

Note to File

Date	April 27, 2023
To	Mr. Andrew Passmore (INDOT), Ms. Erica Tait (FHWA), and Mr. Dave Harmon (KYTC)
From	Susan Castle, Senior Project Manager
Subject	INDOT Lead Des. No. 1702255, Additional Des. Nos. 1702260, 1702254, 1592187, 1702257, 170225 8, 1702259, 1701215, & 1900579 Kentucky Transportation Cabinet (KYTC) Item ID 5-10027

On October 7, 2020, the Federal Highway Administration (FHWA) approved a Categorical Exclusion (CE) Level 4 Document for the proposed Bridge, Associated Approaches, and Road Improvements located on West 5th Street, West Spring Street, and West Elm Street near the I-64 ramps, I-64 and US 150 Sherman Minton Bridge crossing the Ohio River in New Albany, Floyd County, Indiana and in Louisville, Jefferson County, Kentucky, extending from the I-64 / I-264 interchange in Louisville, Kentucky, 3.5 miles to the northwest, to the I-64 / I-265 interchange in New Albany, Indiana. Supplementally, an Additional Information (AI) Document was developed and approved on September 30, 2021 to provide the contractor related details prior to initiation of construction of the project.

The approved CE-4 and AI Documents outlined the preferred alternative for Maintenance of Traffic (MOT) as outlined below:

- One (1) Eastbound (EB) and one (1) Westbound (WB) lane will be closed throughout construction.
- Open travel lanes will shift location on the Sherman Minton Bridge during construction.
- Two (2) EB and (2) WB lanes and associated access ramps will remain open for cross-river traffic and existing access ramps will remain open except for the following allowances:
 - 180 nights per calendar year during which two (2) EB and two (2) WB lanes and associated access ramps will be closed each night approximately from 9 pm to 4 am and 10 pm to 5 am, respectively. Cross-river traffic will be maintained with one (1) EB and one (1) WB travel lane open and a temporary crossover lane in Kentucky for I-64 WB to merge with I-264.
 - Short-term closure of the Sherman Minton Bridge will be allowed for one (1) nine (9) consecutive day period and up to three (3) weekend closures per direction during each calendar year; excluding holidays and community events as outlined in the approved CE-4. During the short-term bridge closure, all I-64 (US 150) cross-river traffic will be diverted to the approved detour routes.


Since the CE-4 and AI were approved and construction has commenced numerous schedule delays have occurred for various reasons that have greatly impacted the construction schedule.

The project team is evaluating all means of schedule recovery, which includes this memorandum outlining a minor change in the approved Traffic Management Plan (TMP). As outlined above, the current TMP allows for one (1) nine (9) consecutive day closure per direction during each calendar year. The project team has proposed to split their 9-day WB closure around the Memorial Day Holiday weekend (May 27, 2023 – May 29, 2023) to facilitate the upcoming change in the traffic pattern, while avoiding impact to public travel during the holiday weekend. This proposed change would involve closure of all WB traffic from May 23, 2023 at 5 am through May 26, 2023 at 5 am at which time traffic will be reopened over the holiday weekend. The team then proposes to resume the 9-day WB closure on May 30, 2023 at 8 pm through June 5, 2023 at 8 pm.

The reason for this proposed change is to facilitate the current schedule while not losing out on the full 9-day closure opportunity due to large scope elements needing to occur close to a major holiday. The project is approaching the transition from Phase 2 to Phase 3 of Traffic Management, which involves a major shift in the traffic pattern that requires reconstruction of the temporary crossover ramps. The crossover currently facilitates the EB contraflow traffic lane on the upper deck which will be altered to facilitate the WB contraflow traffic lane to the lower deck for the next two construction phases of the project. In addition to reconstruction of the crossover, additional work will include shifting of the barrier wall, restriping of the WB lanes and adjustment of the overhead signs. Additionally, the project team will be able to begin demolition work for phase 3 that includes removal of the guardrail, deck saw cutting, and concrete deck slab removal.

All other proposed improvements and commitments in the approved CE-4 document, approved October 7, 2020 and the AI approved on September 30, 2021, remain unchanged. No further environmental investigations are anticipated to be required.

Metric Environmental, LLC



Susan K. Castle
Senior Project Manager

Enclosures: Appendix A - CE-4 approved October 7, 2020
Appendix B - AI Document approved September 30, 2021
Appendix C - Maintenance of Traffic References

APPENDIX A: Approved CE

Indiana Department of Transportation

County Floyd County, IN Jefferson County, KY Route I-64 Des. No. Primary Des. No. 1702255


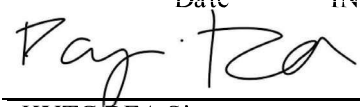
**FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION**


Road No./County:	I-64 / Floyd County, Indiana - Jefferson County, Kentucky
Designation Number:	INDOT Primary Des. No. 1702255, Additional Des. Nos. 1702260, 1702254, 1592187, 1702257, 1702258, 1702259, 1701215, & 1900579. KYTC Item ID 5-64.
Project Description/Termini:	I-64 Sherman Minton Bridge Rehabilitation and associated approaches. The project is located at the Interstate I-64 and US 150 Sherman Minton Bridge crossing of the Ohio River in Floyd County, New Albany, Indiana, and in Jefferson County, Louisville, Kentucky. The lead project termini extend from the I-64 / I-264 interchange in Louisville Kentucky 3.5 miles to the northwest to the I-64 / I-265 interchange in New Albany Indiana.

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager)
	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services Division)
X	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA
	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA

Note: For documents prepared by or for Environmental Services Division, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval	<u>N/A</u>		Digitally signed by Brandon Miller Date: 2020.10.05 16:56:02 -04'00'	<u>10/5/2020</u>
	ESM Signature	Date	INDOT ES Signature	Date
			<u>10/5/2020</u>	
	KYTC DEA Signature	Date		
	<u>Erica Tait</u>	Digitally signed by Erica Tait Date: 2020.10.07 16:14:16 -04'00'	<u>10/7/2020</u>	
	FHWA Signature	Date		

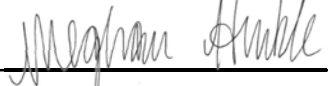
Release for Public Involvement	<u>N/A</u>		<u>6/23/2020</u>
ESM Initials	Date	ES Initials	Date

Certification of Public Involvement

 10/1/20

Office of Public Involvement Date

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

INDOT ES/District Env. Reviewer Signature:  Date: 10-5-2020

Name and Organization of CE/EA Preparer: Mary Pusti, Michael Baker International Inc.

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Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*?	Yes	No
	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If No, then:		
Opportunity for a Public Hearing Required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks:

Notice of Entry Letters
Indiana: Notice of Entry Letters were mailed to potentially affected property owners near the project area on September 13, 2018 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of entry letter for both Indiana and Kentucky is included in Appendix G, page 1.

Kentucky: Notice of Entry Letters were mailed to potentially affected property owners near the project area on September 13, 2018 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter for both Indiana and Kentucky is included in Appendix G, page 1.

Section 106
Indiana: This project qualifies under the Minor Projects Programmatic Agreement (MPPA), therefore does not require additional public involvement under Section 106 in Indiana.

Kentucky: The Kentucky Heritage Council, which is the Kentucky State Historic Preservation Office (SHPO), requirements of Section 106 in Kentucky are satisfied through the public involvement process of this project.

Public Involvement Comments and Responses
 A Project-specific Public Involvement Plan (PIP) was developed that identifies various communication and outreach tools to facilitate project communication, outreach and engagement for the project and is included with the public involvement documentation for the project (Appendix G, page 4). General project communication and outreach for both Indiana and Kentucky, are supported through the use of a project website, social media, traditional media outlets, fact sheets, use of comment cards, newsletters, small group and public information meetings, and a feedback survey:

- Project Website – via <http://shermanmintonrenewal.com>, was designed to constantly update the public on project developments, upcoming events, answers to their questions and to share their feedback; the website has accumulated over approximately 10,442 visitors.
- Social media – via the Sherman Minton Facebook Page and Twitter account, [@ShermanRenewal](#), has recorded over 5,251 engagements (public shares, likes, and comments via a social media post or page), and approximately 97,606 social media views. Local media has covered approximately 120 stories of the project, including an “In Conversation” Radio Call in Program with local radio station WFPL on August 2, 2019.
- Comment Cards – for the project were handed out at outreach events; to date, approximately 170 public comments have been received via the website or completed comment cards available at public open house and information meeting.
- Project Survey – was developed and posted on the website from July 2019 thru November 2019 to elicit feedback on potential Maintenance of Traffic (MOT) Options; there were approximately 3,006 survey responses.
- Community Advisory Committee (CAC) – established to facilitate communication about regional economic and community considerations for both sides of the Sherman Minton Bridge (greater New Albany and west

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Louisville). The CAC is comprised of business, community and municipal representatives from both sides of the Ohio River. Four (4) CAC meetings for the project have been held; September 19, 2018, November 29, 2018, July 16, 2019 and February 6, 2020.

- Environmental Justice (EJ) Technical Advisory Committees (TACs) – due to the distinct characteristics between greater New Albany and west Louisville, two (2) separate EJ TACs were developed to facilitate communication and outreach to these specific populations. Four (4) sets of EJ TAC meetings have been held: the New Albany EJ TAC met on September 18, 2018, December 4, 2018, July 17, 2019, and February 11, 2020 while the west Louisville EJ TAC met on September 18, 2018, November 29, 2018, July 18, 2019, and February 6, 2020.
- Public Official Briefings – offered opportunities for elected officials and their operational constituencies to provide the project team with feedback on important public service considerations; two (2) rounds of public officials’ briefings were included during the project planning phase. Louisville Metro, City of New Albany, Floyd County, Harrison County, Jefferson County and Clark County officials met on September 10, 2018 and July 8, 2019. An additional public officials meeting will be held in the final stages of public involvement.
- Public Open House/Information Meetings – facilitate general communication with the broader public both formally through presentations and more informally through individual conversations with project team members at information stations; to date, two (2) rounds of Public Open Houses were held in both New Albany and west Louisville. Public comments from the Open Houses were facilitated through the project website and comments cards available during meetings. New Albany Public Open Houses were held October 2, 2018 and July 25, 2019. Louisville Public Open Houses were held October 4, 2018 and July 23, 2019.
- Small Group Stakeholder Meetings – allowed for technical interaction between the project team and key stakeholder groups that focused on specific concerns or constituencies; to date, individual meetings/briefings have been held with the following:
 - One Southern Indiana (OSI) met November 7, 2018
 - Develop New Albany met August 21, 2019
 - Greater Louisville Inc (GLI) Transportation Committee met September 24, 2019
 - Transit Authority of River City (TARC), met September 5, 2019 and February 4, 2020
 - One West Community Conversation met on September 5, 2019
 - First Responders Meeting with local emergency service providers took place on August 22, 2019
 - West Jefferson County Community Task Force met March 19, 2019 and August 20, 2019
- Additional Public Involvement – In accordance with the Indiana Department of Transportation (INDOT) and the Kentucky Transportation Cabinet (KYTC) Virtual Public Involvement Procedures, the environmental document was available between July 1, 2020, and August 16, 2020, for public review and comment. Project information materials, including a description of the Preferred Alternative for Maintenance of Traffic (MOT) during construction, project brochure and comment cards were also available to facilitate public understanding of the project and to encourage feedback. Both the environmental document and additional public information materials were available in five public repository locations, on the project website, <http://shermanmintonrenewal.com/> and were offered upon request. A presentation with audio and a project video was available on the project website and via social media channels. Follow-up communications were conducted with the project’s Community Advisory Committee and both Environmental Justice Technical Committees to encourage the dissemination of project information and to request feedback. A public hearing is not required due to the level of environmental documentation; however, the opportunity to request a public hearing by members of the public was advertised during the public review period. Advertisement of the public review period occurred through social media, the project website and local newspaper listings. *The Courier Journal* published a public notice on July 3, 2020 and on July 10, 2020 (Appendix G, page 7). *The New Albany Tribune* published a public notice on July 7, 2020 and July 14, 2020 (Appendix G, page 10). *The Louisville Defender* published a public notice July 2, 2020 and on July 9, 2020 (Appendix G, page 12). No requests for a public hearing were received during the 45-day public review period of July 1, 2020, and August 16, 2020. During the public review and comment period over 350 comment cards and brochures were distributed, and the

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project website had 1,160 views and 13,000 social media impressions were recorded. Social media impressions are defined as are the number of times content is displayed over one social media or multiple social media sources. The public provided a total of 25 comments through a variety of means and methods including the project website (7 comments received), e-mail (8 comments received), telephone hotline (2 comments received), and via comment cards (5 comments received). Appendix G, page 25 identifies comments collected during the public review period. All public involvement related materials, including comments received on the environmental document and corresponding responses are provided in Appendix G.

Applicable public involvement input summaries are included in the Community Impact Assessment (CIA) and EJ Analysis Technical Report (Appendix I, page 18) and the public inquires identified on Appendix G, page 110.

Public Controversy on Environmental Grounds

Will the project involve substantial controversy concerning community and/or natural resource impacts? Yes No

Remarks:

The project was identified early as having potential controversial community impacts to local residences, business and EJ populations. These impacts are based upon required temporary changes in access to and across the Sherman Minton Bridge during construction, all temporary changes of access are considered in the MOT options, and the potential EJ populations within the study:

- The Sherman Minton Bridge has been in service since 1962 and is the non-tolled option of the two Interstate crossing of the Ohio River in the Louisville metro area. The double-decker Sherman Minton Bridge carries approximately 90,000 vehicles per day as part of interstate (I-64) and daily regional (US 150) travel between the City of New Albany, Indiana and the area of west Louisville in Kentucky.
- An emergency closure of the Sherman Minton Bridge was announced and implemented on September 9, 2011 through February 17, 2012. The 2011 closure and related traffic diversions, congestion, uncertainty, and lengthy travel delays for the five months that followed left a lasting impression for many local officials, individuals, communities, and businesses.
- Potential EJ communities were identified early within the project study area and as utilizing the Sherman Minton Bridge with input from the Kentuckiana Regional Planning & Development Agency (KIPDA) regional model.
- Public outreach with local officials, community representatives, and public comments indicated that while there is broad community support for rehabilitating the Sherman Minton Bridge, the temporary impact of traffic diversion, congestion, and heavy trucks on the regional network and local streets, increased travel times and costs (and tolls), and loss of cohesion associated with the project were substantial concerns both regionally and locally. EJ populations were particularly sensitive to potential access and mobility impacts

Based upon early coordination meetings with regulatory agencies and public outreach, at this time, there is no controversy concerning impacts to natural resources.

Consideration to the public and local EJ communities was included in the development of MOT sections, due to the importance of community cohesion between New Albany, IN, and Louisville Kentucky. Minimization measures were part of the project development evaluations detailed in the remarks of the MOT Section, page 18, Community Impact Section, page 41, and EJ Section, page 45 of this document. CAC and EJ constituents along with the general public have been updated as MOT has evolved throughout the project via public meetings, the project website, and social media platforms.

The Preferred Alternative MOT was developed to meet constructability requirements, reflect public involvement input, and to reduce temporary MOT impacts for those that rely on the Sherman Minton Bridge in both Indiana and Kentucky; which should lower potential public controversy for the proposed action. The completion of proposed action will also provide long-term benefits with increased certainty and an extended service life for those that rely on the Sherman Minton Bridge in both Indiana and Kentucky. Furthermore, during the latest public involvement activities the public has not currently expressed concern with the Preferred Alternative MOT.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Indiana Department of Transportation (INDOT) & Kentucky Transportation Cabinet (KYTC) INDOT District: Seymour
 Local Name of the Facility: Interstate I-64 and US 150 Sherman Minton Bridge crossing of the Ohio River KYTC District: District 5

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Funding Source (mark all that apply): Federal State Local Other*

*If other is selected, please identify the funding source: _____

PURPOSE AND NEED:

Describe the transportation problem that the project will address. The solution to the traffic problem should NOT be discussed in this section. (Refer to the CE Manual, Section IV.B.2. Purpose and Need)

Need - The need for the project is due to the deteriorating structural condition of the existing Sherman Minton Bridge over the Ohio River, the deteriorating associated Indiana and Kentucky approaches, and deteriorating pavement of select associated side streets.

Structural elements deficiencies, depicted in Appendix B, page 7, were identified in the October 18, 2017 INDOT Bridge Assessment Report, the November 2019 INDOT Bridge Inspection Reports, and the November 2019 KYTC Bridge Inspection Report and are summarized below. Condition ratings are out of 1 to 9 (poor to good).

Des. Nos. 1702255 & 1592187 (Bridge No. I64-123-04691 D; Sherman Minton Bridge)

- **Bridge Decks** – Approximately 60 years old and were part of the original construction, the bridge decks have internal and external cracking observed at the underside of deck joints. Spalling was identified on limited areas on the upper deck, along the piers, abutments, copings and curbs. High amount of chloride exposure primarily from road salts was identified at the level of the reinforcing steel; 2018 INDOT Deck Condition Assessment confirmed the presence of chlorides as the primary deterioration mechanism. Corrosion identified on reinforcement steel leads to a weakening of the bridge deck and localized spalling and delamination of the concrete. This corrosion eventually leads to potholes. The bridge decks received a rating of 5/fair condition rating on the November 2019 INDOT inspection.
- **Arch and Truss Members** - Members exhibit areas of paint failure throughout the structure, leaving steel elements unprotected, with minor to moderate section loss observed. The arch and truss members received a rating of 5/fair condition rating on the November 2019 INDOT inspection.
- **Cable Hangers** – Are comprised of many individual wires bundled together in one cable, with a minimum of 130 individual wires in each cable. These Cable Hangers and the connectors exhibit surface corrosion due to inadequate protection from natural elements. In addition, one cable was observed to be swelling due to internal corrosion. Most cables exhibit 1 to 3 displaced wires along the length of the cables; and several cables connections exhibit moderate to heavy corrosion or pack rust of the connecting elements. The cable hangers received a rating of 5/fair condition rating on the November 2019 INDOT inspection.
- **Floorbeam Frames** – Carry all the load/weight from the deck out to the supporting hanger cables. Significant widespread paint failure was observed. The paint failure has left the frames vulnerable to corrosion. Corrosion varies from surface corrosion to heavy section loss with some areas approaching 50% of the original thickness. This heavy reduction in the size due to corrosion results in a reduction of the overall load carrying capacity of individual members. No overall load capacity of the bridge is affected at this time. The floorbeam frames received a rating of 5/fair condition rating on the November 2019 INDOT inspection.
- **Steel Stringer** – In place to support the deck between the floor beam frames. Widespread paint failure was observed. The paint failure has left the Steel Stringers vulnerable to corrosion. Corrosion varies from surface corrosion to heavy section loss. Cracking is observed in some stringer ends as a result of section loss due to corrosion and out of plane distortion. The steel stringer received a rating of 5/fair condition rating on the November 2019 INDOT inspection.
- **Substructure (concrete piers and foundations)** – Support of the overlying substructure elements. Limited concrete cracking was identified along the piers and abutments. Some spalling was identified on limited areas along the piers, and abutments. The substructure received a rating of 6/fair condition rating on the November 2019 INDOT inspection.

Des. Nos. 1702254 & 1702260 (Bridge #056B00161N; KY Approach)

- **Bridge Deck and Superstructure elements** – Evidence of internal and external cracking and corrosion was identified. Minor amounts of section loss were present. The bridge deck and superstructure received a rating of 5/fair condition rating on the November 2019 KYTC inspection.
- **Substructure** – Concrete cracking along the piers and abutments and some spalling has been identified. The substructure received a rating of 5/fair condition rating on the November 2019 KYTC inspection.
- **Paint** – Major deterioration of paint was evident giving the structure a poor condition rating. The paint failure has allowed

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exposure to the super structure elements, which increases corrosion on the superstructure. The bridge paint received a rating of 4/poor condition rating on the November 2019 KYTC inspection.

Des. No. 1702257 (Bridge #I64-123-02294 CWBL; I-64 WB over SR 111/Main Street, RR IN Approach WB)

- *Bridge Deck and Superstructure elements* – Evidence of minor internal and external cracking and corrosion. The bridge deck received a rating of 6/fair condition rating and the superstructure received a rating of 5/fair condition rating on the November 2019 INDOT inspection.
- *Substructure* - Minor spalling has been identified. The substructure received a rating of 6/fair condition rating on the November 2019 INDOT inspection.

Des. No. 1702258 (Bridge #I64-123-02294 CEBL; I-64 EB over SR 111/Main Street., RR IN Approach EB)

- *Bridge Deck and Superstructure elements* – Evidence of internal and external cracking and corrosion identified. Minor amounts of section loss were present. The bridge deck received a rating of 6/fair condition rating and the superstructure received a rating of 6/good on the November 2019 INDOT inspection.
- *Substructure* – Minor cracking along the abutments. The superstructure received a rating of 6/good on the November 2019 INDOT inspection.

Des. No. 1702259 (Bridge #I64-123-02294 JCEB; I-64 EB over SR 111/Main Street., RR IN Approach EB)

- *Bridge Deck and Superstructure elements* – Evidence of external cracking and corrosion identified. Minor amounts of section loss were present. The bridge deck and superstructure received a rating of 6/fair condition rating on the November 2019 INDOT inspection.
- *Substructure* – Minor cracking along the abutments. The substructure received a rating of 6/good condition rating on the November 2019 INDOT inspection.

Des. No. 1900579 (Bridge #I64-123-04690 BEBL; I-64 EB over Market Street 0.11 W of SR 111)

- *Paint* – Minor deterioration of paint was evident. The paint failure has allowed exposure to the super structure elements, which could lead to an increase of corrosion on the superstructure. The bridge paint received a rating of 5/fair condition rating on the November 2019 INDOT inspection.

Deterioration was also identified on side streets:

Des. No. 1701215 (Old SR 62 [Elm Street] from I-64 Exit Ramp to State Street)

- *Elm, Spring, and 5th Streets* - Pavement deterioration and non-compliant American Disability Association (ADA) standard curb ramps are identified along Elm, Spring, and 5th Streets of New Albany, IN.

Purpose - The purpose of the Project is to address the deterioration of structural elements listed below; of the Sherman Minton Bridge, the associated Indiana and Kentucky approaches, and select associated side streets with the goal of extending the service life of the I-64 Interstate crossing over the Ohio River up to 30 years.

Des. Nos. 1702255 & 1592187 (Bridge No. I64-123-04691 D; Sherman Minton Bridge)

- Address the deterioration of the Bridge Decks, Arch and Truss Members, Cable Hangers, Floor Frames, Steel Stringer and Substructure; and to protect the structure for future use.

Des. Nos. 1702254 & 1702260 (Bridge #056B00161N; KY Approach)

- Address the deterioration of the Bridge Decks, Superstructure elements and Substructure; and to protect the structure for future use.

Des. No. 1702257 (Bridge #I64-123-02294 CWBL; I-64 WB over SR 111/Main Street, RR IN Approach WB)

- Address the deterioration of the Bridge Decks, Superstructure elements and Substructure.

Des. No. 1702258 (Bridge #I64-123-02294 CEBL; I-64 EB over SR 111/Main Street., RR IN Approach EB)

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- Address the deterioration of the Bridge Decks, Superstructure elements and Substructure.
Des. No. 1702259 (Bridge #164-123-02294 JCEB; I-64 EB over SR 111/Main Street., RR IN Approach EB)
 - Address the deterioration of the Bridge Decks, Superstructure elements and Substructure.
Des. No. 1900579 (Bridge #164-123-04690 BEBL; I-64 EB over Market Street 0.11 W of SR 111)
 - Protect the Superstructure elements for future use.
- Purpose identified on side streets:
Des. No. 1701215 (Old SR 62 [Elm Street] from I-64 Exit Ramp to State Street)
- Address the deterioration of the pavement and provide ADA compliant standard curb ramps along Elm, Spring, and 5th Streets in New Albany, IN.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Floyd, IN & Jefferson, KY Municipality: New Albany, Indiana and Louisville, Kentucky

Limits of Proposed Work: Elm Street from 2nd Street to State Street in Indiana, Spring Street from State Street to 5th Street to Main Street in Indiana, upper and lower decks of the Sherman Minton Bridge to the Kentucky approach crossovers.

Total Work Length: 1.8 Mile(s) Total Work Area: 17.3 Acre(s) (This includes work area measured on both decks of the Sherman Minton Bridge)

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required?

Yes ¹	No
	X
Date: _____	

 If yes, when did the FHWA grant a conditional approval for this project?

¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

In the remarks box below, describe existing conditions, provide in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

Location
 The project location is the I-64 Interstate and US 150 (I-64) over the Ohio River connecting New Albany, Floyd County, Indiana and Louisville, Jefferson County, Kentucky (Appendix B, page 1). The project is centered at 38.278665°N, -85.822237°E, the location of the Sherman Minton Bridge, in the Indiana New Albany Quadrangle, Sections 21, 22, 27, and 28, Township 27N, and Range 8E. The Sherman Minton Bridge connects New Albany, Indiana and the west side of Louisville, Kentucky, which is often referred to as west Louisville throughout this document.

The Sherman Minton Renewal Project (SMRP) logical termini are the system to system I-64/I-264 interchange to the south in Kentucky and the I-64/I-265 interchange to the north in Indiana and have independent utility with the surrounding interstate network. Local side streets, Elm Street, Spring Street, and 5th Street (also referred to as West Elm Street, West Spring Street and West 5th Street), are located within the City of New Albany, IN near the Sherman Minton Bridge. In the past when there have been closures of the Sherman Minton Bridge, traffic diverts onto these local side streets. The side streets were identified as needing improvements including ADA compliant standard curb ramps. Due to these side streets close proximity to the bridge repairs, it was identified by INDOT and recommended to be included in the same contract as the Sherman Minton Bridge. The logical termini for the side street improvements is Elm Street from the I-64 exit ramp and Spring Street from State Street to 5th Street to Main Street (Appendix B, page 3).

Since the project is located within two states, the project has different identifiers for each state. Indiana identifies the project using INDOT Designation numbers (Des. Nos.) and Kentucky identifies the project using Item IDs. There are nine INDOT Des. Nos., 1702255, 1592187, 1702260, 1702254, 1702257, 1702258, 1702259, 1701215, and 1900579, associated with this project and covered in this CE documentation. The lead Des. No. is 1702255. The Kentucky Item ID is 5-64 for the Kentucky approaches, also identified

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under INDOT Des. Nos. 1702260 & 1702254. The INDOT Des. Nos. will be used as the main reference throughout this document.

Specific project locations separated by each INDOT Des. No. and Bridge No. are included in Table 1 and are depicted in graphics in Appendix B.

Table 1. Sherman Minton Renewal Project: Project Location and Termini

Des. No.	Bridge No.	Description	Location
1702255 & 1592187	I64-123-04691 D	Sherman Minton Bridge	Construction extends from 0.32 miles south of the Elm Street south on ramp to 0.45 miles from the I-264 south off ramp MOT elements extend from the I-64 / I-264 interchange 3.5 miles to the northwest to the I-64 / I-265 interchange in New Albany Indiana; and EB I-265 exit ramp to SB I-65
1702260 & 1702254 (KY Item ID 5-64)	056B00161N	KY Approaches	Extend 0.45 miles from the I-264 south off ramp to 0.11 miles from the I-264 off ramp
1702257	I64-123-02294 CWBL	I-64 WB over SR 111/Main Street, RR (IN Approach WB)	0.32 miles south of the Elm Street North off Ramp to 0.17 miles south of the Elm Street North off Ramp
1702258	I64-123-02294 CEBL	I-64 EB over SR 111/Main Street (IN Approach EB)	0.19 miles south of the Elm Street South on ramp to 0.23 miles south of the Elm Street South on Ramp
1702259	I64-123-02294 JCEB	I-64 EB over Southern RR (IN Approach EB over RR)	0.26 miles south of the Elm Street South on ramp to 0.32 miles south of the Elm Street South on Ramp
1701215	n/a Broken into 3 roadway segments.	Old SR 62 (Elm Street) from I-64 Exit Ramp to State Street and Spring Street	0.10 mile of Elm Street from the northbound I-64 exit ramp to 170 ft west of State Street; 0.36 mile of Spring Street from West 5th Street to State Street; 0.19 mile of West 5th Street from the southbound I-64 exit ramp to 65 ft north of SR 111/Main Street;
1900579	I64-123-04690 BEBL	I-64 EB over Market Street 0.11 W of SR 111	0.09 miles south of the Elm Street South on Ramp to 0.13 miles south of the Elm Street South on Ramp

Existing Conditions

The Sherman Minton Bridge has been in service since 1962 and is the non-tolled option of the two Interstate crossings of the Ohio River in the Louisville metro area. The land use in the vicinity of the project is primarily metropolitan and industrial. The project area spans over the Ohio River and contains other water resources identified in this document: part III, section A and in Appendix F. EJ communities and historic districts are identified in the surrounding area of the project. A public recreation area and Ohio River Greenway Trail are located adjacent to the project in New Albany, IN. A public park, Shawnee Park, which includes a public golf course Shawnee Golf Course, and a public trail, Louisville Loop, are adjacent to the project in west Louisville, Kentucky. The double-decker Sherman Minton Bridge carries approximately 90,000 vehicles per day as part of interstate (I-64) and daily regional (US 150) travel between the City of New Albany, Indiana and the area of west Louisville in Kentucky. Other Ohio River crossings in the region include the US 31 Clark Memorial Bridge (non-tolled) in downtown Louisville, the I-65 Kennedy and Lincoln Bridges (tolled), and the SR 265 Lewis and Clark Bridge (tolled) east of Louisville.

