunday.

Kentucky Derby - Work suspended from Thursday at midnight until Monday at 6:00 am.

Harvest Homecoming Festival - First Saturday in October to second Saturday in October.

Based upon early coordination meetings with regulatory agencies and public outreach, at this time, there is no substantial controversy concerning the Preferred Alternative MOT option.

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ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 2,500,000 (2021) Right-of-Way: \$ 0.00 (2021) Construction: \$ 78,114,055 (2021)

Anticipated Start Date of Construction: Spring 2021

Date project incorporated into IN STIP April 15, 2020
 Date project incorporated into KY STIP March 27, 2020

Is the project in an MPO Area? **Yes** **No**

If yes,
 Name of MPO Kentuckiana Regional Planning and Development Agency (KIDPA)

Location of Project in TIP KIPDA 2020-2025 Transportation Improvement Program Page 140

Date of incorporation by reference into the IN STIP April 15, 2020
 Date of incorporation by reference into the KY STIP March 27, 2020

RIGHT OF WAY:

| Land Use Impacts | Amount (acres) | |
|------------------|----------------|-----------|
| | Permanent | Temporary |
| Residential | 0 | 0 |
| Commercial | 0 | 0 |
| Agricultural | 0 | 0 |
| Forest | 0 | 0 |
| Wetlands | 0 | 0 |
| Other: | 0 | 0 |
| Other: | 0 | 0 |
| TOTAL | 0 | 0 |

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition or reacquisition, either known or suspected, and there impacts on the environmental analysis should be discussed.

Remarks: This project will occur within existing right-of-way (ROW). The existing ROW varies throughout the project. No permanent or temporary ROW will be required for this project. The total work area is depicted in Appendix B, page 6. Work within the ROW limits are broken down in Table 4 as follows:

| Table 4. Sherman Minton Renewal Project: Right of Way Breakdown | | | | | |
|---|---------------------|-----------------------|--|---|--|
| Des. No. | Bridge No. / Street | Description | Work Type | Limits | Distance beyond Edge of Pavement to ROW (ft) |
| 1702255 & 1592187 | 164-123-04691D | Sherman Minton Bridge | Bridge Deck Replacement, Structural Repairs, Substructure Repairs, and Bridge Painting | Sherman Minton Bridge 164-123-04691 D extends from 0.32 miles south of the Elm Street south on Ramp to 0.45 miles from the I-264 south off Ramp | 0.0-84.0 |

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| | | | | | |
|-------------------------|------------------------|--|--|--|-----------------|
| 1702260 & 1702254 | 056B00161N | KY Approaches | Bridge Deck Replacement, Substructure Repairs, and Bridge Painting | extend 0.45 miles from the I-264 south off ramp to 0.11 miles from the I-264 off ramp | 0.0 |
| 1702257 | I64-123-02294 CWBL | I-64 WB over SR 111/Main Street, RR (IN Approach WB) | Bridge Deck Overlay and Bridge Painting | 0.32 miles south of the Elm Street North off Ramp to 0.17 miles south of the Elm Street North off Ramp | 0.0 |
| 1702258 | I64-123-02294 CEBL | I-64 EB over SR 111/Main Street (IN Approach EB) | Bridge Deck Overlay and Bridge Painting | 0.19 miles south of the Elm Street South on ramp to 0.23 miles south of the Elm Street South on Ramp | |
| 1702259 | I64-123-02294 JCEB | I-64 EB over Southern RR (IN Approach EB over RR) | Bridge Deck Overlay and Bridge Painting | 0.26 miles south of the Elm Street South on ramp to 0.32 miles south of the Elm Street South on Ramp | |
| 1701215 | Elm Street | Old SR 62 (Elm Street) from I-64 Exit Ramp to State Street and Spring Street | HMA Overlay, Preventative Maintenance, and ADA Ramps | Elm Street from I-64 exit Ramp to 0.04 miles west of State Street. | |
| | 5 th Street | | | 5th Street from 0.02 miles north of SR 111 to Spring Street. | 0.0 |
| | Spring Street | | | Spring Street from W. 5th Street to 0.02 miles West of State Street. | 3.9-10.4 |
| 1900579 | I64-123-04690 BEBL | I-64 EB over Market Street 0.11 W of SR 111 | Bridge Painting | 0.09 miles south of the Elm Street South on Ramp to 0.13 miles south of the Elm Street South on Ramp | 0.0 |
| Total: | | | | | Range: 0.0-84.0 |

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

Streams, Rivers, Watercourses & Jurisdictional Ditches

- Federal Wild and Scenic Rivers
- State Natural, Scenic or Recreational Rivers
- Nationwide Rivers Inventory (NRI) listed
- Outstanding Rivers List for Indiana
- Navigable Waterways

Presence

| |
|---|
| X |
| |
| |
| |
| |
| X |

Impacts

| Yes | No |
|-----|----|
| | X |
| | |
| | |
| | |
| | |
| | X |

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Remarks:

Based on a desktop review, a site visit on September 18-20, 2018 and May 6-7, 2019 by *Kaskaskia Engineering*, the aerial map of the project area (Appendix B, page 4), and the water resources map in the Red Flag Investigation (RFI) report (Appendix E, page 10) there are three rivers and streams located within the 0.5 mile search radius of the Sherman Minton Bridge. There is one river present within the project area. No Federal, Wild and Scenic Rivers or National Park Service's Nationwide River Inventory (NRI) List waterways are located within the project area.

A *Waters of the U.S. Determination / Wetland Delineation Report* was completed for the project and the INDOT Ecology and Waterway Permitting Office approved the report on September 20, 2019. The *Waters of the U.S. Determination / Wetland Delineation Report's* investigated area covers both Indiana and Kentucky and is larger than the SMRP area limits. Please refer to Appendix F, page 3 for the *Waters of the U.S. Determination / Wetland Delineation Report*. The entire project area, both in Indiana and Kentucky, falls under the jurisdiction of the Louisville U.S. Army Corps of Engineers (USACE). It was determined that six (6) Waters of the U.S. (federally regulated) streams are located within the project area. No isolated (state regulated) streams are located within the project area. One (1) river, the Ohio River, borders both Indiana and Kentucky. The remaining five (5) streams are located within Indiana. The USACE makes all final determinations regarding jurisdiction.

Both Indiana & Kentucky

Ohio River: Borders Indiana and Kentucky as depicted in Appendix F, page 52. An estimated 176 LF of the Ohio River is within the project area. The Ohio River is a perennial river. The Ordinary High-Water Mark (OHWM) is approximately 1,700 feet wide and the depth is unknown. The Louisville USACE classifies the Ohio River as a navigable Waters of the U.S. and a Section 10 waterway.

The Ohio River is listed as an Indiana Department of Environmental Management (IDEM) 303d impaired waters for dioxin, E. coli, Total Mercury in water, polychlorinated biphenyls (PCBs) in water and PCBs in fish tissue. The Ohio River is also listed as a Kentucky Energy and Environment Cabinet (EEC) 303d impaired waters for dioxin, E. coli, and PCBs. Workers who are working in or near water with E. coli should take care to wear appropriate (Personal Protective Equipment) PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure. If there will be sediment and/or soils disturbed by construction, additional investigation may be necessary. Coordination with INDOT ES and KYTC will occur. These recommendations are included as firm commitments in the Environmental Commitments section of this document.

No work is anticipated within the Ohio River or along the Indiana or Kentucky banks of the Ohio River. Therefore, no impacts to the Ohio River are anticipated. If the Design-Build Contractor requires work within the Ohio River, the Design-Build Contractor will be responsible for assessing impacts, applying for permits, any associated documentation, and coordination with the necessary agencies. It has not been decided if a causeway or barge will be used, however; if the Design-Build Contractor requires the need, the Contractor will be responsible for assessing impacts, applying for permits, any associated documentation, coordination with necessary agencies and following the environmental commitments set forth by the agencies.

Indiana

No State Natural, Scenic, Recreational Rivers and Outstanding Rivers for Indiana waterways are present in the project area. Five (5) jurisdictional streams were identified on the Indiana side of the project area as per the Waters of the U.S. Report.

Valley View Creek: an estimated 1,664 linear feet (LF) of Valley View Creek is within the investigated area. Located north of the I-64/Spring Street intersection and depicted on graphic in Appendix F, page 50. Valley View Creek would likely be classified as perennial because it has in-channel structure (riffles and pools) and base flow. The substrate in the stream is dominated by gravel and cobble. An ordinary high water mark (OHWM) was observed that was 18 feet wide and 15 inches deep. No impact is anticipated.

Falling Run: an estimated 1,075 LF of Falling Run is within the investigated area. Falling Run flows from northeast to southwest under I-64 along the north side of the I-64 and Spring Street interchange as depicted in Appendix F, page 50. It would likely be classified as perennial because it has in-channel structure and base flow. An OHWM was observed that was 34 feet wide and 15 inches deep. No impact is anticipated.

Unnamed Tributary (UNT) 1 to Falling Run: an estimated 969 LF of UNT 1 to Falling Run is within the investigated area. UNT 1 to Falling Run flows northwest along the east side of the I-64 Spring Street interchange as depicted in Appendix F, page 50 & 51. It would likely be classified as intermittent. Water sits within the channel but does not appear to have base flow. An OHWM was observed that was 12 feet wide and 24 inches deep. No impact is anticipated.

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UNT 2 to Falling Run: an estimated 689 LF of UNT 2 to Falling Run is within the investigated area. UNT 2 to Falling Run flows east to west from within the I-64 and Spring Street interchange and through Wetlands 6 and 5. Location is depicted in Appendix F, page 50. It would likely be classified as intermittent. An OHWM was observed that was 3 feet wide and 8 inches deep. No impact is anticipated.

UNT to Ohio River: an estimated 206 LF of UNT to the Ohio River is within the investigated area. UNT to Ohio River is a short channel that drains from a culvert under the railroad tracks along the Ohio River as depicted in Appendix F, page 52. An OHWM was observed that was 3.5 feet wide and 10 inches deep. No impact is anticipated.

No work is anticipated within any waterway. Therefore, no impacts are anticipated. If the Design-Build Contractor requires work within any waterway, the Design-Build Contractor will be responsible for assessing impacts, applying for permits, any associated documentation, and coordination with the necessary agencies. It has not been decided yet if a causeway or barge will be used, however; if the Design-Build Contractor requires the need, the Design-Build Contractor will be responsible for assessing impacts, applying for permits, any associated documentation, coordination with necessary agencies and following the environmental commitments set forth by the agencies.

Kentucky
No State Natural, Scenic, Recreational Rivers and Outstanding Rivers for Kentucky are present in the project area. No waterways, besides the Ohio River, is located within the project area on the Kentucky side as per the Waters of the U.S. Report. The Ohio River description is listed above. No impacts are anticipated.

KYTC Division of Environmental Analysis inserted a special note in accordance to Section 7 of the Endangered Species Act, for construction activities on March 6, 2020, stating the following, "No impacts due to construction, including pieces falling from construction will occur to the Ohio River. Should impacts be unavoidable the contractor will be responsible for coordination to obtain clearance for section." This is included as a firm commitment.

Early Coordination for both Indiana and Kentucky
Early coordination letters were sent on December 11, 2018. U.S. Environmental Protection Agency (USEPA) responded on February 7, 2019 with comments and recommendations (Appendix C, page 4). EPA recommends the environmental review identify and assess potential impacts to water resources, identify best management practices (BMPs) and mitigation measures that will be used to prevent/reduce potential impacts associated with renewal activities. United States Army Corps of Engineers (USACE) responded January 14, 2019, and May 6, 2020, with comments (Appendix C, page 13). USACE comments avoidance, minimization, or potential mitigation will be required to minimize adverse impact to aquatic resources. USACE recommends the submittal of a 33 CFR 322 Department of the Army (DA) permit application in the case an individual permit is to be required. United States Fish and Wildlife Service (USFWS) responded February 20, 2019 (Appendix C, page 23), with a list of recommendations in the case a causeway or barge must be used during construction. USFWS also recommends restricting work below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slops around the bridge abutments and placement of riprap. United States Coast Guard (USCG) District 8 responded December 18, 2018. USCG stated a Coast Guard Bridge Permit will not be required (Appendix C, page 78). Indiana Department of Environmental Management (IDEM) did not formally respond, however; a standard automated response (Appendix C, page 80) was generated March 19, 2019. A Response received from Indiana Department of Natural Resources (IDNR) on March 11, 2019, recommending as list of measures to be implemented to avoid, minimize, or compensate for impacts should they occur to fish wildlife and botanical resources (Appendix C, page 17). No response received from Kentucky Division of Water (KDOW). All applicable USEPA, USACE, USFWS, and IDNR recommendations are included in the Environmental Commitments section of this CE document.

| Other Surface Waters | <u>Presence</u> | <u>Impacts</u> | |
|-----------------------------------|-------------------------------------|--------------------------|-------------------------------------|
| | | <u>Yes</u> | <u>No</u> |
| Reservoirs | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Lakes | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Farm Ponds | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Detention Basins | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Storm Water Management Facilities | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Other: <u>Golf Course Pond</u> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

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Remarks: Based on a desktop review, a site visit on September 18-20, 2018 and May 6-7, 2019 by Kaskaskia Engineering, the aerial map of the project area (Appendix B, page 4), and the water resources map in the RFI report (Appendix E, page 10) there are three (3) lakes located within the 0.5 mile search radius of the Sherman Minton Bridge. There is one (1) other surface waters (referred to as open water resources in the *Waters of the U.S. Determination/Wetland Delineation Report*) located within the project area.

A *Waters of the U.S. Determination / Wetland Delineation Report* was completed for the project area, including both Indiana and Kentucky, and the INDOT Ecology and Waterway Permitting Office approved the report on September 20, 2019. Please refer to Appendix F, page 3 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that one (1) jurisdictional open water resource is located within the project area. This one (1) open water resource, identified as OW 1, is located within Kentucky. No open water resources and other surface waters are located within the project area on the Indiana side. The USACE makes all final determinations regarding jurisdiction.