Because of the age and condition of these structures, the frequency of both planned and unplanned (urgent) repair projects to keep the Sherman Minton Bridge safely in service have been increasing. For example, the 2011 5-month emergency closure of the Sherman Minton Bridge for repair of cracks in structural members, the 2013 bridge deck expansion, joint replacement and steel repairs, the 2017 unplanned urgent repair project to strengthen steel floor system elements due to corrosion and section loss and the 2018 unplanned urgent repair project occurred to repair holes and deterioration identified in the bridge decks. Currently the October 18, 2017 INDOT Bridge Assessment Report, and the November 2019 INDOT Inspection Reports, and the November 2019 KYTC Bridge Inspection Report identify the following in a brief summary:

Des. Nos. 1702255 & 1592187 (Bridge No. I64-123-04691 D; Sherman Minton Bridge)

- Bridge decks have evidence of cracking, spalling, road salt damage, and corrosion.

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- Arch and Truss Members exhibit widespread paint failure throughout the structure, leaving the structure unprotected and susceptible to corrosion.
- Cable Hangers and the connectors exhibit heavy surface corrosion, minor internal corrosion, and displaced wires.
- Floorbeam Frames and Steel Stringers exhibit widespread paint failure, heavy corrosion, and section loss.
- Substructure has evidence of minor cracking and spalling along the piers, and abutments.

Des. Nos. 1702254 & 1702260 (Bridge #056B00161N; KY Approach)

- Bridge Deck and Superstructure elements have evidence of cracking, corrosion and minor amounts of section loss were present along with major paint deterioration.
- Substructure has evidence of cracking, spalling and paint deterioration.

Des. No. 1702257 (Bridge #I64-123-02294 CWBL; I-64 WB over SR 111/Main Street, RR IN Approach WB)

- Bridge Deck and Superstructure elements have evidence of cracking and corrosion.
- Substructure has evidence of spalling.

Des. No. 1702258 (Bridge #I64-123-02294 CEBL; I-64 EB over SR 111/Main Street, RR IN Approach EB)

- Bridge Deck and Superstructure elements have evidence of cracking, corrosion and minor amount of section loss.
- Substructure has minor cracking along the abutments.

Des. No. 1702259 (Bridge #I64-123-02294 JCEB; I-64 EB over SR 111/Main Street., RR IN Approach EB)

- Bridge Deck and Superstructure elements have evidence of cracking, corrosion and minor amounts of section loss.
- Substructure has Minor cracking along the abutments.

Des. No. 1900579 (Bridge #I64-123-04690 BEBL; I-64 EB over Market Street 0.11 W of SR 111)

- Superstructure and substructure exhibit deterioration of paint.

Des. No. 1701215 (Old SR 62 [Elm Street] from I-64 Exit Ramp to State Street)

- Elm, Spring, and 5th Streets have evidence of pavement deterioration and non-compliant ADA standard curb ramps at intersections identified along Elm, Spring, and 5th Streets.

Preferred Alternative

The SMRP is the rehabilitation of the Sherman Minton Bridge and related approaches in Indiana and Kentucky. The goal of SMRP is to address the deteriorating structural condition of the existing bridges to extend the service life of the bridges up to 30 years. Project elements include bridge deck replacements and bridge deck overlays, structural repairs, replacement lighting, bridge painting, local streets Hot Mix Asphalt (HMA) overlay and ADA ramp reconstruction, and interstate ramp frictionalization. SMRP is a joint effort between INDOT and KYTC. INDOT is leading SMRP in close collaboration with key staff from KYTC. In efforts of avoiding repetition in this document the structural repairs, lighting replacements, bridge drain components, and structural elements included on this project are further detailed in the remarks of the Design Criteria Section, page 13 of this document.

Since construction zones and temporary lane/ramp closures are required for construction on the Sherman Minton Bridge structure, additional maintenance work is included as part of SMRP: deck overlay on I-64 westbound and eastbound over SR 111/Main Street and I-64 eastbound over Southern Railroad, painting of the I-64 EB bridge over Market Street, and local street work including HMA overlay, preventative maintenance, and ADA curb ramp reconstruction on Elm Street, Spring Street, and 5th Street near the I-64 ramps in New Albany, IN. The SMRP construction area is from the I-64 exit ramp to State Street in Indiana to the I-64/I-264 interchange in Kentucky, for a total of approximately 1.5 miles. Appendix B, page 3 depicts the geographic location of the various individual elements of SMRP. Appendix B, page 26-110 includes the draft plan sets per structure. Table 2 below provides additional details for the proposed improvements that are anticipated to be completed as part of SMRP.

Table 2. Sherman Minton Renewal Project: Individual Project Elements

Des. No.	Bridge No.	Description	Work Type
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1702255	I64-123-04691 D	Sherman Minton Bridge	Bridge Deck Replacement, Structural Repairs and Substructure Repairs
1592187	I64-123-04691 D	Sherman Minton Bridge	Bridge Painting
1702260 (KY Item ID 5-64)	056B00161N	KY Approach	Bridge Deck Replacement, Substructure Repairs, and Bridge Painting
1702254 (KY Item ID 5-64)	056B00161N	KY Approach	Bridge Deck Replacement, Substructure Repairs, and Bridge Painting
1702257	I64-123-02294 CWBL	I-64 WB over SR 111/Main Street, RR (IN Approach WB)	Bridge Deck Overlay and Bridge Painting
1702258	I64-123-02294 CEBL	I-64 EB over SR 111/Main Street (IN Approach EB)	Bridge Deck Overlay and Bridge Painting
1702259	I64-123-02294 JCEB	I-64 EB over Southern RR (IN Approach EB over RR)	Bridge Deck Overlay and Bridge Painting
1701215	n/a	Old SR 62 (Elm Street) from I-64 Exit Ramp to State Street and Spring Street	HMA Overlay, Preventative Maintenance, and ADA Curb Ramp
1900579	I64-123-04690 BEBL	I-64 EB over Market Street 0.11 W of SR 111	Bridge Painting

The roadway project spans three segments of side streets: Elm Street from I-64 exit ramp to 0.04 miles west of State Street; 5th Street from 0.02 miles north of SR 111 to Spring Street; and Spring Street from W. 5th Street to 0.02 miles West of State Street in the City of New Albany, IN. The HMA resurface includes 1.5-inch mill and HMA overlay of the existing pavement (Appendix B, page 85). In addition to the HMA overlay activities, traffic signal loops will be reestablished at the west approach of Elm Street and Scribner Drive and the east approach of Spring Street and Scribner Drive. A total of eleven (11) intersections will have ADA curb ramp work completed in order to meet ADA-compliant standards. The intersections where ADA curb ramp work will occur are provided in Table 3 below:

Table 3. Sherman Minton Renewal Project: Intersections with ADA Curb Ramp Work		
Intersections:		Quadrants:
1	West 5 th Street & SR 111/Main Street	All four corners
2	West 5 th Street & Market Street	All four corners
3	West 5 th Street & Spring Street	NW, SW and SE corners
4	West 4 th Street & Spring Street	SW and SE corners
5	Washington Place & Spring Street	SW and SE corners
6	Scribner Drive & Spring Street	All four corners
7	West 1 st Street & Spring Street	All four corners
8	State Street & Spring Street	All four corners
9	Scribner Drive & Elm Street	NE and SE corners
10	West 1 st Street & Elm Street	All four corners
11	State Street & Elm Street	All four corners

Six MOT options have been analyzed and considered for this project. The six MOT options are further detailed on page 18 in the MOT section of this document and included as exhibits in Appendix B, pages 10-18. Minimization measures were part of project development, evaluations, and a combination of MOT options to reduced impacts. While the MOT 1 option had the most positive public involvement comments, fewest changes from existing travel patterns and local access, lowest induced traffic diversions and related environmental impacts; project constructability requirements were not entirely met. The Preferred Alternative MOT combined the predominant use of MOT 1, limited off-peak use of MOT-2 (*allowance for about 180 nights per construction year*), and short-term use of MOT-5. Detour routes are listed below and included in Appendix B, pages 19-26.

Indiana

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- I-64 through traffic will be detoured via I-265 to I-65 paired Kennedy and Lincoln bridges through Louisville and the I-265 (SR 265) Lewis and Clark Bridge to the east of Louisville.
- Local access to New Albany IN would follow the same detour route or remain on the local roadway network; State Street will be the detour route during closure of the Spring Street access ramps in New Albany IN.

Kentucky

- Both I-264 and I-64 through traffic will be detoured to either the I-65 paired Kennedy and Lincoln bridges through Louisville or the I-265 (SR 265) Lewis and Clark Bridge to the east of Louisville.
- Local access to west Louisville KY would follow the same detour route or remain on the local arterial network.

The completion of proposed action will also provide long-term benefits with increased certainty and an extended service life for those that rely on the Sherman Minton Bridge in both Indiana and Kentucky. The Sherman Minton Bridge is the most heavily travelled crossing of the Ohio River in Louisville, and as such, many people are affected by increasingly frequent repairs and associated lane closures. A portion of the traffic that uses this bridge is commercial and lane closures can cause delays due to queuing at the bridge or detouring around the bridge. The rehabilitation of the Bridge maximizes use of existing structures, while extending their service life and meets the purpose and need.

No permanent or temporary ROW is anticipated for this project. No impacts to natural resources are anticipated.

Implementation

This project will be implemented as a Design-Build Best Value contract. INDOT and KYTC will select the Design-Build Contractor team based upon evaluation including but not limited to factors such as: contractor approach, cost, time and impacts minimization of environmental and public.

The Design-Build Contractor will be responsible for compliance with all approved NEPA Documents. While no impacts are anticipated, this is dependent on the MOT option and construction methods proposed by the Design-Build Contractor; further resource analysis and permits may be required. All permits, associated documentation, and coordination will be the responsibility of the Design-Build Contractor. Any applicable recommendations made by resource agencies during initial coordination are included in the Environmental Commitments section of this CE document on page 51.

OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

No Build Alternative: The “No-Build” is an avoidance alternative that would have no work on the existing Sherman Minton Bridge and approach structures, no immediate expenditure of federal funds, and would result in no environmental impact. Without rehabilitation, the existing Sherman Minton Bridge and approach structures would continue to deteriorate, require more frequent inspections, maintenance work, additional closures over time to keep the bridge functioning safely, and increase community impact. Eventually, without rehabilitation, it will become too costly to maintain safe travel conditions and result in the permanent closure of the bridge. As the most heavily travelled crossing of the Ohio River in the Louisville area, the increasingly frequent repairs and associated lane closures have negatively affected local traffic patterns and adjacent communities. Permanent closure would disrupt regional traffic patterns for thousands of interstate and local users and affect local EJ populations. This alternative would not meet the purpose and need of the project, therefore; this alternative has been dismissed.

Replacement Alternative: Replacement of the existing double-decker bridge would have new environmental impacts; expanded evaluation, justification, and permitting requirements; require new Right of Way (ROW) and potential relocations; a longer timeframe to address deteriorating conditions, and significantly higher financial costs. Replacement was not carried forward by INDOT or FHWA as an alternative in the project’s planning stage therefore, replacement has not been carried over in the project scope.

The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

- It would not correct existing capacity deficiencies;
 - It would not correct existing safety hazards;
 - It would not correct the existing roadway geometric deficiencies;
 - It would not correct existing deteriorated conditions and maintenance problems; or
 - It would result in serious impacts to the motoring public and general welfare of the economy.
- Other (Describe)

X
X

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ROADWAY CHARACTER:

I-64

Functional Classification: Interstate
 Current ADT: 79,541 VPD (2019) Design Year ADT: 98,025 VPD (2040)
 Design Hour Volume (DHV): 7,402 Truck Percentage (%) 10
 Designed Speed (mph): 55 Legal Speed (mph): 55

Existing		Proposed	
Number of Lanes:	3 NB, 3 SB		3 NB, 3 SB
Type of Lanes:	Travel Lanes		Travel Lanes
Pavement Width:	88	ft.	88
Shoulder Width:	4 min	ft.	4 min
Median Width:	N/A	ft.	N/A
Sidewalk Width:	N/A	ft.	N/A

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway

Spring Street

Functional Classification: Minor Arterial
 Current ADT: 18,817 VPD (2018) Design Year ADT: 23,421 VPD (2040)
 Design Hour Volume (DHV): 1,882 Truck Percentage (%) 2
 Designed Speed (mph): 25 Legal Speed (mph): 25

Existing		Proposed	
Number of Lanes:	5		5
Type of Lanes:	3 travel lanes, 2 parking lanes		3 travel lanes, 2 parking lanes
Pavement Width:	52	ft.	52
Shoulder Width:	N/A	ft.	N/A
Median Width:	N/A	ft.	N/A
Sidewalk Width:	6 min	ft.	6 min

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

Elm Street

Functional Classification: Minor Arterial
 Current ADT: 23,329 VPD (2018) Design Year ADT: 23,347 VPD (2040)
 Design Hour Volume (DHV): 2,332 Truck Percentage (%) 2
 Designed Speed (mph): 25 Legal Speed (mph): 25

Existing		Proposed	
Number of Lanes:	3		3
Type of Lanes:	3 travel lanes		3 travel lanes
Pavement Width:	52	ft.	52
Shoulder Width:	N/A	ft.	N/A
Median Width:	N/A	ft.	N/A
Sidewalk Width:	4 min	ft.	4 min

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

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DESIGN CRITERIA FOR BRIDGES:

Structure/NBI Number(s): 5 Structures, NBI number listed below Sufficiency Rating: 9-1, INDOT Bridge Inspection Report
 (Rating, Source of Information)

(2019 INDOT Bridge Inspection Report)

Bridge (Inspection Date)	Component	Condition Rating								
		Good			Fair		Poor			
		9	8	7	6	5	4	3	2	1
<u>I64-123-04691 D</u> Sherman Minton Bridge (Nov 2019)	Deck					✓				
	Superstructure					✓				
	Substructure				✓					
	Paint						✓			
<u>I64-123-02294 CWBL</u> I-64 WB over SR 111/Main Street, RR IN Approach WB (Nov 2019)	Deck				✓					
	Superstructure					✓				
	Substructure					✓				
	Paint			✓						
<u>I64-123-02294 CEBL</u> I-64 EB over SR 111/Main Street IN Approach EB (Nov 2019)	Deck				✓					
	Superstructure			✓						
	Substructure				✓					
	Paint		✓							
<u>I64-123-02294 JCEB</u> I-64 EB over Southern RR IN Approach EB (Nov 2019)	Deck				✓					
	Superstructure				✓					
	Substructure			✓						
	Paint				✓					
<u>056B00161N</u> KY Approach (Sept 2019)	Deck					✓				
	Superstructure					✓				
	Substructure					✓				
	Paint						✓			
<u>I64-123-04690 BEBL</u> I-64 EB over Market Street (Nov 2019)	Deck			✓						
	Superstructure				✓					
	Substructure			✓						
	Paint					✓				

9-7	Excellent to Good condition with none to some minor problems noted.
6-5	Satisfactory to fair condition, all primary structural elements found but minor deterioration, section loss is present
4-1	Poor to "imminent" failure condition, Advanced to Major deterioration to primary structure elements, Fatigue cracks may be present, or scour could be damaging to support.
0	Failed condition the bridge is out-of-service.

Source: INDOT and KYTC Bridge Inspection Reports

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I64-123-04691 D	Existing	Proposed
Bridge Type:	Steel through arch, Steel through/deck truss	Steel through arch, Steel through/deck truss
Number of Spans:	5 spans	5 spans
Weight Restrictions:	Open (no restrictions)	Open (no restrictions)
Height Restrictions:	17.58 (from SI&A) ft.	17.58 (from SI&A) ft.
Curb to Curb Width:	42 ft.	42 ft.
Outside to Outside Width:	45 ft.	45 ft.
Shoulder Width:	3 (both sides) ft.	3 (both sides) ft.
Length of Channel Work:	N/A	N/A

Remarks: Structure No. I64-123-04691 D
 This is the lead structure number for the Sherman Minton Bridge. The Sherman Minton bridge is a double-deck bridge carrying I-64 traffic over the Ohio River, connecting New Albany, Indiana and Louisville, Kentucky. The proposed work type for this structure is a bridge deck replacement, replacement of lighting components, structural repairs, and cleaning and painting of all steel (Appendix B, page 29).

The bridge deck replacement work includes demolition of the existing bridge decks, cleaning the top flanges of the stringers, installing shear stud connectors, and casting a new reinforced concrete bridge deck with new expansion joints. The proposed bridge decks will feature bridge railings, longitudinal grooving, snow-plowable raised pavement markers, barrier delineators, and new pavement markings. The proposed decks will be surface sealed.

The structural repair work includes repair or replacement of selected structural steel members. Specifically, the following members included in the work are:

- The hanger cable assemblies will be replaced.
- Stringer ends will be repaired.
- Floorbeams and floorbeam frames will be repaired.
- The inspection access system including the catwalk, fall-arrest cables, and associated hardware will be repaired or replaced as needed to restore the safe working capacity of these systems. In addition, the inspection access system will be enhanced through the addition of inspection access ladders from the tops of the piers to the upper chords of the arches, and a safety cable will be added to the top chord of the arches.
- Deteriorated bolts and rivets will be replaced with new bolts throughout the bridge.
- Grinding and bolted repairs will be performed at floorbeam frames to reduce the fatigue sensitivity of certain steel details.

The cleaning and painting work include removal of the existing paint system and application of a new paint system on all steel members of the bridge, above the tops of the substructures. The metal drill shavings at the bottom of the arch ties will be addressed during the cleaning of the steel in these areas.

Substructure repair work will be performed on the pier cap at Pier 6. Epoxy injection grouting repair will be performed at a crack in the pier cap. This work is to be completed under Des. No. 1702255 and 1592187.

Will the structure be rehabilitated or replaced as part of the project?
 Yes
 No
 N/A

I64-123-02294 CWBL	Existing	Proposed
Bridge Type:	Steel multi-girder	Steel multi-girder
Number of Spans:	9 spans	9 spans
Weight Restrictions:	Open (no restrictions)	Open (no restrictions)
Height Restrictions:	99.99 (from SI&A means no restrictions) ft.	99.99 (from SI&A means no restrictions) ft.
Curb to Curb Width:	42 ft.	42 ft.
Outside to Outside Width:	45 ft.	45 ft.
Shoulder Width:	3 (both sides) ft.	3 (both sides) ft.
Length of Channel Work:	N/A	N/A

Remarks: Structure Nos. I64-123-02294 CWBL
 This structure is the I-64 westbound lane (WBL) Indiana approach over SR 111/Main Street connecting to the Sherman Minton Bridge. The proposed work type for this structure is bridge deck overlay, replacement of lighting

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components, structural repairs, and partial bridge cleaning and painting (Appendix B, page 43).

The bridge deck overlay work includes milling the existing bridge deck to remove concrete cover and unsound concrete, deck and barrier concrete repairs, replacement of bridge deck expansion joints, and casting a new latex modified concrete overlay. The proposed work also includes installation of snow-plowable raised pavement markers, barrier delineators, and new pavement markings. The existing reinforced concrete approach slabs will be demolished, and new reinforced concrete approach slabs will be constructed.

The structural repair work includes repair or replacement of selected structural steel members. Specifically, the following members included in the work are:

- The bearings will be replaced.
- Girder ends will be repaired.

The partial cleaning and painting work include local repairs of the existing paint system and painting of areas disturbed by steel repair work. This work is to be completed under Des. No. 1702257.

Will the structure be rehabilitated or replaced as part of the project? **Yes** **No** **N/A**

	Existing	Proposed
Bridge Type:	Steel continuous multi-girder	Steel continuous multi-girder
Number of Spans:	3 spans	3 spans
Weight Restrictions:	Open (no restrictions)	Open (no restrictions)
Height Restrictions:	99.99 (from SI&A means no restrictions) ft.	99.99 (from SI&A means no restrictions) ft.
Curb to Curb Width:	51'-1 3/4" to 58'-7 1/2"	51'-1 3/4" to 58'-7 1/2"
Outside to Outside Width:	54'-1 3/4" to 61'-7 1/2"	54'-1 3/4" to 61'-7 1/2"
Shoulder Width:	Varies (left 3'-6" to 6', right 4'-7 1/2")	Varies (left 3'-6" to 6', right 4'-7 1/2")
Length of Channel Work:	N/A	N/A ft.

Remarks: Structure Nos. I64-123-02294 CEBL
 This structure is the I-64 eastbound lane (EBL) Indiana approach over SR 111/Main Street connecting to the Sherman Minton Bridge. The proposed work type for this structure is deck overlay, structural repairs and painting (Appendix B, page 36).

The bridge deck overlay work includes milling the existing bridge deck to remove concrete cover and unsound concrete, deck and barrier concrete repairs, replacement of bridge deck expansion joints, and casting a new latex modified concrete overlay. The proposed work also includes installation of snow-plowable raised pavement markers, barrier delineators, and new pavement markings. The existing reinforced concrete approach slabs will be demolished, and new reinforced concrete approach slabs will be constructed.

The structural repair work includes repair or replacement of selected structural steel members. Specifically, the following members included in the work are:

- The bearings will be replaced.
- Girder ends will be repaired.

The partial cleaning and painting work include local repairs of the existing paint system and painting of areas disturbed by steel repair work. This work is to be completed under Des. No. 1702258.

Will the structure be rehabilitated or replaced as part of the project? **Yes** **No** **N/A**

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I64-123-04690 BEBL	Existing		Proposed	
Bridge Type:	Steel multi girder		Steel multi girder	
Number of Spans:	3		3	
Weight Restrictions:	Open (no restrictions)		Open (no restrictions)	
Height Restrictions:	99.99 (from SI&A means no restrictions)	ft.	99.99 (from SI&A means no restrictions)	ft.
Curb to Curb Width:	59.0	ft.	59.0	ft.
Outside to Outside Width:	61.6	ft.	61.6	ft.
Shoulder Width:	6	ft.	6	ft.
Length of Channel Work:	N/A		N/A	

Remarks: I64-123-04690 BEBL
 This structure is the I-64 eastbound lane (EBL) over Market Street 0.11 W of SR 111. This structure has a fair condition rating for the deck, superstructure, and substructure. The proposed work type for this structure is bridge painting (Appendix B, page 77). This work is to be completed under Des. No. 1900579.

Yes
 No
 N/A

Will the structure be rehabilitated or replaced as part of the project?

I64-123-02294 JCEB	Existing		Proposed	
Bridge Type:	Steel multi-girder		Steel multi-girder	
Number of Spans:	4 spans		4 spans	
Weight Restrictions:	Open (no restrictions)		Open (no restrictions)	
Height Restrictions:	15.05 (from SI&A)	ft.	15.05 (from SI&A)	ft.
Curb to Curb Width:	42	ft.	42	ft.
Outside to Outside Width:	45	ft.	45	ft.
Shoulder Width:	3 (both sides)	ft.	3 (both sides)	ft.
Length of Channel Work:	N/A		N/A	

Remarks: Structure Nos. I64-123-02294 JCEB
 This structure is the I-64 eastbound lane (EBL) Indiana approach over Southern RR connecting to the Sherman Minton Bridge. The proposed work type for this structure is bridge deck overlay, replacement of lighting components, structural repairs, and partial bridge cleaning and painting (Appendix B, page 81).

The bridge deck overlay work includes milling the existing bridge deck to remove concrete cover and unsound concrete, deck and barrier concrete repairs, replacement of bridge deck expansion joints, and casting a new latex modified concrete overlay. The proposed work also includes installation of snow-plowable raised pavement markers, barrier delineators, and new pavement markings. The existing reinforced concrete approach slabs will be demolished, and new reinforced concrete approach slabs will be constructed.

The structural repair work includes repair or replacement of selected structural steel members. Specifically, the following members included in the work are:

- The bearings will be replaced.
- Girder ends will be repaired.

The partial cleaning and painting work include local repairs of the existing paint system and painting of areas disturbed by steel repair work. This work is to be completed under Des. No. 1702259.

Yes
 No
 N/A

Will the structure be rehabilitated or replaced as part of the project?

056B00161N	Existing		Proposed	
Bridge Type:	Steel multi girder		Steel multi girder	
Number of Spans:	27 spans (14 WB + 13 EB)		27 spans (14 WB + 13 EB)	
Weight Restrictions:	Open (no restrictions)		Open (no restrictions)	
Height Restrictions:	16'-3"		16'-3"	
Curb to Curb Width:	42	ft.	42	ft.

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Outside to Outside Width:	48'-3"	ft.	48'-3"	ft.
Shoulder Width:	3 (both sides)		3 (both sides)	
Length of Channel Work:	N/A		N/A	

Describe bridges and structures; provide specific location information for small structures.

Remarks: Structure No. 056B00161N

This structure is the I-64 eastbound lane (EBL) and westbound lane (WBL) Kentucky approach connecting to the Sherman Minton Bridge. The proposed work type for this structure is a bridge deck replacement and structural repairs (Appendix B, page 52).

The bridge deck replacement work includes demolition of the existing bridge decks, cleaning the top flanges of the girders, installing shear stud connectors, and casting a new reinforced concrete bridge deck with new expansion joints. The proposed bridge decks will feature bridge railings, longitudinal grooving, snow-plowable raised pavement markers, barrier delineators, and new pavement markings. The proposed decks will be surface sealed. The existing reinforced concrete approach slabs will be demolished, and new reinforced concrete approach slabs will be constructed.

The structural repair work includes repair or replacement of selected structural steel members. Specifically, the following members included in the work are:

- Repair of cracks in steel members.
- The lateral restraints will be repaired or replaced.
- Deteriorated bolts and rivets will be replaced with new bolts throughout the bridge.

The cleaning and painting work include removal of the existing paint system and application of a new paint system on all steel members of the bridge, above the tops of the substructures. This work will be completed under Des. Nos. 1702254 and 1702260, and KY Item ID 5-64.

Substructure repair work will be performed on the piers supporting this bridge. Proposed pier cap work includes:

- Concrete cover will be removed, reinforcement will be repaired as needed, deeper concrete repairs will be performed as needed, passive cathodic protection will be installed, and new cover concrete will be cast to restore the original surface of the pier caps. Pier caps beneath expansion joints will be surface sealed.

Columns, from bottom of pier caps down to existing ground line:

- These surfaces will receive substructure concrete repairs as needed. Reinforcement repair will be performed as needed, and passive cathodic protection will be installed in new concrete in the repaired areas.

	Yes	No	N/A
Will the structure be rehabilitated or replaced as part of the project?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

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Remarks: MOT with temporary construction access for work on the bridge and both the Indiana and Kentucky sides of the Ohio River will be required throughout the duration of SMRP. Construction zone speed limit, travel lane, and access reductions and/or closures will reduce traffic on the Sherman Minton Bridge as well as divert traffic along detour routes and the local roadway network. A summary of the full traffic analysis is identified in Appendix I, page 11. Six MOT options were developed and evaluated during SMRP design engineering, traffic modeling, Community Impact Assessment (CIA) and EJ Analysis (in Section G – Community Impacts of this document (page 43) and Appendix I, page 90); MOT graphics are identified in Appendix B, page 10.

MOT 1 – Two lanes open, both decks (existing EB-eastbound and WB-westbound decks)
One (1) EB and one (1) WB lane will be closed for construction. Two (2) EB and two (2) WB travel lanes will remain open but will shift location on the bridge during each of the three (3) construction phases; existing access ramps will remain open.

MOT 2 – One lane open, both decks (EB and WB decks)
Two (2) EB and two (2) WB lanes will be closed for construction. One (1) EB and one (1) WB travel lanes will remain open but will shift location on the bridge during two (2) construction phases; existing access ramps will remain open. A temporary Kentucky crossover lane for to I-64 WB merge with I-264 will be required.

MOT 3 – Alternating three one-way lanes (AM-EB / PM-WB) open on one deck
One (1) bridge deck with three (3) lanes will be closed for construction during two (2) construction phases. The remaining bridge deck will be open with all three (3) travel lanes open in one direction only (EB 1AM to Noon), all travel will be closed during a 1-hour transition before reopening for travel in the opposite direction (WB 1PM to Midnight) Access ramp closures and a Kentucky crossover for I-64 WB merge with I-264 will be required.

MOT 4 – Reversible center lane (AM-EB / PM-WB) and one-way EB/WB lanes open on one deck
One (1) bridge deck with three (3) lanes will be closed for construction during two (2) construction phases. The remaining bridge deck will be open with one (1) EB and one (1) WB travel lanes, and movable barrier system that will change a (1) center lane from one-way EB (Midnight to Noon) to the opposite direction one-way WB (Noon to Midnight) each day. Access ramp closures and a Kentucky crossover for I-64 WB merge with I-264 will be required.

MOT 5 – Full Duration Closure of all six lanes and both decks
Both bridge decks, all six (6) lanes, and associated access ramps will be closed for construction; all traffic will be diverted to detour routes.

MOT 6 – One Direction/Phase three one-way lanes (WB-Phase 1/EB-Phase 2) open on one deck
One (1) bridge deck with three (3) lanes will be closed for construction during two (2) construction phases. During Phase 1, the remaining bridge deck will be open with all three (3) travel lanes open for only EB flow and all WB traffic will be diverted to detour routes. In Phase 2, the remaining bridge deck will be open with all three (3) travel lanes open for only WB flow and all EB traffic will be diverted to detour routes. Access ramp closures would be required.

As detailed in the Community Impact and EJ Analysis (in Section G - Community Impacts, page 43 of this document and Appendix I, page 90), public involvement, community profiles, and EJ population and demographics were evaluated for induced traffic diversions and related travel times, travel costs, congestion, and cross-river connectivity impacts. Quality of Life Factors (Air Quality and Noise Impacts) and Safety Factors (driver expectancy, emergency incident response access, and work zone safety) had similar and/or minimal temporary changes for the overall traveling public and local communities.

Minimization measures were part of project development, evaluations, and a combination of MOT options to reduced impacts. While the MOT 1 option had the most positive public involvement comments, fewest changes from existing travel patterns and local access, lowest induced traffic diversions and related environmental impacts; project constructability requirements were not entirely met. The Preferred Alternative MOT combined the predominant use of MOT 1, limited off-peak use of MOT-2 (*allowance for about 180 nights per construction year*), and short-term use of MOT-5. Detour routes are listed below and included in Appendix B, pages 19-26.

Preferred Alternative MOT –

- One (1) EB and one (1) WB lane will be closed throughout construction.

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- Open travel lanes will shift location on the Sherman Minton Bridge during construction.
- Two (2) EB and two (2) WB travel lanes will remain open for cross-river traffic and existing access ramps will remain open except for the following allowances:
 - 180 nights per construction year during which two (2) EB and two (2) WB lanes and associated access ramps will be closed each night approximately from 9 pm to 4 am and 10 pm to 5 am, respectively. Cross-river traffic will be maintained with one (1) EB and one (1) WB travel lane open and a temporary crossover lane in Kentucky for I-64 WB to merge with I-264.
 - Short-term closure of the Sherman Minton Bridge will be allowed for one (1) nine (9) consecutive day period and up to three (3) weekend closures during each construction year; excluding holidays and community events detailed below. During the short-term bridge closure, all I-64 (US 150) cross-river traffic will be diverted to detour routes.
- Provisions are included for local traffic access and through-traffic dependent businesses by retaining existing access ramps in Indiana and Kentucky; through-traffic dependent businesses by maintaining cross-river travel lanes in both directions; public notification, signage according to MOT, and posting requirements during construction; and detour routes that remain within the interstate system to alternate local river crossings (Appendix B, page 19):

Indiana

- I-64 through traffic will be detoured via I-265 and the I-65 paired Kennedy/Lincoln bridges
- Local access to New Albany would follow the same detour route or remain on the local roadway network; State Street will be the detour route during Spring Street access ramp closures.

Kentucky

- Both I-264 and I-64 through traffic will be detoured via the I-65 paired Kennedy/Lincoln bridges
- Local access to west Louisville would follow the same detour route or remain on the local arterial network

- Minimization measures included additional accommodation for local special events and festivals with the exclusion of bridge closure work during the following:

New Year's Day - If New Year's Day falls on a Sunday, work shall be suspended from noon December 31 until sunrise January 3. or if New Year's Day falls on a Monday through Saturday, work shall be suspended from noon December 31 until sunrise January 2.

Good Friday - Work shall be suspended from noon on Good Friday until sunrise Monday.

Memorial Day - Work shall be suspended from noon the Friday before Memorial Day until sunrise Tuesday, the day after Memorial Day.

Independence Day - If Independence Day falls on a:

Sunday - work shall be suspended from noon Friday, July 2, until sunrise Tuesday, July 6.

Monday - work shall be suspended from noon Friday, July 1, until sunrise Tuesday, July 5.

Tuesday - work shall be suspended from noon Friday, June 30, until sunrise Wednesday, July 5.

Wednesday - work shall be suspended from sunset on Tuesday, July 3, until sunrise Thursday, July 5.

Thursday - work shall be suspended from noon Wednesday, July 3, until sunrise Monday, July 8.

Friday - work shall be suspended from noon Thursday, July 3, until sunrise Monday, July 7.

Saturday - work shall be suspended from noon Thursday, July 2, until sunrise Monday, July 6.

Labor Day - Work shall be suspended from noon the Friday before Labor Day until sunrise Tuesday, the day after Labor Day.

Thanksgiving Day - Work shall be suspended from noon the Wednesday before Thanksgiving Day until sunrise the Monday after Thanksgiving Day.

Christmas Day - Work shall be suspended from noon December 24 until sunrise December 27.

Thunder Over Louisville - Work suspended from Midnight Friday till 6:00 a.m. Sunday.

Kentucky Derby - Work suspended from Thursday at midnight until Monday at 6:00 am.

Harvest Homecoming Festival - First Saturday in October to second Saturday in October.

Based upon early coordination meetings with regulatory agencies and public outreach, at this time, there is no substantial controversy concerning the Preferred Alternative MOT option.

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ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 2,500,000 (2021) Right-of-Way: \$ 0.00 (2021) Construction: \$ 78,114,055 (2021)

Anticipated Start Date of Construction: Spring 2021

Date project incorporated into IN STIP April 15, 2020
 Date project incorporated into KY STIP March 27, 2020

Is the project in an MPO Area? **Yes** **No**

If yes,
 Name of MPO Kentuckiana Regional Planning and Development Agency (KIDPA)

Location of Project in TIP KIPDA 2020-2025 Transportation Improvement Program Page 140

Date of incorporation by reference into the IN STIP April 15, 2020
 Date of incorporation by reference into the KY STIP March 27, 2020

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0	0
Commercial	0	0
Agricultural	0	0
Forest	0	0
Wetlands	0	0
Other:	0	0
Other:	0	0
TOTAL	0	0

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition or reacquisition, either known or suspected, and there impacts on the environmental analysis should be discussed.

Remarks: This project will occur within existing right-of-way (ROW). The existing ROW varies throughout the project. No permanent or temporary ROW will be required for this project. The total work area is depicted in Appendix B, page 6. Work within the ROW limits are broken down in Table 4 as follows:

Des. No.	Bridge No. / Street	Description	Work Type	Limits	Distance beyond Edge of Pavement to ROW (ft)
1702255 & 1592187	I64-123-04691D	Sherman Minton Bridge	Bridge Deck Replacement, Structural Repairs, Substructure Repairs, and Bridge Painting	Sherman Minton Bridge 164-123-04691 D extends from 0.32 miles south of the Elm Street south on Ramp to 0.45 miles from the I-264 south off Ramp	0.0-84.0

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1702260 & 1702254	056B00161N	KY Approaches	Bridge Deck Replacement, Substructure Repairs, and Bridge Painting	extend 0.45 miles from the I-264 south off ramp to 0.11 miles from the I-264 off ramp	0.0
1702257	I64-123-02294 CWBL	I-64 WB over SR 111/Main Street, RR (IN Approach WB)	Bridge Deck Overlay and Bridge Painting	0.32 miles south of the Elm Street North off Ramp to 0.17 miles south of the Elm Street North off Ramp	0.0
1702258	I64-123-02294 CEBL	I-64 EB over SR 111/Main Street (IN Approach EB)	Bridge Deck Overlay and Bridge Painting	0.19 miles south of the Elm Street South on ramp to 0.23 miles south of the Elm Street South on Ramp	
1702259	I64-123-02294 JCEB	I-64 EB over Southern RR (IN Approach EB over RR)	Bridge Deck Overlay and Bridge Painting	0.26 miles south of the Elm Street South on ramp to 0.32 miles south of the Elm Street South on Ramp	
1701215	Elm Street	Old SR 62 (Elm Street) from I-64 Exit Ramp to State Street and Spring Street	HMA Overlay, Preventative Maintenance, and ADA Ramps	Elm Street from I-64 exit Ramp to 0.04 miles west of State Street.	
	5 th Street			5th Street from 0.02 miles north of SR 111 to Spring Street.	0.0
	Spring Street			Spring Street from W. 5th Street to 0.02 miles West of State Street.	3.9-10.4
1900579	I64-123-04690 BEBL	I-64 EB over Market Street 0.11 W of SR 111	Bridge Painting	0.09 miles south of the Elm Street South on Ramp to 0.13 miles south of the Elm Street South on Ramp	0.0
Total:					Range: 0.0-84.0

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

Streams, Rivers, Watercourses & Jurisdictional Ditches
 Federal Wild and Scenic Rivers
 State Natural, Scenic or Recreational Rivers
 Nationwide Rivers Inventory (NRI) listed
 Outstanding Rivers List for Indiana
 Navigable Waterways

	Presence	Impacts	
		Yes	No
	X		X
	X		X

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Remarks:

Based on a desktop review, a site visit on September 18-20, 2018 and May 6-7, 2019 by *Kaskaskia Engineering*, the aerial map of the project area (Appendix B, page 4), and the water resources map in the Red Flag Investigation (RFI) report (Appendix E, page 10) there are three rivers and streams located within the 0.5 mile search radius of the Sherman Minton Bridge. There is one river present within the project area. No Federal, Wild and Scenic Rivers or National Park Service's Nationwide River Inventory (NRI) List waterways are located within the project area.

A *Waters of the U.S. Determination / Wetland Delineation Report* was completed for the project and the INDOT Ecology and Waterway Permitting Office approved the report on September 20, 2019. The *Waters of the U.S. Determination / Wetland Delineation Report's* investigated area covers both Indiana and Kentucky and is larger than the SMRP area limits. Please refer to Appendix F, page 3 for the *Waters of the U.S. Determination / Wetland Delineation Report*. The entire project area, both in Indiana and Kentucky, falls under the jurisdiction of the Louisville U.S. Army Corps of Engineers (USACE). It was determined that six (6) Waters of the U.S. (federally regulated) streams are located within the project area. No isolated (state regulated) streams are located within the project area. One (1) river, the Ohio River, borders both Indiana and Kentucky. The remaining five (5) streams are located within Indiana. The USACE makes all final determinations regarding jurisdiction.

Both Indiana & Kentucky

Ohio River: Borders Indiana and Kentucky as depicted in Appendix F, page 52, An estimated 176 LF of the Ohio River is within the project area. The Ohio River is a perennial river. The Ordinary High-Water Mark (OHWM) is approximately 1,700 feet wide and the depth is unknown. The Louisville USACE classifies the Ohio River as a navigable Waters of the U.S. and a Section 10 waterway.

The Ohio River is listed as an Indiana Department of Environmental Management (IDEM) 303d impaired waters for dioxin, E. coli, Total Mercury in water, polychlorinated biphenyls (PCBs) in water and PCBs in fish tissue. The Ohio River is also listed as a Kentucky Energy and Environment Cabinet (EEC) 303d impaired waters for dioxin, E. coli, and PCBs. Workers who are working in or near water with E. coli should take care to wear appropriate (Personal Protective Equipment) PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure. If there will be sediment and/or soils disturbed by construction, additional investigation may be necessary. Coordination with INDOT ES and KYTC will occur. These recommendations are included as firm commitments in the Environmental Commitments section of this document.

No work is anticipated within the Ohio River or along the Indiana or Kentucky banks of the Ohio River. Therefore, no impacts to the Ohio River are anticipated. If the Design-Build Contractor requires work within the Ohio River, the Design-Build Contractor will be responsible for assessing impacts, applying for permits, any associated documentation, and coordination with the necessary agencies. It has not been decided if a causeway or barge will be used, however; if the Design-Build Contractor requires the need, the Contractor will be responsible for assessing impacts, applying for permits, any associated documentation, coordination with necessary agencies and following the environmental commitments set forth by the agencies.

Indiana

No State Natural, Scenic, Recreational Rivers and Outstanding Rivers for Indiana waterways are present in the project area. Five (5) jurisdictional streams were identified on the Indiana side of the project area as per the Waters of the U.S. Report.

Valley View Creek: an estimated 1,664 linear feet (LF) of Valley View Creek is within the investigated area. Located north of the I-64/Spring Street intersection and depicted on graphic in Appendix F, page 50. Valley View Creek would likely be classified as perennial because it has in-channel structure (riffles and pools) and base flow. The substrate in the stream as dominated by gravel and cobble. An ordinary high water mark (OHWM) was observed that was 18 feet wide and 15 inches deep. No impact is anticipated.

Falling Run: an estimated 1,075 LF of Falling Run is within the investigated area. Falling Run flows from northeast to southwest under I-64 along the north side of the I-64 and Spring Street interchange as depicted in Appendix F, page 50. It would likely be classified as perennial because it has in-channel structure and base flow. An OHWM was observed that was 34 feet wide and 15 inches deep. No impact is anticipated.

Unnamed Tributary (UNT) 1 to Falling Run: an estimated 969 LF of UNT 1 to Falling Run is within the investigated area. UNT 1 to Falling Run flows northwest along the east side of the I-64 Spring Street interchange as depicted in Appendix F, page 50 & 51. It would likely be classified as intermittent. Water sits within the channel but does not appear to have base flow. An OHWM was observed that was 12 feet wide and 24 inches deep. No impact is anticipated.

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UNT 2 to Falling Run: an estimated 689 LF of UNT 2 to Falling Run is within the investigated area. UNT 2 to Falling Run flows east to west from within the I-64 and Spring Street interchange and through Wetlands 6 and 5. Location is depicted in Appendix F, page 50. It would likely be classified as intermittent. An OHWM was observed that was 3 feet wide and 8 inches deep. No impact is anticipated.

UNT to Ohio River: an estimated 206 LF of UNT to the Ohio River is within the investigated area. UNT to Ohio River is a short channel that drains from a culvert under the railroad tracks along the Ohio River as depicted in Appendix F, page 52. An OHWM was observed that was 3.5 feet wide and 10 inches deep. No impact is anticipated.

No work is anticipated within any waterway. Therefore, no impacts are anticipated. If the Design-Build Contractor requires work within any waterway, the Design-Build Contractor will be responsible for assessing impacts, applying for permits, any associated documentation, and coordination with the necessary agencies. It has not been decided yet if a causeway or barge will be used, however; if the Design-Build Contractor requires the need, the Design-Build Contractor will be responsible for assessing impacts, applying for permits, any associated documentation, coordination with necessary agencies and following the environmental commitments set forth by the agencies.

Kentucky
 No State Natural, Scenic, Recreational Rivers and Outstanding Rivers for Kentucky are present in the project area. No waterways, besides the Ohio River, is located within the project area on the Kentucky side as per the Waters of the U.S. Report. The Ohio River description is listed above. No impacts are anticipated.

KYTC Division of Environmental Analysis inserted a special note in accordance to Section 7 of the Endangered Species Act, for construction activities on March 6, 2020, stating the following, "No impacts due to construction, including pieces falling from construction will occur to the Ohio River. Should impacts be unavoidable the contractor will be responsible for coordination to obtain clearance for section." This is included as a firm commitment.

Early Coordination for both Indiana and Kentucky
 Early coordination letters were sent on December 11, 2018. U.S. Environmental Protection Agency (USEPA) responded on February 7, 2019 with comments and recommendations (Appendix C, page 4). EPA recommends the environmental review identify and assess potential impacts to water resources, identify best management practices (BMPs) and mitigation measures that will be used to prevent/reduce potential impacts associated with renewal activities. United States Army Corps of Engineers (USACE) responded January 14, 2019, and May 6, 2020, with comments (Appendix C, page 13). USACE comments avoidance, minimization, or potential mitigation will be required to minimize adverse impact to aquatic resources. USACE recommends the submittal of a 33 CFR 322 Department of the Army (DA) permit application in the case an individual permit is to be required. United States Fish and Wildlife Service (USFWS) responded February 20, 2019 (Appendix C, page 23), with a list of recommendations in the case a causeway or barge must be used during construction. USFWS also recommends restricting work below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slops around the bridge abutments and placement of riprap. United States Coast Guard (USCG) District 8 responded December 18, 2018. USCG stated a Coast Guard Bridge Permit will not be required (Appendix C, page 78). Indiana Department of Environmental Management (IDEM) did not formally respond, however; a standard automated response (Appendix C, page 80) was generated March 19, 2019. A Response received from Indiana Department of Natural Resources (IDNR) on March 11, 2019, recommending as list of measures to be implemented to avoid, minimize, or compensate for impacts should they occur to fish wildlife and botanical resources (Appendix C, page 17). No response received from Kentucky Division of Water (KDOW). All applicable USEPA, USACE, USFWS, and IDNR recommendations are included in the Environmental Commitments section of this CE document.

Other Surface Waters	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Reservoirs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Lakes	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Detention Basins	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Storm Water Management Facilities	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other: <u>Golf Course Pond</u>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Remarks: Based on a desktop review, a site visit on September 18-20, 2018 and May 6-7, 2019 by Kaskaskia Engineering, the aerial map of the project area (Appendix B, page 4), and the water resources map in the RFI report (Appendix E, page 10) there are three (3) lakes located within the 0.5 mile search radius of the Sherman Minton Bridge. There is one (1) other surface waters (referred to as open water resources in the *Waters of the U.S. Determination/Wetland Delineation Report*) located within the project area.

A *Waters of the U.S. Determination / Wetland Delineation Report* was completed for the project area, including both Indiana and Kentucky, and the INDOT Ecology and Waterway Permitting Office approved the report on September 20, 2019. Please refer to Appendix F, page 3 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that one (1) jurisdictional open water resource is located within the project area. This one (1) open water resource, identified as OW 1, is located within Kentucky. No open water resources and other surface waters are located within the project area on the Indiana side. The USACE makes all final determinations regarding jurisdiction.

Indiana
Based on review of the water resource map in the RFI report (Appendix E, page 10), there are three (3) lakes located within the 0.5 mile search radius of the Sherman Minton Bridge on the Indiana side. There are no lakes or other surface waters present within or adjacent to the project area on the Indiana side. Therefore, no impacts are anticipated.

Kentucky
One (1) open water was identified on the Kentucky side of the project area in the *Waters of the U.S. Determination/Wetland Delineation Report* (Appendix F, page 3).

Open Water (OW) 1: OW 1 is a man-made golf course pond located within the Shawnee Golf Course. The quality of the pond was considered poor due to surrounding non-native vegetation and due to its primary source of hydrology via runoff from the golf course as well as the nearby I-64 roadway slope. This feature does not drain to or from a likely water of the U.S. and is not adjacent to a likely water of the U.S. However, since it is within the floodplain of the Ohio River, approximately 980 feet from the OHWM of the Ohio River, this feature is likely a jurisdictional water feature. OW 1 is not classified as an NWI wetland. No vegetation exists within the pond.

No work is anticipated within the open water pond and therefore no impact is anticipated. The Design-Build Contractor is to remain within the existing roadway ROW. If the Design-Build Contractor deviates from the existing roadway ROW, the Contractor will be responsible for assessing impacts, applying for the necessary permits, associated documentation, and any mitigation.

Early Coordination for both Indiana and Kentucky
Early coordination letters were sent on December 11, 2018. USEPA responded on February 7, 2019 with no comments and recommendations pertaining to other surface waters (Appendix C, page 4). USACE responded January 14, 2019 and May 6, 2020, with no comments or recommendations pertaining to other surface waters. (Appendix C, page 13). USFWS responded February 20, 2019 (Appendix C, page 23), no comments or recommendations pertaining to other surface waters. USCG District 8 responded December 18, 2018 (Appendix C, page 78). IDEM did not formally respond, however; a standard automated response (Appendix C, page 80) was generated March 19, 2019. A response was received from IDNR March 11, 2019, with no comments or recommendations pertaining to other surface waters, (Appendix C, Page 17). No response was received from KDOW.

	<u>Presence</u>	<u>Impacts</u>	
	<input checked="" type="checkbox"/>	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>

Wetlands

Total wetland area: 6.31 acre(s) Total wetland area impacted: 0.0 acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments
Wetland 5	Palustrine Forested	3.52	0.00	Located in IN
Wetland 6	Palustrine Forested	0.06	0.00	Located in IN
Wetland 7	Palustrine Scrub-Shrub	0.27	0.00	Located in IN

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Wetland 8	Palustrine Forested	0.32	0.00	Located in IN
Wetland 9	Palustrine Emergent	0.31	0.00	Located in IN
Wetland 10	Palustrine Forested	0.56	0.00	Located in KY
Wetland 11	Palustrine Emergent	0.60	0.00	Located in KY
Wetland 12	Palustrine Scrub-Shrub	0.15	0.00	Located in KY
Wetland 13	Palustrine Forested	0.52	0.00	Located in KY

Documentation

ES Approval Dates

Wetlands (Mark all that apply)

- Wetland Determination
- Wetland Delineation
- USACE Isolated Waters Determination
- Mitigation Plan

X
X

September 20, 2019
September 20, 2019

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.

Measures to avoid, minimize, and mitigate wetland impacts need to be discussed in the remarks box.

Remarks: Based on a desktop review of the National Wetlands Inventory (NWI) online mapper (<https://www.fws.gov/wetlands/data/Mapper.html>), a site visit on September 18-20, 2018 and May 6-7, 2019 by Kaskaskia Engineering, the USGS topographic (Appendix E, page 8), and the RFI report (Appendix E, page 10) there are four (4) wetlands located within the 0.5 mile search radius of the Sherman Minton Bridge. There are three (3) wetlands present adjacent to the project area.

A *Waters of the U.S. Determination / Wetland Delineation Report* was completed for the project area, including both Indiana and Kentucky, and the INDOT Ecology and Waterway Permitting Office approved the report on September 20, 2019. Please refer to Appendix F, page 3 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that nine (9) jurisdictional wetlands are located within the project area, five (5) wetlands are located within Indiana and four (4) wetlands are located within Kentucky. The USACE makes all final determinations regarding jurisdiction.

Indiana
Based on a desktop review of the RFI report (Appendix E, page 10), four (4) wetlands are located within the 0.5 mile search radius. The *Waters of the U.S. Determination / Wetland Delineation Report* identified five (5) wetlands within the project area on the Indiana side.

Wetland 5: as depicted in Appendix F, Page 50. Wetland 5 is an approximately 3.52 acre palustrine forested wetland located west of the I-64 eastbound exit ramp to Spring Street, within the Midwest Region. A portion of the western section of Wetland 5 is classified as an NWI wetland (PFO1A). Wetland 5 is within the floodplain of Falling Run. No impact is anticipated.

Wetland 6: as depicted in Appendix F, Page 50. Wetland 6 is an approximately 0.06 acre palustrine forested wetland located in a drainage depression adjacent to the I-64 EB exit ramp to Spring Street, within the Midwest Region. Wetland 6 is not classified as an NWI wetland. No impact is anticipated.

Wetland 7: as depicted in Appendix F, Page 50. Wetland is an approximately 0.27 acre palustrine shrub-scrub wetland located east of I-64, near Anderson Park, within the Midwest Region. Wetland 7 is not classified as an NWI wetland. Wetland 7 is within the floodplain of Falling Run. No impact is anticipated.

Wetland 8: as depicted in Appendix F, Page 50 & 51. Wetland 8 is an approximately 0.32 acre palustrine forested

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wetland located in the NE quadrant of the I-64/Spring Street interchange, near Billy Herman Fields, within the Midwest Region. Wetland 8 is not classified as an NWI wetland. Wetland 8 is within the floodplain of Falling Run. Due to its hydrological connection to UNT 1 to Falling Run, this feature is likely a jurisdictional wetland. No impact is anticipated.

Wetland 9: as depicted in Appendix F, Page 50& 51. Wetland 9 is an approximately 0.31 acre palustrine emergent wetland located between the WB I-64 entrance and exit ramps at I-64/Spring Street interchange, within the Midwest Region. Wetland 9 is not classified as an NWI wetland. No impact is anticipated.

No impacts to any wetlands in Indiana are anticipated, as the Design-Build Contractor is to remain within the existing roadway ROW, where no wetlands were identified. If the Design-Build Contractor deviates from the existing roadway ROW, the Design-Build Contractor will be responsible for assessing impacts, applying for the necessary permits, associated documentation, and any mitigation.

Kentucky:
The *Waters of the U.S. Determination / Wetland Delineation Report* identified four (4) wetlands within the project area on the Kentucky side.

Wetland 10: as depicted in Appendix F, Page 53. Wetland 10 is an approximately 0.56 acre palustrine forested wetland located adjacent to the Kentucky east bank of the Ohio River, within the Eastern Mountains and Piedmont Region. A portion of Wetland 10 is classified as an NWI wetland (PFO1Ah). It is within the floodplain of the Ohio River. No impact is anticipated.

Wetland 11: as depicted in Appendix F, Page 54. Wetland 11 is an approximately 0.6 acre palustrine emergent wetland under the WB lanes of the Sherman Minton Bridge and extending along the north side EB I-64 within the Eastern Mountains and Piedmont region. Wetland 11 is not classified as an NWI wetland. It is within the floodplain of the Ohio River. No impact is anticipated.

Wetland 12: as depicted in Appendix F, Page 54. Wetland 12 is an approximately 0.15 acre shrub-scrub wetland north of the I-64 WB I-264 interchange, at the edge of a golf course. Wetland 12 is not classified as an NWI wetland. It is within the floodplain of the Ohio River. No impact is anticipated.

Wetland 13: as depicted in Appendix F, Page 54. Wetland 13 is an approximately 0.52 acre palustrine forested wetland north of the I-64 WB I-264 interchange, at the edge of a golf course within the Eastern Mountains and Piedmont region. Wetland 13 is not classified as an NWI wetland. It is within the floodplain of the Ohio River. No impact is anticipated.

No impacts to any wetlands in Kentucky are anticipated, as the Design-Build Contractor is to remain within the existing roadway ROW, where no wetlands were identified. If the Design-Build Contractor deviates from the existing roadway ROW, the Design-Build Contractor will be responsible for assessing impacts, applying for the necessary permits, associated documentation, and any mitigation.

Early Coordination for both Indiana and Kentucky
Early coordination letters were sent on December 11, 2018. USEPA responded on February 7, 2019 with no comments and recommendations pertaining to other wetlands (Appendix C, page 4). USACE responded January 14, 2019 and May 6, 2020, with no comments or recommendations pertaining to other surface waters. (Appendix C, page 13). USFWS responded February 20, 2019 (Appendix C, page 23), no comments or recommendations pertaining wetlands. USCG District 8 responded December 18, 2018 (Appendix C, page 78). No response was received from KDOW. IDEM did not formally respond, however; a standard automated response (Appendix C, page 80) was generated March 19, 2019. A response was received from IDNR March 11, 2019, with comments or recommendations pertaining to wetlands, (Appendix C, Page 17).

	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Terrestrial Habitat	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Unique or High Quality Habitat	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Use the remarks box to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).

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Remarks:

Indiana
 Based on a desktop review, a site visit on July 18, 2019 by Michael Baker International, and the aerial map of the project area (Appendix B, page 4), there is terrestrial habitat present on the Indiana side of the project. There are trees located along the bank of the Ohio River underneath the Sherman Minton Bridge along with maintained lawns.

An early coordination IDNR response letter dated March 11, 2019 stated according to the Natural Heritage Database the following have been documented within 0.5 mile of the project area (on the Indiana side): pretty dodder (*Cuscuta indecora*) an Indiana state endangered plant, long beak arrowhead (*Sagittaria asustralis*) an Indiana state rare plant, and the purple passion-flower (*Passiflora incarnata*) an Indiana state watchlist plant. The Division of Natural Preserves does not foresee any impacts to the plant species (Appendix C, Page 17).

No work will take place outside of the existing ROW and no tree clearing is anticipated. Any terrestrial habitats present on then Indiana side of the project area face no foreseeable impacts to habitat or plant species. If the Design-Build Contractor requires work outside of the existing ROW, the Design-Build Contractor will be responsible for assessing impacts, coordination with the necessary agencies, and preparing any necessary documentation.

Kentucky
 Based on a desktop review, a site visit on July 18, 2019 by Michael Baker International, and the aerial map of the project area (Appendix B, page 4), there is terrestrial habitat present on the Kentucky side of the project. A riparian forest corridor is located along the Ohio River underneath the Sherman Minton Bridge. There is also a public city park, Shawnee Park, which includes the public Shawnee Golf Course, that contain stands of trees and maintained lawns. The riparian forest is dominated by silver maple (*Acer saccharinum*). Other trees identified within the project area and within the Shawnee Golf Course include green ash (*Fraxinus pennsylvanica*) and eastern cottonwood (*Populus deltoides*).