Indiana
Based on review of the water resource map in the RFI report (Appendix E, page 10), there are three (3) lakes located within the 0.5 mile search radius of the Sherman Minton Bridge on the Indiana side. There are no lakes or other surface waters present within or adjacent to the project area on the Indiana side. Therefore, no impacts are anticipated.

Kentucky
One (1) open water was identified on the Kentucky side of the project area in the *Waters of the U.S. Determination/Wetland Delineation Report* (Appendix F, page 3).

Open Water (OW) 1: OW 1 is a man-made golf course pond located within the Shawnee Golf Course. The quality of the pond was considered poor due to surrounding non-native vegetation and due to its primary source of hydrology via runoff from the golf course as well as the nearby I-64 roadway slope. This feature does not drain to or from a likely water of the U.S. and is not adjacent to a likely water of the U.S. However, since it is within the floodplain of the Ohio River, approximately 980 feet from the OHWM of the Ohio River, this feature is likely a jurisdictional water feature. OW 1 is not classified as an NWI wetland. No vegetation exists within the pond.

No work is anticipated within the open water pond and therefore no impact is anticipated. The Design-Build Contractor is to remain within the existing roadway ROW. If the Design-Build Contractor deviates from the existing roadway ROW, the Contractor will be responsible for assessing impacts, applying for the necessary permits, associated documentation, and any mitigation.

Early Coordination for both Indiana and Kentucky
Early coordination letters were sent on December 11, 2018. USEPA responded on February 7, 2019 with no comments and recommendations pertaining to other surface waters (Appendix C, page 4). USACE responded January 14, 2019 and May 6, 2020, with no comments or recommendations pertaining to other surface waters. (Appendix C, page 13). USFWS responded February 20, 2019 (Appendix C, page 23), no comments or recommendations pertaining to other surface waters. USCG District 8 responded December 18, 2018 (Appendix C, page 78). IDEM did not formally respond, however; a standard automated response (Appendix C, page 80) was generated March 19, 2019. A response was received from IDNR March 11, 2019, with no comments or recommendations pertaining to other surface waters, (Appendix C, Page 17). No response was received from KDOW.

| | | | |
|-----------------|-------------------------------------|--------------------------|-------------------------------------|
| | <u>Presence</u> | <u>Impacts</u> | |
| | <input checked="" type="checkbox"/> | Yes | No |
| Wetlands | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Total wetland area: 6.31 acre(s) Total wetland area impacted: 0.0 acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

| Wetland No. | Classification | Total Size (Acres) | Impacted Acres | Comments |
|-------------|------------------------|--------------------|----------------|---------------|
| Wetland 5 | Palustrine Forested | 3.52 | 0.00 | Located in IN |
| Wetland 6 | Palustrine Forested | 0.06 | 0.00 | Located in IN |
| Wetland 7 | Palustrine Scrub-Shrub | 0.27 | 0.00 | Located in IN |

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| | | | | |
|------------|------------------------|------|------|---------------|
| Wetland 8 | Palustrine Forested | 0.32 | 0.00 | Located in IN |
| Wetland 9 | Palustrine Emergent | 0.31 | 0.00 | Located in IN |
| Wetland 10 | Palustrine Forested | 0.56 | 0.00 | Located in KY |
| Wetland 11 | Palustrine Emergent | 0.60 | 0.00 | Located in KY |
| Wetland 12 | Palustrine Scrub-Shrub | 0.15 | 0.00 | Located in KY |
| Wetland 13 | Palustrine Forested | 0.52 | 0.00 | Located in KY |

Documentation

ES Approval Dates

Wetlands (Mark all that apply)

- Wetland Determination
- Wetland Delineation
- USACE Isolated Waters Determination
- Mitigation Plan

| |
|---|
| X |
| X |
| |
| |

| |
|--------------------|
| September 20, 2019 |
| September 20, 2019 |
| |
| |

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.

| |
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| |

Measures to avoid, minimize, and mitigate wetland impacts need to be discussed in the remarks box.

Remarks: Based on a desktop review of the National Wetlands Inventory (NWI) online mapper (<https://www.fws.gov/wetlands/data/Mapper.html>), a site visit on September 18-20, 2018 and May 6-7, 2019 by Kaskaskia Engineering, the USGS topographic (Appendix E, page 8), and the RFI report (Appendix E, page 10) there are four (4) wetlands located within the 0.5 mile search radius of the Sherman Minton Bridge. There are three (3) wetlands present adjacent to the project area.

A *Waters of the U.S. Determination / Wetland Delineation Report* was completed for the project area, including both Indiana and Kentucky, and the INDOT Ecology and Waterway Permitting Office approved the report on September 20, 2019. Please refer to Appendix F, page 3 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that nine (9) jurisdictional wetlands are located within the project area, five (5) wetlands are located within Indiana and four (4) wetlands are located within Kentucky. The USACE makes all final determinations regarding jurisdiction.

Indiana
Based on a desktop review of the RFI report (Appendix E, page 10), four (4) wetlands are located within the 0.5 mile search radius. The *Waters of the U.S. Determination / Wetland Delineation Report* identified five (5) wetlands within the project area on the Indiana side.

Wetland 5: as depicted in Appendix F, Page 50. Wetland 5 is an approximately 3.52 acre palustrine forested wetland located west of the I-64 eastbound exit ramp to Spring Street, within the Midwest Region. A portion of the western section of Wetland 5 is classified as an NWI wetland (PFO1A). Wetland 5 is within the floodplain of Falling Run. No impact is anticipated.

Wetland 6: as depicted in Appendix F, Page 50. Wetland 6 is an approximately 0.06 acre palustrine forested wetland located in a drainage depression adjacent to the I-64 EB exit ramp to Spring Street, within the Midwest Region. Wetland 6 is not classified as an NWI wetland. No impact is anticipated.

Wetland 7: as depicted in Appendix F, Page 50. Wetland is an approximately 0.27 acre palustrine shrub-scrub wetland located east of I-64, near Anderson Park, within the Midwest Region. Wetland 7 is not classified as an NWI wetland. Wetland 7 is within the floodplain of Falling Run. No impact is anticipated.

Wetland 8: as depicted in Appendix F, Page 50 & 51. Wetland 8 is an approximately 0.32 acre palustrine forested

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wetland located in the NE quadrant of the I-64/Spring Street interchange, near Billy Herman Fields, within the Midwest Region. Wetland 8 is not classified as an NWI wetland. Wetland 8 is within the floodplain of Falling Run. Due to its hydrological connection to UNT 1 to Falling Run, this feature is likely a jurisdictional wetland. No impact is anticipated.

Wetland 9: as depicted in Appendix F, Page 50& 51. Wetland 9 is an approximately 0.31 acre palustrine emergent wetland located between the WB I-64 entrance and exit ramps at I-64/Spring Street interchange, within the Midwest Region. Wetland 9 is not classified as an NWI wetland. No impact is anticipated.

No impacts to any wetlands in Indiana are anticipated, as the Design-Build Contractor is to remain within the existing roadway ROW, where no wetlands were identified. If the Design-Build Contractor deviates from the existing roadway ROW, the Design-Build Contractor will be responsible for assessing impacts, applying for the necessary permits, associated documentation, and any mitigation.

Kentucky:
The *Waters of the U.S. Determination / Wetland Delineation Report* identified four (4) wetlands within the project area on the Kentucky side.

Wetland 10: as depicted in Appendix F, Page 53. Wetland 10 is an approximately 0.56 acre palustrine forested wetland located adjacent to the Kentucky east bank of the Ohio River, within the Eastern Mountains and Piedmont Region. A portion of Wetland 10 is classified as an NWI wetland (PFO1Ah). It is within the floodplain of the Ohio River. No impact is anticipated.

Wetland 11: as depicted in Appendix F, Page 54. Wetland 11 is an approximately 0.6 acre palustrine emergent wetland under the WB lanes of the Sherman Minton Bridge and extending along the north side EB I-64 within the Eastern Mountains and Piedmont region. Wetland 11 is not classified as an NWI wetland. It is within the floodplain of the Ohio River. No impact is anticipated.

Wetland 12: as depicted in Appendix F, Page 54. Wetland 12 is an approximately 0.15 acre shrub-scrub wetland north of the I-64 WB I-264 interchange, at the edge of a golf course. Wetland 12 is not classified as an NWI wetland. It is within the floodplain of the Ohio River. No impact is anticipated.

Wetland 13: as depicted in Appendix F, Page 54. Wetland 13 is an approximately 0.52 acre palustrine forested wetland north of the I-64 WB I-264 interchange, at the edge of a golf course within the Eastern Mountains and Piedmont region. Wetland 13 is not classified as an NWI wetland. It is within the floodplain of the Ohio River. No impact is anticipated.

No impacts to any wetlands in Kentucky are anticipated, as the Design-Build Contractor is to remain within the existing roadway ROW, where no wetlands were identified. If the Design-Build Contractor deviates from the existing roadway ROW, the Design-Build Contractor will be responsible for assessing impacts, applying for the necessary permits, associated documentation, and any mitigation.

Early Coordination for both Indiana and Kentucky
Early coordination letters were sent on December 11, 2018. USEPA responded on February 7, 2019 with no comments and recommendations pertaining to other wetlands (Appendix C, page 4). USACE responded January 14, 2019 and May 6, 2020, with no comments or recommendations pertaining to other surface waters. (Appendix C, page 13). USFWS responded February 20, 2019 (Appendix C, page 23), no comments or recommendations pertaining wetlands. USCG District 8 responded December 18, 2018 (Appendix C, page 78). No response was received from KDOW. IDEM did not formally respond, however; a standard automated response (Appendix C, page 80) was generated March 19, 2019. A response was received from IDNR March 11, 2019, with comments or recommendations pertaining to wetlands, (Appendix C, Page 17).

Terrestrial Habitat
Unique or High Quality Habitat

| <u>Presence</u> | <u>Impacts</u> | |
|-----------------|----------------|-----------|
| | <u>Yes</u> | <u>No</u> |
| X | | X |
| | | |

Use the remarks box to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).

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Remarks:

Indiana
 Based on a desktop review, a site visit on July 18, 2019 by Michael Baker International, and the aerial map of the project area (Appendix B, page 4), there is terrestrial habitat present on the Indiana side of the project. There are trees located along the bank of the Ohio River underneath the Sherman Minton Bridge along with maintained lawns.

An early coordination IDNR response letter dated March 11, 2019 stated according to the Natural Heritage Database the following have been documented within 0.5 mile of the project area (on the Indiana side): pretty dodder (*Cuscuta indecora*) an Indiana state endangered plant, long beak arrowhead (*Sagittaria asustralis*) an Indiana state rare plant, and the purple passion-flower (*Passiflora incarnata*) an Indiana state watchlist plant. The Division of Natural Preserves does not foresee any impacts to the plant species (Appendix C, Page 17).

No work will take place outside of the existing ROW and no tree clearing is anticipated. Any terrestrial habitats present on then Indiana side of the project area face no foreseeable impacts to habitat or plant species. If the Design-Build Contractor requires work outside of the existing ROW, the Design-Build Contractor will be responsible for assessing impacts, coordination with the necessary agencies, and preparing any necessary documentation.

Kentucky
 Based on a desktop review, a site visit on July 18, 2019 by Michael Baker International, and the aerial map of the project area (Appendix B, page 4), there is terrestrial habitat present on the Kentucky side of the project. A riparian forest corridor is located along the Ohio River underneath the Sherman Minton Bridge. There is also a public city park, Shawnee Park, which includes the public Shawnee Golf Course, that contain stands of trees and maintained lawns. The riparian forest is dominated by silver maple (*Acer saccharinum*). Other trees identified within the project area and within the Shawnee Golf Course include green ash (*Fraxinus pennsylvanica*) and eastern cottonwood (*Populus deltoides*).

The USFWS List of threatened and endangered species identified that the endangered running buffalo clover (*Trifolium stoloniferum*) has the potential to be within the project area (Appendix C, page 59). The USFWS Kentucky Ecological Services Field Office has running buffalo clover habitat assessment guidance (Appendix C, page 73). No critical habitat of the species has been identified within the project area. KTYC reviewed the project area and USFWS coordination and has agreed no impacts to terrestrial habitats are anticipated (Appendix C, Page 73)

The Design-Build Contractor is to remain within the existing ROW and no tree clearing is anticipated. Therefore, no impacts are expected. If the Design-Build Contractor deviates from the existing ROW, the Design-Build Contractor will be responsible for identifying and conducting any necessary habitat assessments, coordination with the necessary agencies, and preparing any other required documentation. The guidelines for a habitat assessment are identified in (Appendix C, page 70).

Early Coordination for both Indiana and Kentucky
 Early coordination letters were sent on December 11, 2018. USEPA responded on February 7, 2019 with no comments and recommendations pertaining to terrestrial habitat (Appendix C, page 4). USACE responded January 14, 2019 and May 6, 2020, with no comments pertaining to terrestrial habitat (Appendix C, page 13). USFWS responded February 20, 2019 (Appendix C, page 23), with recommendations including do not clear trees or understory vegetation outside of the construction zone boundaries. USCG District 8 responded December 18, 2018 with no comments pertaining to terrestrial habitat (Appendix C, page 78). IDEM did not formally respond, however; a standard automated response (Appendix C, page 80) was generated March 19, 2019. A response from IDNR received March 11, 2019, included recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible (Appendix C, page 17). No response was received KDOW. All applicable USFWS and IDNR recommendations are included in the Environmental Commitments section of this document.