The USFWS List of threatened and endangered species identified that the endangered running buffalo clover (*Trifolium stoloniferum*) has the potential to be within the project area (Appendix C, page 59). The USFWS Kentucky Ecological Services Field Office has running buffalo clover habitat assessment guidance (Appendix C, page 73). No critical habitat of the species has been identified within the project area. KTYC reviewed the project area and USFWS coordination and has agreed no impacts to terrestrial habitats are anticipated (Appendix C, Page 73)

The Design-Build Contractor is to remain within the existing ROW and no tree clearing is anticipated. Therefore, no impacts are expected. If the Design-Build Contractor deviates from the existing ROW, the Design-Build Contractor will be responsible for identifying and conducting any necessary habitat assessments, coordination with the necessary agencies, and preparing any other required documentation. The guidelines for a habitat assessment are identified in (Appendix C, page 70).

Early Coordination for both Indiana and Kentucky
 Early coordination letters were sent on December 11, 2018. USEPA responded on February 7, 2019 with no comments and recommendations pertaining to terrestrial habitat (Appendix C, page 4). USACE responded January 14, 2019 and May 6, 2020, with no comments pertaining to terrestrial habitat (Appendix C, page 13). USFWS responded February 20, 2019 (Appendix C, page 23), with recommendations including do not clear trees or understory vegetation outside of the construction zone boundaries. USCG District 8 responded December 18, 2018 with no comments pertaining to terrestrial habitat (Appendix C, page 78). IDEM did not formally respond, however; a standard automated response (Appendix C, page 80) was generated March 19, 2019. A response from IDNR received March 11, 2019, included recommendations to avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible (Appendix C, page 17). No response was received KDOW. All applicable USFWS and IDNR recommendations are included in the Environmental Commitments section of this document.

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

Karst

	Yes	No
Is the proposed project located within or adjacent to the potential Karst Area of Indiana?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Are karst features located within or adjacent to the footprint of the proposed project?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is the proposed project located within or adjacent to the potential Karst Area of Kentucky?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Are karst features located within or adjacent to the footprint of the proposed project?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If yes, will the project impact any of these karst features?	<input type="checkbox"/>	<input type="checkbox"/>

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Use the remarks box to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

Remarks: **Indiana**
 Based on a desktop review, the project is located outside the designated karst region of Indiana as outlined in the October 13, 1993 Memorandum of Understanding (MOU). According to the topo map of the project area (Appendix B, page 3) and the RFI report (Appendix E, page 1), there are no karst features identified within or adjacent to the project area. In the early coordination response, the Indiana Geological Survey (IGS) did not indicate that karst features exist in the project area (Appendix C, page 85). IGS identified geological hazards consisting of floodway and high liquefaction potential. IGS also identifies high potential of sand and gravel resources in the area. Response from IGS will be communicated with the Design-Build Contractor through a commitment, included in the Environmental Commitments section of this document. No impacts are expected.

Kentucky
 Based on a desktop review, the project is located outside a designated karst region of Kentucky according to Kentucky Geological Survey (KGS) viewer map; <https://kgs.uky.edu/kgsmmap/kgsgeoserver/viewer.asp>. According to KGS, no karst features are identified within or adjacent to the project area. The project area consists of Glacial Outwash with primary lithology of sand, gravel, silt and clay. No Impacts are expected.

Threatened or Endangered Species	Presence	Impacts	
		Yes	No
Within the known range of any federal species	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Any critical habitat identified within project area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Federal species found in project area (based upon informal consultation)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
State species found in project area (based upon consultation with IDNR)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Is Section 7 formal consultation required for this action?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	
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Remarks: **Indiana**
 Based on a desktop review and the RFI report (Appendix E, page 5), completed by Parsons on March 28, 2019, the IDNR Floyd County Endangered, Threatened and Rare (ETR) Species List has been checked and is included in (Appendix E, page 14). The highlighted species on the list reflect the federal and state identified ETR species located within the county. An early coordination letter response was received from IDNR Division of Fish and Wildlife (DFW) on March 11, 2019. According to the Natural Heritage Database, the Kirtland's Snake (*Clonophis kirtlandii*) an Indiana state endangered species, has been documented within a half mile of the project area. The Division of Natural Preserves does not foresee any impacts to the Kirtland's Snake as a result of this project. (Appendix C, Page 17). If the Design-Build Contractor proposes to do any work outside of the existing ROW within Indiana, coordination with the IDNR-DFW shall occur.

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, page 43). The Project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). Other species were found within or adjacent to the project area along with the Indiana and northern long-eared bat.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on August 28, 2019, and based on the responses provided, the project was found to have "no effect" to the Indiana bat and/or the NLEB (Appendix C, page 30).

The official species list generated from IPaC on August 28, 2019, indicated two (2) other species present, one (1) mammal and one (1) clam, within the project area of Indiana. No critical habitat was identified. The species include the following:

Mammal identified:

- Gray Bat (*Myotis grisescens*) is an endangered species. The Gray Bat was identified in the official species list and though the USFWS response on February 20, 2019 (Appendix C, page 23). The Gray Bat is a southern species

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which inhabits caves year-round and typically migrates between winter hibernation caves and summer cave roots used for reproduction and foraging. Preferred foraging habitat is typically along wooded stream corridors and their forage base often includes a high percentage of aquatic insects. There is only one significant summer maternity colony known in Indiana, in southern Clark County. Previous studies through USFWS have shown that Silver Creek and Muddy Fork are the main foraging habitat for this colony. Based on the project description and location, USFWS does not anticipate impacts to gray bats or their habitat. The official's species list indicates no critical habitat has been designated for this species.

Clam identified:

- Sheepsnose Mussel (*Plethobasus cyphus*) also known as is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 60. USFWS response on February 20, 2019 (Appendix C, page 23) identifies the Sheepsnose mussel as a medium-sized mussel that grows to about 5 inches in length. It lives in larger rivers and streams where it is usually found in hallow areas with moderate to swift flowing currents flowing over coarse sand and gravel. USFWS considers the sheepsnose mussel extant in the Ohio River in Floyd County.

No further coordination with Indiana USFWS is needed at this time. No work is anticipated to take place outside of existing roadway ROW or within any waterway. Therefore, no impacts are anticipated.

Kentucky:

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, page 50). The Project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). Other species were found within or adjacent to the project area along with the Indiana and northern long-eared bat.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on August 28, 2019, and based on the responses provided, the project was found to have "no effect" to the Indiana bat and/or the NLEB (Appendix C, page 30).

The official Species list generated from IPaC on August 28, 2019, indicated thirteen (13) other species present within the project area of Kentucky. The species include the following:

Mammal identified:

- Gray Bat (*Myotis grisescens*) is an endangered species. The Gray Bat was identified in the official species list and though the USFWS response on February 20, 2019 (Appendix C, page 23). The Gray Bat is a southern species which inhabits caves year-round and typically migrates between winter hibernation caves and summer cave roots used for reproduction and foraging. Preferred foraging habitat is typically along wooded stream corridors and their forage base often includes a high percentage of aquatic insects. There is only one significant summer maternity colony known in Indiana, in southern Clark County. Previous studies through USFWS have shown that Silver Creek and Muddy Fork are the main foraging habitat for this colony. Based on the project description and location, USFWS does not anticipate impacts to gray bats or their habitat. The official's species list indicates no critical habitat has been designated for this species.

Bird identified:

- Least Tern (*Sterna antillarum*) is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area.

Clams identified:

- Clubshell (*Pleurobema clava*) is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Fanshell (*Cyprogenia stegaria*) is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Northern Riffleshell (*Epioblasma torulosa rangiana*) is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Orangefoot Pimpleback (*Plethobasus cooperianus*), also known as is an endangered species considered to exist in

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the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.

- Purple Cat's Paw (*Epioblasma obliquata obliquata*), also known as is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Rabbitsfoot (*Quadrula cylindrica cylindrica*), also known as is a threatened species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Ring Pink mussel (*Obovaria retusa*) also known as is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Rough Pigtoe (*Pleurobema plenum*) also known as is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Sheepnose Mussel (*Plethobasus cyphus*) also known as is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.
- Spectaclecase mussel (*Cumberlandia monodonta*) also known as is an endangered species considered to exist in the geographical area of the project. No critical habitat has been designated for this species in the project area. General project design guidelines are identified on Appendix C, page 58.

On March 6, 2020 KYTC Division of Environmental Analysis concurred with USFWS Consistency Letter "No effect" finding for the Indiana bat, NLEB and other species listed, (Appendix C, page 30). KYTC states the finding is contingent upon no effects to the Ohio River, (Appendix C, page 77). In follow up a special note for construction activities was placed and will be found in the Environmental Commitments section of this CE document.

No early coordination letter response was received from Kentucky Department of Natural Resources (KDNR). If the Design-Build Contractor proposes to do any work outside of the existing ROW within Kentucky, coordination with the KDNR shall occur.

No further coordination with Kentucky USFWS is needed at this time. No work is anticipated to take place outside of existing roadway ROW or within any waterway. Therefore, impacts are not anticipated.

Both Indiana and Kentucky

It has not been decided yet if a causeway or barge will be used, however; if the Design-Build Contractor requires the need, coordination will be required. A USFWS letter dated February 20, 2019, stated, recommend that remediation/rehabilitation work be performed from the bridge or via barges where possible and the use of causeways and other in-stream construction be avoided. Causeways can cause substantial changes in flow patterns and restrict fish passage at lower flows; they can also adversely affect mussels within and near the causeway construction area. During low-flow conditions excessive channel blockage can create stagnant water upstream of the causeway and dissolved oxygen deficits downstream in lateral portions of the channel. If a causeway or barge must be used, then recommendations should be followed. These recommendations can be found in the Environmental Commitments section of this CE document.

This precludes the need for further consultation as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, if the project plans are changed, or if the Contractor goes outside of the existing roadway ROW as previously mentioned, USFWS will be contacted for consultation.

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SECTION B – OTHER RESOURCES

Drinking Water Resources	Presence	Impacts	
		Yes	No
Wellhead Protection Area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Public Water System(s)	X	<input type="checkbox"/>	X
Residential Well(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Source Water Protection Area(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Sole Source Aquifer (SSA)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

If a SSA is present, answer the following:

	Yes	No
Is the Project in the St. Joseph Aquifer System?	<input type="checkbox"/>	<input type="checkbox"/>
Is the FHWA/EPA SSA MOU Applicable?	<input type="checkbox"/>	<input type="checkbox"/>
Initial Groundwater Assessment Required?	<input type="checkbox"/>	<input type="checkbox"/>
Detailed Groundwater Assessment Required?	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

Sole Source Aquifer
 The project is located in Floyd County, Indiana and in Jefferson County, Kentucky. No legally designated sole source aquifers in Indiana and Kentucky are within or near the study area. Therefore, the FHWA/USEPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project. Therefore, a detailed groundwater assessment is not needed, and no impacts are expected.

Wellhead Protection Area and/or Source Water Area
 The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on March 12, 2019 by Michael Baker International. This project is not located within a Wellhead Protection Area or Source Water Area. The IDEM automated response on March 12, 2019, doesn't not identify if the project is or is not located within a wellhead area (Appendix C, page 80). No impacts are expected.

According to KDOW Water Maps Porta <https://watermaps.ky.gov/#SWP>, accessed on March 15, 2019, no Source Water Protection Areas, Wellhead Protection Areas, Permitted Water Withdrawals, and Public Water Systems via Water Wells and/or Springs are located within or near the SMRP area.

Water Wells
 The IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on October 18, 2019 by Michael Baker International. No wells are located near this project, the closest location is approximately 0.52 mile north of the project. Therefore, no impacts are expected.

The KDOW Record Database website (<https://watermaps.ky.gov/>) was accessed on October 18, 2019, by KYTC. No wells are located near this project, Louisville Water Company has a wellfield, but it's over a mile upstream of the Sherman Minton. Therefore, no impacts are expected.

Urban Area Boundary (UAB)
 Based on a desktop review of the INDOT MS4 website (<https://entapps.indot.in.gov/MS4/>) by Michael Baker International on March 12, 2019, and the RFI report, this project is located in an UAB location in Indiana. An early coordination letter was mailed on March 29, 2019, to the New Albany MS4 coordinator and no response was received within the 30-day time frame. Communication with the City of New Albany, Floyd County officials has been ongoing since September 10, 2018.

KYTC is involved pursuant to 40 CFR 122.32(a), which requires state transportation departments to participate in the MS4 program. KYTC is required to report to KDOW regarding the KYTC MS4 program. Project managers are to coordinate the development of highway plans with local government requirements. Communications with Louisville officials, Jefferson County officials, and KDOW has been on-going since September 10, 2018.

Public Water System
 Based on a desktop review, a site visit on March 12, 2019 by Michael Baker International. and the aerial map of the

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project area (Appendix B, page 4), this project is located in New Albany, IN, where there is a public water system (<https://cityofnewalbany.com/residents/municipal-utilities/>). The public water system will not be affected by this project due to location and no significant excavation. Therefore, no impacts are expected. Coordination with the City of New Albany, IN, has been ongoing since September 10, 2018.

Based on a desktop review, a site visit on March 12, 2019, Michael Baker International. the aerial map of the project area (Appendix B, page 4), this project is located in Louisville, KY, where there is a public water system (<https://www.louisvillewater.com/customer-service/>). The public water system will not be affected by this project due to location and no significant excavation. Therefore, no impacts are expected. Coordination with the City of Louisville, KY, has been ongoing since September 10, 2018.

	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Flood Plains			
Longitudinal Encroachment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transverse Encroachment	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Project located within a regulated floodplain	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Homes located in floodplain within 1000' up/downstream from project	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks: **Indiana**
 Based on a desktop review of The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) by Michael Baker International. on March 12, 2019, and the RFI report; this project is located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix E, page 10). Coordination with the floodplain administrator for this project has occurred. The floodplain administrator for this project is a member of the CAC for this project and has attended CAC meetings.

This project qualifies as a Category 1 per the current INDOT CE Manual, which states "Although this project involves work within the horizontal limits of the 100-year floodplain, no work is being performed below the 100-year flood elevation and as a result this project does not encroach upon the base floodplain."

Kentucky
 The project is located in a regulatory floodplain, (<https://watermaps.ky.gov/RiskPortal/>). Coordination with KDOW has occurred and is ongoing. At the agency coordination field meeting held on July 18, 2019, KDOW indicated a construction in floodway permit will likely be required for this project. The Permit Section of this document is on page 50.

	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Farmland			
Agricultural Lands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006* _____
**If 160 or greater, see CE Manual for guidance.*

See CE Manual for guidance to determine which NRCS form is appropriate for your project.

Remarks: Based on a desktop review, a site visit on July 18, 2019 by Michael Baker International, the aerial map of the project area (Appendix B, page 4), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area in Indiana or Kentucky. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on December 11, 2018 to Natural Resources Conservation Services (NRCS). NRCS responded on December 17, 2018, with confirmation the project will not cause a conversion of prime farmland. (Appendix C, page 79).

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SECTION C – CULTURAL RESOURCES

Minor Projects PA Clearance	Category A-1 B-1 B-12	INDOT Approval Dates 1/10/2020 5/7/2020	N/A
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IN Eligible and/or Listed Resource Present KY Eligible and/or Listed Resource Present

Results of Research

Archaeology	
NRHP Buildings/Site(s)	X
NRHP District(s)	X
NRHP Bridge(s)	

IN Project Effect

No Historic Properties Affected No Adverse Effect Adverse Effect

KY Project Effect

No Historic Properties Affected No Adverse Effect Adverse Effect

IN Prepared Documentation KY Prepared Documentation

Documentation (mark all that apply)

	ES/FHWA Approval Date(s)	SHPO Approval Date(s)
Historic Properties Short Report		
Historic Property Report		
Archaeological Records Check/ Review		
Archaeological Phase Ia Survey Report	X	KY-SHPO March 27, 2020
Archaeological Phase Ic Survey Report		
Archaeological Phase II Investigation Report		
Archaeological Phase III Data Recovery		
APE, Eligibility and Effect Determination		
800.11 Documentation		

MOA Signature Dates (List all signatories)

Memorandum of Agreement (MOA)

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

Remarks:

Indiana
On January 10, 2020 and May 7, 2020, the INDOT Cultural Resource Office (CRO) determined that this project falls within the guidelines of Category A, Type 1, Category B, Type 1, and Category B, Type 12 under the MPPA (Appendix D, page 8). Below is a description of each category types as defined in the MPPA:

Category A, Type 1 includes:
Any work on bridges limited to substructure or both superstructure elements without replacing widening or elevating the

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superstructure under the conditions listed below. Conditions A and B must be met. This category does not include bridge replacement projects (when both super structures are removed):

- A. The project takes place in previously disturbed soils and
- B. With regard to the bridges, at least one of the conditions (i, ii or iii) listed below must be satisfied;
 - i. The bridge is not identified in the latest Historic Bridge Inventory as a National Register-listed or National Register-eligible (see <http://www.in.gov/indot/2531.htm>); This does not apply for the project.
 - ii. The bridge was built after 1945, and is a common type as defined in Section V. of the *Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges* issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment - - remains in effect AND the considerations listed in Section IV of the Program Comment do not apply; This does not apply for the project.
 - iii. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect. This applies for the project.

Category B, Type 1 includes:

Replacement, repair, or installation of curbs, curb ramps, or sidewalks, including when such projects are associated with roadway work such as surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking, under the following conditions [BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]:

Condition A (Archaeological Resources) One of the two conditions listed below must be satisfied (EITHER Condition i or Condition ii must be satisfied):

- i. Work occurs in previously disturbed soils; OR
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the Division of Historic Preservation and Archaeology (DHPA) and any archaeological site form information will be entered directly into the State Historic Architectural and Archaeological Database (SHAARD) by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources) One of the two conditions listed below must be satisfied (EITHER Condition i or Condition ii must be satisfied):

- i. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; OR
- ii. Work occurs adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource under one of the two additional conditions listed below (EITHER Condition a OR Condition b must be met and field work and documentation must be completed as described below):
 - a. No unusual features, including but not limited to historic brick or stone sidewalks, curbs or curb ramps, stepped or elevated sidewalks and historic brick or stone retaining walls are present in the project area adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; OR
 - b. Unusual features, including but not limited to historic brick or stone sidewalks, curbs or curb ramps, stepped or elevated sidewalks and historic brick or stone retaining walls are present in the project area adjacent to or within a National Register-listed or National Register-eligible individual above-ground resource or district and ANY ONE of the conditions (1, 2, or 3) listed below must be fulfilled:
 - 1. Unusual features described above will not be impacted by the project. Firm commitments regarding the avoidance of these features must be listed in the MPPA determination form and the NEPA document and must be entered into the INDOT Project Commitments Database. These projects will also be flagged for quality assurance reviews by INDOT Cultural Resources Office during/after project construction. Revised Appendices A and B February 13, 2019 Page 6 of 13
 - 2. Unusual features described above have been determined not to contribute to the significance of the historic resource by INDOT Cultural Resources Office in consultation with the SHPO based on an analysis and justification prepared by their staff or review of such information from other qualified professional historians.

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3. Impacts to unusual features described above have been determined by INDOT Cultural Resources Office to be so minimal that they do not diminish any of the characteristics that contribute to the significance of the historic resource, based on an analysis and justification prepared by their staff or review of such information from other qualified professional historians.

Category B, Type 12 includes:

Replacement, widening, or raising the elevation of the superstructure on existing bridges, and bridge replacement projects (when both the superstructure and substructure are removed), under the following conditions [BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]:

Condition A (Archaeological Resources) One of the two conditions listed below must be met (EITHER Condition i or Condition ii must be satisfied):

- i. Work occurs in previously disturbed soils; This applies to the project.

Condition B (Above-Ground Resources) The conditions listed below must be met (BOTH Condition i and Condition ii must be satisfied) Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; AND

- ii. With regard to the subject bridge, at least one of the conditions listed below is satisfied (AT LEAST one of the conditions a, b or c, must be fulfilled):
 - a. The latest Historic Bridge Inventory did not identify the bridge as a National Register-listed or National Register-eligible (see <http://www.in.gov/indot/2531.htm>);
 - b. The bridge was built after 1945, and is a common type as defined in Section V. of the Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post- 1945 Concrete and Steel Bridges issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment remains in effect AND the considerations listed in Section IV of the Program Comment do not apply;
 - c. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect. This applies for the project.

As part of the MPPA determination, a review was conducted of the National Register of Historic Places and the Indiana Register of Indiana Historic Sites and Structures (IHSSI). The National Register & IHSSI information is available in the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries (IHBBC) map.

There are no recorded SHAARD archaeological sites in or adjacent to the proposed project area. No National Register-listed districts or individual resources were identified within or adjacent to the project area in Indiana. One (1) IHSSI district, New Albany Downtown Historic District, was identified adjacent to the project area. An ADA curb ramp modification will take place adjacent to the historic district, at the southwest corner of West Spring Street and West 1st Street. One (1) IHSSI individual property, Reyse-Friend House located at 229 West Spring Street (IHSSI 043-446-34204, rated Outstanding), was also identified within the project area. An ADA curb ramp modification will take place adjacent to the Reyse-Friend House, at the southwest corner of West Spring Street and Washington Place. This ADA curb ramp work is covered under the Category B, Type 1 (Appendix D, page 12).

The land surrounding the project area is densely urban/industrial/residential. Structures within or adjacent to the project area range in age from mid-to-late nineteenth to early twenty-first century. The assigned INDOT CRO historian performed a desktop street view survey of the project area. None of the structures appear to possess the significance and integrity necessary to be considered eligible for the National Register, and no previously un-surveyed potentially eligible structures are located within or adjacent to the project area.

The subject structure (Bridge No. I64-123-04691D/NBI No. 034520) is a steel continuous thru-arch bridge constructed in 1961 and reconstructed in 1997. The bridge has three (3) approach spans and a concrete cast-in-place deck. The bridge was not surveyed for or included in the 1976/2006 *Floyd County Interim Report*. As a border bridge, the structure was not included in the 2010 INDOT-sponsored Historic Bridge Inventory (HBI). As part of the Interstate system (per MPPA, Category B-12 (ii) (b)), "...the bridge...was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for as long as that Exemption remains in effect..."

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Table 5 below shows each individual project element and which part of the MPPA it falls under.

Table 5. Sherman Minton Renewal Project: MPPA				
Des. No.	Bridge No. / Street	Description	Work Type	MPPA
1702255	I64-123-04691 D	Sherman Minton Bridge	Bridge Deck Replacement, Structural Repairs, and Substructure Repairs	B-12 Ai
1592187	I64-123-04691 D	Sherman Minton Bridge	Bridge Painting	A-1 iii
1702257	I64-123-02294 CWBL	I-64 WB over SR 111/Main Street, RR (IN Approach WB)	Bridge Deck Overlay and Bridge Painting	A-1 iii
1702258	I64-123-02294 CEBL	I-64 EB over SR 111/Main Street (IN Approach EB)	Bridge Deck Overlay and Bridge Painting	A-1 iii
1702259	I64-123-02294 JCEB	I-64 EB over Southern RR (IN Approach EB)	Bridge Deck Overlay and Bridge Painting	A-1 iii
1701215	Elm Street 5 th Street Spring Street	Old SR 62 (Elm Street) from I-64 Exit Ramp to State Street and Spring Street	HMA Overlay, Preventative Maintenance, and ADA Curb Ramp	B-1
1900579	I64-123-04690 BEBL	I-64 EB over Market Street 0.11 W of SR 111.	Bridge Painting	A-1 iii

The MPPA concluded that no above-ground concerns exist as long as the project scope does not change. It was also concluded there are no archaeological concerns because the proposed project is limited to minor rehabilitation work and painting of an existing interstate bridge, in previously disturbed soils. No further consultation is required. If the Design-Build Contractor proposes work outside of the existing ROW, coordination with INDOT Cultural Resources will be required. This completes the Section 106 process for the Indiana side of the project and the responsibilities of the FHWA under Section 106 have been fulfilled.

Kentucky

There is a National Register historic district, Portland Historic District, and historic property, Shawnee Park, located within the Kentucky Area of Potential Effect (APE). Kentucky Historic Preservation Office (KY SHPO) responded to early coordination (via a telephone conversation) on December 20, 2018 stating the office finds the half mile APE to be appropriate for the above-ground resources (Appendix D, page 18). Through coordination with KYTC it was determined that a Phase I Archaeological Survey would be required for approximately 2.5 acres beneath the Kentucky approach to the Sherman Minton Bridge within the Shawnee Golf Course (located within the National Register listed Shawnee Park).

A Phase I Archeology Survey was completed and summarized in a letter dated March 26, 2020 by Wood Environment & Infrastructure Solutions, Inc. The survey included background research which revealed that no archaeological sites have been recorded within or directly adjacent to the APE. The field survey, conducted on March 23, and 24, 2020, consisted of a total of 28 Shovel test probe (STP)s and six bucket augers excavated within the APE. Two newly recorded archeological sites, FS-1 and FS-2, were identified during the survey. No features or significant artifact concentrations were identified at either site. Of the 28 STPs, there was one positive STP identified at Site FS-1; however, it was concluded that sites FS-1 and FS-2 are recommended as not eligible for the National Register Historic Property (NRHP) and the survey concluded that no further archaeological investigations within the project APE are recommended (Appendix D, page 30).

KYTC coordinated with KY-SHPO on March 27, 2020, requesting concurrence for the finding of No Historic Properties Affected for archaeological resources. KY-SHPO responded on March 27, 2020 concurring with the finding, conditional upon stipulations listed below (Appendix D, page 36):

- 1) OSA site numbers will be requested for FS-1 and FS-2 by Wood;
- 2) Three bound copies of an acceptable Phase I report be submitted to our office for review and comment no later than June 30, 2020; and

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3) Should there be any additions or modifications to the APE, this office will be consulted, and additional archaeological survey may be required.

Additionally, KYTC coordinated with KY-SHPO on March 26, 2020, stating it is the determination of KYTC on behalf of FHWA that the Sherman Minton Rehabilitation project as proposed will have a No Adverse Effect on the Shawnee Golf Course, Shawnee Park, the Northwestern Parkway, and the Clark Memorial Bridge (where diverted traffic may go). The Kentucky bridge approach spans above the golf course and staging areas in or adjacent to the park will be subject to additional review once they are identified. The project as proposed does not have the potential to adversely affect any other historic sites within the APE.

KY-SHPO provided a No Adverse Effect concurrence finding on March 30, 2020 contingent on the following (Appendix D, page 35):

- 1) Since this project is a design-build contract, once an alternative has been chosen KYTC shall provide SHPO with the chosen alternative and final effects and final effects recommendation.
- 2) Once the alternative and APE has been chosen, KYTC shall identify and reach out to the appropriate consulting parties based upon the chosen APE.

KY-SHPO provided a No Historic Properties Affected finding on July 27, 2020, upon additional review of the Phase 1 Archaeological Survey (Appendix D, page 41).

All applicable KY-SHPO commitments are included in the Environmental Commitments section of this CE document. This completes the Section 106 process for the Kentucky side of the project and the responsibilities of the FHWA under Section 106 have been fulfilled.

The park (including the golf course) and trail are not anticipated to be impacted by the project. The project will not use this resource by taking permanent ROW and will not alter the environment in such a way as to constitute constructive use of this resource. Therefore, no use or permanent impact is expected. If the Design-Build Contractor determines that they want temporary use of the trail or park during construction, it is the Design-Build Contractor's responsibility to coordinate with KYTC, FHWA, the City of Louisville, and the Louisville Parks and Recreation. Additional archaeology, wetland, or other analysis may be required.

SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement (mark all that apply)

Parks & Other Recreational Land

- Publicly owned park
- Publicly owned recreation area
- Other (school, state/national forest, bikeway, etc.)

Presence

X
X
X

Use

Yes	No
	X
	X
	X

Evaluations

Prepared

- Programmatic Section 4(f)*
- "De minimis" Impact*
- Individual Section 4(f)

FHWA

Approval date

--

Wildlife & Waterfowl Refuges

- National Wildlife Refuge
- National Natural Landmark
- State Wildlife Area
- State Nature Preserve

Presence

Use

Yes	No

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**Evaluations
Prepared**

Programmatic Section 4(f)*
"De minimis" Impact*
Individual Section 4(f)

**FHWA
Approval date**

Historic Properties

Sites eligible and/or listed on the NRHP

Presence

X

Use

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Evaluations
Prepared**

Programmatic Section 4(f)*
"De minimis" Impact*
Individual Section 4(f)

**FHWA
Approval date**

**FHWA approval of the environmental document also serves as approval of any Section 4f Programmatic and/or De minimis evaluation(s) discussed below.*

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the remarks box below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, "de minimis" and Individual Section 4(f) evaluations please refer to the "Procedural Manual for the Preparation of Environmental Studies". Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks:

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, a site visit on July 18, 2019 by Michael Baker International, the aerial map of the project area (Appendix B, page 4), and the RFI report (Appendix E, page 10) there are four (4) Section 4(f) resources located within the project area. There are two (2) located in Indiana and two (2) located in Kentucky.

Indiana

There are two (2) Section 4(f) resources located within or adjacent to the project area on the Indiana side (Appendix B, page 27). One (1) resource is a public recreation area that includes public facilities, such as the New Albany Riverfront Amphitheater and overlooks, located east of the Sherman Minton Bridge. These public facilities are owned by the City of New Albany. One (1) is a public trail, the Ohio River Greenway Trail, that runs under the Sherman Minton Bridge and is operated by the Ohio River Greenway Commission.