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

Karst

| | Yes | No |
|---|--------------------------|-------------------------------------|
| Is the proposed project located within or adjacent to the potential Karst Area of Indiana? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Are karst features located within or adjacent to the footprint of the proposed project? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Is the proposed project located within or adjacent to the potential Karst Area of Kentucky? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Are karst features located within or adjacent to the footprint of the proposed project? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| If yes, will the project impact any of these karst features? | <input type="checkbox"/> | <input type="checkbox"/> |

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Use the remarks box to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

Remarks: **Indiana**
 Based on a desktop review, the project is located outside the designated karst region of Indiana as outlined in the October 13, 1993 Memorandum of Understanding (MOU). According to the topo map of the project area (Appendix B, page 3) and the RFI report (Appendix E, page 1), there are no karst features identified within or adjacent to the project area. In the early coordination response, the Indiana Geological Survey (IGS) did not indicate that karst features exist in the project area (Appendix C, page 85). IGS identified geological hazards consisting of floodway and high liquefaction potential. IGS also identifies high potential of sand and gravel resources in the area. Response from IGS will be communicated with the Design-Build Contractor through a commitment, included in the Environmental Commitments section of this document. No impacts are expected.

Kentucky
 Based on a desktop review, the project is located outside a designated karst region of Kentucky according to Kentucky Geological Survey (KGS) viewer map; <https://kgs.uky.edu/kgsmmap/kgsgeoserver/viewer.asp>. According to KGS, no karst features are identified within or adjacent to the project area. The project area consists of Glacial Outwash with primary lithology of sand, gravel, silt and clay. No Impacts are expected.

| Threatened or Endangered Species | Presence | Impacts | |
|--|-------------------------------------|--------------------------|-------------------------------------|
| | | Yes | No |
| Within the known range of any federal species | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Any critical habitat identified within project area | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Federal species found in project area (based upon informal consultation) | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| State species found in project area (based upon consultation with IDNR) | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

| | | |
|--|-------------------------------------|---|
| Is Section 7 formal consultation required for this action? | <input type="checkbox"/> Yes | <input checked="" type="checkbox"/> No |
|--|-------------------------------------|---|

Remarks: **Indiana**
 Based on a desktop review and the RFI report (Appendix E, page 5), completed by Parsons on March 28, 2019, the IDNR Floyd County Endangered, Threatened and Rare (ETR) Species List has been checked and is included in (Appendix E, page 14). The highlighted species on the list reflect the federal and state identified ETR species located within the county. An early coordination letter response was received from IDNR Division of Fish and Wildlife (DFW) on March 11, 2019. According to the Natural Heritage Database, the Kirtland's Snake (*Clonophis kirtlandii*) an Indiana state endangered species, has been documented within a half mile of the project area. The Division of Natural Preserves does not foresee any impacts to the Kirtland's Snake as a result of this project. (Appendix C, Page 17). If the Design-Build Contractor proposes to do any work outside of the existing ROW within Indiana, coordination with the IDNR-DFW shall occur.

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, page 43). The Project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). Other species were found within or adjacent to the project area along with the Indiana and northern long-eared bat.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on August 28, 2019, and based on the responses provided, the project was found to have "no effect" to the Indiana bat and/or the NLEB (Appendix C, page 30).

The official species list generated from IPaC on August 28, 2019, indicated two (2) other species present, one (1) mammal and one (1) clam, within the project area of Indiana. No critical habitat was identified. The species include the following:

Mammal identified:

- Gray Bat (*Myotis grisescens*) is an endangered species. The Gray Bat was identified in the official species list and though the USFWS response on February 20, 2019 (Appendix C, page 23). The Gray Bat is a southern species

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which inhabits caves year-round and typically migrates between winter hibernation caves and summer cave roots used for reproduction and foraging. Preferred foraging habitat is typically along wooded stream corridors and their forage base often includes a high percentage of aquatic insects. There is only one significant summer maternity colony known in Indiana, in southern Clark County. Previous studies through USFWS have shown that Silver Creek and Muddy Fork are the main foraging habitat for this colony. Based on the project description and location, USFWS does not anticipate impacts to gray bats or their habitat. The official's species list indicates no critical habitat has been designated for this species.

Clam identified:

- Sheepnose Mussel (*Plethobasus cyphus*) also known as is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 60. USFWS response on February 20, 2019 (Appendix C, page 23) identifies the Sheepnose mussel as a medium-sized mussel that grows to about 5 inches in length. It lives in larger rivers and streams where it is usually found in hallow areas with moderate to swift flowing currents flowing over coarse sand and gravel. USFWS considers the sheepnose mussel extant in the Ohio River in Floyd County.

No further coordination with Indiana USFWS is needed at this time. No work is anticipated to take place outside of existing roadway ROW or within any waterway. Therefore, no impacts are anticipated.

Kentucky:

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, page 50). The Project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). Other species were found within or adjacent to the project area along with the Indiana and northern long-eared bat.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on August 28, 2019, and based on the responses provided, the project was found to have "no effect" to the Indiana bat and/or the NLEB (Appendix C, page 30).

The official Species list generated from IPaC on August 28, 2019, indicated thirteen (13) other species present within the project area of Kentucky. The species include the following:

Mammal identified:

- Gray Bat (*Myotis grisescens*) is an endangered species. The Gray Bat was identified in the official species list and though the USFWS response on February 20, 2019 (Appendix C, page 23). The Gray Bat is a southern species which inhabits caves year-round and typically migrates between winter hibernation caves and summer cave roots used for reproduction and foraging. Preferred foraging habitat is typically along wooded stream corridors and their forage base often includes a high percentage of aquatic insects. There is only one significant summer maternity colony known in Indiana, in southern Clark County. Previous studies through USFWS have shown that Silver Creek and Muddy Fork are the main foraging habitat for this colony. Based on the project description and location, USFWS does not anticipate impacts to gray bats or their habitat. The official's species list indicates no critical habitat has been designated for this species.

Bird identified:

- Least Tern (*Sterna antillarum*) is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area.

Clams identified:

- Clubshell (*Pleurobema clava*) is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Fanshell (*Cyprogenia stegaria*) is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Northern Riffleshell (*Epioblasma torulosa rangiana*) is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Orangefoot Pimpleback (*Plethobasus cooperianus*), also known as is an endangered species considered to exist in

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the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.

- Purple Cat's Paw (*Epioblasma obliquata obliquata*), also known as is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Rabbitsfoot (*Quadrula cylindrica cylindrica*), also known as is a threatened species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Ring Pink mussel (*Obovaria retusa*) also known as is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Rough Pigtoe (*Pleurobema plenum*) also known as is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Sheepnose Mussel (*Plethobasus cyphus*) also known as is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Spectaclecase mussel (*Cumberlandia monodonta*) also known as is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.

On March 6, 2020 KYTC Division of Environmental Analysis concurred with USFWS Consistency Letter "No effect" finding for the Indiana bat, NLEB and other species listed, (Appendix C, page 30). KYTC states the finding is contingent upon no effects to the Ohio River, (Appendix C, page 77). In follow up a special note for construction activities was placed and will be found in the Environmental Commitments section of this CE document.

No early coordination letter response was received from Kentucky Department of Natural Resources (KDNR). If the Design-Build Contractor proposes to do any work outside of the existing ROW within Kentucky, coordination with the KDNR shall occur.

No further coordination with Kentucky USFWS is needed at this time. No work is anticipated to take place outside of existing roadway ROW or within any waterway. Therefore, impacts are not anticipated.

Both Indiana and Kentucky

It has not been decided yet if a causeway or barge will be used, however; if the Design-Build Contractor requires the need, coordination will be required. A USFWS letter dated February 20, 2019, stated, recommend that remediation/rehabilitation work be performed from the bridge or via barges where possible and the use of causeways and other in-stream construction be avoided. Causeways can cause substantial changes in flow patterns and restrict fish passage at lower flows; they can also adversely affect mussels within and near the causeway construction area. During low-flow conditions excessive channel blockage can create stagnant water upstream of the causeway and dissolved oxygen deficits downstream in lateral portions of the channel. If a causeway or barge must be used, then recommendations should be followed. These recommendations can be found in the Environmental Commitments section of this CE document.

This precludes the need for further consultation as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, if the project plans are changed, or if the Contractor goes outside of the existing roadway ROW as previously mentioned, USFWS will be contacted for consultation.

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SECTION B – OTHER RESOURCES

| Drinking Water Resources | Presence | Impacts | |
|---------------------------------|--------------------------|--------------------------|--------------------------|
| | | Yes | No |
| Wellhead Protection Area | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Public Water System(s) | X | <input type="checkbox"/> | X |
| Residential Well(s) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Source Water Protection Area(s) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Sole Source Aquifer (SSA) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

If a SSA is present, answer the following:

| | Yes | No |
|--|--------------------------|--------------------------|
| Is the Project in the St. Joseph Aquifer System? | <input type="checkbox"/> | <input type="checkbox"/> |
| Is the FHWA/EPA SSA MOU Applicable? | <input type="checkbox"/> | <input type="checkbox"/> |
| Initial Groundwater Assessment Required? | <input type="checkbox"/> | <input type="checkbox"/> |
| Detailed Groundwater Assessment Required? | <input type="checkbox"/> | <input type="checkbox"/> |

Remarks:

Sole Source Aquifer
 The project is located in Floyd County, Indiana and in Jefferson County, Kentucky. No legally designated sole source aquifers in Indiana and Kentucky are within or near the study area. Therefore, the FHWA/USEPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project. Therefore, a detailed groundwater assessment is not needed, and no impacts are expected.

Wellhead Protection Area and/or Source Water Area
 The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on March 12, 2019 by Michael Baker International. This project is not located within a Wellhead Protection Area or Source Water Area. The IDEM automated response on March 12, 2019, doesn't not identify if the project is or is not located within a wellhead area (Appendix C, page 80). No impacts are expected.

According to KDOW Water Maps Porta <https://watermaps.ky.gov/#SWP>, accessed on March 15, 2019, no Source Water Protection Areas, Wellhead Protection Areas, Permitted Water Withdrawals, and Public Water Systems via Water Wells and/or Springs are located within or near the SMRP area.

Water Wells
 The IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on October 18, 2019 by Michael Baker International. No wells are located near this project, the closest location is approximately 0.52 mile north of the project. Therefore, no impacts are expected.

The KDOW Record Database website (<https://watermaps.ky.gov/>) was accessed on October 18, 2019, by KYTC. No wells are located near this project, Louisville Water Company has a wellfield, but it's over a mile upstream of the Sherman Minton. Therefore, no impacts are expected.

Urban Area Boundary (UAB)
 Based on a desktop review of the INDOT MS4 website (<https://entapps.indot.in.gov/MS4/>) by Michael Baker International on March 12, 2019, and the RFI report, this project is located in an UAB location in Indiana. An early coordination letter was mailed on March 29, 2019, to the New Albany MS4 coordinator and no response was received within the 30-day time frame. Communication with the City of New Albany, Floyd County officials has been ongoing since September 10, 2018.

KYTC is involved pursuant to 40 CFR 122.32(a), which requires state transportation departments to participate in the MS4 program. KYTC is required to report to KDOW regarding the KYTC MS4 program. Project managers are to coordinate the development of highway plans with local government requirements. Communications with Louisville officials, Jefferson County officials, and KDOW has been on-going since September 10, 2018.

Public Water System
 Based on a desktop review, a site visit on March 12, 2019 by Michael Baker International. and the aerial map of the

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project area (Appendix B, page 4), this project is located in New Albany, IN, where there is a public water system (<https://cityofnewalbany.com/residents/municipal-utilities/>). The public water system will not be affected by this project due to location and no significant excavation. Therefore, no impacts are expected. Coordination with the City of New Albany, IN, has been ongoing since September 10, 2018.

Based on a desktop review, a site visit on March 12, 2019, Michael Baker International. the aerial map of the project area (Appendix B, page 4), this project is located in Louisville, KY, where there is a public water system (<https://www.louisvillewater.com/customer-service/>). The public water system will not be affected by this project due to location and no significant excavation. Therefore, no impacts are expected. Coordination with the City of Louisville, KY, has been ongoing since September 10, 2018.

| | <u>Presence</u> | <u>Impacts</u> | |
|---|-------------------------------------|--------------------------|-------------------------------------|
| | | Yes | No |
| Flood Plains | | | |
| Longitudinal Encroachment | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Transverse Encroachment | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Project located within a regulated floodplain | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| Homes located in floodplain within 1000' up/downstream from project | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks: **Indiana**
 Based on a desktop review of The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<http://dnrmaps.dnr.in.gov/appsphp/fdms/>) by Michael Baker International. on March 12, 2019, and the RFI report; this project is located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix E, page 10). Coordination with the floodplain administrator for this project has occurred. The floodplain administrator for this project is a member of the CAC for this project and has attended CAC meetings.

This project qualifies as a Category 1 per the current INDOT CE Manual, which states "Although this project involves work within the horizontal limits of the 100-year floodplain, no work is being performed below the 100-year flood elevation and as a result this project does not encroach upon the base floodplain."

Kentucky
 The project is located in a regulatory floodplain, (<https://watermaps.ky.gov/RiskPortal/>). Coordination with KDOW has occurred and is ongoing. At the agency coordination field meeting held on July 18, 2019, KDOW indicated a construction in floodway permit will likely be required for this project. The Permit Section of this document is on page 50.

| | <u>Presence</u> | <u>Impacts</u> | |
|---------------------------|--------------------------|--------------------------|--------------------------|
| | | Yes | No |
| Farmland | | | |
| Agricultural Lands | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| Prime Farmland (per NRCS) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

Total Points (from Section VII of CPA-106/AD-1006* _____
**If 160 or greater, see CE Manual for guidance.*

See CE Manual for guidance to determine which NRCS form is appropriate for your project.

Remarks: Based on a desktop review, a site visit on July 18, 2019 by Michael Baker International, the aerial map of the project area (Appendix B, page 4), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area in Indiana or Kentucky. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on December 11, 2018 to Natural Resources Conservation Services (NRCS). NRCS responded on December 17, 2018, with confirmation the project will not cause a conversion of prime farmland. (Appendix C, page 79).

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SECTION C – CULTURAL RESOURCES

| | | | |
|-----------------------------|---------------------------------------|--|------------|
| Minor Projects PA Clearance | Category A-1 B-1 B-12 | INDOT Approval Dates 1/10/2020 5/7/2020 | N/A |
|-----------------------------|---------------------------------------|--|------------|

IN Eligible and/or Listed Resource Present KY Eligible and/or Listed Resource Present

Results of Research

| | | | | | |
|------------------------|--|--|---|--|--|
| Archaeology | | | | | |
| NRHP Buildings/Site(s) | | | X | | |
| NRHP District(s) | | | X | | |
| NRHP Bridge(s) | | | | | |

IN Project Effect

No Historic Properties Affected No Adverse Effect Adverse Effect

KY Project Effect

No Historic Properties Affected No Adverse Effect Adverse Effect

IN Prepared Documentation KY Prepared Documentation

Documentation (mark all that apply)

| | | ES/FHWA Approval Date(s) | | SHPO Approval Date(s) |
|--|---|--------------------------------|--|--------------------------|
| Historic Properties Short Report | | | | |
| Historic Property Report | | | | |
| Archaeological Records Check/ Review | | | | |
| Archaeological Phase Ia Survey Report | X | | | KY-SHPO March 27, 2020 |
| Archaeological Phase Ic Survey Report | | | | |
| Archaeological Phase II Investigation Report | | | | |
| Archaeological Phase III Data Recovery | | | | |
| APE, Eligibility and Effect Determination | | | | |
| 800.11 Documentation | | | | |

MOA (List all signatories)

**Signature
Dates**

Memorandum of Agreement (MOA)

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

Remarks:

Indiana
On January 10, 2020 and May 7, 2020, the INDOT Cultural Resource Office (CRO) determined that this project falls within the guidelines of Category A, Type 1, Category B, Type 1, and Category B, Type 12 under the MPPA (Appendix D, page 8). Below is a description of each category types as defined in the MPPA:

Category A, Type 1 includes:
Any work on bridges limited to substructure or both superstructure elements without replacing widening or elevating the

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superstructure under the conditions listed below. Conditions A and B must be met. This category does not include bridge replacement projects (when both super structures are removed):

- A. The project takes place in previously disturbed soils and
- B. With regard to the bridges, at least one of the conditions (i, ii or iii) listed below must be satisfied;
 - i. The bridge is not identified in the latest Historic Bridge Inventory as a National Register-listed or National Register-eligible (see <http://www.in.gov/indot/2531.htm>); This does not apply for the project.
 - ii. The bridge was built after 1945, and is a common type as defined in Section V. of the *Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges* issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment - - remains in effect AND the considerations listed in Section IV of the Program Comment do not apply; This does not apply for the project.
 - iii. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect. This applies for the project.

Category B, Type 1 includes:

Replacement, repair, or installation of curbs, curb ramps, or sidewalks, including when such projects are associated with roadway work such as surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking, under the following conditions [BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]:

Condition A (Archaeological Resources) One of the two conditions listed below must be satisfied (EITHER Condition i or Condition ii must be satisfied):

- i. Work occurs in previously disturbed soils; OR
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the Division of Historic Preservation and Archaeology (DHPA) and any archaeological site form information will be entered directly into the State Historic Architectural and Archaeological Database (SHAARD) by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources) One of the two conditions listed below must be satisfied (EITHER Condition i or Condition ii must be satisfied):

- i. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; OR
- ii. Work occurs adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource under one of the two additional conditions listed below (EITHER Condition a OR Condition b must be met and field work and documentation must be completed as described below):
 - a. No unusual features, including but not limited to historic brick or stone sidewalks, curbs or curb ramps, stepped or elevated sidewalks and historic brick or stone retaining walls are present in the project area adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; OR
 - b. Unusual features, including but not limited to historic brick or stone sidewalks, curbs or curb ramps, stepped or elevated sidewalks and historic brick or stone retaining walls are present in the project area adjacent to or within a National Register-listed or National Register-eligible individual above-ground resource or district and ANY ONE of the conditions (1, 2, or 3) listed below must be fulfilled:
 - 1. Unusual features described above will not be impacted by the project. Firm commitments regarding the avoidance of these features must be listed in the MPPA determination form and the NEPA document and must be entered into the INDOT Project Commitments Database. These projects will also be flagged for quality assurance reviews by INDOT Cultural Resources Office during/after project construction. Revised Appendices A and B February 13, 2019 Page 6 of 13
 - 2. Unusual features described above have been determined not to contribute to the significance of the historic resource by INDOT Cultural Resources Office in consultation with the SHPO based on an analysis and justification prepared by their staff or review of such information from other qualified professional historians.

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3. Impacts to unusual features described above have been determined by INDOT Cultural Resources Office to be so minimal that they do not diminish any of the characteristics that contribute to the significance of the historic resource, based on an analysis and justification prepared by their staff or review of such information from other qualified professional historians.

Category B, Type 12 includes:

Replacement, widening, or raising the elevation of the superstructure on existing bridges, and bridge replacement projects (when both the superstructure and substructure are removed), under the following conditions [BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]:

Condition A (Archaeological Resources) One of the two conditions listed below must be met (EITHER Condition i or Condition ii must be satisfied):

- i. Work occurs in previously disturbed soils; This applies to the project.

Condition B (Above-Ground Resources) The conditions listed below must be met (BOTH Condition i and Condition ii must be satisfied) Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; AND

- ii. With regard to the subject bridge, at least one of the conditions listed below is satisfied (AT LEAST one of the conditions a, b or c, must be fulfilled):
 - a. The latest Historic Bridge Inventory did not identify the bridge as a National Register-listed or National Register-eligible (see <http://www.in.gov/indot/2531.htm>);
 - b. The bridge was built after 1945, and is a common type as defined in Section V. of the Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post- 1945 Concrete and Steel Bridges issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment remains in effect AND the considerations listed in Section IV of the Program Comment do not apply;
 - c. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect. This applies for the project.

As part of the MPPA determination, a review was conducted of the National Register of Historic Places and the Indiana Register of Indiana Historic Sites and Structures (IHSSI). The National Register & IHSSI information is available in the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries (IHBBC) map.

There are no recorded SHAARD archaeological sites in or adjacent to the proposed project area. No National Register-listed districts or individual resources were identified within or adjacent to the project area in Indiana. One (1) IHSSI district, New Albany Downtown Historic District, was identified adjacent to the project area. An ADA curb ramp modification will take place adjacent to the historic district, at the southwest corner of West Spring Street and West 1st Street. One (1) IHSSI individual property, Reyse-Friend House located at 229 West Spring Street (IHSSI 043-446-34204, rated Outstanding), was also identified within the project area. An ADA curb ramp modification will take place adjacent to the Reyse-Friend House, at the southwest corner of West Spring Street and Washington Place. This ADA curb ramp work is covered under the Category B, Type 1 (Appendix D, page 12).

The land surrounding the project area is densely urban/industrial/residential. Structures within or adjacent to the project area range in age from mid-to-late nineteenth to early twenty-first century. The assigned INDOT CRO historian performed a desktop street view survey of the project area. None of the structures appear to possess the significance and integrity necessary to be considered eligible for the National Register, and no previously un-surveyed potentially eligible structures are located within or adjacent to the project area.

The subject structure (Bridge No. I64-123-04691D/NBI No. 034520) is a steel continuous thru-arch bridge constructed in 1961 and reconstructed in 1997. The bridge has three (3) approach spans and a concrete cast-in-place deck. The bridge was not surveyed for or included in the 1976/2006 *Floyd County Interim Report*. As a border bridge, the structure was not included in the 2010 INDOT-sponsored Historic Bridge Inventory (HBI). As part of the Interstate system (per MPPA, Category B-12 (ii) (b)), "...the bridge...was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for as long as that Exemption remains in effect..."

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Table 5 below shows each individual project element and which part of the MPPA it falls under.

| Table 5. Sherman Minton Renewal Project: MPPA | | | | |
|--|---|--|---|---------|
| Des. No. | Bridge No. / Street | Description | Work Type | MPPA |
| 1702255 | I64-123-04691 D | Sherman Minton Bridge | Bridge Deck Replacement, Structural Repairs, and Substructure Repairs | B-12 Ai |
| 1592187 | I64-123-04691 D | Sherman Minton Bridge | Bridge Painting | A-1 iii |
| 1702257 | I64-123-02294 CWBL | I-64 WB over SR 111/Main Street, RR (IN Approach WB) | Bridge Deck Overlay and Bridge Painting | A-1 iii |
| 1702258 | I64-123-02294 CEBL | I-64 EB over SR 111/Main Street (IN Approach EB) | Bridge Deck Overlay and Bridge Painting | A-1 iii |
| 1702259 | I64-123-02294 JCEB | I-64 EB over Southern RR (IN Approach EB) | Bridge Deck Overlay and Bridge Painting | A-1 iii |
| 1701215 | Elm Street 5 th Street Spring Street | Old SR 62 (Elm Street) from I-64 Exit Ramp to State Street and Spring Street | HMA Overlay, Preventative Maintenance, and ADA Curb Ramp | B-1 |
| 1900579 | I64-123-04690 BEBL | I-64 EB over Market Street 0.11 W of SR 111. | Bridge Painting | A-1 iii |

The MPPA concluded that no above-ground concerns exist as long as the project scope does not change. It was also concluded there are no archaeological concerns because the proposed project is limited to minor rehabilitation work and painting of an existing interstate bridge, in previously disturbed soils. No further consultation is required. If the Design-Build Contractor proposes work outside of the existing ROW, coordination with INDOT Cultural Resources will be required. This completes the Section 106 process for the Indiana side of the project and the responsibilities of the FHWA under Section 106 have been fulfilled.

Kentucky

There is a National Register historic district, Portland Historic District, and historic property, Shawnee Park, located within the Kentucky Area of Potential Effect (APE). Kentucky Historic Preservation Office (KY SHPO) responded to early coordination (via a telephone conversation) on December 20, 2018 stating the office finds the half mile APE to be appropriate for the above-ground resources (Appendix D, page 18). Through coordination with KYTC it was determined that a Phase I Archaeological Survey would be required for approximately 2.5 acres beneath the Kentucky approach to the Sherman Minton Bridge within the Shawnee Golf Course (located within the National Register listed Shawnee Park).

A Phase I Archeology Survey was completed and summarized in a letter dated March 26, 2020 by Wood Environment & Infrastructure Solutions, Inc. The survey included background research which revealed that no archaeological sites have been recorded within or directly adjacent to the APE. The field survey, conducted on March 23, and 24, 2020, consisted of a total of 28 Shovel test probe (STP)s and six bucket augers excavated within the APE. Two newly recorded archeological sites, FS-1 and FS-2, were identified during the survey. No features or significant artifact concentrations were identified at either site. Of the 28 STPs, there was one positive STP identified at Site FS-1; however, it was concluded that sites FS-1 and FS-2 are recommended as not eligible for the National Register Historic Property (NRHP) and the survey concluded that no further archaeological investigations within the project APE are recommended (Appendix D, page 30).

KYTC coordinated with KY-SHPO on March 27, 2020, requesting concurrence for the finding of No Historic Properties Affected for archaeological resources. KY-SHPO responded on March 27, 2020 concurring with the finding, conditional upon stipulations listed below (Appendix D, page 36):

- 1) OSA site numbers will be requested for FS-1 and FS-2 by Wood;
- 2) Three bound copies of an acceptable Phase I report be submitted to our office for review and comment no later than June 30, 2020; and

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3) Should there be any additions or modifications to the APE, this office will be consulted, and additional archaeological survey may be required.

Additionally, KYTC coordinated with KY-SHPO on March 26, 2020, stating it is the determination of KYTC on behalf of FHWA that the Sherman Minton Rehabilitation project as proposed will have a No Adverse Effect on the Shawnee Golf Course, Shawnee Park, the Northwestern Parkway, and the Clark Memorial Bridge (where diverted traffic may go). The Kentucky bridge approach spans above the golf course and staging areas in or adjacent to the park will be subject to additional review once they are identified. The project as proposed does not have the potential to adversely affect any other historic sites within the APE.

KY-SHPO provided a No Adverse Effect concurrence finding on March 30, 2020 contingent on the following (Appendix D, page 35):

- 1) Since this project is a design-build contract, once an alternative has been chosen KYTC shall provide SHPO with the chosen alternative and final effects and final effects recommendation.
- 2) Once the alternative and APE has been chosen, KYTC shall identify and reach out to the appropriate consulting parties based upon the chosen APE.

KY-SHPO provided a No Historic Properties Affected finding on July 27, 2020, upon additional review of the Phase 1 Archaeological Survey (Appendix D, page 41).

All applicable KY-SHPO commitments are included in the Environmental Commitments section of this CE document. This completes the Section 106 process for the Kentucky side of the project and the responsibilities of the FHWA under Section 106 have been fulfilled.

The park (including the golf course) and trail are not anticipated to be impacted by the project. The project will not use this resource by taking permanent ROW and will not alter the environment in such a way as to constitute constructive use of this resource. Therefore, no use or permanent impact is expected. If the Design-Build Contractor determines that they want temporary use of the trail or park during construction, it is the Design-Build Contractor's responsibility to coordinate with KYTC, FHWA, the City of Louisville, and the Louisville Parks and Recreation. Additional archaeology, wetland, or other analysis may be required.

SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement (mark all that apply)

Parks & Other Recreational Land

- Publicly owned park
- Publicly owned recreation area
- Other (school, state/national forest, bikeway, etc.)