Coordination with the City of New Albany and members of the Ohio River Greenway Commission has occurred through CAC meetings to provide project updates and address concerns on keeping facilities and trails open for the public. Coordination with the City of New Albany and the Ohio River Greenway Commission is to be maintained by the Design-Build Contractor on project updates to ensure the safety of trail users.

The public facility and trail are not anticipated to be impacted by the project. The project will not use this resource by taking permanent ROW and will not alter the environment in such a way as to constitute constructive use of this resource. Therefore, no use or permanent impact is expected. If the Design-Build Contractor determines that they want temporary use of the trail during construction, it is the Design-Build Contractor's responsibility to coordinate with INDOT, FHWA, the City New Albany, and the Ohio River Greenway Commission. Additional archaeology, wetland, or other analysis may be required.

Kentucky

There are two (2) Section 4(f) resources located within or adjacent to the project area on the Kentucky side (Appendix B, page 27). One (1) resource is the public Shawnee Park, which includes the public Shawnee Golf Course, located underneath the Sherman Minton Bridge approaches. The park and golf course are owned by the City of Louisville and operated by the Louisville Parks and Recreation. One (1) resource is the Louisville Loop (also referred to as Louisville Riverwalk) trail that runs underneath the Sherman Minton Bridge approaches. The Louisville Loop trail is maintained by

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the Louisville Parks and Recreation.

Coordination with the City of Louisville and Louisville Parks and Recreation has occurred through CAC and teleconference meetings to provide project updates and address concerns on keeping the trails open for the public. Coordination with the Louisville Parks and Recreation is to be maintained by the Design-Build Contractor with project updates to ensure the safety of trail users.

The park (including the golf course) and trail are not anticipated to be impacted by the project. The project will not use this resource by taking permanent ROW and will not alter the environment in such a way as to constitute constructive use of this resource. Therefore, no use or permanent impact is expected. If the Design-Build Contractor determines that they want temporary use of the trail or park during construction, it is the Design-Build Contractor's responsibility to coordinate with KYTC, FHWA, the City of Louisville, and the Louisville Parks and Recreation. Additional archaeology, wetland, or other analysis may be required.

Section 6(f) Involvement

Presence

Use

Section 6(f) Property

Yes

No

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks:

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

Indiana
A review of 6(f) properties on the Land and Water Conservation Fund (LWCF) website at <https://www.lwcfcoalition.com/tools> revealed a total of three (3) properties in Floyd County, Indiana (Appendix H, page 21). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources in Indiana as a result of this project.

Kentucky
A review of 6(f) properties on the Land and Water Conservation Fund (LWCF) website at <https://www.lwcfcoalition.com/tools> revealed a total of 84 properties in Jefferson County, Kentucky (Appendix H, page 22). Two (2) properties are listed within the project area in Kentucky, the Shawnee Park Pavilion and the Shawnee Golf Course Maintenance Building. These properties are owned by the City of Louisville and operated by the Louisville Parks and Recreation.

The Shawnee Park and the Shawnee Golf Course are not anticipated to be impacted by the project. The project will not use this resource by taking permanent ROW and will not alter the environment in such a way as to constitute constructive use of this resource. Therefore, no use or permanent impact is expected. If the Design-Build Contractor determines that they want temporary use of the park or golf course during construction, it is the Design-Build Contractor's responsibility to coordinate with KYTC, FHWA, the City of Louisville, and the Louisville Parks and Recreation. Additional archaeology, wetland, or other analysis may be required.

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SECTION E – Air Quality

Air Quality

Conformity Status of the Project

	Yes	No
Is the project in an air quality non-attainment or maintenance area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If YES, then:		
Is the project in the most current MPO TIP?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Is the project exempt from conformity?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If the project is NOT exempt from conformity, then:		
Is the project in the Transportation Plan (TP)?	<input type="checkbox"/>	<input type="checkbox"/>
Is a hot spot analysis required (CO/PM)?	<input type="checkbox"/>	<input type="checkbox"/>
Level of MSAT Analysis required?		

Level 1a Level 1b Level 2 Level 3 Level 4 Level 5

Remarks:

Indiana
 The FY 2020-2024 IN STIP is listed based on the lead Des. No. in the contract. The lead Des. No. for this contract is 1702255. The FY 2020-2024 STIP includes Des. No. 1702255 by reference with the contract number 4071 (Appendix H, page 16).

This project is located in Floyd County, Indiana, which is currently a “Marginal” nonattainment area for the 2015 8-Hour Ozone Standard (0.070 ppm), according to <https://www.in.gov/idem/airquality/2339.htm>. This project has been identified as being exempt from air quality analysis in accordance with 40 CFR Part 93.126 and this project is not a project of air quality concern (40 CFR Part 93.123). Therefore, the project will have no significant impact on air quality in Indiana.

Kentucky
 The FY 2019-2022 KY STIP is listed based on Item ID. The Item ID for this contract is 5-64 (Appendix H, page 17).

This project is also located in Jefferson County, Kentucky which is currently a nonattainment area for the 2015 8-Hour Ozone Standard (0.070 ppm), according to https://www3.epa.gov/airquality/greenbook/anayo_ky.html. This project has been identified as being exempt from air quality analysis in accordance with 40 CFR Part 93.126 and this project is not a project of air quality concern (40 CFR Part 93.123). Therefore, the project will have no significant impact on air quality in Kentucky.

Both Indiana and Kentucky
 An air quality summary memo has been prepared by Michael Baker International as an initial response to comments raised in USEPA’s early coordination response letter dated February 7, 2019, identified in Appendix C, page 4. Air quality concerns specifically mentioned in the letter include the potential for project level air quality conformity analysis, air quality considerations during the NEPA process and air quality concerns during construction. The air quality summary memo is identified in Appendix H, page 19.

MPO Process
 The Kentuckiana Regional Planning and Development Agency (KIPDA) serves as the Metropolitan Planning Organization (MPO) for the Louisville/Jefferson County KY-IN urbanized area. As the MPO, KIPDA is responsible for the Metropolitan Transportation Plan (MTP) that includes all federally funded surface transportation projects within the MPO Metropolitan Planning Area (MPA). The updated MTP as of July 2020, Connecting Kentuckiana 2040 (<https://www.kipda.org/transportation/core-products/metropolitan-transportation-plan/>), provides a vision of the regional transportation network and the projects that are being considered for funding to fulfill the plans goals and objectives. KIPDA also manages the four-year Transportation Improvement Program (TIP), the fiscal programming document of the MTP that assigns funding to regional projects (Appendix H, page 1).

Air Quality Status and Conformity
 Recently, most of the KIPDA region (Clark and Floyd counties in Indiana and Bullitt, Jefferson, and Oldham counties in Kentucky) was designated as being in nonattainment for the 2015 8-hour Ozone National Ambient Air Quality Standards (NAAQS). This area, which is known as the Louisville KY-IN 2015 8-hour Ozone nonattainment area, is

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subject to the requirements of conformity, and KIPDA is the agency responsible for fulfilling the federal air quality conformity requirements associated with the MTP. In order to support a conformity determination for an MTP update or amendment, KIPDA must prepare a conformity analysis to show that expected emissions of Ozone precursors are less than the limits (budgets) established for the area when it became attainment of the 1997 8-hour Ozone standard (there are currently no budgets for the 2015 8-hour Ozone standard, therefore the 1997 budgets are the only applicable budgets). The use of the 1997 budgets also supports a conformity determination for the 1997 8-hour Ozone Standard (which is necessary per February 16, 2018 D.C. Circuit Court decision in South Coast Air Quality Management District versus USEPA). The region meets the NAAQS for all other regulated pollutants.

SMRP is located in the Louisville, KY-IN nonattainment area, therefore, a transportation project-level conformity determination under the Clean Air Act (CAA) is required. The SMRP is included in the region's long-range transportation plan, MTP Connecting Kentuckiana 2040. SMRP is primarily a rehabilitation (reconstruction) project. It is classified as exempt (as per 40 CFR 93.126), and it does not need to be explicitly modeled as part of the region's air quality conformity analysis. Additionally, the SMRP is included in the FY 2020 – 2025 Transportation Improvement Program (TIP) adopted February 27, 2020 as well as in subsequent addendums.

In summary, the SMRP is included in a current and conforming transportation plan and TIP. Further, because the SMRP is not within a CO or Particulate Matter (PM) maintenance or nonattainment area, a hotspot analysis is also not required. Thus, the transportation conformity requirements under the CAA have been met.

Requirements Under NEPA

Air quality considerations under NEPA must also be considered. SMRP is not within a Carbon Monoxide (CO) maintenance area and is classified as an exempt project pursuant to Section 93.126, because it is a safety or maintenance project that will correct, improve, or eliminate a hazardous location or feature. As such it can be confidently asserted that the project in and of itself will not result in a new exceedance of the CO National Ambient Air Quality Standards (NAAQS).

The purpose of this project is to address structural deficiencies and safety/geometric issues without adding additional capacity. This project has been determined to generate minimal air quality impacts for Clean Air Act criteria pollutants and has not been linked with any special mobile source air toxic (MSAT) concerns. As such, this project will not result in changes in traffic volumes, vehicle mix, basic project location, or any other factor that would cause a meaningful increase in MSAT impacts of the project from that of the no-build alternative.

Moreover, USEPA regulations for vehicle engines and fuels will cause overall MSAT emissions to decline significantly over the next several decades. Based on regulations now in effect, an analysis of national trends with USEPA's MOVES2014 model forecasts a combined reduction of over 90 percent in the total annual emissions rate for the priority MSAT from 2010 to 2050 while vehicle-miles of travel are projected to increase by over 45 percent (Updated Interim Guidance on Mobile Source Air Toxic Analysis in NEPA Documents, Federal Highway Administration, October 12, 2016). This will both reduce the background level of MSAT as well as the possibility of even minor MSAT emissions from this project.

Construction Emissions

Emissions may be produced in the construction of this project from heavy equipment and vehicle travel to and from the site, as well as from fugitive sources. Construction emissions are short term or temporary in nature. Based on available information, temporary construction activities are not expected to generate high enough emissions during the anticipated construction duration to result in a new exceedance of the NAAQS for these pollutants.

SECTION F - NOISE

Noise
Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy? Yes No

	No	Yes/ Date
ES Review of Noise Analysis	X	

Remarks: **Indiana**
This project is a Type III project. In accordance with 23 CFR 772 and the current Indiana Department of Transportation Traffic Noise Analysis Procedure, this action does not require a formal noise analysis.

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Kentucky
This project is not considered a Type I project; therefore, no further noise considerations are required.

SECTION G – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

- Will the proposed action comply with the local/regional development patterns for the area?
- Will the proposed action result in substantial impacts to community cohesion?
- Will the proposed action result in substantial impacts to local tax base or property values?
- Will construction activities impact community events (festivals, fairs, etc.)?
- Does the community have an approved transition plan?
If No, are steps being made to advance the community's transition plan?
- Does the project comply with the transition plan? (explain in the remarks box)

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Remarks:

A Community Impact Assessment (CIA) was conducted to the SMRP as part of the CIA and EJ Analysis Technical Report. An executive summary of the CIA report is included in Appendix I. Based upon community profiles developed for the Project Study Area and surrounding jurisdictions, those on the Indiana side of the Ohio River are more likely to experience greater temporary impacts:

Indiana – Floyd and Clark counties, cities of New Albany and Jeffersonville, and Town of Clarksville

- o 40% and 60% of those residing in Floyd and Clark counties in the Project Study Area commute out of state to Kentucky, with an even larger percentage of residents in New Albany and Clarksville doing so (up to 80%).
- o Indiana residents have a greater reliance on the Sherman Minton Bridge (also referred to SMB throughout the CIA and EJ Analysis Technical Report), with a 2.5:1 ratio of Indiana to Kentucky residents crossing the Ohio River for jobs.
- o Silver Creek limits the New Albany network to three east-west crossings (I-265, Blackiston Mill Road, and Spring Street/Providence Way and Spring Street/Brown Station Way). This is likely to increase potential congestion on local routes as travelers find alternate crossings into Louisville.

Kentucky – Jefferson County and the City of Louisville

- o About 20% of Kentucky residents commute to Indiana.
- o I-64 provides a high-capacity Interstate connection directly along the river between the Sherman Minton Bridge to the west and the downtown bridges to the east.
- o West Louisville has a classic arterial grid roadway network that allows for efficient travel options between these bridges. This network currently has excess capacity that could accommodate a temporary increase in traffic volumes.

The proposed SMRP action complies with local/regional development patterns for the area as demonstrated by:

- Inclusion of the SMRP in the Kentuckiana Regional Planning & Development Agency (KIPDA) MPO 2020-2025 Transportation Improvement Program (TIP),
- Participation of local/regional planning stakeholders during project development (Part I – Public Involvement), and,
- Incorporation of KIPDA planning, demographic, travel demand model (TDM), and EJ resources data.

Minimization measures were part of project development, evaluations, and a combination of MOT options to reduced impacts. While the MOT 1 option had the most positive public involvement comments, fewest changes from existing travel patterns and local access, lowest induced traffic diversions and related environmental impacts; project constructability requirements were not entirely met. The Preferred Alternative MOT combined the predominant use of MOT 1, limited off-peak use of MOT-2 (*allowance for about 180 nights per construction year*), and short-term use of MOT-5. Detour routes are listed below and included in Appendix B, pages 19-26.

Based upon public involvement input, retaining cross-river travel lanes on the Sherman Minton Bridge and local access, and the MOT with the lowest induced traffic diversions (with related travel times, travel costs, and congestion) and disruptions to community access, and mobility, the SMRP will not result in substantial impacts to community cohesion.

Since the SMRP consists of the rehabilitation of existing infrastructure elements within existing ROW, does not increase capacity or alter travel patterns upon completion, and community impacts are limited to temporary traffic disruptions

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during construction, the proposed action will not result in substantial impacts to the local tax based or property values.

Minimization measures included additional accommodation for local special events and festivals with the exclusion of bridge closure work during the following:

New Year's Day - If New Year's Day falls on a Sunday, work shall be suspended from noon December 31 until sunrise January 3. or if New Year's Day falls on a Monday through Saturday, work shall be suspended from noon December 31 until sunrise January 2.

Good Friday - Work shall be suspended from noon on Good Friday until sunrise Monday.

Memorial Day - Work shall be suspended from noon the Friday before Memorial Day until sunrise Tuesday, the day after Memorial Day.

Independence Day - If Independence Day falls on a:

Sunday - work shall be suspended from noon Friday, July 2, until sunrise Tuesday, July 6.

Monday - work shall be suspended from noon Friday, July 1, until sunrise Tuesday, July 5.

Tuesday - work shall be suspended from noon Friday, June 30, until sunrise Wednesday, July 5.

Wednesday - work shall be suspended from sunset on Tuesday, July 3, until sunrise Thursday, July 5.

Thursday - work shall be suspended from noon Wednesday, July 3, until sunrise Monday, July 8.

Friday - work shall be suspended from noon Thursday, July 3, until sunrise Monday, July 7.

Saturday - work shall be suspended from noon Thursday, July 2, until sunrise Monday, July 6.

Labor Day - Work shall be suspended from noon the Friday before Labor Day until sunrise Tuesday, the day after Labor Day.

Thanksgiving Day - Work shall be suspended from noon the Wednesday before Thanksgiving Day until sunrise the Monday after Thanksgiving Day.

Christmas Day - Work shall be suspended from noon December 24 until sunrise December 27.

Thunder Over Louisville - Work suspended from Midnight Friday till 6:00 a.m. Sunday.

Kentucky Derby - Work suspended from Thursday at midnight until Monday at 6:00 am.

Harvest Homecoming Festival - First Saturday in October to second Saturday in October.

The SMRP complies with the local/regional development MPO and transition plan and community impacts are limited to temporary traffic disruptions during construction.

Based on the CIA evaluations, there will be temporary MOT related impacts for both interstate and local travel to community events within and adjacent to the Project Study Area; however, none of these impacts are unique to a particular community or event. The completion of SMRP will provide long-term benefits with increased certainty and an extended service life for the Sherman Minton Bridge and approaches to these same communities and events in both Indiana and Kentucky.

Indirect and Cumulative Impacts

Will the proposed action result in substantial indirect or cumulative impacts?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

Indirect impacts are effects which are caused by the action and are later in time or farther removed in distance but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency or person undertakes such actions

Indirect Impacts - there may be indirect impacts beyond the SMRP study area from induced traffic diversions to other Ohio River crossings and increased interstate, regional, and public transportation network travel times, travel costs, and congestion. However, since the project impacts are temporary and will not change traffic patterns, access and mobility following construction completion, potential impacts to regional growth, patterns of land use, population density, or the growth rate are minimal.

Cumulative Impacts - since SMRP impacts are temporary and will not change traffic patterns, access and mobility following construction completion, the potential cumulative impacts to the environment from incremental impact of the project when added to other past, present, and reasonably foreseeable future actions are minimal.

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Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities? Discuss how the maintenance of traffic will affect public facilities and services.

Yes No

Remarks:

Indiana
Based on a desktop review, a site visit on July 18, 2019 by Michael Baker International, the aerial map of the project area (Appendix B, page 4), and the RFI and limited RFI reports (Appendix E, page 1), there are two recreational facilities located within the immediate vicinity of the project area in Indiana. The public recreation area located east of the Sherman Minton Bridge, contains public facilities including the New Albany Riverfront Amphitheater and overlooks, and is owned by the City of New Albany. There is also the public Ohio River Greenway trail that runs underneath the Sherman Minton Bridge, operated by the Ohio River Greenway Commission.
Coordination with the Ohio River Greenway Commission and City of New Albany occurred through CAC and teleconference meetings to provide project updates and address concerns on keeping the recreational facility open for the public. Coordination with the City of New Albany and the Ohio River Greenway Commission is to be maintained by the Design-Build Contractor on project updates to ensure the safety of park users. No permanent impacts are anticipated and access to the recreational facilities will be maintained during construction. The project will not use this resource by taking permanent ROW and will not alter the environment in such a way as to constitute constructive use of this resource. Therefore, no use or permanent impact is expected. If the Design-Build Contractor determines that they want temporary use of the trail during construction, it is the Design-Build Contractor's responsibility to coordinate with INDOT, FHWA, the City of New Albany, and the Ohio River Greenway Commission. Additional archaeology, wetland, or other analysis may be required.
No educational facilities are located within a 0.5 mile radius of the project area. Three (3) religious facilities are located within the 0.5 mile radius of the project area. Access to and from the facilities are not impacted by the project; therefore no impact is expected. No airports are identified within the 0.5 mile radius of the project area; therefore, no impact is expected.
In Indiana there are no utilities running parallel to I-64; however, I-64 bridges have several utilities identified alongside the interstate. A utility coordination kick-off meeting was held in September 2019. Project plans have been provided to utilities for their review and comment. No utility impacts are anticipated within rehabilitation of the Sherman Minton Bridge nor the New Albany resurface project.
An active rail line is located under the Indiana Approach of the Sherman Minton Bridge and is an owned and operated by Norfolk Southern Railroad. Norfolk Southern has reviewed project plans and provided comments. All comments are being incorporated into the project documents. No impact is anticipated to the railroad.
Kentucky
Based on a desktop review, a site visit on July 18, 2019 by Michael Baker International, the aerial map of the project area (Appendix B, page 4), and the RFI and limited RFI reports (Appendix E, page 1), there is one(1) recreational facility, the public Shawnee Golf Course, located within the immediate vicinity of the project area in Kentucky.
Coordination with the City of Louisville and Louisville Parks and Recreation occurred through CAC and teleconference meetings to provide project updates and address concerns on keeping the recreational facility open for the public. Coordination with the City of Louisville and Louisville Parks and Recreation is to be maintained by the Design-Build Contractor on project updates to ensure the safety of park users. No permanent impacts are anticipated due to access to the recreational facility will be maintained during construction. The project will not use this resource by taking permanent ROW and will not alter the environment in such a way as to constitute constructive use of this resource. Therefore, no use or permanent impact is expected. If the Design-Build Contractor determines that they want temporary use of the facility during construction, it is the Design-Build Contractor's responsibility to coordinate with KYTC, FHWA, the City of Louisville, and the Louisville Parks and Recreation. Additional archaeology, wetland, or other analysis may be required.
Two (2) educational facilities are located within a 0.5 mile radius of the project area. These schools may face temporary traffic impacts; however, access to and from the school is maintained. One (1) religious facility is located within the 0.5 mile radius of the project area. Access to and from the facility is not impacted by the project; therefore, no impact is expected. No airports are identified within the 0.5 mile radius of the project area; therefore, no impact is expected.

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Utilities located in Kentucky are located outside of the existing Limited Access Right-of-Way. There are utilities located along local roadways of Northwestern Parkway and Bank Street which travel under I-264 just south of the I-64/I-264 interchange. A utility coordination kick-off meeting was held in September 2019. Project plans have been provided to utilities for their review and comment. No utility impacts are anticipated with the project work.

Both Indiana and Kentucky

Local first responders and emergency service providers force have been in coordination with the project team as a part of public outreach, this includes Floyd memorial hospital with concerns of heliport map. A meeting was held August 22, 2019 with a local Freeway Incident Management team, which included both New Albany and Louisville representatives of first responders and emergency service providers. The purpose of this meeting was to obtain feedback and concerns from those representatives during project development in approach to construction. Concerns mentioned in this meeting centered around the MOT options. It was mentioned that many first responders cross the bridge to answer a call of service regardless of state or city affiliation. If the bridge is only open to one lane, (MOT 2), this can leave emergency teams in difficult positions due to access, not to mention if multiple vehicles needed. An additional concern of access for emergency teams and hazardous units were also brought up in the case of an incident on the bridge itself. These concerns and comments were taken into consideration by the project team for further analysis into the MOT options and recommendations.

The public transit system serving the Project Study Area is operated by the Transit Authority of River City (TARC). TARC representatives have indicated that while all MOT options would have some effect on their operations, additional coordination would allow for service adjustments to ensure continued served for their ridership.

- **Indiana:**

Increased congestion on the local New Albany roadway network and at the river crossings under any of the MOT options could affect headways for TARC Routes.

- **Kentucky:**

Because of Louisville Metro area's robust roadway network, TARC riders on routes that do not cross the river are not anticipated to experience travel time delays under any of the MOT Options in the Louisville Metro area.

TARC routes were evaluated for impacts. Routes 71, 72, 82, and 65X were identified as potentially affected routes dependent upon MOT (Appendix I, page 123).

- Route 71 (Jeffersonville-Louisville-IUS) TARC Route 71, at its easternmost point within the Project Study Area, starts in west Jeffersonville, accesses I-64 via North 22nd Street, Crosses the SMB, then exits I-64 into downtown New Albany via the Spring Street Exit where it connects to State Street, Pearl Street, and Grant Line Road. Route 71 would be most affected because, in a single complete trip, this route crosses the Ohio River twice: once on the SMB and once on the Clark Memorial Bridge which, in the Base case, is already at capacity.
- Route 72 (Clarksville) - This route connects Clarksville to downtown Louisville via the Clark Memorial Bridge (2nd Street Bridge). Starting in Clarksville just west of I-65, southbound along routes that include Veterans Parkway, Greentree Boulevard, and Eastern Boulevard, to its crossing of the Ohio River via the Clark Memorial Bridge, then south into Downtown Louisville.
- Express 65 (Sellersburg Express) This route operates *Monday through Friday*, -providing an express connection between Ivy Tech in Sellersburg, IN to downtown Louisville. crossing via Clark Memorial Bridge. One stop at Ivy Tech on Indiana side. Several Downtown Louisville.

MOT Options 1, 2 and 4 maintain continuous travel on the Sherman Minton Bridge in both directions and would be less disruptive to TARC services and operations. MOT Option 5 (full closure) would require rerouting of TARC Route 71. Travel time delays may require service adjustments such as number stops, temporary detours taken, number of detours, and adjustments to the transit schedules, which could lead to further communication delays to riders. If TARC is unable to provide alternative and timely routes for riders, the impact to riders needing to cross the river could be significant. Since EJ populations near the Sherman Minton Bridge depend on TARC services, that rerouting diversions to other bridges was determined to be "disproportionately high and adverse effects", and therefore MOT conditions that increased this potential were not carried forward.

Minimization measures were part of project development, evaluations, and a combination of MOT options to reduced impacts. While the MOT 1 option had the most positive public involvement comments, fewest changes from existing travel patterns and local access, lowest induced traffic diversions and related environmental impacts; project constructability requirements were not entirely met. The Preferred Alternative MOT combined the predominant use of

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MOT 1, limited off-peak use of MOT-2 (*allowance for about 180 nights per construction year*), and short-term use of MOT-5. Detour routes are listed below and included in Appendix B, pages 19-26.

The SMRP will impact interstate and local travel to public health and educational facilities, public and private utilities, emergency services, religious institutions, and airports within and adjacent to the project area, however, these impacts are temporary and none of these impacts are unique to a particular facility. The completion of proposed action will also provide long-term benefits with increased certainty and an extended service life for facilities and services that rely on the Sherman Minton Bridge in both Indiana and Kentucky.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Will the project result in adversely high or disproportionate impacts to EJ populations?

Remarks:

An EJ evaluation was conducted to the SMRP as part of the CIA and EJ Analysis Technical Report (Appendix I, page 1). This analysis covered both Indiana and Kentucky.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is *Floyd County, New Albany, Indiana and Jefferson County, Louisville, Kentucky*. The COC is then broken into smaller, more manageable units based on Census Tracts (CTs). The community that overlaps the project area is called the affected community (AC). For the SMRP, the ACs includes all of the CTs that are located in the SMRP study area. (Appendix I, page 61). Demographic data at the COC and AC levels come from the U.S. Census Bureau and its more frequently updated American Community Survey (ACS). CT data were obtained from the 2013-2017 ACS five-year estimates and from the US Census Bureau Website <https://factfinder.census.gov/> on November 12, 2019 by Michael Baker International. The data collected from the 59 census tracts for minority and low-income populations within the AC are summarized in Appendix I, page 85.

An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. EJ populations are present in Project Study Area:

Indiana

- Clark County consists of 8 Census Tracts where 5 are minority, 4 are low income, and 4 are both;
- Floyd County consists of 13 Census Tracts where 4 are minority, 8 are low income, and 4 are both;

Kentucky

- Jefferson County consists of 38 Census Tracts where 29 are minority, 35 are low-income, and 29 are both.

Project Area COC, AC, and CT level demographic data were incorporated into a Travel Demand Model (TDM) for insights to likely detour routes, travel time changes, changes in travel costs, and roadways likely to experience increased congestion. The TDM utilized both EJ and Non-EJ Traffic Analysis Zones (TAZs; as determined by KIPDA) based on whether or not the zone was within an EJ community. The model outputs were categorized by the trip origin as either an EJ Trip (those trips originating from within a Study Area EJ TAZ) or a Non-EJ Trip (those trips originating outside of a Study Area EJ TAZ). Potential EJ impacts are detected by locating minority populations and low-income populations in and near the project area, calculating their percentage in the area relative to a reference population (i.e. Project COCs, and determining whether there will be adverse impacts to them. For this project, disparities between non-EJ and EJ populations were examined comparing existing data with implementation of the proposed MOT Options. Potential disparities between non-EJ and EJ populations were analyzed along with community impacts for the following factors:

Traffic Impacts

- Diversions – Average Daily Traffic (ADT) vehicles diverted from the Sherman Minton Bridge and percentage change (total vehicles and EJ area origin passenger vehicles).
- Congestion - under capacity, near capacity, or at capacity conditions based on estimated peak hour traffic at any of the four selected local street network locations.
- Change in average AM Peak trip length (miles) and time (minutes) for Non-EJ (total vehicles) and EJ (passenger vehicles by origin area).