Presence

| |
|---|
| X |
| X |
| X |

Use

| Yes | No |
|-----|----|
| | X |
| | X |
| | X |

Evaluations

Prepared

- Programmatic Section 4(f)*
- "De minimis" Impact*
- Individual Section 4(f)

| |
|--|
| |
| |
| |

FHWA

Approval date

| |
|--|
| |
|--|

Wildlife & Waterfowl Refuges

- National Wildlife Refuge
- National Natural Landmark
- State Wildlife Area
- State Nature Preserve

Presence

| |
|--|
| |
| |
| |
| |

Use

| Yes | No |
|-----|----|
| | |
| | |
| | |
| | |

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**Evaluations
Prepared**

Programmatic Section 4(f)*
"De minimis" Impact*
Individual Section 4(f)

| |
|--|
| |
| |
| |

**FHWA
Approval date**

Historic Properties

Sites eligible and/or listed on the NRHP

Presence

X

Use

| | |
|---|--|
| Yes | No |
| <div style="border: 1px solid black; width: 50px; height: 20px;"></div> | <div style="border: 1px solid black; width: 50px; height: 20px; text-align: center;">X</div> |

**Evaluations
Prepared**

Programmatic Section 4(f)*
"De minimis" Impact*
Individual Section 4(f)

| |
|--|
| |
| |
| |

**FHWA
Approval date**

**FHWA approval of the environmental document also serves as approval of any Section 4f Programmatic and/or De minimis evaluation(s) discussed below.*

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the remarks box below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, "de minimis" and Individual Section 4(f) evaluations please refer to the "Procedural Manual for the Preparation of Environmental Studies". Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks:

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, a site visit on July 18, 2019 by Michael Baker International, the aerial map of the project area (Appendix B, page 4), and the RFI report (Appendix E, page 10) there are four (4) Section 4(f) resources located within the project area. There are two (2) located in Indiana and two (2) located in Kentucky.

Indiana

There are two (2) Section 4(f) resources located within or adjacent to the project area on the Indiana side (Appendix B, page 27). One (1) resource is a public recreation area that includes public facilities, such as the New Albany Riverfront Amphitheater and overlooks, located east of the Sherman Minton Bridge. These public facilities are owned by the City of New Albany. One (1) is a public trail, the Ohio River Greenway Trail, that runs under the Sherman Minton Bridge and is operated by the Ohio River Greenway Commission.

Coordination with the City of New Albany and members of the Ohio River Greenway Commission has occurred through CAC meetings to provide project updates and address concerns on keeping facilities and trails open for the public. Coordination with the City of New Albany and the Ohio River Greenway Commission is to be maintained by the Design-Build Contractor on project updates to ensure the safety of trail users.

The public facility and trail are not anticipated to be impacted by the project. The project will not use this resource by taking permanent ROW and will not alter the environment in such a way as to constitute constructive use of this resource. Therefore, no use or permanent impact is expected. If the Design-Build Contractor determines that they want temporary use of the trail during construction, it is the Design-Build Contractor's responsibility to coordinate with INDOT, FHWA, the City New Albany, and the Ohio River Greenway Commission. Additional archaeology, wetland, or other analysis may be required.

Kentucky

There are two (2) Section 4(f) resources located within or adjacent to the project area on the Kentucky side (Appendix B, page 27). One (1) resource is the public Shawnee Park, which includes the public Shawnee Golf Course, located underneath the Sherman Minton Bridge approaches. The park and golf course are owned by the City of Louisville and operated by the Louisville Parks and Recreation. One (1) resource is the Louisville Loop (also referred to as Louisville Riverwalk) trail that runs underneath the Sherman Minton Bridge approaches. The Louisville Loop trail is maintained by

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the Louisville Parks and Recreation.

Coordination with the City of Louisville and Louisville Parks and Recreation has occurred through CAC and teleconference meetings to provide project updates and address concerns on keeping the trails open for the public. Coordination with the Louisville Parks and Recreation is to be maintained by the Design-Build Contractor with project updates to ensure the safety of trail users.

The park (including the golf course) and trail are not anticipated to be impacted by the project. The project will not use this resource by taking permanent ROW and will not alter the environment in such a way as to constitute constructive use of this resource. Therefore, no use or permanent impact is expected. If the Design-Build Contractor determines that they want temporary use of the trail or park during construction, it is the Design-Build Contractor's responsibility to coordinate with KYTC, FHWA, the City of Louisville, and the Louisville Parks and Recreation. Additional archaeology, wetland, or other analysis may be required.

Section 6(f) Involvement

Presence

Use

Yes

No

Section 6(f) Property

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks:

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

Indiana
A review of 6(f) properties on the Land and Water Conservation Fund (LWCF) website at <https://www.lwcfcoalition.com/tools> revealed a total of three (3) properties in Floyd County, Indiana (Appendix H, page 21). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources in Indiana as a result of this project.

Kentucky
A review of 6(f) properties on the Land and Water Conservation Fund (LWCF) website at <https://www.lwcfcoalition.com/tools> revealed a total of 84 properties in Jefferson County, Kentucky (Appendix H, page 22). Two (2) properties are listed within the project area in Kentucky, the Shawnee Park Pavilion and the Shawnee Golf Course Maintenance Building. These properties are owned by the City of Louisville and operated by the Louisville Parks and Recreation.

The Shawnee Park and the Shawnee Golf Course are not anticipated to be impacted by the project. The project will not use this resource by taking permanent ROW and will not alter the environment in such a way as to constitute constructive use of this resource. Therefore, no use or permanent impact is expected. If the Design-Build Contractor determines that they want temporary use of the park or golf course during construction, it is the Design-Build Contractor's responsibility to coordinate with KYTC, FHWA, the City of Louisville, and the Louisville Parks and Recreation. Additional archaeology, wetland, or other analysis may be required.

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SECTION E – Air Quality

Air Quality

Conformity Status of the Project

| | Yes | No |
|--|-------------------------------------|--------------------------|
| Is the project in an air quality non-attainment or maintenance area? | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| If YES, then: | | |
| Is the project in the most current MPO TIP? | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Is the project exempt from conformity? | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| If the project is NOT exempt from conformity, then: | | |
| Is the project in the Transportation Plan (TP)? | <input type="checkbox"/> | <input type="checkbox"/> |
| Is a hot spot analysis required (CO/PM)? | <input type="checkbox"/> | <input type="checkbox"/> |
| Level of MSAT Analysis required? | | |

Level 1a Level 1b Level 2 Level 3 Level 4 Level 5

Remarks:

Indiana
 The FY 2020-2024 IN STIP is listed based on the lead Des. No. in the contract. The lead Des. No. for this contract is 1702255. The FY 2020-2024 STIP includes Des. No. 1702255 by reference with the contract number 4071 (Appendix H, page 16).

This project is located in Floyd County, Indiana, which is currently a “Marginal” nonattainment area for the 2015 8-Hour Ozone Standard (0.070 ppm), according to <https://www.in.gov/idem/airquality/2339.htm>. This project has been identified as being exempt from air quality analysis in accordance with 40 CFR Part 93.126 and this project is not a project of air quality concern (40 CFR Part 93.123). Therefore, the project will have no significant impact on air quality in Indiana.

Kentucky
 The FY 2019-2022 KY STIP is listed based on Item ID. The Item ID for this contract is 5-64 (Appendix H, page 17).

This project is also located in Jefferson County, Kentucky which is currently a nonattainment area for the 2015 8-Hour Ozone Standard (0.070 ppm), according to https://www3.epa.gov/airquality/greenbook/anayo_ky.html. This project has been identified as being exempt from air quality analysis in accordance with 40 CFR Part 93.126 and this project is not a project of air quality concern (40 CFR Part 93.123). Therefore, the project will have no significant impact on air quality in Kentucky.

Both Indiana and Kentucky
 An air quality summary memo has been prepared by Michael Baker International as an initial response to comments raised in USEPA’s early coordination response letter dated February 7, 2019, identified in Appendix C, page 4. Air quality concerns specifically mentioned in the letter include the potential for project level air quality conformity analysis, air quality considerations during the NEPA process and air quality concerns during construction. The air quality summary memo is identified in Appendix H, page 19.

MPO Process
 The Kentuckiana Regional Planning and Development Agency (KIPDA) serves as the Metropolitan Planning Organization (MPO) for the Louisville/Jefferson County KY-IN urbanized area. As the MPO, KIPDA is responsible for the Metropolitan Transportation Plan (MTP) that includes all federally funded surface transportation projects within the MPO Metropolitan Planning Area (MPA). The updated MTP as of July 2020, Connecting Kentuckiana 2040 (<https://www.kipda.org/transportation/core-products/metropolitan-transportation-plan/>), provides a vision of the regional transportation network and the projects that are being considered for funding to fulfill the plans goals and objectives. KIPDA also manages the four-year Transportation Improvement Program (TIP), the fiscal programming document of the MTP that assigns funding to regional projects (Appendix H, page 1).

Air Quality Status and Conformity
 Recently, most of the KIPDA region (Clark and Floyd counties in Indiana and Bullitt, Jefferson, and Oldham counties in Kentucky) was designated as being in nonattainment for the 2015 8-hour Ozone National Ambient Air Quality Standards (NAAQS). This area, which is known as the Louisville KY-IN 2015 8-hour Ozone nonattainment area, is

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subject to the requirements of conformity, and KIPDA is the agency responsible for fulfilling the federal air quality conformity requirements associated with the MTP. In order to support a conformity determination for an MTP update or amendment, KIPDA must prepare a conformity analysis to show that expected emissions of Ozone precursors are less than the limits (budgets) established for the area when it became attainment of the 1997 8-hour Ozone standard (there are currently no budgets for the 2015 8-hour Ozone standard, therefore the 1997 budgets are the only applicable budgets). The use of the 1997 budgets also supports a conformity determination for the 1997 8-hour Ozone Standard (which is necessary per February 16, 2018 D.C. Circuit Court decision in South Coast Air Quality Management District versus USEPA). The region meets the NAAQS for all other regulated pollutants.

SMRP is located in the Louisville, KY-IN nonattainment area, therefore, a transportation project-level conformity determination under the Clean Air Act (CAA) is required. The SMRP is included in the region's long-range transportation plan, MTP Connecting Kentuckiana 2040. SMRP is primarily a rehabilitation (reconstruction) project. It is classified as exempt (as per 40 CFR 93.126), and it does not need to be explicitly modeled as part of the region's air quality conformity analysis. Additionally, the SMRP is included in the FY 2020 – 2025 Transportation Improvement Program (TIP) adopted February 27, 2020 as well as in subsequent addendums.

In summary, the SMRP is included in a current and conforming transportation plan and TIP. Further, because the SMRP is not within a CO or Particulate Matter (PM) maintenance or nonattainment area, a hotspot analysis is also not required. Thus, the transportation conformity requirements under the CAA have been met.

Requirements Under NEPA

Air quality considerations under NEPA must also be considered. SMRP is not within a Carbon Monoxide (CO) maintenance area and is classified as an exempt project pursuant to Section 93.126, because it is a safety or maintenance project that will correct, improve, or eliminate a hazardous location or feature. As such it can be confidently asserted that the project in and of itself will not result in a new exceedance of the CO National Ambient Air Quality Standards (NAAQS).

The purpose of this project is to address structural deficiencies and safety/geometric issues without adding additional capacity. This project has been determined to generate minimal air quality impacts for Clean Air Act criteria pollutants and has not been linked with any special mobile source air toxic (MSAT) concerns. As such, this project will not result in changes in traffic volumes, vehicle mix, basic project location, or any other factor that would cause a meaningful increase in MSAT impacts of the project from that of the no-build alternative.

Moreover, USEPA regulations for vehicle engines and fuels will cause overall MSAT emissions to decline significantly over the next several decades. Based on regulations now in effect, an analysis of national trends with USEPA's MOVES2014 model forecasts a combined reduction of over 90 percent in the total annual emissions rate for the priority MSAT from 2010 to 2050 while vehicle-miles of travel are projected to increase by over 45 percent (Updated Interim Guidance on Mobile Source Air Toxic Analysis in NEPA Documents, Federal Highway Administration, October 12, 2016). This will both reduce the background level of MSAT as well as the possibility of even minor MSAT emissions from this project.

Construction Emissions

Emissions may be produced in the construction of this project from heavy equipment and vehicle travel to and from the site, as well as from fugitive sources. Construction emissions are short term or temporary in nature. Based on available information, temporary construction activities are not expected to generate high enough emissions during the anticipated construction duration to result in a new exceedance of the NAAQS for these pollutants.

SECTION F - NOISE

Noise
Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy? Yes No

| | No | Yes/ Date |
|------------------------------------|----------|-----------|
| ES Review of Noise Analysis | X | |

Remarks: **Indiana**
This project is a Type III project. In accordance with 23 CFR 772 and the current Indiana Department of Transportation Traffic Noise Analysis Procedure, this action does not require a formal noise analysis.

This is page 41 of 54 Project name: I-64 Bridge Rehabilitation Sherman Minton Renewal Project Date: October 2, 2020

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Kentucky
This project is not considered a Type I project; therefore, no further noise considerations are required.

SECTION G – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

- Will the proposed action comply with the local/regional development patterns for the area?
- Will the proposed action result in substantial impacts to community cohesion?
- Will the proposed action result in substantial impacts to local tax base or property values?
- Will construction activities impact community events (festivals, fairs, etc.)?
- Does the community have an approved transition plan?
If No, are steps being made to advance the community's transition plan?
- Does the project comply with the transition plan? (explain in the remarks box)

| Yes | No |
|-------------------------------------|-------------------------------------|
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <input type="checkbox"/> | <input type="checkbox"/> |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Remarks:

A Community Impact Assessment (CIA) was conducted to the SMRP as part of the CIA and EJ Analysis Technical Report. An executive summary of the CIA report is included in Appendix I. Based upon community profiles developed for the Project Study Area and surrounding jurisdictions, those on the Indiana side of the Ohio River are more likely to experience greater temporary impacts:

Indiana – Floyd and Clark counties, cities of New Albany and Jeffersonville, and Town of Clarksville

- o 40% and 60% of those residing in Floyd and Clark counties in the Project Study Area commute out of state to Kentucky, with an even larger percentage of residents in New Albany and Clarksville doing so (up to 80%).
- o Indiana residents have a greater reliance on the Sherman Minton Bridge (also referred to SMB throughout the CIA and EJ Analysis Technical Report), with a 2.5:1 ratio of Indiana to Kentucky residents crossing the Ohio River for jobs.
- o Silver Creek limits the New Albany network to three east-west crossings (I-265, Blackiston Mill Road, and Spring Street/Providence Way and Spring Street/Brown Station Way). This is likely to increase potential congestion on local routes as travelers find alternate crossings into Louisville.

Kentucky – Jefferson County and the City of Louisville

- o About 20% of Kentucky residents commute to Indiana.
- o I-64 provides a high-capacity Interstate connection directly along the river between the Sherman Minton Bridge to the west and the downtown bridges to the east.
- o West Louisville has a classic arterial grid roadway network that allows for efficient travel options between these bridges. This network currently has excess capacity that could accommodate a temporary increase in traffic volumes.

The proposed SMRP action complies with local/regional development patterns for the area as demonstrated by:

- Inclusion of the SMRP in the Kentuckiana Regional Planning & Development Agency (KIPDA) MPO 2020-2025 Transportation Improvement Program (TIP),
- Participation of local/regional planning stakeholders during project development (Part I – Public Involvement), and,
- Incorporation of KIPDA planning, demographic, travel demand model (TDM), and EJ resources data.

Minimization measures were part of project development, evaluations, and a combination of MOT options to reduced impacts. While the MOT 1 option had the most positive public involvement comments, fewest changes from existing travel patterns and local access, lowest induced traffic diversions and related environmental impacts; project constructability requirements were not entirely met. The Preferred Alternative MOT combined the predominant use of MOT 1, limited off-peak use of MOT-2 (*allowance for about 180 nights per construction year*), and short-term use of MOT-5. Detour routes are listed below and included in Appendix B, pages 19-26.

Based upon public involvement input, retaining cross-river travel lanes on the Sherman Minton Bridge and local access, and the MOT with the lowest induced traffic diversions (with related travel times, travel costs, and congestion) and disruptions to community access, and mobility, the SMRP will not result in substantial impacts to community cohesion.

Since the SMRP consists of the rehabilitation of existing infrastructure elements within existing ROW, does not increase capacity or alter travel patterns upon completion, and community impacts are limited to temporary traffic disruptions

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during construction, the proposed action will not result in substantial impacts to the local tax based or property values.

Minimization measures included additional accommodation for local special events and festivals with the exclusion of bridge closure work during the following:

New Year's Day - If New Year's Day falls on a Sunday, work shall be suspended from noon December 31 until sunrise January 3. or if New Year's Day falls on a Monday through Saturday, work shall be suspended from noon December 31 until sunrise January 2.

Good Friday - Work shall be suspended from noon on Good Friday until sunrise Monday.

Memorial Day - Work shall be suspended from noon the Friday before Memorial Day until sunrise Tuesday, the day after Memorial Day.

Independence Day - If Independence Day falls on a:

Sunday - work shall be suspended from noon Friday, July 2, until sunrise Tuesday, July 6.
Monday - work shall be suspended from noon Friday, July 1, until sunrise Tuesday, July 5.
Tuesday - work shall be suspended from noon Friday, June 30, until sunrise Wednesday, July 5.
Wednesday - work shall be suspended from sunset on Tuesday, July 3, until sunrise Thursday, July 5.
Thursday - work shall be suspended from noon Wednesday, July 3, until sunrise Monday, July 8.
Friday - work shall be suspended from noon Thursday, July 3, until sunrise Monday, July 7.
Saturday - work shall be suspended from noon Thursday, July 2, until sunrise Monday, July 6.

Labor Day - Work shall be suspended from noon the Friday before Labor Day until sunrise Tuesday, the day after Labor Day.

Thanksgiving Day - Work shall be suspended from noon the Wednesday before Thanksgiving Day until sunrise the Monday after Thanksgiving Day.

Christmas Day - Work shall be suspended from noon December 24 until sunrise December 27.

Thunder Over Louisville - Work suspended from Midnight Friday till 6:00 a.m. Sunday.

Kentucky Derby - Work suspended from Thursday at midnight until Monday at 6:00 am.

Harvest Homecoming Festival - First Saturday in October to second Saturday in October.

The SMRP complies with the local/regional development MPO and transition plan and community impacts are limited to temporary traffic disruptions during construction.

Based on the CIA evaluations, there will be temporary MOT related impacts for both interstate and local travel to community events within and adjacent to the Project Study Area; however, none of these impacts are unique to a particular community or event. The completion of SMRP will provide long-term benefits with increased certainty and an extended service life for the Sherman Minton Bridge and approaches to these same communities and events in both Indiana and Kentucky.

Indirect and Cumulative Impacts

Will the proposed action result in substantial indirect or cumulative impacts?

| | |
|--------------------------|-------------------------------------|
| Yes | No |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Remarks:

Indirect impacts are effects which are caused by the action and are later in time or farther removed in distance but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency or person undertakes such actions

Indirect Impacts - there may be indirect impacts beyond the SMRP study area from induced traffic diversions to other Ohio River crossings and increased interstate, regional, and public transportation network travel times, travel costs, and congestion. However, since the project impacts are temporary and will not change traffic patterns, access and mobility following construction completion, potential impacts to regional growth, patterns of land use, population density, or the growth rate are minimal.

Cumulative Impacts - since SMRP impacts are temporary and will not change traffic patterns, access and mobility following construction completion, the potential cumulative impacts to the environment from incremental impact of the project when added to other past, present, and reasonably foreseeable future actions are minimal.

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Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities? *Discuss how the maintenance of traffic will affect public facilities and services.*

| | |
|---------------------------------|---|
| Yes <input type="checkbox"/> | No <input checked="" type="checkbox"/> |
|---------------------------------|---|

Remarks:

Indiana
Based on a desktop review, a site visit on July 18, 2019 by Michael Baker International, the aerial map of the project area (Appendix B, page 4), and the RFI and limited RFI reports (Appendix E, page 1), there are two recreational facilities located within the immediate vicinity of the project area in Indiana. The public recreation area located east of the Sherman Minton Bridge, contains public facilities including the New Albany Riverfront Amphitheater and overlooks, and is owned by the City of New Albany. There is also the public Ohio River Greenway trail that runs underneath the Sherman Minton Bridge, operated by the Ohio River Greenway Commission.

Coordination with the Ohio River Greenway Commission and City of New Albany occurred through CAC and teleconference meetings to provide project updates and address concerns on keeping the recreational facility open for the public. Coordination with the City of New Albany and the Ohio River Greenway Commission is to be maintained by the Design-Build Contractor on project updates to ensure the safety of park users. No permanent impacts are anticipated and access to the recreational facilities will be maintained during construction. The project will not use this resource by taking permanent ROW and will not alter the environment in such a way as to constitute constructive use of this resource. Therefore, no use or permanent impact is expected. If the Design-Build Contractor determines that they want temporary use of the trail during construction, it is the Design-Build Contractor's responsibility to coordinate with INDOT, FHWA, the City of New Albany, and the Ohio River Greenway Commission. Additional archaeology, wetland, or other analysis may be required.

No educational facilities are located within a 0.5 mile radius of the project area. Three (3) religious facilities are located within the 0.5 mile radius of the project area. Access to and from the facilities are not impacted by the project; therefore no impact is expected. No airports are identified within the 0.5 mile radius of the project area; therefore, no impact is expected.

In Indiana there are no utilities running parallel to I-64; however, I-64 bridges have several utilities identified alongside the interstate. A utility coordination kick-off meeting was held in September 2019. Project plans have been provided to utilities for their review and comment. No utility impacts are anticipated within rehabilitation of the Sherman Minton Bridge nor the New Albany resurface project.

An active rail line is located under the Indiana Approach of the Sherman Minton Bridge and is an owned and operated by Norfolk Southern Railroad. Norfolk Southern has reviewed project plans and provided comments. All comments are being incorporated into the project documents. No impact is anticipated to the railroad.

Kentucky
Based on a desktop review, a site visit on July 18, 2019 by Michael Baker International, the aerial map of the project area (Appendix B, page 4), and the RFI and limited RFI reports (Appendix E, page 1), there is one(1) recreational facility, the public Shawnee Golf Course, located within the immediate vicinity of the project area in Kentucky.

Coordination with the City of Louisville and Louisville Parks and Recreation occurred through CAC and teleconference meetings to provide project updates and address concerns on keeping the recreational facility open for the public. Coordination with the City of Louisville and Louisville Parks and Recreation is to be maintained by the Design-Build Contractor on project updates to ensure the safety of park users. No permanent impacts are anticipated due to access to the recreational facility will be maintained during construction. The project will not use this resource by taking permanent ROW and will not alter the environment in such a way as to constitute constructive use of this resource. Therefore, no use or permanent impact is expected. If the Design-Build Contractor determines that they want temporary use of the facility during construction, it is the Design-Build Contractor's responsibility to coordinate with KYTC, FHWA, the City of Louisville, and the Louisville Parks and Recreation. Additional archaeology, wetland, or other analysis may be required.

Two (2) educational facilities are located within a 0.5 mile radius of the project area. These schools may face temporary traffic impacts; however, access to and from the school is maintained. One (1) religious facility is located within the 0.5 mile radius of the project area. Access to and from the facility is not impacted by the project; therefore, no impact is expected. No airports are identified within the 0.5 mile radius of the project area; therefore, no impact is expected.

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Utilities located in Kentucky are located outside of the existing Limited Access Right-of-Way. There are utilities located along local roadways of Northwestern Parkway and Bank Street which travel under I-264 just south of the I-64/I-264 interchange. A utility coordination kick-off meeting was held in September 2019. Project plans have been provided to utilities for their review and comment. No utility impacts are anticipated with the project work.

Both Indiana and Kentucky

Local first responders and emergency service providers force have been in coordination with the project team as a part of public outreach, this includes Floyd memorial hospital with concerns of heliport map. A meeting was held August 22, 2019 with a local Freeway Incident Management team, which included both New Albany and Louisville representatives of first responders and emergency service providers. The purpose of this meeting was to obtain feedback and concerns from those representatives during project development in approach to construction. Concerns mentioned in this meeting centered around the MOT options. It was mentioned that many first responders cross the bridge to answer a call of service regardless of state or city affiliation. If the bridge is only open to one lane, (MOT 2), this can leave emergency teams in difficult positions due to access, not to mention if multiple vehicles needed. An additional concern of access for emergency teams and hazardous units were also brought up in the case of an incident on the bridge itself. These concerns and comments were taken into consideration by the project team for further analysis into the MOT options and recommendations.

The public transit system serving the Project Study Area is operated by the Transit Authority of River City (TARC). TARC representatives have indicated that while all MOT options would have some effect on their operations, additional coordination would allow for service adjustments to ensure continued served for their ridership.

- **Indiana:**

Increased congestion on the local New Albany roadway network and at the river crossings under any of the MOT options could affect headways for TARC Routes.

- **Kentucky:**

Because of Louisville Metro area's robust roadway network, TARC riders on routes that do not cross the river are not anticipated to experience travel time delays under any of the MOT Options in the Louisville Metro area.

TARC routes were evaluated for impacts. Routes 71, 72, 82, and 65X were identified as potentially affected routes dependent upon MOT (Appendix I, page 123).

- Route 71 (Jeffersonville-Louisville-IUS) TARC Route 71, at its easternmost point within the Project Study Area, starts in west Jeffersonville, accesses I-64 via North 22nd Street, Crosses the SMB, then exits I-64 into downtown New Albany via the Spring Street Exit where it connects to State Street, Pearl Street, and Grant Line Road. Route 71 would be most affected because, in a single complete trip, this route crosses the Ohio River twice: once on the SMB and once on the Clark Memorial Bridge which, in the Base case, is already at capacity.
- Route 72 (Clarksville) - This route connects Clarksville to downtown Louisville via the Clark Memorial Bridge (2nd Street Bridge). Starting in Clarksville just west of I-65, southbound along routes that include Veterans Parkway, Greentree Boulevard, and Eastern Boulevard, to its crossing of the Ohio River via the Clark Memorial Bridge, then south into Downtown Louisville.
- Express 65 (Sellersburg Express) This route operates *Monday through Friday*, -providing an express connection between Ivy Tech in Sellersburg, IN to downtown Louisville. crossing via Clark Memorial Bridge. One stop at Ivy Tech on Indiana side. Several Downtown Louisville.

MOT Options 1, 2 and 4 maintain continuous travel on the Sherman Minton Bridge in both directions and would be less disruptive to TARC services and operations. MOT Option 5 (full closure) would require rerouting of TARC Route 71. Travel time delays may require service adjustments such as number stops, temporary detours taken, number of detours, and adjustments to the transit schedules, which could lead to further communication delays to riders. If TARC is unable to provide alternative and timely routes for riders, the impact to riders needing to cross the river could be significant. Since EJ populations near the Sherman Minton Bridge depend on TARC services, that rerouting diversions to other bridges was determined to be "disproportionately high and adverse effects", and therefore MOT conditions that increased this potential were not carried forward.

Minimization measures were part of project development, evaluations, and a combination of MOT options to reduced impacts. While the MOT 1 option had the most positive public involvement comments, fewest changes from existing travel patterns and local access, lowest induced traffic diversions and related environmental impacts; project constructability requirements were not entirely met. The Preferred Alternative MOT combined the predominant use of

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MOT 1, limited off-peak use of MOT-2 (*allowance for about 180 nights per construction year*), and short-term use of MOT-5. Detour routes are listed below and included in Appendix B, pages 19-26.