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Transit impacts

- o Potential for a TARC route change (low to high)
- o Travel time delay for a TARC route change (minimal to 3 times existing)

Economic impacts

- o Relative construction duration (years)
- o Change in average AM Peak trip user cost and toll cost (\$ Non-EJ total vehicles and \$ EJ passenger vehicles by origin area)
- o Temporary disruption for local businesses (low to high)

Social impacts

- o Local access (ramp closures) and cross-river connectivity (maintained, reduced, or closed)

EJ impacts

- o Evaluated per FHWA Order 6640.23A (5)(g) that defines a “Disproportionately High and Adverse Effect on Minority and Low-Income Populations” as “an adverse effect that (1) is predominately borne by a minority population and/or a low-income population; or (2) will be suffered by the minority population and/or low-income population and is appreciably more severe or greater in magnitude than the adverse effect that will be suffered by the nonminority population and/or non-low-income population.”
 - All of the MOT options have some degree of EJ impacts, based upon diversion to toll facilities and increased network user costs
 - Disproportionately high and adverse effects to EJ populations vary by MOT option (yes/no)

MOT 1 – Two lanes open, both decks (EB and WB)

- Traffic impacts – 7,400 ADT diversions (8% total; 19% EJ), congestion (under capacity), change in trip length (miles – 0.2 non-EJ; 0.2 EJ) and trip time (minutes – 0.9 non-EJ; 0.9 EJ).
- Transit impacts – disruption of TARC routes (few); increase in rider travel time (low)
- Economic impacts – duration (3+ years), change in average AM Peak trip user cost (\$0.52 Non-EJ; \$0.41 EJ) and toll cost (\$0.20 Non-EJ; \$0.14 EJ)
- Social impacts - access closures (none) and cross-river connectivity (maintained)
- EJ impacts - disproportionately high and adverse effects (no)

MOT 2 – One lane open, both decks (EB and WB)

- Traffic impacts – 33,400 ADT diversions (37% total; 21% EJ), congestion (near capacity), change in trip length (miles – 0.1 non-EJ; 0.2 EJ) and trip time (minutes – 2.0 non-EJ; 2.6 EJ).
- Transit impacts – disruption of TARC routes (few); increase in rider travel time (moderate)
- Economic impacts – duration (2 years), change in average AM Peak trip user cost (\$1.37 Non-EJ; \$1.18 EJ) and toll cost (\$0.59 Non-EJ; \$0.35 EJ)
- Social impacts - access ramps (3 closures) and cross-river cohesion (maintained)
- EJ impacts - disproportionately high and adverse effects (no)

MOT 3 – Alternating three one-way lanes (AM-EB / PM-WB) open on one deck

- Traffic impacts – 40,600 diversions (45% total; 28% EJ), congestion (under capacity), change in trip length (miles – 0.2 non-EJ; 0.8 EJ) and trip time (minutes – 0.3 non-EJ; 3.2 EJ).
- Transit impacts – disruption of TARC routes (high); increase in rider travel time (high)
- Economic impacts – duration (2.5 years), change in average AM Peak trip user cost (\$0.57 Non-EJ; \$01.32 EJ) and toll cost (\$0.41 Non-EJ; \$0.07 EJ)
- Social impacts - access ramps (4 closures) and cross-river cohesion (reduced)
- EJ impacts - disproportionately high and adverse effects (yes)

MOT 4 – Reversible center lane (AM-EB / PM-WB) and one-way EB/WB lanes open on one deck

- Traffic impacts – 19,700 diversions (22% total; 26% EJ), congestion (under capacity), change in trip length (miles – 0.1 non-EJ; 0.1 EJ) and trip time (minutes – 1.0 non-EJ; 2.0 EJ).
- Transit impacts – disruption of TARC routes (none); increase in rider travel time (none)

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- Economic impacts – duration (2.5 years), change in average AM Peak trip user cost (\$0.68 Non-EJ; \$0.88 EJ) and toll cost (\$0.29 Non-EJ; \$0.20 EJ)
- Social impacts - access ramps (5 closures) and cross-river cohesion (maintained)
- EJ impacts - disproportionately high and adverse effects (no)

MOT 5 – Full Closure* of all six lanes and both decks

- Traffic impacts – 90,000 diversions (100% total; 18% EJ), congestion (at capacity), change in trip length (miles – 1.1 non-EJ; 1.3 EJ) and trip time (minutes – 3.3 non-EJ; 6.0 EJ).
- Transit impacts – disruption of TARC routes (none); increase in rider travel time (none)
- Economic impacts – duration (1.5 years*), change in average AM Peak trip user cost (\$2.77 Non-EJ; \$2.98 EJ) and toll cost (\$1.24 Non-EJ; \$0.72 EJ)
- Social impacts - access ramps (4 closures) and cross-river cohesion (closed)
- EJ impacts - disproportionately high and adverse effects (yes*)

* based on 1.5 years duration

MOT 6 – One Direction/Phase three one-way lanes (WB-Phase 1/EB-Phase 2) open on one deck

- Traffic impacts – 46,600 ADT diversions (52% total; 21% EJ), congestion (at capacity), change in trip length (miles – 0.5 non-EJ; 0.6 EJ) and trip time (minutes – 1.7 non-EJ; 3.2 EJ).
- Transit impacts – disruption of TARC routes (none); increase in rider travel time (none)
- Economic impacts – duration (2.5 years), change in average AM Peak trip user cost (\$1.43 Non-EJ; \$1.56 EJ) and toll cost (\$0.64 Non-EJ; \$0.36 EJ)
- Social impacts - access ramps (3 closures) and cross-river cohesion (reduced)
- EJ impacts - disproportionately high and adverse effects (yes)

Since 75% of the EJ Trips did not cross the river (via a passenger vehicle using the Sherman Minton Bridge); EJ populations within the Project Study Area are not disproportionately more reliant on the existing Sherman Minton Bridge nor would these populations be disproportionately affected by having to use an alternative river crossing. Similarly, regardless of MOT Option, the increased use of tolled river crossings for EJ Trips would be less than that of Non-EJ Trips. Table 6 below summarizes the potential impacts per MOT at full duration.

Table 6. Sherman Minton Renewal Project: Potential MOT Impacts

TEMPORARY IMPACT CATEGORY	SUB-CATEGORY	MOT 1		MOT 2		MOT 3		MOT 4		MOT 5		MOT 6	
		Disp High	Adv	Disp High	Adv	Disp High	Adv	Disp High	Adv	Disp High	Adv	Disp High	Adv
Traffic	Diversions									X			
	Access & Congestion									X			X
	Travel Distance												
	Travel Time												
Transit	TARC Riders*					X	X			X	X	X	X
Economic	Diversion to Tolls		X		X		X		X		X		X
	User Costs – Network		X		X		X		X		X		X
	User Costs – Local						X				X		X
	Local Businesses**						X		X		X		X
Social	Access, Mobility, Cohesion						X		X	X	X		X
	Quality of Life (Air/Noise)												
Overall Potential (Yes/No)		No		No		Yes		No		Yes		Yes	

* Applies primarily to cross-river riders on TARC Route 71

** Applies primarily to businesses in downtown New Albany

Minimization measures were part of project development, evaluations, and a combination of MOT options to reduced

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impacts. While the MOT 1 option had the most positive public involvement comments, fewest changes from existing travel patterns and local access, lowest induced traffic diversions and related environmental impacts; project constructability requirements were not entirely met. The Preferred Alternative MOT combined the predominant use of MOT 1, limited off-peak use of MOT-2 (*allowance for about 180 nights per construction year*), and short-term use of MOT-5. Detour routes are listed below and included in Appendix B, pages 19-26.

According to the CIA and EJ evaluations, Project Study Area EJ populations would be adversely affected in all six MOT options for diversion to tolled facilities and increased network user costs. The completion of proposed action will also provide long-term benefits with increased certainty and an extended service life for EJ populations and communities that rely on the Sherman Minton Bridge in both Indiana and Kentucky. All meeting summaries are included in Appendix G.

Based upon public involvement comments, EJ Outreach input, comparison to COC populations, and inclusion of the MOT Option that maintains cross-river community connectivity the lowest impacts; the project will not result in "Disproportionately High and Adverse" impacts for Study Area EJ populations.

Relocation of People, Businesses or Farms

	Yes	No
Will the proposed action result in the relocation of people, businesses or farms?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a Business Information Survey (BIS) required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a Conceptual Stage Relocation Study (CSRS) required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Has utility relocation coordination been initiated for this project?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Number of relocations: Residences: _____ Businesses: _____ Farms: _____ Other: _____

If a BIS or CSRS is required, discuss the results in the remarks box.

Remarks: No relocations of people, utilities, businesses, or farms in Indiana or Kentucky will take place as a result of this project.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Documentation

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation	<input checked="" type="checkbox"/>
Phase I Environmental Site Assessment (Phase I ESA)	<input type="checkbox"/>
Phase II Environmental Site Assessment (Phase II ESA)	<input type="checkbox"/>
Design/Specifications for Remediation required?	<input type="checkbox"/>

	No	Yes/ Date
ES Review of Investigations	<input type="checkbox"/>	April 3, 2019
ES Review of Investigations	<input type="checkbox"/>	Limited RFI October 11, 2019

Include a summary of findings for each investigation.

Remarks: Based on a review of GIS and available public records, a RFI was completed on March 29, 2019 by Parsons (Appendix E, Page 1) and approved April 3, 2019, specific to the Sherman Minton Bridge location. No hazmat sites were identified in or within 0.5 mile search radius of the Sherman Minton Bridge that will impact the project.

Indiana
 A Limited RFI was completed on October 9, 2019, by Parsons (Appendix E, Page 16) fixated on Elm, Spring, 4th, and 5th Streets of New Albany, IN. Nine (9) underground storage tanks, one (1) state cleanup site, ten (10) leaking underground storage tanks, nine (9) brownfields and five (5) institutional control are located within the 0.5 mile search radius of the project area. One (1) underground storage tank (UST) site is located within the project area.

This UST site is the Kentuckiana Mack Sales and Service at 44 W. 5th Street, New Albany, IN(Agency ID #30115), located at the southeast corner of the intersection. The IDEM received an *Underground Storage Tank Notification*, dated August 10, 1988 that documented two USTs containing diesel as "permanently out of use" with anticipated removal in 1988; no further information was available. In addition to petroleum contamination, it is likely that lead would be in the soil/groundwater. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Before

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proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. No work is proposed at this intersection. Therefore, no impacts are expected. Further investigation for hazardous material concerns or regulated substances is not required at this time.

Kentucky

A desktop screening was conducted for the Kentucky side of the project. No concerns were identified within the project limits. The proposed project will stay within existing previously disturbed roadway ROW; therefore, no impacts are expected. Further investigation for hazardous material concerns or regulated substances is not required at this time.

Both Indiana and Kentucky

The Ohio River is listed as an Indiana Department of Environmental Management (IDEM) 303d impaired waters for dioxin, E. coli, Total Mercury in water, polychlorinated biphenyls (PCBs) in water and PCBs in fish tissue. The Ohio River is also listed as a Kentucky Energy and Environment Cabinet (EEC) 303d impaired waters for dioxin, E. coli, and PCBs. Workers who are working in or near water with E. coli should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure. If there will be sediment and/or soils disturbed by construction, additional investigation may be necessary. Coordination with INDOT ES and KYTC will occur. These recommendations are included as firm commitments in the Environmental Commitments section of this document.

SECTION I – PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Individual Permit (IP)	<input type="checkbox"/>
Nationwide Permit (NWP)	<input type="checkbox"/>
Regional General Permit (RGP)	<input type="checkbox"/>
Pre-Construction Notification (PCN)	<input type="checkbox"/>
Other	<input type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>

IDEM

Section 401 WQC	<input type="checkbox"/>
Isolated Wetlands determination	<input type="checkbox"/>
Rule 5	<input type="checkbox"/>
Other	<input type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>

IDNR

Construction in a Floodway	X
Navigable Waterway Permit	<input type="checkbox"/>
Lake Preservation Permit	<input type="checkbox"/>
Other	<input type="checkbox"/>
Mitigation Required	<input type="checkbox"/>

KDOW

Construction in a Floodway	X
401 Water Quality Certification	<input type="checkbox"/>

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the remarks box below)

<input type="checkbox"/>
<input type="checkbox"/>

Remarks:

Applicable recommendations provided by USACE, IDNR, KDOW, USEPA and USCG are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

USACE agency coordination response letter received May 8, 2020, (Appendix C, page 13) identifies recommendations and commitments for the Design-Build Contractor to follow. The following commitments section include the applicable recommendations.

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IDNR early coordination letter response received March 11, 2019, (Appendix C, page 17), indicates the I-64 Sherman Minton Bridge work will require the formal approval for construction in a floodway under the Flood Control Act IC 1-28-1.

KDOW indicated on July 18, 2019, at the agency field visit a KDOW Construction and Floodplain Permit will be required. If the Design-Build Contractor decides to access or work within any waterway, a 401 Water Quality Certification will also be required.

USCG early coordination letter response received on December 18, 2018, (Appendix C, page 78) indicates USCG will not require a Coast Guard Permit; However once a contractor has been selected, USCG will require work plans to be reviewed and a conditions letter will be issued before work can commence.

It is the responsibility of the Design-Build Contractor to identify and obtain all required permits.

SECTION J- ENVIRONMENTAL COMMITMENTS

The following information should be provided below: List all commitments, name of agency/organization requesting the commitment(s) and indicating which are firm and which are for further consideration. The commitments should be numbered.

Remarks: The following commitments are based upon those provided or requested by environmental regulatory agencies. Additional details and requirements set forth by regulatory agencies, INDOT and KYTC are conveyed in the Design-Build Best Value Technical Provisions.

Firm:

- 1) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT District)
- 2) It is the responsibility of the project sponsor to notify school corporations, hospitals and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
- 3) Workers who are working in or near water with E. coli should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure. (INDOT SAM)
- 4) If there will be sediment and/or soils disturbed in the Ohio River by construction, additional investigation may be necessary. Coordination with INDOT ESD and KYTC will occur. (INDOT SAM)
- 5) If excavation occurs in or near 44 W. 5th Street, New Albany, IN, it is likely that petroleum contamination will be encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. (INDOT SAM)
- 6) Accommodations will be provided for the following special events and festivals and full bridge closures will not occur on: New Year’s Day, Good Friday, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, Christmas Day, Thunder over Louisville, Kentucky Derby, and Harvest Homecoming Festival. (INDOT ESD)
- 7) Coordination with the Louisville Parks and Recreation is to be maintained by the Design-Build Contractor with project updates to ensure the safety of trail users. (KYTC-DEA)
- 8) If the Design-Build Contractor requires temporary access or use of any Section 4(f) or 6(f) resource during construction, it is the Design-Build Contractor’s responsibility to coordinate with necessary agencies including but not limited to INDOT, KYTC, FHWA, the City of New Albany, the City of Louisville, the Louisville Parks and Recreation, and the Ohio River Greenway Commission, as Section 4(f) or Section 6(f) analysis may be required. (Technical Provision Section 7)
- 9) Early coordination response information received from Indiana Geological Survey is to be reviewed by the Design-Build Contractor. (Technical Provision Section 7)

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- 10) Once a contractor has been selected, United States Coast Guard will require them to submit a work plan to be reviewed by our office and a work conditions letter will need to be issued from our office before any work can commence. (USCG)
- 11) No impacts due to construction will occur to the Ohio River. Should impacts be unavoidable the contractor will be responsible for coordination to obtain clearance for section. If there are any questions regarding this note, please contact Danny Peake, Director, Division of Environmental Analysis, 200 Mero Street, Frankfort, KY 40601; Phone (502) 564-7250. (KYTC-DEA)
- 12) The Design-Build Contractor must coordinate the chosen alternative with KYTC. Once an alternative has been chosen KYTC shall provide SHPO with the chosen alternative and final archeological effects recommendation. (KY-SHPO)
- 13) Once an Alternative and APE has been chosen, KYTC shall identify and reach out to the appropriate consulting parties based upon the chosen APE. (KY-SHPO)
- 14) If the design-build contractor proposes work outside of the existing ROW, coordination with INDOT Cultural Resources will be required. (INDOT-CRO)
- 15) Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of the riprap. (USFWS)
- 16) Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat. (USFWS)
- 17) Implement pollution prevention and control measures during all construction activities to reduce the potential for hazardous spills or other materials entering the Ohio River. This will include the placement of refueling staging areas, fuel storage, and hazardous materials away from the river, and may also require specific containment measures for painting, sanding, etc. (USFWS)
- 18) If a causeway must be used, then locate the causeway primarily outside of the cobble/gravel substrate area, which is the most suitable habitat for many mussel species. (USFWS)
- 19) Install culverts/pipes within the causeway to allow continued flow of water through the area to prevent pooling and stagnation. (USFWS)
- 20) The height of the causeway should be kept to a minimum to allow over-topping during heavy rain events to prevent upstream flooding. (USFWS)
- 21) Use clean fill material and remove immediately once project is completed. (USFWS)
- 22) The structure should not be in the stream longer than a year in order to minimize disruption of the mussel and host fish reproductive cycle. (USFWS)
- 23) All equipment to be used in the river should be inspected using accepted protocols and determined free of zebra mussel adults and veligers. (USFWS)
- 24) In the event a barge is used, all barge equipment maintenance will be conducted away from the river, whenever possible. Fuel storage shall be contained/maintained in an area where leakage and spilling into the river will be avoided. (USFWS)
- 25) Excavation for the deadman anchors and steel cables would be performed in a manner to minimize the amount of surface disturbance, and appropriate measures would be implemented to prevent the discharge of material into the river channel. During excavation, temporary silt fence will be installed around each deadman anchor site during excavation and installation. Extreme caution will be exercised during excavation/installation activities to prevent sediment from being washed into the Ohio River. (USFWS)

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- 26) The towboat will be operated at as low of RPM's as practicable when approaching and leaving the work site to minimize river bottom scouring and downstream siltation. (USFWS)
- 27) Minimize impacts to shoreline and substrate via barge grounding. (USFWS)
- 28) Align the road along or through previously disturbed and degraded areas and disturb as narrow an area as possible to minimize negative impacts. Avoid tree removal to the greatest extent possible. Plant native hardwood trees to replace the vegetation destroyed during construction. (IDNR)
- 29) All plant material, mud, and debris should be removed, and all water drained from equipment before entering or leaving the waterway to prevent the spread of aquatic and terrestrial invasive species. (IDNR)
- 30) Avoid staging and construction access within or wooded areas to the extent possible. (IDNR)
- 31) Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to nonwetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10 inches dbh or greater (5:1 mitigation based on the number of large trees). (IDNR)
- 32) Section 404 of the Clean Water Act (CWA) establishes a program to regulate the discharge of dredged or fill material into waters of the United States, including wetlands. Section 10 of the Rivers and Harbors Act of 1899 requires authorization for the construction of any structure in, over or under any navigable water of the United States. The proposed project would require two applications to be submitted for authorization under Section 404 of the Clean Water Act and Section 10 of the Rivers and Harbors act - one application for impacts to waters of the U.S. in Kentucky and one application for impacts in Indiana. INDOT and KYTC have funded positions in Louisville District's Regulatory Division under the Water Resources Development Act (WRDA). These personnel would process the Section 404/Section 10 permits for this project. Under the WRDA funding agreements, the permits cannot be issued to any entity other than to those agencies funding the positions, thus Louisville District will have to process two separate actions for the proposed project. The WRDA funding agreement also requires KYTC and INDOT to be co-applicants with the contractor when the contractor applies for the permits to each state, respectively. KYTC and INDOT, along with the contractor, assume all responsibilities and risks associated with the permit and the contractor's work. (USACE).
- 33) If barges would be moored on the Ohio River or doing any work on the river, a Section 10 permit would be required as stated above. The permit would be part of the Section 404 permit. A map showing the location of barges would be required, along with drawings stamped by a professional engineer showing the locations and mooring configurations (including locations of deadmen that would be installed). Include a narrative/description of the mooring configuration and work to be performed. (USACE)
- 34) Work within the river would require Regulatory to coordinate with the Navigation Branch of the Louisville District Corps, which may necessitate the Corps permit being conditioned directing the permittees to notify Navigation 30 days prior to the commencement of work/mooring on the river for Navigation to issue a Notice to Navigation Interests. The Corps permit, if issued, would provide the contact information. (USACE)
- 35) The permit application must include the location, size and work for any staging, borrow and/or waste sites, with a description of work at those locations' areas; temporary work to be performed, including the installation of temporary mats, cofferdams, etc. (USACE)
- 36) The permit that would be issued for this project would require the contractor to notify the Corps if potential endangered species or historic/archeological resources are encountered during the course of work. This should also be included in the contract letting documents. (USACE)
- 37) The Corps must be notified of any modifications to the authorized work. Please ensure this requirement is included in the contract. (USACE)
- 38) Please ensure the Corps is supplied with either a U.S. Coast Guard (USCG) permit or correspondence from the USCG stating a permit is not required. The Corps will require either of these documents prior to issuance of any Corps permits. (USACE)

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39) Based upon the Navigable Waters Protection Rule that took effect in June 2020, Wetlands/open waters will need to be reevaluated throughout the project under the Navigable Waters Protection Rule to determine if they are jurisdictional waters. (USACE)

40) All future correspondence with the Corps should reference the Corps ID number for this project. Which is LRL-2018-1114. (USACE)

41) Advance notice must be provided to Metro Public Works, Emergency Management, and Transit Authority of River City (TARC) prior to any closures that extend more than 24 hours. (INDOT)

For Further Consideration:

N/A

SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received. INDOT and FHWA are automatically considered early coordination participants and should only be listed if a response is received.

Remarks:

The following agencies were sent early coordination letters on December 12, 2018, as a part of the development of this Environmental Study. Additional coordination letters were sent February 15, 2019. Early coordination Letters are found in Appendix C.

Agency	Response Date	Appendix C
FHWA Indiana	No Response	Not Included
FHWA Kentucky	No Response	Not Included
INDOT Environmental Services	No Response	Not Included
INDOT Cultural Resources	No Response	Not Included
INDOT Public Works	No Response	Not Included
KYTC Division of Environmental Analysis	No Response	Not Included
KYTC Division of Cultural Historic Branch	No Response	Not Included
USEPA Region 5 Chicago	February 7, 2019	C4
USEPA Region 4 Atlanta	February 7, 2019	C4
USACE Louisville District	January 14, 2019	C13
USFWS IN-Bloomington Field Office	February 20, 2019	C23
USFWS KY	No Response	Not Included
United States Coast Guard 8 th District	December 18, 2018	C78
National Resources Conservation Office / USDA	December 17, 2018	C79
IDEM	Auto response generated March 19, 2019	C80
IDNR	March 11, 2019	C17
Kentucky Division of Water (KDOW)	No response	Not Included
IN SHPO	January 7, 2019	D1
KY SHPO	December 20, 2018	D17
National Parks Service	No Response	Not Included
US. – HUD	No Response	Not Included
Indiana Geological Survey	Auto generated response March 12, 2019	C85

APPENDIX B: Approved AI



DATE: September 7, 2021

TO: Mr. Ron Bales, Environmental Policy Manager
Indiana Department of Transportation (INDOT) Environmental Services Division (ESD)

Ms. Michelle Allen, Team Leader
Federal Highway Administration (FHWA) – Indiana Division

Mr. Dave Harmon
Kentucky Transportation Cabinet (KYTC) Division of Environmental Analysis (DEA)

COPY: Mr. Eric Rothermel
FHWA – Kentucky Division

FROM: Susan Castle, Senior Scientist
Metric Environmental, LLC

RE: INDOT Lead Des. No. 1702255, Additional Des. Nos. 1702260, 1702254, 1592187, 1702257, 170225 8, 1702259, 1701215, & 1900579 Kentucky Transportation Cabinet (KYTC) Item ID 5-64

Additional Information to the Final Categorical Exclusion (CE) Level 4 Document, approved October 7, 2020, for the proposed Bridge, Associated Approaches, and Road Improvements located on West 5th Street, West Spring Street, and West Elm Street near the I-64 ramps, I-64 and US 50 Sherman Minton Bridge crossing the Ohio River in New Albany, Floyd County, Indiana and in Louisville, Jefferson County, Kentucky, extending from the I-62 / I-264 interchange in Louisville Kentucky, 3.5 miles to the northwest, to the I-64 / I-265 interchange in New Albany, Indiana.

ADDITIONAL INFORMATION

This memorandum provides additional information to the approved CE document, for the bridge and roadway improvements on Sherman Minton Bridge and side streets in Floyd County, Indiana and Jefferson County, Kentucky. Unless specifically discussed in this document, the impacts as identified in the approved CE document, approved October 7, 2020, remain unchanged. The approved CE document, without attachments, is located in Appendix G, pages G-1 to G-54.

Purpose and Need

The need for the project is due to the deteriorating structural condition of the existing Sherman Minton Bridge over the Ohio River, the deteriorating associated Indiana and Kentucky approaches, and deteriorating pavement of select associated side streets.

The purpose of the project is to address the deterioration of structural elements of the Sherman Minton Bridge, the associated Indiana and Kentucky approaches, and select associated side streets with the goal of extending the service life of the I-64 Interstate crossing over the Ohio River up to 30 years.

Please refer to Appendix G, pages G-5 to G-7 for the Purpose and Need, in its entirety.

Project and Environmental Document History

The approved CE document identifies this project as the Sherman Minton Renewal Project (SMRP); however, prior to request for proposals and selection of the Design-Build Team, the official name of the project was changed to Sherman Minton Corridor Project (SMCP). The project name was changed due to adding asphalt overlay and preventative maintenance work on West 5th Street, West Spring Street, and West Elm Street near the I-64 ramps in New Albany, Indiana, which are included in the approved CE document. SMCP shall be the reference to the project throughout this document.

The SMCP is the rehabilitation of the Sherman Minton Bridge, related approaches, and selected side streets in Indiana and Kentucky. The goal of SMCP is to address the deteriorating structural condition of the existing bridges to extend the service life of the bridges up to 30 years. Project elements include bridge deck replacements, bridge deck overlays, structural repairs, replacement lighting, bridge painting, local streets Hot Mix Asphalt (HMA) overlay and Americans with Disabilities Act (ADA) ramp reconstruction, and interstate ramp frictionalization. SMCP is a joint effort between INDOT and KYTC. INDOT is leading SMCP in close collaboration with key staff from KYTC. The entire description of the preferred alternative is located in Appendix G, pages G-9 to G-11.

Since the CE was approved on October 7, 2020, additional work has been added to this project as discussed below:

Additional work in Kentucky

A temporary access road will be installed on the levee, southwest of the bridge in Louisville, Kentucky, in order to provide access beneath the Kentucky approach bridges to perform structural repairs. A Location Map, Topographic Map, 2019 Aerial Photo Location Map, and Site Photographs are located in Appendix A, pages A-1 to A-12. The access road shall extend from levee Stations 329+00 to 342+00 and adjacent to I-64 Station 65+00 (Appendix A, page A-20).

The temporary access road will be approximately 12 feet (ft.) wide, 1 ft. thick, and 1,440 ft. long and comprised of temporary aggregate over geotextile fabric. The levee access road will then traverse down the slope into the project right-of-way (ROW). Project access areas beneath the bridge extend 40 ft. beyond the drip line along both sides of the Kentucky approach bridges up to Station 315+00 where the work area is restricted to 25 ft. beyond the drip line along both sides of the structure (Appendix A, page A-14). This restriction extends all the way to the culmination of the work zone adjacent to the Ohio River.

Construction of the access road and aggregate pads under the approach spans will require approximately 20 loads of stone per day for four days, one Cat D6 bulldozer to strip topsoil to a depth of six to eight inches and spread stone for four days, one 72-inch smooth drum roller to compact stone for four days, and an occasional pickup truck in and out of the site during the levee road access construction process. The stripped topsoil will be stored and stabilized for the duration of the project.

After the access road is constructed, normal day-to-day operations in the project area will include water trucks making passes as needed for dust control, occasional pickup trucks entering and exiting the site, one to two concrete trucks with light loads (two to three cubic yards) utilizing the access road for pier cap patching, and an occasional low-boy trailer will be needed to bring equipment in and out (anticipated to be an infrequent occurrence after the initial mobilization).

During hydro-demolition activities one additional water truck will utilize the access road emptying frac tanks from hydro-demo operation site to be hauled offsite, and occasional pickup trucks will enter and leave the site.

During removal of the access road, aggregate pads, and subsequent stabilization one track-hoe for loading dump trucks will be required for four days, approximately 20 dump truck loads will occur per day for four days, one Cat D6 bulldozer will be used for two days redressing the levee, one hydro seeder will be needed for one day to stabilize and plant grass seed on the levee once the work is complete, and occasional pickup trucks will be entering and leaving the site.

The full area of the levee surveyed consists of approximately 6.77 acres and includes the levee within KYTC ROW and beyond. The area of disturbance associated with the temporary access road is approximately 2.11 acres. Upon completion of construction activities, the aggregate and geotextile fabric will be removed, and the topsoil that had been placed in storage will be replaced. The topsoil will be graded and seeded to restore it to preconstruction conditions. This will include application of seed and mulch to facilitate final stabilization of the replaced topsoil. No permanent alterations or impacts to the levee are anticipated.

Two separate staging and lay down areas are proposed to be utilized within the I-64 and I-264 interchange infield areas (Appendix A, page A-3 and A-14).

There will be no work below the ordinary high water mark (OHWM) of the Ohio River; however, there will be temporary impacts to an emergent wetland (0.16 acre) and a forested wetland (0.32 acre) beneath the Kentucky approach bridges for access beneath the structures to allow staging, bridge deck replacement, structural steel painting, and concrete pier rehabilitation. Please refer to Waters Determination / Wetlands Delineation under the Supplemental Resource Review section below for additional information.

The Kentucky side of the project is bound on both sides of the ROW by the Shawnee Park Golf Course (SPGC). The SPGC is a U.S. Department of Transportation Section 4(f) resource as a recreational facility. The SPGC is also a Land and Water Conservation Fund (LWCF) Section 6(f) resource due to use of a LWCF grant for improvements to the SPGC maintenance facility and the park pavilion. Additionally, the Louisville Riverwalk, also known as the Louisville Loop, a Section 4(f) resource, is located along the edge of the SPGC proper between the SPGC and the Ohio River. Because the Louisville Loop is on property considered part of the SPGC, it is also considered a Section 6(f) resource.