The SMRP will impact interstate and local travel to public health and educational facilities, public and private utilities, emergency services, religious institutions, and airports within and adjacent to the project area, however, these impacts are temporary and none of these impacts are unique to a particular facility. The completion of proposed action will also provide long-term benefits with increased certainty and an extended service life for facilities and services that rely on the Sherman Minton Bridge in both Indiana and Kentucky.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

| Yes | No |
|-------------------------------------|--------------------------|
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

| | |
|-------------------------------------|--------------------------|
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Will the project result in adversely high or disproportionate impacts to EJ populations?

Remarks:

An EJ evaluation was conducted to the SMRP as part of the CIA and EJ Analysis Technical Report (Appendix I, page 1). This analysis covered both Indiana and Kentucky.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is *Floyd County, New Albany, Indiana and Jefferson County, Louisville, Kentucky*. The COC is then broken into smaller, more manageable units based on Census Tracts (CTs). The community that overlaps the project area is called the affected community (AC). For the SMRP, the ACs includes all of the CTs that are located in the SMRP study area. (Appendix I, page 61). Demographic data at the COC and AC levels come from the U.S. Census Bureau and its more frequently updated American Community Survey (ACS). CT data were obtained from the 2013-2017 ACS five-year estimates and from the US Census Bureau Website <https://factfinder.census.gov/> on November 12, 2019 by Michael Baker International. The data collected from the 59 census tracts for minority and low-income populations within the AC are summarized in Appendix I, page 85.

An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. EJ populations are present in Project Study Area:

Indiana

- Clark County consists of 8 Census Tracts where 5 are minority, 4 are low income, and 4 are both;
- Floyd County consists of 13 Census Tracts where 4 are minority, 8 are low income, and 4 are both;

Kentucky

- Jefferson County consists of 38 Census Tracts where 29 are minority, 35 are low-income, and 29 are both.

Project Area COC, AC, and CT level demographic data were incorporated into a Travel Demand Model (TDM) for insights to likely detour routes, travel time changes, changes in travel costs, and roadways likely to experience increased congestion. The TDM utilized both EJ and Non-EJ Traffic Analysis Zones (TAZs; as determined by KIPDA) based on whether or not the zone was within an EJ community. The model outputs were categorized by the trip origin as either an EJ Trip (those trips originating from within a Study Area EJ TAZ) or a Non-EJ Trip (those trips originating outside of a Study Area EJ TAZ). Potential EJ impacts are detected by locating minority populations and low-income populations in and near the project area, calculating their percentage in the area relative to a reference population (i.e. Project COCs, and determining whether there will be adverse impacts to them. For this project, disparities between non-EJ and EJ populations were examined comparing existing data with implementation of the proposed MOT Options. Potential disparities between non-EJ and EJ populations were analyzed along with community impacts for the following factors:

Traffic Impacts

- Diversions – Average Daily Traffic (ADT) vehicles diverted from the Sherman Minton Bridge and percentage change (total vehicles and EJ area origin passenger vehicles).
- Congestion - under capacity, near capacity, or at capacity conditions based on estimated peak hour traffic at any of the four selected local street network locations.
- Change in average AM Peak trip length (miles) and time (minutes) for Non-EJ (total vehicles) and EJ (passenger vehicles by origin area).

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Transit impacts

- o Potential for a TARC route change (low to high)
- o Travel time delay for a TARC route change (minimal to 3 times existing)

Economic impacts

- o Relative construction duration (years)
- o Change in average AM Peak trip user cost and toll cost (\$ Non-EJ total vehicles and \$ EJ passenger vehicles by origin area)
- o Temporary disruption for local businesses (low to high)

Social impacts

- o Local access (ramp closures) and cross-river connectivity (maintained, reduced, or closed)

EJ impacts

- o Evaluated per FHWA Order 6640.23A (5)(g) that defines a “Disproportionately High and Adverse Effect on Minority and Low-Income Populations” as “an adverse effect that (1) is predominately borne by a minority population and/or a low-income population; or (2) will be suffered by the minority population and/or low-income population and is appreciably more severe or greater in magnitude than the adverse effect that will be suffered by the nonminority population and/or non-low-income population.”
 - All of the MOT options have some degree of EJ impacts, based upon diversion to toll facilities and increased network user costs
 - Disproportionately high and adverse effects to EJ populations vary by MOT option (yes/no)

MOT 1 – Two lanes open, both decks (EB and WB)

- Traffic impacts – 7,400 ADT diversions (8% total; 19% EJ), congestion (under capacity), change in trip length (miles – 0.2 non-EJ; 0.2 EJ) and trip time (minutes – 0.9 non-EJ; 0.9 EJ).
- Transit impacts – disruption of TARC routes (few); increase in rider travel time (low)
- Economic impacts – duration (3+ years), change in average AM Peak trip user cost (\$0.52 Non-EJ; \$0.41 EJ) and toll cost (\$0.20 Non-EJ; \$0.14 EJ)
- Social impacts - access closures (none) and cross-river connectivity (maintained)
- EJ impacts - disproportionately high and adverse effects (no)

MOT 2 – One lane open, both decks (EB and WB)

- Traffic impacts – 33,400 ADT diversions (37% total; 21% EJ), congestion (near capacity), change in trip length (miles – 0.1 non-EJ; 0.2 EJ) and trip time (minutes – 2.0 non-EJ; 2.6 EJ).
- Transit impacts – disruption of TARC routes (few); increase in rider travel time (moderate)
- Economic impacts – duration (2 years), change in average AM Peak trip user cost (\$1.37 Non-EJ; \$1.18 EJ) and toll cost (\$0.59 Non-EJ; \$0.35 EJ)
- Social impacts - access ramps (3 closures) and cross-river cohesion (maintained)
- EJ impacts - disproportionately high and adverse effects (no)

MOT 3 – Alternating three one-way lanes (AM-EB / PM-WB) open on one deck

- Traffic impacts – 40,600 diversions (45% total; 28% EJ), congestion (under capacity), change in trip length (miles – 0.2 non-EJ; 0.8 EJ) and trip time (minutes – 0.3 non-EJ; 3.2 EJ).
- Transit impacts – disruption of TARC routes (high); increase in rider travel time (high)
- Economic impacts – duration (2.5 years), change in average AM Peak trip user cost (\$0.57 Non-EJ; \$0.32 EJ) and toll cost (\$0.41 Non-EJ; \$0.07 EJ)
- Social impacts - access ramps (4 closures) and cross-river cohesion (reduced)
- EJ impacts - disproportionately high and adverse effects (yes)

MOT 4 – Reversible center lane (AM-EB / PM-WB) and one-way EB/WB lanes open on one deck

- Traffic impacts – 19,700 diversions (22% total; 26% EJ), congestion (under capacity), change in trip length (miles – 0.1 non-EJ; 0.1 EJ) and trip time (minutes – 1.0 non-EJ; 2.0 EJ).
- Transit impacts – disruption of TARC routes (none); increase in rider travel time (none)

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- Economic impacts – duration (2.5 years), change in average AM Peak trip user cost (\$0.68 Non-EJ; \$0.88 EJ) and toll cost (\$0.29 Non-EJ; \$0.20 EJ)
- Social impacts - access ramps (5 closures) and cross-river cohesion (maintained)
- EJ impacts - disproportionately high and adverse effects (no)

MOT 5 – Full Closure* of all six lanes and both decks

- Traffic impacts – 90,000 diversions (100% total; 18% EJ), congestion (at capacity), change in trip length (miles – 1.1 non-EJ; 1.3 EJ) and trip time (minutes – 3.3 non-EJ; 6.0 EJ).
- Transit impacts – disruption of TARC routes (none); increase in rider travel time (none)
- Economic impacts – duration (1.5 years*), change in average AM Peak trip user cost (\$2.77 Non-EJ; \$2.98 EJ) and toll cost (\$1.24 Non-EJ; \$0.72 EJ)
- Social impacts - access ramps (4 closures) and cross-river cohesion (closed)
- EJ impacts - disproportionately high and adverse effects (yes*)

* based on 1.5 years duration

MOT 6 – One Direction/Phase three one-way lanes (WB-Phase 1/EB-Phase 2) open on one deck

- Traffic impacts – 46,600 ADT diversions (52% total; 21% EJ), congestion (at capacity), change in trip length (miles – 0.5 non-EJ; 0.6 EJ) and trip time (minutes – 1.7 non-EJ; 3.2 EJ).
- Transit impacts – disruption of TARC routes (none); increase in rider travel time (none)
- Economic impacts – duration (2.5 years), change in average AM Peak trip user cost (\$1.43 Non-EJ; \$1.56 EJ) and toll cost (\$0.64 Non-EJ; \$0.36 EJ)
- Social impacts - access ramps (3 closures) and cross-river cohesion (reduced)
- EJ impacts - disproportionately high and adverse effects (yes)

Since 75% of the EJ Trips did not cross the river (via a passenger vehicle using the Sherman Minton Bridge); EJ populations within the Project Study Area are not disproportionately more reliant on the existing Sherman Minton Bridge nor would these populations be disproportionately affected by having to use an alternative river crossing. Similarly, regardless of MOT Option, the increased use of tolled river crossings for EJ Trips would be less than that of Non-EJ Trips. Table 6 below summarizes the potential impacts per MOT at full duration.

Table 6. Sherman Minton Renewal Project: Potential MOT Impacts

| TEMPORARY IMPACT CATEGORY | SUB-CATEGORY | MOT 1 | | MOT 2 | | MOT 3 | | MOT 4 | | MOT 5 | | MOT 6 | |
|----------------------------|-----------------------------|-----------|-----|-----------|-----|-----------|-----|-----------|-----|-----------|-----|-----------|-----|
| | | Disp High | Adv | Disp High | Adv | Disp High | Adv | Disp High | Adv | Disp High | Adv | Disp High | Adv |
| Traffic | Diversions | | | | | | | | | X | | | |
| | Access & Congestion | | | | | | | | | X | | | X |
| | Travel Distance | | | | | | | | | | | | |
| | Travel Time | | | | | | | | | | | | |
| Transit | TARC Riders* | | | | | X | X | | | X | X | X | X |
| Economic | Diversion to Tolls | | X | | X | | X | | X | | X | | X |
| | User Costs – Network | | X | | X | | X | | X | | X | | X |
| | User Costs – Local | | | | | | X | | | | X | | X |
| | Local Businesses** | | | | | | X | | X | | X | | X |
| Social | Access, Mobility, Cohesion | | | | | | X | | X | X | X | | X |
| | Quality of Life (Air/Noise) | | | | | | | | | | | | |
| Overall Potential (Yes/No) | | No | | No | | Yes | | No | | Yes | | Yes | |

* Applies primarily to cross-river riders on TARC Route 71

** Applies primarily to businesses in downtown New Albany

Minimization measures were part of project development, evaluations, and a combination of MOT options to reduced

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impacts. While the MOT 1 option had the most positive public involvement comments, fewest changes from existing travel patterns and local access, lowest induced traffic diversions and related environmental impacts; project constructability requirements were not entirely met. The Preferred Alternative MOT combined the predominant use of MOT 1, limited off-peak use of MOT-2 (*allowance for about 180 nights per construction year*), and short-term use of MOT-5. Detour routes are listed below and included in Appendix B, pages 19-26.

According to the CIA and EJ evaluations, Project Study Area EJ populations would be adversely affected in all six MOT options for diversion to tolled facilities and increased network user costs. The completion of proposed action will also provide long-term benefits with increased certainty and an extended service life for EJ populations and communities that rely on the Sherman Minton Bridge in both Indiana and Kentucky. All meeting summaries are included in Appendix G.

Based upon public involvement comments, EJ Outreach input, comparison to COC populations, and inclusion of the MOT Option that maintains cross-river community connectivity the lowest impacts; the project will not result in "Disproportionately High and Adverse" impacts for Study Area EJ populations.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?
 Is a Business Information Survey (BIS) required?
 Is a Conceptual Stage Relocation Study (CSRS) required?
 Has utility relocation coordination been initiated for this project?

| Yes | No |
|--------------------------|-------------------------------------|
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Number of relocations: Residences: _____ Businesses: _____ Farms: _____ Other: _____

If a BIS or CSRS is required, discuss the results in the remarks box.

Remarks: No relocations of people, utilities, businesses, or farms in Indiana or Kentucky will take place as a result of this project.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Documentation

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation
 Phase I Environmental Site Assessment (Phase I ESA)
 Phase II Environmental Site Assessment (Phase II ESA)
 Design/Specifications for Remediation required?

| | No | Yes/ Date |
|------------------------------------|--------------------------|------------------------------|
| ES Review of Investigations | <input type="checkbox"/> | April 3, 2019 |
| ES Review of Investigations | <input type="checkbox"/> | Limited RFI October 11, 2019 |

Include a summary of findings for each investigation.

Remarks: Based on a review of GIS and available public records, a RFI was completed on March 29, 2019 by Parsons (Appendix E, Page 1) and approved April 3, 2019, specific to the Sherman Minton Bridge location. No hazmat sites were identified in or within 0.5 mile search radius of the Sherman Minton Bridge that will impact the project.

Indiana
 A Limited RFI was completed on October 9, 2019, by Parsons (Appendix E, Page 16) fixated on Elm, Spring, 4th, and 5th Streets of New Albany, IN. Nine (9) underground storage tanks, one (1) state cleanup site, ten (10) leaking underground storage tanks, nine (9) brownfields and five (5) institutional control are located within the 0.5 mile search radius of the project area. One (1) underground storage tank (UST) site is located within the project area.

This UST site is the Kentuckiana Mack Sales and Service at 44 W. 5th Street, New Albany, IN(Agency ID #30115), located at the southeast corner of the intersection. The IDEM received an *Underground Storage Tank Notification*, dated August 10, 1988 that documented two USTs containing diesel as "permanently out of use" with anticipated removal in 1988; no further information was available. In addition to petroleum contamination, it is likely that lead would be in the soil/groundwater. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Before

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proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. No work is proposed at this intersection. Therefore, no impacts are expected. Further investigation for hazardous material concerns or regulated substances is not required at this time.

Kentucky

A desktop screening was conducted for the Kentucky side of the project. No concerns were identified within the project limits. The proposed project will stay within existing previously disturbed roadway ROW; therefore, no impacts are expected. Further investigation for hazardous material concerns or regulated substances is not required at this time.

Both Indiana and Kentucky

The Ohio River is listed as an Indiana Department of Environmental Management (IDEM) 303d impaired waters for dioxin, E. coli, Total Mercury in water, polychlorinated biphenyls (PCBs) in water and PCBs in fish tissue. The Ohio River is also listed as a Kentucky Energy and Environment Cabinet (EEC) 303d impaired waters for dioxin, E. coli, and PCBs. Workers who are working in or near water with E. coli should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure. If there will be sediment and/or soils disturbed by construction, additional investigation may be necessary. Coordination with INDOT ES and KYTC will occur. These recommendations are included as firm commitments in the Environmental Commitments section of this document.

SECTION I – PERMITS CHECKLIST

Permits (mark all that apply) Likely Required

| | |
|---|-------------------------------------|
| Army Corps of Engineers (404/Section10 Permit) | |
| Individual Permit (IP) | <input type="checkbox"/> |
| Nationwide Permit (NWP) | <input type="checkbox"/> |
| Regional General Permit (RGP) | <input type="checkbox"/> |
| Pre-Construction Notification (PCN) | <input type="checkbox"/> |
| Other | <input type="checkbox"/> |
| Wetland Mitigation required | <input type="checkbox"/> |
| Stream Mitigation required | <input type="checkbox"/> |
| IDEM | |
| Section 401 WQC | <input type="checkbox"/> |
| Isolated Wetlands determination | <input type="checkbox"/> |
| Rule 5 | <input type="checkbox"/> |
| Other | <input type="checkbox"/> |
| Wetland Mitigation required | <input type="checkbox"/> |
| Stream Mitigation required | <input type="checkbox"/> |
| IDNR | |
| Construction in a Floodway | <input checked="" type="checkbox"/> |
| Navigable Waterway Permit | <input type="checkbox"/> |
| Lake Preservation Permit | <input type="checkbox"/> |
| Other | <input type="checkbox"/> |
| Mitigation Required | <input type="checkbox"/> |
| KDOW | |
| Construction in a Floodway | <input checked="" type="checkbox"/> |
| 401 Water Quality Certification | <input type="checkbox"/> |
| US Coast Guard Section 9 Bridge Permit | |
| Others (Please discuss in the remarks box below) | |
| | <input type="checkbox"/> |

Remarks: Applicable recommendations provided by USACE, IDNR, KDOW, USEPA and USCG are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

USACE agency coordination response letter received May 8, 2020, (Appendix C, page 13) identifies recommendations and commitments for the Design-Build Contractor to follow. The following commitments section include the applicable recommendations.

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IDNR early coordination letter response received March 11, 2019, (Appendix C, page 17), indicates the I-64 Sherman Minton Bridge work will require the formal approval for construction in a floodway under the Flood Control Act IC 1-28-1.

KDOW indicated on July 18, 2019, at the agency field visit a KDOW Construction and Floodplain Permit will be required. If the Design-Build Contractor decides to access or work within any waterway, a 401 Water Quality Certification will also be required.

USCG early coordination letter response received on December 18, 2018, (Appendix C, page 78) indicates USCG will not require a Coast Guard Permit; However once a contractor has been selected, USCG will require work plans to be reviewed and a conditions letter will be issued before work can commence.

It is the responsibility of the Design-Build Contractor to identify and obtain all required permits.

SECTION J- ENVIRONMENTAL COMMITMENTS

The following information should be provided below: List all commitments, name of agency/organization requesting the commitment(s) and indicating which are firm and which are for further consideration. The commitments should be numbered.

Remarks: The following commitments are based upon those provided or requested by environmental regulatory agencies. Additional details and requirements set forth by regulatory agencies, INDOT and KYTC are conveyed in the Design-Build Best Value Technical Provisions.

Firm:

- 1) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT District)
- 2) It is the responsibility of the project sponsor to notify school corporations, hospitals and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
- 3) Workers who are working in or near water with E. coli should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure. (INDOT SAM)
- 4) If there will be sediment and/or soils disturbed in the Ohio River by construction, additional investigation may be necessary. Coordination with INDOT ESD and KYTC will occur. (INDOT SAM)
- 5) If excavation occurs in or near 44 W. 5th Street, New Albany, IN, it is likely that petroleum contamination will be encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. (INDOT SAM)
- 6) Accommodations will be provided for the following special events and festivals and full bridge closures will not occur on: New Year’s Day, Good Friday, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, Christmas Day, Thunder over Louisville, Kentucky Derby, and Harvest Homecoming Festival. (INDOT ESD)
- 7) Coordination with the Louisville Parks and Recreation is to be maintained by the Design-Build Contractor with project updates to ensure the safety of trail users. (KYTC-DEA)
- 8) If the Design-Build Contractor requires temporary access or use of any Section 4(f) or 6(f) resource during construction, it is the Design-Build Contractor’s responsibility to coordinate with necessary agencies including but not limited to INDOT, KYTC, FHWA, the City of New Albany, the City of Louisville, the Louisville Parks and Recreation, and the Ohio River Greenway Commission, as Section 4(f) or Section 6(f) analysis may be required. (Technical Provision Section 7)
- 9) Early coordination response information received from Indiana Geological Survey is to be reviewed by the Design-Build Contractor. (Technical Provision Section 7)

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- 10) Once a contractor has been selected, United States Coast Guard will require them to submit a work plan to be reviewed by our office and a work conditions letter will need to be issued from our office before any work can commence. (USCG)
- 11) No impacts due to construction will occur to the Ohio River. Should impacts be unavoidable the contractor will be responsible for coordination to obtain clearance for section. If there are any questions regarding this note, please contact Danny Peake, Director, Division of Environmental Analysis, 200 Mero Street, Frankfort, KY 40601; Phone (502) 564-7250. (KYTC-DEA)
- 12) The Design-Build Contractor must coordinate the chosen alternative with KYTC. Once an alternative has been chosen KYTC shall provide SHPO with the chosen alternative and final archeological effects recommendation. (KY-SHPO)
- 13) Once an Alternative and APE has been chosen, KYTC shall identify and reach out to the appropriate consulting parties based upon the chosen APE. (KY-SHPO)
- 14) If the design-build contractor proposes work outside of the existing ROW, coordination with INDOT Cultural Resources will be required. (INDOT-CRO)
- 15) Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of the riprap. (USFWS)
- 16) Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat. (USFWS)
- 17) Implement pollution prevention and control measures during all construction activities to reduce the potential for hazardous spills or other materials entering the Ohio River. This will include the placement of refueling staging areas, fuel storage, and hazardous materials away from the river, and may also require specific containment measures for painting, sanding, etc. (USFWS)
- 18) If a causeway must be used, then locate the causeway primarily outside of the cobble/gravel substrate area, which is the most suitable habitat for many mussel species. (USFWS)
- 19) Install culverts/pipes within the causeway to allow continued flow of water through the area to prevent pooling and stagnation. (USFWS)
- 20) The height of the causeway should be kept to a minimum to allow over-topping during heavy rain events to prevent upstream flooding. (USFWS)
- 21) Use clean fill material and remove immediately once project is completed. (USFWS)
- 22) The structure should not be in the stream longer than a year in order to minimize disruption of the mussel and host fish reproductive cycle. (USFWS)
- 23) All equipment to be used in the river should be inspected using accepted protocols and determined free of zebra mussel adults and veligers. (USFWS)
- 24) In the event a barge is used, all barge equipment maintenance will be conducted away from the river, whenever possible. Fuel storage shall be contained/maintained in an area where leakage and spilling into the river will be avoided. (USFWS)
- 25) Excavation for the deadman anchors and steel cables would be performed in a manner to minimize the amount of surface disturbance, and appropriate measures would be implemented to prevent the discharge of material into the river channel. During excavation, temporary silt fence will be installed around each deadman anchor site during excavation and installation. Extreme caution will be exercised during excavation/installation activities to prevent sediment from being washed into the Ohio River. (USFWS)

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- 26) The towboat will be operated at as low of RPM's as practicable when approaching and leaving the work site to minimize river bottom scouring and downstream siltation. (USFWS)
- 27) Minimize impacts to shoreline and substrate via barge grounding. (USFWS)
- 28) Align the road along or through previously disturbed and degraded areas and disturb as narrow an area as possible to minimize negative impacts. Avoid tree removal to the greatest extent possible. Plant native hardwood trees to replace the vegetation destroyed during construction. (IDNR)
- 29) All plant material, mud, and debris should be removed, and all water drained from equipment before entering or leaving the waterway to prevent the spread of aquatic and terrestrial invasive species. (IDNR)
- 30) Avoid staging and construction access within or wooded areas to the extent possible. (IDNR)
- 31) Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to nonwetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10 inches dbh or greater (5:1 mitigation based on the number of large trees). (IDNR)
- 32) Section 404 of the Clean Water Act (CWA) establishes a program to regulate the discharge of dredged or fill material into waters of the United States, including wetlands. Section 10 of the Rivers and Harbors Act of 1899 requires authorization for the construction of any structure in, over or under any navigable water of the United States. The proposed project would require two applications to be submitted for authorization under Section 404 of the Clean Water Act and Section 10 of the Rivers and Harbors act - one application for impacts to waters of the U.S. in Kentucky and one application for impacts in Indiana. INDOT and KYTC have funded positions in Louisville District's Regulatory Division under the Water Resources Development Act (WRDA). These personnel would process the Section 404/Section 10 permits for this project. Under the WRDA funding agreements, the permits cannot be issued to any entity other than to those agencies funding the positions, thus Louisville District will have to process two separate actions for the proposed project. The WRDA funding agreement also requires KYTC and INDOT to be co-applicants with the contractor when the contractor applies for the permits to each state, respectively. KYTC and INDOT, along with the contractor, assume all responsibilities and risks associated with the permit and the contractor's work. (USACE).
- 33) If barges would be moored on the Ohio River or doing any work on the river, a Section 10 permit would be required as stated above. The permit would be part of the Section 404 permit. A map showing the location of barges would be required, along with drawings stamped by a professional engineer showing the locations and mooring configurations (including locations of deadmen that would be installed). Include a narrative/description of the mooring configuration and work to be performed. (USACE)
- 34) Work within the river would require Regulatory to coordinate with the Navigation Branch of the Louisville District Corps, which may necessitate the Corps permit being conditioned directing the permittees to notify Navigation 30 days prior to the commencement of work/mooring on the river for Navigation to issue a Notice to Navigation Interests. The Corps permit, if issued, would provide the contact information. (USACE)
- 35) The permit application must include the location, size and work for any staging, borrow and/or waste sites, with a description of work at those locations' areas; temporary work to be performed, including the installation of temporary mats, cofferdams, etc. (USACE)
- 36) The permit that would be issued for this project would require the contractor to notify the Corps if potential endangered species or historic/archeological resources are encountered during the course of work. This should also be included in the contract letting documents. (USACE)
- 37) The Corps must be notified of any modifications to the authorized work. Please ensure this requirement is included in the contract. (USACE)
- 38) Please ensure the Corps is supplied with either a U.S. Coast Guard (USCG) permit or correspondence from the USCG stating a permit is not required. The Corps will require either of these documents prior to issuance of any Corps permits. (USACE)

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39) Based upon the Navigable Waters Protection Rule that took effect in June 2020, Wetlands/open waters will need to be reevaluated throughout the project under the Navigable Waters Protection Rule to determine if they are jurisdictional waters. (USACE)

40) All future correspondence with the Corps should reference the Corps ID number for this project. Which is LRL-2018-1114. (USACE)

41) Advance notice must be provided to Metro Public Works, Emergency Management, and Transit Authority of River City (TARC) prior to any closures that extend more than 24 hours. (INDOT)

For Further Consideration:

N/A

SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received. INDOT and FHWA are automatically considered early coordination participants and should only be listed if a response is received.

Remarks:

The following agencies were sent early coordination letters on December 12, 2018, as a part of the development of this Environmental Study. Additional coordination letters were sent February 15, 2019. Early coordination Letters are found in Appendix C.

| Agency | Response Date | Appendix C |
|--|--|--------------|
| FHWA Indiana | No Response | Not Included |
| FHWA Kentucky | No Response | Not Included |
| INDOT Environmental Services | No Response | Not Included |
| INDOT Cultural Resources | No Response | Not Included |
| INDOT Public Works | No Response | Not Included |
| KYTC Division of Environmental Analysis | No Response | Not Included |
| KYTC Division of Cultural Historic Branch | No Response | Not Included |
| USEPA Region 5 Chicago | February 7, 2019 | C4 |
| USEPA Region 4 Atlanta | February 7, 2019 | C4 |
| USACE Louisville District | January 14, 2019 | C13 |
| USFWS IN-Bloomington Field Office | February 20, 2019 | C23 |
| USFWS KY | No Response | Not Included |
| United States Coast Guard 8 th District | December 18, 2018 | C78 |
| National Resources Conservation Office / USDA | December 17, 2018 | C79 |
| IDEM | Auto response generated March 19, 2019 | C80 |
| IDNR | March 11, 2019 | C17 |
| Kentucky Division of Water (KDOW) | No response | Not Included |
| IN SHPO | January 7, 2019 | D1 |
| KY SHPO | December 20, 2018 | D17 |
| National Parks Service | No Response | Not Included |
| US. – HUD | No Response | Not Included |
| Indiana Geological Survey | Auto generated response March 12, 2019 | C85 |