The I-64 roadway is elevated above the Section 4(f) and Section 6(f) resources at this location, with several bridge approach spans on piers crossing the SPGC. Two golf cart paths within SPGC converge within the ROW and pass under the I-64 approach spans between holes 1 and 2, and between holes 7 and 8. The Louisville Loop also passes under an approach span within the highway ROW. The Louisville Loop is not being maintained at this location and has been closed to public use since approximately 2009/2010, with barricades across the trail. The area of the trail within the ROW is approximately 640 square ft. (Appendix A, page A-16).

During active project construction, from July 2021 to June 2023, the cart paths will merge before entering the work zone, and a single path will pass under the approach spans under a protective canopy. Bridge deck repairs and painting will occur above the cart path, hence the canopy to shield the golfers. Impact will occur to approximately 5,500 square ft. (0.13 acre) of cart path. There will be repair and rehabilitation of the piers and associated structures during the construction period, within the highway ROW. There will be no change in the recreational use of the SPGC, and access will be maintained during all times the SPGC is open to the public. The cart paths will be returned to their pre-construction condition once construction is completed. There will be no permanent or temporary ROW needed from the facility (Appendix A, page A-16. Please refer to Section 4(f) and Section 6(f) under the Supplemental Resource Review section below for additional information.

All work will be completed in previously disturbed soils in existing ROW. Although suitable habitat is present, no trees are currently planned to be trimmed or cleared during construction activities. In the event it becomes necessary to trim or clear trees during construction, coordination shall occur with all appropriate parties prior to initiation of work.

Additional work in Indiana

Temporary access beneath the Indiana approach bridges will be required for staging and access for structural repairs. Access beneath the Indiana approach bridges will be attained via local streets. Temporary aggregate fill, approximately 12 inches in depth, over geotextile fabric, will be installed beneath the approach bridges, within the ROW, adjacent to the Ohio River. The temporary impacted area will be restored using Seed Mix, Type Floodplain, per INDOT Standard Specifications. No permanent impacts shall occur in association with this work (Appendix A, page A-18).

The temporary access beneath the Indiana approach bridges will temporarily result in minor impacts to two local trails, the Levee Trail and the Ohio River Greenway Trail, and require Section 4(f) coordination and approval. Both the Levee Trail near Jaycee Park and the Ohio River Greenway Trail terminate within the ROW beneath the Indiana approaches and will be closed for nearly the full duration of construction, approximately July 2021 to June 2023 (Appendix A, page A-18). Please refer to Section 4(f) and Section 6 (f) under the Supplemental Resource Review section below for additional information.

An Intelligent Transportation Systems (ITS) is an advanced application which aims to provide innovative services relating to different modes of transport and traffic management. The ITS will provide real-time sensor data that feeds INDOT's traffic wise mobile and web map. Data is collected from roadway cameras, digital message sign text for each sign in the State, portable digital message signs current major roadway incidents recorded in Hoosier Helper dispatch system, color road segments of highway speeds, and travel time sign values.

The ITS will be installed on the southwest side of I-64 between the railroad tracks and W. Main Street. The type and actual placement of the ITS is not known at this time (Appendix A, page A-18). The Ohio Valley Truck and Tractor (Agency ID No. 107467) is located at 303 to 305 West Main Street, approximately 90 ft. west of this ITS location. Based on the INDOT Red Flag Investigation (RFI), approved by INDOT SAM on April 3, 2019, in February 2014, the Ohio Valley Truck and Tractor facility was issued a Brownfields Petroleum Eligibility Letter, stating the landowners and the New Albany Redevelopment Commission may perform site assessment activities at the property. No further records are available on the VFC. No impact is expected. The RFI can be viewed in the approved October 7th CE appendices (E-1 to E-15) which can be found in the originally approved document currently located on the <https://shermanmintonrenewal.com> website or can be requested by contacting INDOT environmental services.

The ITS will also be installed at the northeast and southwest sides of I-64 between W. Main Street and W. Market Street, between the I-64 westbound on and off ramps, the northeast and southwest sides of I-64 between W. Market Street and W. Spring Street, and between W. Spring Street and the I-64 eastbound on ramp and westbound off ramp. The type and actual placement of the ITS is not known at this time (Appendix A, pages A-18 to A-19). Based on the INDOT RFI approved by INDOT SAM on April 3, 2019, and the INDOT Limited RFI, approved by INDOT SAM on October 11, 2019, there are no hazardous materials within or near the ITS project areas. Therefore, no impact is expected. The RFI and Limited RFI can be viewed in the approved October 7th CE appendices (E-1 to E-22) which can be found in the originally approved document currently located on the <https://shermanmintonrenewal.com> website or can be requested by contacting INDOT environmental services.

Additionally, based on the Waters of the U.S. Determination Report, approved by INDOT Ecology Waterway and Permitting Office (EWPO) on September 20, 2019, there are no water resources within any of the ITS installation locations. Therefore, no impacts are expected. The Waters of the U.S. Determination Report can be viewed in the approved October 7th CE appendices (F-1 to F-54) which can be found in the originally approved document currently located on the <https://shermanmintonrenewal.com> website or can be requested by contacting INDOT environmental services.

There will be a staging area within the Spring Street-I-64 Eastbound (EB) on-ramp (Appendix A, page A-19).

All work will be completed in previously disturbed soils in existing ROW. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10 inches dbh or greater (5:1 mitigation based on the number of large trees).

Justification for Additional Information

Due to the additional work described above, additional services to include Maintenance of Traffic (MOT), Waters Determination / Wetland Delineation, Threatened or Endangered Species, Section 106 Cultural Resources, Section 4(f) / Section 6(f) resources, and Permits were required. No other additions or changes to the project are anticipated.

SUPPLEMENTAL RESOURCE REVIEW:

Public Involvement - Since approval of the original CE on October 7, 2020,

A combined effort of DLZ and C2 Strategic Communications, the following public involvement activities have been conducted, on behalf of the Indiana Finance Authority (IFA).

- On March 16, 2021, the start of bridge inspection was advertised.
- On April 19, 2021, the start of ramp improvements and description of work was advertised.
- On April 22, 2021, a major update was made to the project website.
- On June 9, 2021, a presentation of the project was given in southern Indiana by INDOT.
- On July 12, 2021, an elected officials briefing for Indiana and Kentucky were held virtually.
- On July 21, 2021, an update for start of painting and brief summary of work was advertised.
- On July 30, 2021, upcoming weekend closures was advertised.
- August 13, 2021, the beginning of Phase 1 construction was advertised.

All of the above public involvement advertisements were distributed to multiple print and broadcast media including the following in Louisville, Kentucky: Courier-Journal, Courier-Journal Online Louisville, LouisvilleKY.com, Business First, WFPL-FM, Louisville Defender, WHAS-TV, WHAS-TV Online, WHAS-AM, Total Traffic, Transport Topics, WDRB-TV, The Lane Report, WLKY-TV, Louisville Public Radio, WGTK/WRVI Radio, Kentucky Today, Spectrum News, Spectrum News 1, al dia en America, WAVE-TV, Louisville Business First, LEO Weekly, Oldham Era, as well as The Indianapolis Star, Extol Magazine in Southern Indiana, WRTV Indianapolis Indiana, News and Tribune Associated Press in New Albany and Louisville, and Gov Delivery (C2 Strategic Communications e-newsletter and text updating system).

Additionally, a weekly notice is sent to the SPGC representative of planned construction activities and media updates are provided that will or do identify anticipated construction traffic in neighborhoods on either side of the river. Additional public outreach may be conducted in the future with the neighborhood adjacent to the SPGC.

Maintenance of Traffic (MOT) During Construction

As discussed in the approved CE document, October 7, 2020, six MOT options were developed and evaluated during design engineering, traffic modeling, Community Impact Assessment (CIA) and Environmental Justice (EJ) Analysis. As design progressed, the contractor has proposed to utilize MOT option 1 throughout construction. MOT option 1 consists of the following:

The MOT for the project will require one EB and one westbound (WB) lane will be closed throughout construction. Open travel lanes will shift location on the Sherman Minton Bridge during construction. Two EB and two WB travel lanes will remain open for cross-river traffic and existing access ramps will remain open except for the following allowances:

180 nights per construction year during which two EB and two WB lanes and associated access ramps will be closed each night approximately from 9 pm to 4 am and 10 pm to 5 am, respectively. Cross-river traffic will be maintained with one EB and one WB travel lane open and a temporary crossover lane in Kentucky for I-64 WB to merge with I-264. See Permitted Mainline Interstate Off-Peak Movement Closures below for off peak closures and times.

Table 12-1: Permitted Mainline Interstate Off-Peak Movement Closures

Roadway	From	To	Minimum Number of Lanes Maintained on the Bridges	Number of Off-Peak Periods	Restrictions
I-64 EB	I-265	I-264	1 lane	[360] Prior to Substantial Completion	Between 9:00 p.m. and 5:00 a.m. each night - Sunday night through Thursday night
				[60] After Substantial Completion for approved Punch List work only.	Between 10:00 p.m. and 6:00 a.m. Friday night Between 10:00 p.m. and 9:00 a.m. Saturday night
I-64 WB	I-264	I-265	1 lane	[360] Prior to Substantial Completion	Between 10:00 p.m. and 6:00 a.m. each night - Sunday night through Thursday night
				[60] after Substantial Completion for approved Punch List work only.	Between 10:00 p.m. and 9:00 a.m. Friday night Between 10:00 p.m. and 10:00 a.m. Saturday night

Short-term closure of the Sherman Minton Bridge will be allowed for one (1) nine (9) consecutive day period and up to three (3) weekend closures during each construction year; excluding holidays and community events detailed below. During the short-term bridge closure, all I-64 (US 150) cross-river traffic will be diverted to detour routes.

Table 10-1-1	
Allowable Mainline Interstate Movement Closures	
Segment	Maximum Movement Closures
I-64 EB (from approximate MM 121.4 to approximate 123.8 (Kentucky State Line) and from MM 0.0 (Kentucky State Line) to approximate MM 1.4)	<p>One (1) nine (9) consecutive day closure per calendar year period;</p> <p>Up to three (3) weekend periods per calendar year (Work may commence no earlier than Friday at 10 p.m. and conclude no later than Monday at 5 a.m.); and</p> <p>Subject to the approved IHCP Exception Request obtained in accordance with <u>Section 12.3.11.1.4</u> of the Technical Provisions, one (1) lane for one (1) fifteen (15) consecutive day closure for As-Built Bridge Reference Document Verification inspection.</p>
I-64 WB (from approximate MM 121.4 to approximate 123.8 (Kentucky State Line) and from MM 0.0 (Kentucky State Line) to approximate MM 1.4)	<p>One (1) nine (9) consecutive day closure per calendar year period;</p> <p>Up to three (3) weekend periods per calendar year (Work may commence no earlier than Friday at 10 p.m. and conclude no later than Monday at 6 a.m.); and</p> <p>Subject to the approved IHCP Exception Request obtained in accordance with <u>Section 12.3.11.1.4</u> of the Technical Provisions, one (1) lane for one (1) fifteen (15) consecutive day closure for As-Built Bridge Reference Document Verification inspection.</p>

I-64 EB & WB MOT – Shoulder rehabilitation for travel lane reconfiguration and Indiana Crossover; no pavement footprint widening.

I-64 / I-265 Interchange MOT – Mill and fill shoulder rehabilitation to existing pavement footprint on I-265 WB to I-64 WB and I-64 EB to I-265 EB; existing one-lane ramps will be restriped and converted to 2-lane ramps. The ramps will be returned to their current configurations when construction is complete.

I-65 / I-265 Interchange MOT – Mill and fill shoulder rehabilitation to existing pavement footprint on EB to SB ramp; existing one-lane ramp will be restriped and converted to a 2-lane ramp. The ramps will be returned to their current configurations when construction is complete.

Provisions are included for local traffic access and through-traffic dependent businesses by retaining existing access ramps in Indiana and Kentucky; through-traffic dependent businesses by maintaining cross-river travel lanes in both directions; public notification, signage according to MOT, and posting requirements during construction; and detour routes that remain within the interstate system to alternate local river crossings. The MOT plans are located in Appendix A, pages A-30 to A-53.

Indiana (Alternate Routes)

- I-64 through traffic may use I-265 and the I-65 paired Kennedy/Lincoln bridges.
- Local access to New Albany could follow the same alternate route or remain on the local roadway network; State Street will be the alternate route during Spring Street access ramp closures.

Kentucky (Alternate Route)

- Both I-264 and I-64 through traffic may use I-65 paired Kennedy/Lincoln bridges.
- Local access to west Louisville could follow the same alternate route or remain on the local arterial network.

Minimization measures included additional accommodation for local special events and festivals with the exclusion of bridge closure work during the following:

New Year's Day - If New Year's Day falls on a Sunday, work shall be suspended from noon December 31 until sunrise January 3. or if New Year's Day falls on a Monday through Saturday, work shall be suspended from noon December 31 until sunrise January 2.

Good Friday - Work shall be suspended from noon on Good Friday until sunrise Monday.

Memorial Day - Work shall be suspended from noon the Friday before Memorial Day until sunrise Tuesday, the day after Memorial Day.

Independence Day - If Independence Day falls on a:

Sunday - work shall be suspended from noon Friday, July 2, until sunrise Tuesday, July 6. Monday - work shall be suspended from noon Friday, July 1, until sunrise Tuesday, July 5. Tuesday - work shall be suspended from noon Friday, June 30, until sunrise Wednesday, July 5.

Wednesday - work shall be suspended from sunset on Tuesday, July 3, until sunrise Thursday, July 5. Thursday - work shall be suspended from noon Wednesday, July 3, until sunrise Monday, July 8.

Friday - work shall be suspended from noon Thursday, July 3, until sunrise Monday, July 7. Saturday - work shall be suspended from noon Thursday, July 2, until sunrise Monday, July 6.

Labor Day - Work shall be suspended from noon the Friday before Labor Day until sunrise Tuesday, the day after Labor Day.

Thanksgiving Day - Work shall be suspended from noon the Wednesday before Thanksgiving Day until sunrise the Monday after Thanksgiving Day.

Christmas Day - Work shall be suspended from noon December 24 until sunrise December 27.

Thunder Over Louisville - Work suspended from Midnight Friday till 6:00 a.m. Sunday.

Kentucky Derby -Work suspended from Thursday at midnight until Monday at 6:00 am.

Harvest Homecoming Festival - First Saturday in October to second Saturday in October.

Construction Traffic

The area for construction access traffic was identified in the approved CE-4 on appendix page I-21 as an area where minority and low income (EJ) populations reside.

To access the project beneath the Kentucky approach structures, construction traffic will be routed off of I-264 via the Bank Street Exit. Construction traffic will then utilize N. 38th Street to Northwestern Parkway to the Levee access point, adjacent to the SPGC. Upon leaving the construction project traffic will be routed via Bank Street to I-264.

The temporary impacts to the EJ populations include additional traffic (light to heavy construction vehicles) along N. 38th Street and Northwestern Parkway, which could cause increased vehicular air pollution and noise. The impacts will be temporary in nature, as described in the four phases below:

1. Construction of the access road and aggregate pads under the approach spans
 - Approximately 20 loads of stone / day for 4 days
 - 1 - Cat D6 Dozer stripping topsoil / spreading stone for 4 days
 - 1 - 72" smooth drum roller compacting stone for 4 days
 - Occasional pickup trucks in and out of the site
2. Normal day to day operations throughout the project
 - Water Truck making passes as needed for dust control
 - Occasional Pickup trucks in and out of the site
 - 1 to 2 concrete trucks with light loads (2 CY to 3 CY) for pier cap patching
 - Occasional low boy trailer bringing equipment as needed in an out for KCC and North Star (this will be infrequent after the initial mobilization)
3. Hydro demolition activities
 - 1 Additional water truck emptying frac tanks from hydro-demo operation and hauling offsite
 - Occasional pickup trucks in and out of the site
4. Removal of the access road and aggregate pads and subsequent stabilization
 - 1 - Track-hoe loading dump trucks for 4 days
 - Approximately 20 dump truck loads / day for 4 days
 - 1 - D6 Dozer for 2 days redressing the levee
 - Occasional pickup trucks in and out of the site
 - 1 - Hydro seeder for 1 day to stabilize the levee once we are complete

Most of the construction period, a small number of vehicles will be traveling along N. 38th Street, Northwestern Parkway, and Bank Street each day. Therefore, this would not cause disproportionately high or adverse effects to EJ populations.

Waters Determination / Wetlands Delineation

As discussed in the approved CE document, October 7, 2020, A *Waters of the U.S. Determination / Wetland Delineation Report* was completed by Kaskaskia Engineering for the project area, including both Indiana and Kentucky. The INDOT Ecology and Waterway Permitting Office approved the report on September 20, 2019.

The *Waters of the U.S. Determination / Wetland Delineation Report* identified two wetlands (wetland 10 and wetland 11) beneath the Kentucky approach bridges. At the time the CE document was prepared and approved, no impacts to any wetlands were anticipated. As design progressed, it was determined that access beneath the Kentucky approach bridges will be necessary and will result in temporary placement of fill within wetland 10 and wetland 11 in order to provide stable access for construction equipment. Overall, there will be a total of 975 cubic yards over 0.48 acre of temporary impact to the two wetlands. Upon completion of construction activities all temporary fill materials shall be removed. However, due to the wetlands existing within maintained highway right of way (ROW), USACE requested mitigation associated with these impacts be provided since the restored wetlands cannot be guaranteed to be maintained in perpetuity. The wetlands will be restored using early successional seed mix and will be mitigated with the purchase of wetland bank credits at a 2:1 ratio. (Appendix A, pages A-15 to A-16).

Wetland 10: As depicted in Appendix A, page A-16, wetland 10 is an approximately 0.56 acre palustrine forested wetland located adjacent to the Kentucky east bank of the Ohio River, within the Eastern Mountains and Piedmont Region. A portion of Wetland 10 is classified as a National Wetland Inventory (NWI) wetland, Palustrine Forested Broad-Leaved Deciduous Temporary Flooded Diked/Impounded (PFO1Ah). It is within the floodplain of the Ohio River. Wetland 10 will be temporarily impacted during construction due to the placement of a 1 ft. thick temporary coarse aggregate pad over geotextile fabric resulting in approximately 516 cubic yards of temporary aggregate fill over 0.32 acre.

Wetland 11: As depicted in Appendix A, page A-15, wetland 11 is an approximately 0.60 acre palustrine emergent wetland under the WB lanes of the Sherman Minton Bridge and extending along the north side EB I-64 within the Eastern Mountains and Piedmont region. Wetland 11 is not classified as an NWI wetland. It is within the floodplain of the Ohio River. Wetland 11 will be temporarily impacted during construction due to the placement of a 1 ft. thick temporary coarse aggregate pad over geotextile fabric. In addition, an approximate 12-inch diameter by 252 ft. long temporary steel pipe shall be placed within wetland 11 to maintain drainage through the project area. These temporary impacts will result in approximately 459 cubic yards of fill material over 0.16 acre. Upon project completion, the temporary impacts will be removed.

In conformity with the requirements of Section 401 of the Clean Water Act, a Kentucky General Certification of the Nationwide Permit 14 for linear transportation projects and a USACE 404 Nationwide permit are in the process of being obtained for this work. A Kentucky Department of Water (KY DOW) exemption for the floodplain permit was applied and no permit was deemed necessary for the temporary impacts within the floodway on the Kentucky side of the project.

On March 19, 2021, Metric conducted a field reconnaissance to determine the absence or presence of Waters of the U.S., Waters of the State, and potential wetlands within the levee area to be used for temporary access to the Kentucky approach bridges.

A *Waters of the U.S. Determination / Wetland Delineation Report* was prepared by Metric Environmental, dated March 31, 2021. It was determined no wetlands and no streams were identified within the project study limits. On June 4, 2021, KYTC concurred with Metric's findings in the *Waters Determination Report*. (Appendix D, pages D-1 to D-20).

Terrestrial Habitat

Tree clearing or trimming may become necessary during construction, In the event tree clearing or trimming becomes necessary, coordination shall occur with all appropriate agencies prior to initiating any activities. This firm commitment is included in the Additional Firm Environmental Commitments section of this AI.

Threatened or Endangered Species

Re-coordination was not required with the Indiana Section 7 determination because the impacts are on the Kentucky side. None of the determination key questions change on the Indiana side; therefore, the project is still found to have “no effect” to the Indiana bat and/or the Northern Long-eared bat (Appendix B, pages B-1 to B-3).

On May 19, 2021, Metric Environmental coordinated with KYTC to determine if additional coordination needs to occur for USFWS/IPaC documentation as it relates to the use of the levee access road, wetland impacts or potential temporary lighting on the Kentucky side of the project. On May 26, 2021, KYTC responded the new access along the levee did not change any determination they made in regard to Section 7. Coordination is located in Appendix B, pages B-4 to B-6.

A new species list was generated on May 27, 2021, which identifies the entire project area, including the levee access road, which has been added to this project. The bird, Least tern (*Sterna antillarum*) and clam, Purple cat’s paw (*Epioblasma obliquata obliquata*) are no longer included on the species list. The clam, Pink mucket (*Lampsilis abrupta*) has been added to the species list since originally generated on August 27, 2019. As no work or impacts are proposed within the river, there is no potential for impact to this species, thus no coordination with USFWS has occurred. No critical habitat has been designated for this species in the project area. The updated list of threatened and endangered species list is located in Appendix B, pages B-7 to B-15.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Section 106 Cultural Resources

Due to the subsequent design change in Kentucky, the original Area of Potential Effect (APE) has been widened as part of the “reasonable and good faith effort” that is required by the Section 106 process to identify historic properties (36 CFR Part § 800.4(b)(1)). The APE includes most of the SPGC, an USACE earthen levee, a portion of the Shawnee neighborhood along the Northwestern Parkway corridor, a portion of the Portland neighborhood on the north side of Rudd Avenue, and a large portion of river frontage that contains the McAlpine Locks and Dam Visitors Area, the Portland Wharf Park, and the Sherman Minton approach bridges. Northwestern Parkway and the Portland Historic District are both listed on the NRHP. And, as stated previously, the SPGC and the earthen levee have been previously determined NRHP eligible. Though the McAlpine Locks and Dam Visitors Center is within the APE, the engineering system itself lies outside of the APE, and thus was not evaluated for this project. Since the Sherman Minton Bridge is part of the Interstate system, it has previously been determined not eligible for the NRHP under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005. The APE is located in Appendix C, pages C-32 to C-33.

Archaeological Investigation Summary:

A Qualified Professional archaeologist from Metric Environmental, who meets the Secretary of the Interior’s Professional Qualifications Standards for Section 106 work per 36 CFR Part § 61, reviewed the proposed newly added project area by conducting a literature review. The expanded APE required for the temporary access road encompasses the full limits of the levee including 6.77 acres located immediately adjacent to and southwest of the area examined during the original archaeological investigation for the project in 2020. Based on review of the documentation Metric received regarding the previous investigation, there are no previously recorded archaeological resources within the additional acreage nor any sites eligible for listing in the National Register of Historic Places (NRHP) within its vicinity.

The Qualified Professional conducted a literature review utilizing online information from the United States Department of Agriculture, Natural Resource Conservation Service Web Soil Survey, as well as historical county soil surveys. Mapped soils within the additional acreage consist entirely of Urban land-Udorthents complex, smoothed, 0 to 50 percent slopes; udorthents comprise a deep and very deep mixture of geologic and artificial materials that have been graded and smoothed. The 6.77 acres that encompass the levee have been heavily disturbed from the levee's construction, and the levee itself is composed of artificial fill. For these reasons, Metric Environmental determined no archaeological investigations were warranted as the area within the expanded APE was not considered to have the potential to contain archaeological resources.

An archaeological summary report was submitted to the KYTC in early May 2021, and on May 13, 2021, the KYTC agreed with the findings and recommended that a "No Historic Properties Affected" determination was still appropriate for the undertaking (Appendix C, pages C-85 to C-90). On May 17, 2021, the Kentucky Heritage Council (KHC) also gave their concurrence with this determination (Appendix C, page C-86).

Above Ground Investigation Summary:

A Qualified Professional historian who meets the Secretary of the Interior's Professional Qualifications Standards for Section 106 work per 36 CFR Part § 61, conducted a literature review of the project area and established a revised APE to include the addition of the levee access road. The revised APE was submitted to KYTC on May 24, 2021, for review and submittal to the KHC for their review and comment. In a letter dated June 3, 2021, KHC acknowledged receipt of the revised APE from the KYTC and expressed their office's concurrence with the APE's adequacy for this undertaking (Appendix C, page C-91). The KHC "strongly suggest(s) avoidance/minimization to avoid the NRHP-eligible or listed resources within the proposed APE" to avoid adverse effects to those resources.

On June 29, 2021, an on-site meeting was held to discuss potential impacts to historic properties within the APE. The meeting summary is located in Appendix F, pages F-1 to F-3.

The Qualified Professional recommended the following resources within the APE as NRHP eligible: the Shawnee Golf Course, as a contributing resource to the previously-listed Olmsted Park System of Louisville Historic District (NPS File No. 82002715) under Criteria A and C; the earthen levee west of I-64 as a contributing resource for the proposed Louisville Reach of the Louisville Metro Flood Protection System HD, under Criteria A; the Shawnee Residential Historic District under Criteria A and C; and the continuing NRHP eligibility of the Portland Historic District (NPS File No. 80001615). Please see Appendix C, pages C-16 to C-24.

There are no anticipated physical changes occurring to the Shawnee Golf Course and Northwestern Parkway, Shawnee Residential Historic District, and the Portland Historic District as part of this undertaking, and no temporary or permanent right-of-way will be acquired. Construction traffic will utilize Northwestern Parkway at certain times during construction. Kokosing will document existing roadway conditions via video camera before construction begins and will compare and make patches as needed after the project is completed.

Impacts to the earthen levee include temporary physical changes due to the construction of a temporary access road. The proposed access road to be built on top of the levee would be constructed of temporary gravel aggregate over geotextile fabric, and it would measure 12 ft. wide, one ft. thick, and 1,440 ft. long. The stripped topsoil will be stored and stabilized for the duration of the project. Total temporary right-of-way needed for the access road is approximately 6.77 acres. Upon completion of construction activities, the aggregate and geotextile fabric would be removed, and the topsoil that had been placed in storage would be replaced. The topsoil would be graded and seeded to restore it to pre-construction conditions. This will

include seeding the topsoil. No permanent alterations or impacts to the levee are anticipated. A water truck will be on site to manage dust control and will be used on an as-needed basis.

Based upon the project's temporary and minor impacts to the NRHP-eligibility resources within the APE, the recommended effect finding for the project was No Adverse Effect. Please see Appendix C, pages C-24 to C-29.

The additional project information, including NRHP Evaluation and Effects Determination Recommendations was provided to the KHC for a 30-day review and comment period on July 16, 2021. In a letter dated July 27, 2021, the KHC provided concurrence with a revised conditional finding of No Adverse Effect, which includes the following:

- A revised Cultural Historic that addresses the issues outlined via email on July 27th, 2021, is submitted and accepted by KHC office no later than six months from the date of this letter. KHC office is withholding comment on eligibility discussions included within the report until the revised copy is received. This will include a paper copy of the revised report, if requested by KHC office.
- Survey forms, likely to be submitted by a different consultant, are submitted and accepted by KHC office for all resources within the APE no later than one year from the date of this letter. This should be coordinated with KHC office, and follow the guidelines discussed via email on July 27th, 2021. This will include a paper copy of the survey forms, if requested by KHC office.
- Use of the NRHP-eligible SPGC, Shawnee Residential Historic District and Northwestern Parkway are temporary, with any damages to the roadway caused by construction vehicles to be repaired by the end of the project.
- Use of the NRHP-eligible Louisville Floodwall levee for construction vehicle access shall protect the resource from adverse effect, with no more than one foot of topsoil removed from the levee, before the aggregate and geotextile road is set up. All area of removed matter shall be returned to the state in which it was found at the end of the project duration, conforming to pre-existing dimensions including height, width, and slope of the levee, as this will be temporarily altered by the construction of a road for contractor access. This shall be confirmed with SHPO at the end of this undertaking.

The KHC letter response is located in Appendix C, pages C-1 to C-2.

Section 4(f) de minimis impacts to historical resources (SPGC, Levee, Northwestern Parkway, Shawnee Residential and Portland Historic Districts) did not require notification due to an existing Memorandum of Understanding (MOU) between KHC and KYTC (Appendix C, pages C-128 to C-129).

Section 4(f)

Kentucky: There are two Section 4(f) resources located within the project area which include the SPGC and Louisville Loop (Appendix A, page A-3 and A-16).

In a letter, dated August 2, 2021, the KY FHWA proposed the Section 4(f) impact to the SPGC qualifies for a Temporary Occupancy Exemption, because the impact is minor, the cart path will be returned to its pre-construction condition, and the impact lasts less than the entire duration of the project, which is from July 2021 to August 2023. It is proposed the impact to the Louisville Loop will also qualify as a Temporary Occupancy Exemption since the trail is not open to the public at this time, no permanent changes will occur, and that status will not change. This effectively means there is 'no use' of either resource. On August 2, 2021, Louisville Parks and Recreation Department, the official with jurisdiction (OWJ) over the SPGC

including the Louisville Loop for Section 4(f), concurred with the Section 4(f) determination of temporary occupancy ‘no use’ (Appendix F, pages F-4 to F-5).

Indiana: There are two Section 4(f) resources located within the project area which include the Levee Trail and Ohio Greenway Trail (Appendix A, page A-3 and A-18).

In a letter dated August 10, 2021, the City of New Albany, official with jurisdiction (OWJ) over the Levee Trail and Ohio River Greenway Trail, understands that the *de minimis* use of the Section 4(f) resource is necessary and is in the opinion that such work will not permanently adversely affect the activities, features, or attributes of the recreational resource. The OWJ concurs with the FHWA Section 4(f) *de minimis* finding for this project (Appendix F, pages F-6 to F-7).

A public notice was advertised in the News and Tribune newspaper on July 13, 2021, describing the temporary effects of the Levee Trail and the Ohio River Greenway Trail, within the Indiana side of the SMCP. The Proof of Publication and Public Notice are located in Appendix F, pages F-8 to F-9. No comments were received by the comment deadline date of July 26, 2021.

Section 6(f)

As discussed in the approved CE document, dated October 7, 2020, the Shawnee Park Pavilion and the SPGC Maintenance Building within the SPGC are Section 6(f) resources. The Louisville Loop is attached to the SPGC and therefore, should be considered part of the Section 6(f) resource. These properties are owned by the City of Louisville and operated by the Louisville Parks and Recreation.

On July 12, 2021, FHWA proposed the Section 6(f) finding for both the SPGC and Louisville Loop resources will be ‘no conversion’ because there will be no constructive changes to the current use of either facility. There will be no conversion of recreational land to a non-recreational use, and no change in ownership. The trail is closed to the public and will remain closed.

On July 20, 2021, LWCF “Recreational Trails Program, Federal Program Specialist, Kentucky Department for Local Governments concurred with the Section 6(f) ‘no conversion’ for the SPGC including the Louisville Loop (Appendix F, page F-11).

Addendum to Red Flag Investigation (RFI)

Indiana:

As the original RFI for this project was approved by INDOT Site Assessment and Management (SAM) on April 1, 2019, more than two years ago, Metric Environmental conducted a review of the findings to evaluate any changes.

On June 4, 2021, Metric Environmental coordinated with INDOT SAM regarding the evaluation. There was no new infrastructure, water resources, mining and mineral resources, and hazardous material concerns within or near the project area. On June 16, 2021, INDOT SAM responded that it does not appear an RFI Addendum is warranted. Coordination is located in Appendix E, page E-1 to E-2.

Kentucky:

Construction of the access road along the levee will require excavation of approximately 1 ft.; however, the levee is approximately 30 ft. above ground level. Therefore, no coordination with Kentucky Energy and Environment Cabinet was conducted. No impacts of hazardous materials are anticipated.

Permits

A US Army Corps of Engineers (USACE) Section 408 levee permit and Louisville MSD Flood Protection System Modification permit are in the process of being obtained due to performing work on the levee on the Kentucky side of the project. The Louisville MSD Flood Protection System Modification permit will provide temporary permission to access and temporarily alter the levee for construction activities.

Although impacts will occur within the floodplain on the Kentucky side of the project, correspondence was received from KY DOW indicating that the proposed construction activities are exempt from the KY DOW floodplain permit. This email correspondence is included in Appendix B, pages B-16 to B-17.

A KY DOW Section 401 general permit has been obtained and a USACE Section 404 Water Quality Certification is in the process of being obtained for the temporary impacts to regulated wetlands on the Kentucky side of the project.

There will be earth disturbance, greater than 1 acre, associated with the MOT and staging within the I-264 and I-64 interchange ramps. A National Pollutant Discharge Elimination System (NPDES) General Permit was approved via the Kentucky KYR10 permit process. Permit coverage extends from July 23, 2021 through July 23, 2023. An extension of permit coverage will be requested prior to expiration due to construction activities planned to extend through September 2023. In addition, a Louisville MSD local site disturbance permit is in the process of being obtained for the disturbed areas on the levee that extend beyond the KYTC ROW.

An Indiana Department of Natural Resources (IDNR) Construction in a Floodway (CIF) permit is in the process of being obtained for temporary impacts beneath the Indiana approach bridges.

A USACE Section 408 levee permit is in the process of being obtained due to performing work within 50 ft. of the levee on the Indiana side of the project.

Earth disturbance for the project will result in more than one (1) acre of disturbance; therefore, an Indiana Department of Environmental Management (IDEM) Rule 5 Notice of Intent is in the process of being obtained for the project.

Additional Firm Environmental Commitments:

1. In the event tree clearing or trimming becomes necessary, coordination shall occur with all appropriate agencies prior to initiating any activities. (KYTC and INDOT).
2. A revised Cultural Historic Report that addresses the issues outlined by KY-SHPO in email communication to KYTC on July 27, 2021 shall be provided to KY-SHPO no later than January 27, 2022. The revised report will be drafted by Metric Environmental on behalf of Kokosing Construction Company and submitted by KYTC to KY-SHPO.
3. Survey forms for all historical resources within the revised APE shall be submitted and accepted by the KY-SHPO no later than July 27, 2022. Submittal of the survey forms shall be coordinated with KY-SHPO prior to submittal and shall follow the guidelines discussed in email communication to KYTC on July 27, 2021. This will be the responsibility of KYTC.
4. Any damages incurred by construction vehicles to the NRHP-eligible Northwestern Parkway shall be repaired prior to project completion and shall be the responsibility of Kokosing Construction Company.
5. The NRHP-eligible Louisville Floodwall levee is to be used for temporary vehicle access throughout construction. No more than one foot of topsoil is to be removed from the levee to facilitate construction operations. All area of removed matter shall be returned to the state in which it was found prior to project completion, conforming to pre-existing dimensions including height,

width, and slope of the levee. This shall be the responsibility of Kokosing Construction Company and shall be confirmed by KY-SHPO prior to project completion.

CONCLUSIONS:

Adding the temporary access road in Kentucky and the staging areas and ITS in Indiana to this project has no effect. Adding the staging area beneath the Kentucky approach bridges will temporarily impact approximately 0.32 acre of wetland 10 and 0.16 acre of wetland 11. No additional impacts are expected during construction of this project.

All other proposed improvements identified in the approved CE document, dated October 7, 2020, remain unchanged. Therefore, supplemental review of environmental resources has been completed.

Name and organization of AI Preparer: Susan Castle, Metric Environmental, LLC

Approval	Brandon Miller <small>Digitally signed by Brandon Miller Date: 2021.09.07 13:51:34 -04'00'</small>	<u>9/7/2021</u>
	INDOT ESD Signature	Date
	Erica Tait <small>Digitally signed by Erica Tait Date: 2021.09.08 13:55:27 -04'00'</small>	<u>9/8/2021</u>
	INDIANA FHWA Signature	Date
	<i>David Harmon</i>	<u>9/7/2021</u>
	KYTC DEA Signature	Date

APPENDIX C: MOT References



SHERMAN
MINTON
RENEWAL

MAINTENANCE

OF TRAFFIC APPROACH

2020

INTRODUCTION

The Sherman Minton Renewal is a \$90+ million bridge rehabilitation and painting project that will significantly extend the service life of the 57-year-old bridge.

The double-decked bridge carries six lanes of traffic (I-64 and US 150) over the Ohio River connecting Louisville, KY and New Albany, IN.

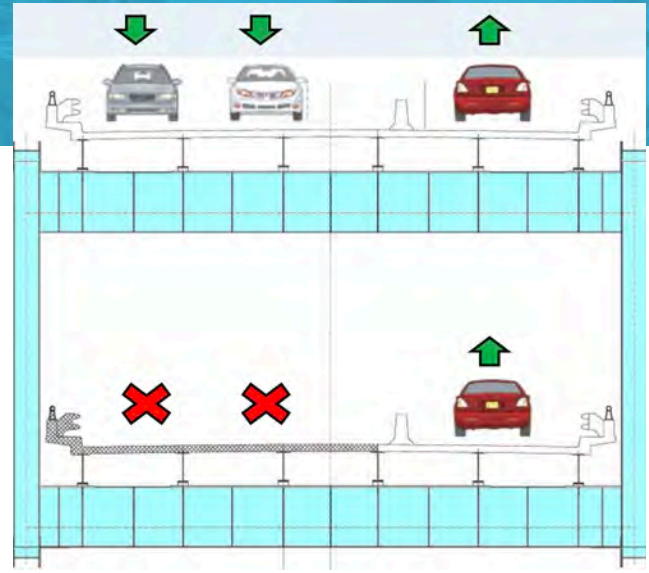
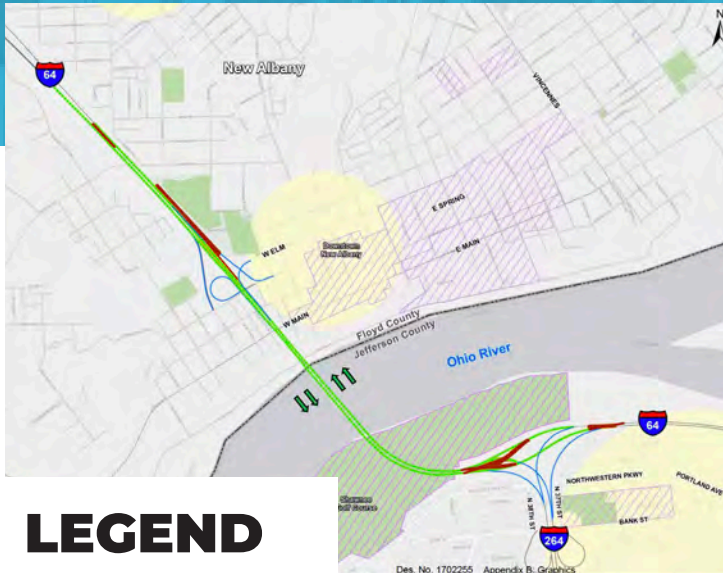
The project includes replacement or refurbishment of all bridge decks, rehabilitation or replacement of structural steel elements and hanger cables, new lighting, drainage repairs and painting of steel components. The long-term repairs, along with normal preventive maintenance, will add up to 30 years of service life to the bridge.

MAINTENANCE OF TRAFFIC PREFERRED ALTERNATIVE







The recommended approach reflects the public's preference for maintaining access across the bridge to the fullest extent possible during the estimated three-year construction process, while providing a safe environment for the driving public and the design-build contractor's team.

The Preferred Alternative will ensure at least one lane of traffic in each direction will remain open for nearly 95% of the construction process. Closures will not be in one consecutive period, but will be limited to 9 consecutive days per direction in a calendar year and up to three 3-day weekends per direction per calendar year. The recommended plan will limit full closure of the bridge to only 54 total days of the estimated 834 total days of work.

- ▶ Cross-river traffic will be maintained with two eastbound and two westbound travel lanes open during the majority of construction.
- ▶ Temporary crossovers and widening to ramp lanes will facilitate traffic flow.
- ▶ One eastbound and one westbound lane will be closed throughout construction.
- ▶ Existing access ramps will remain open during the majority of construction.
- ▶ Two eastbound and/or two westbound lanes and associated access ramps may be closed nightly during construction for up to 180 nights per calendar year.



LEGEND

- | | | | |
|---|--------------------|---|---------------------------|
|  | Reduced to 2-lanes |  | Historic District |
|  | Existing Ramp |  | Park |
|  | Temporary Pavement |  | Business/Community Center |

Short-Term Full Closures and Overnight Closures (In Each Direction)

Short-term closure of the Sherman Minton Bridge will be allowed for nine consecutive days for one period and up to three 3-day weekends per direction per calendar year. During the short-term bridge closure, the I-64 (US 150) cross-river traffic will be diverted to alternate interstate river crossings.

During construction, two eastbound and/or two westbound lanes and associated access ramps may be closed nightly.

Maintaining Access and Notifications

- ▶ Cross-river travel lanes will be maintained in both directions.
- ▶ Existing access ramps will be maintained for local traffic access in Kentucky and Indiana.
- ▶ Ramp closures and second lane restrictions will occur overnight.
- ▶ Many public notification methods will be used during construction, including the project website, social media channels, traditional media and traffic signage.

Holidays and Special Events

The design-build contractor will be permitted to work 24 hours per day, seven days per week. Full closures and added restrictions will not be allowed during holidays and select community events.

DETOUR OPTIONS



Indiana

- ▶ I-64 through traffic will be detoured via I-265 to the I-65 paired Kennedy/Lincoln bridges.
- ▶ Local access to New Albany would follow the same detour route.



Kentucky

- ▶ I-264 and I-64 through traffic will be detoured to the I-65 paired Kennedy/Lincoln bridges.
- ▶ Local access to west Louisville would follow the same detour route.

These detour options for Indiana and Kentucky will be in place during full closures.

SUBMIT COMMENTS OR QUESTIONS

To submit a comment or question about the Sherman Minton Renewal project, please visit www.shermanmintonrenewal.com or call the project hotline at **(502) 329-8320**.



/ShermanMintonRenewal



@ShermanRenewal



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Sherman Minton Renewal



PRSR STD
US POSTAGE
PAID
PERMIT NO. 555
LOUISVILLE, KY

Michael Baker International

Attention: Mary Jo Hamman
1650 Lyndon Farm Ct. #101
Louisville, KY 40223



COMMENTS

PROJECT	DESIGNATION
1702255	1702255
CONTRACT	
B-40719	

INDIANA DEPARTMENT OF TRANSPORTATION



MAINTENANCE OF TRAFFIC PLANS BUILDABLE UNIT 2B

ROUTE: I-64 AT: SHERMAN MINTON BRIDGE

PROJECT NO. 1702255 P.E.

NO ADDITIONAL RIGHT OF WAY
REQUIRED FOR THIS PROJECT

R/W

1702255 CONST.

Project Description: Ramp Modifications for the Rehabilitation of Sherman Minton Bridge over Ohio River and associated Indiana and Kentucky Approach Bridges Section 2, T-3-S, R-6-E, New Albany Township, Floyd County, Indiana



SHERMAN MINTON BRIDGE
REHABILITATION



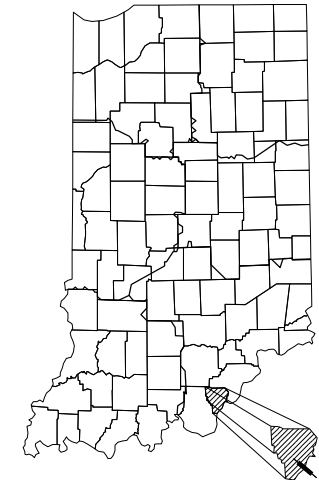
SCALE: 1" = 2000'

TRAFFIC DATA (I-64)

A.A.D.T.	(2018)	45,200 V.P.D. (EB)	44,800 V.P.D. (WB)
A.A.D.T.	(2031)	45,490 V.P.D. (EB)	42,500 V.P.D. (WB)
D.H.V	(2031)	4,090 V.P.H. (EB)	3,830 V.P.H. (WB)
DIRECTIONAL DISTRIBUTION		50.3 % EB	
TRUCKS		11 % A.A.D.T. 6 % D.H.V.	

DESIGN DATA

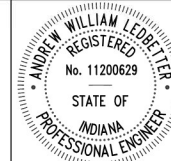
DESIGN SPEED	SEE GENERAL NOTES AND INDEX SHEET
PROJECT DESIGN CRITERIA	3R (FREEWAY)
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL
RURAL/URBAN	URBAN
TERRAIN	LEVEL
ACCESS CONTROL	FULL



PROJECT LOCATION SHOWN BY
FLOYD COUNTY

LATITUDE: 38°16'43" N LONGITUDE: 85°49'20" W

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS



PLANS PREPARED BY: Jacobs Engineering (314) 335-4000
PHONE NUMBER
CERTIFIED BY: Andrew M. Ledbetter 8/20/2021
DATE
RECOMMENDED FOR LETTING: _____
INDIANA DEPARTMENT OF TRANSPORTATION DATE

DESIGNATION	
1702255	
BUILDABLE UNIT	SHEETS
2B	1 of 59
CONTRACT	PROJECT
B-40719	1702255

REVISIONS

SHEET NO.	DATE	DESCRIPTION
2-1	12/16/21	Sheet Added
40-1	12/16/21	Sheet Added

REV	DATE	DESCRIPTION
1	12/16/21	Sheet Added

Page 82 of 186



RECOMMENDED FOR APPROVAL	<i>Andrew W. Ledbetter</i>	12/16/2021
	DESIGN ENGINEER	DATE
DESIGNED: AWL	12/16/21	DRAWN: AWL 12/16/21
CHECKED: DPS	12/16/21	CHECKED: DPS 12/16/21

INDIANA DEPARTMENT OF TRANSPORTATION
REVISIONS

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	2-1 of 59
CONTRACT	PROJECT
B-40719	1702255

UTILITIES

Kentucky Utilities

Communication AT&T - Distribution Scott Roche 3719 Bardstown Rd Louisville, KY 40218 (502) 348-4528 SR8832@att.com KYTC KYTC Lighting Lonny Hamblin (502) 338-5273 lonny.hamblin@ky.gov	Gas and Electric Louisville Gas and Electric Caroline Justice 820 W. Broadway Louisville, KY 40202 (502) 627-3708 Caroline.Justice@LGE-KU.com KYTC Traffic Signals (Outside of Jefferson Co.) David Huber david.huber@ky.gov	Water and Sewer Louisville Water Eric Walls 550 S. Third Street Louisville, KY 40202 (502) 333-4858 ewalls@lwcky.com MSD David Mulloy/Josh Dickerson 700 W. Liberty Street Louisville, KY 40203 (502) 540-6615 david.mulloy@louisvillemsd.org Local Louisville Metro Traffic Signals Stacy Keith stacy.keith@louisvilleky.gov Jason Yeager jason.yeager@louisvilleky.gov (502) 794-6928
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Indiana Utilities

Gas and Electric Centerpoint Energy Distribution Timmy Langston (812) 348-6703 timothy.langston@centerpointenergy.com Centerpoint Energy Transmission Raj Brahmabhatt, PE 28100 Torch Pkwy Warrenville, IL 60555 (630) 225-6086 rbrahmbhatt@enengineering.com Duke Energy Distribution Quentin Knight quentin.knight@duke-energy.com (812) 277-3112 Duke Energy Transmission Zach Boston dei-tline-coord@duke-energy.com (317) 838-1053 Duke Energy Telecom Chris Gere (980) 373-6842 Chris.Gere@duke-energy.com	Water Ed Bergsiker ed.bergsiker@amwater.com (317) 626-8560 Communication Centurylink (Lumen) Rob Smith 1409 E. Grace Street Rensselaer, IN 47978 relocations@Lumen.com Nationalrelo@centurylink.com MCI/Verizon Rick Pickens rick.pickens@tcscomm.com (502) 791-2725 Zayo Waylon Higgins 9209 Castlegate Drive Indianapolis, IN 46256 INKY_relo@zayo.com Windstream Jeremy Smiddie jeremy.smiddie@windstream.com (618) 383-4032	Local Housing Authority - New Albany Dwayne Gadd (812) 948-2319 dgadd@nahain.com City of New Albany Scott Wilkinson (812) 948-5320 swilkinson@cityofnewalbany.com INDOT INDOT ITS Drew Sorenson 8620 E. 21st Street Indianapolis, IN 46219 ITS_Locates@indot.in.gov INDOT Traffic Signal Steve Gann (812) 524-3777 sgann@indot.in.gov
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INDIANA UNDERGROUND
1-800-382-5544 OR CALL 811
24 HOURS A DAY 7 DAYS A WEEK

Not all utilities listed are members of the Indiana Underground (811) locate system. Contractor is to verify all utilities have been contacted before excavations can proceed.

- Phase 1**
Construction Activities
1) Remove concrete median barrier between EB and WB I-64 Sta. 123+00 to Sta. 128+63 on the Indiana side of the Sherman Minton Bridge and construct crossover.
2) Construct crossover on the Kentucky side of the Sherman Minton Bridge between EB and WB I-64.
3) Complete Sherman Minton Bridge repairs and associated I-64 roadway work on the 2 closed left lanes of EB I-64 (Lower Deck).
Traffic Revisions
1) Close 1 lane of WB I-64 to allow for 1 lane of EB I-64 in the WB I-64 lanes.
2) Utilize crossovers and shift 1 lane of EB I-64 to WB I-64.
3) Close 2 left lanes of EB I-64 across the Sherman Minton Bridge.
4) Utilize nighttime lane closures to close the lower deck of the Sherman Minton Bridge. Use INDOT Standard Drawing E 801-TCLC-01 and close the right lane of EB I-64 beginning at Sta. 198+00 "B". Maintain exit 123 to Spring Street. Close EB I-64 On-ramp at Spring Street.
- Phase 2**
Construction Activities
1) Maintain crossovers constructed in Phase 1.
2) Complete Sherman Minton Bridge repairs and associated I-64 roadway work on the remaining right lane of EB I-64 (Lower Deck).
Traffic Revisions
1) Maintain 1 lane of EB I-64 traffic in the WB direction from Phase 1.
2) Shift 1 lane of EB I-64 traffic across the Sherman Minton Bridge to the newly constructed EB lanes from Phase 1.
3) Close 2 right lanes of EB I-64 across the Sherman Minton Bridge.
4) Utilize nighttime lane closures to close the lower deck of the Sherman Minton Bridge. Use INDOT Standard Drawing E 801-TCLC-01 and close the right lane of EB I-64 beginning at Sta. 198+00 "B". Maintain exit 123 to Spring Street. Close EB I-64 On-ramp at Spring Street.

GENERAL NOTES

- Traffic shall be maintained in accordance with the Indiana Manual on Uniform Traffic Control Devices (2011 Edition), the InDOT Construction Standards and Project Technical Provisions. Daytime and nighttime shoulder closures, lane closures, and other temporary operations not detailed in the traffic control plans shall follow the standard details found in the InDOT Manual on Uniform Traffic Control Devices (2011 Edition).
- Along I-64 a 15-mile-per-hour speed reduction within the work zone shall be posted when a single 11 foot lane is maintained in any direction. A 10 mile-per-hour speed reduction within the work zone shall be posted in all other instances along I-64.
- The minimum MOT lane width shall be 11 feet on I-64, I-264, and I-265.
- The minimum MOT lane width for multiple lane ramps shall be 11.5 feet.
- The minimum MOT lane width for single lane ramps shall be 16 feet.
- The minimum MOT lane width on local streets shall match existing lane widths up to 11 feet.
- The minimum MOT lane width within multi-lane shifts on interstate mainline and ramps shall be 13 feet.
- A minimum horizontal clearance of 1 foot between barrier and edge of travel lane shall be provided.
- Opposing traffic lanes of I-64, I-264, or I-265 traffic shall be separated by Type 1 traffic barrier if the existing open median width is reduced or otherwise cannot be provided.
- For TTB used to separate two directions of traffic in a crossover, glare screens shall be used to eliminate headlight glare.
- Design-Build Contractor shall maintain positive drainage on all active travel lanes during Construction Work.
- Barriers shall not impede snow removal operations outside the limits of the Project. To facilitate snow removal operations by INDOT and KYTC, from November 1 through March 31 each year, Design-Build Contractor shall maintain open areas along interstate routes and ramps matching existing shoulder widths. Within the limits of the Project, shoulders may be reduced to the widths shown in the Design-Build Contractor's approved MOT plans, while work is progressing.
- Design-Build Contractor shall remove all existing rumble strips and replace with asphalt pavement prior to moving any traffic onto shoulders. Vehicles cannot straddle rumble strips at any point of construction. Before substantial project completion, all rumble strips shall be replaced in their existing location.
- Existing striping which conflicts with temporary markings within 12-feet of construction shall be water blasted and removed prior to any traffic shifts. All areas shown in the Design-Build Contractor's approved MOT plans shall be returned to their original configuration prior to Final Acceptance.
- All existing raised pavement markers within 12-feet of construction shall be disabled prior to any traffic shifts.
- Temporary minimum vertical clearance shall be 14'-6" for I-64 during construction.
- Access to all businesses and residences shall be maintained at all times.
- Design-Build Contractor shall maintain existing or detoured pedestrian and/or bicycle access on all sidewalks, trails, transit facilities and at all intersections that are open to traffic. Design-Build Contractor shall also maintain safe access and passage for all pedestrian facilities.
- Design-Build Contractor shall provide an alternate accessible pedestrian route that complies with the MUTCD, PROWAG, and ADA requirements where accessible pedestrian and/or bicycle routes are allowed to be closed by Design-Build Contractor during construction.
- Design-Build Contractor shall not park motor vehicles or construction vehicles, locate construction signs, or place equipment on a pedestrian sidewalk or path open to pedestrian traffic; or use the pedestrian sidewalk or path for loading operations, stockpiling of materials, or allowing demolished or spoil materials to be deposited on the surface of a pedestrian and/or bicycle sidewalk or path.
- Design-Build Contractor shall provide temporary access for the Shawnee Golf Course through the Work area under the Kentucky Approach Bridge at all times. If Work areas encroach upon the golf course access, a minimum clear width of 15 feet wide by 16 feet high shall be provided.
- All temporary golf course access points shall be coordinated with the management of the Shawnee Golf Course a minimum of 14 days prior to implementation of temporary access.
- Design Build Contractor shall always maintain two I-64 traffic lanes in each direction on the Indiana Approach Bridges, the Sherman Minton Bridge, and the Kentucky Approach Bridge. However, I-64 may be reduced to one lane in each direction during off-peak hours per Table 12-1 of the Project Technical Provisions shown on this sheet.
- Use InDOT Type B borrow material per 211.02 and compacted per 211.04 to construct temporary crossover in Kentucky.

INDEX

SHEET NO.	DRAWINGS INDEX
1	TITLE
2	INDEX AND GENERAL NOTES
3	MAINTENANCE OF TRAFFIC - TYPICALS - PHASE 1
4	MAINTENANCE OF TRAFFIC - TYPICALS - PHASE 2
5	MAINTENANCE OF TRAFFIC - KEY MAP
6 - 19	MAINTENANCE OF TRAFFIC - I-64 PLAN SHEETS - PHASE 1
20 - 24	MAINTENANCE OF TRAFFIC - I-64 PLAN SHEETS - PHASE 2
25	MAINTENANCE OF TRAFFIC - DETAIL - KENTUCKY CROSSOVER
26 - 27	MAINTENANCE OF TRAFFIC - DETOUR - WB I-64 CLOSURE
28 - 29	MAINTENANCE OF TRAFFIC - DETOUR - EB I-64 CLOSURE
30 - 38	MAINTENANCE OF TRAFFIC - DETAILS - ZONEGUARD BARRIER
39	MAINTENANCE OF TRAFFIC - DETAILS - TEMPORARY BRIDGE BARRIER TRANSITION
40	MAINTENANCE OF TRAFFIC - DETAILS - TEMPORARY BARRIER CONNECTOR
41 - 49	MAINTENANCE OF TRAFFIC - CROSS SECTIONS - KENTUCKY CROSSOVER
50 - 59	MAINTENANCE OF TRAFFIC - CROSS SECTIONS - INDIANA CROSSOVER

DESIGN SPEEDS

Roadway	Posted Speed	Construction Speed
I-64	55 MPH	45 MPH*
I-65	65 MPH	65 MPH
I-264	55 MPH	55 MPH
I-265	65 MPH	65 MPH
Ramp A (I-64/I-265)	40 MPH	30 MPH
Ramp C (I-64/I-265)	40 MPH	30 MPH
Ramp EB-R-FUT/ SW-R (I-265/I-65)	45 MPH	35 MPH
Ramp A1	25 MPH	25 MPH
Ramp B1	25 MPH	25 MPH
Ramp C1	40 MPH	40 MPH
Ramp D1	45 MPH	45 MPH
Ramp W-S (I-64/I-264)	50 MPH	50 MPH
Ramp PR S-W (I-264/I-64)	45 MPH	45 MPH
Ramp E-S (I-64/I-264)	45 MPH	45 MPH
Ramp S-E (I-264/I-64)	45 MPH	45 MPH
Local Streets	25 MPH	25 MPH

*See General Note 2.

INDOT STANDARD DRAWINGS

INDOT Standard Drawings (2020) used:
 E 715-SLDR Slotted Drain Pipe
 E 720-EDCA Earth Ditch Casting Type 7
 E 720-INST Inlet Standard
 E 801-TCCO Temporary Construction Crossover
 E 801-TCDV Traffic Control Devices
 E 801-TCLC Traffic Control Lane Closures
 E 801-TCLG Traffic Control Legend
 E 801-TCSC Traffic Control Shoulder Closure
 E 801-TCSN Traffic Control Signs
 E 808-DLIM Dotted Line Markings

Table 12-1 Permitted Mainline Interstate Off-Peak Movement Closures

Roadway	From	To	Minimum Number of Lanes Maintained on the Bridges	Number of Off-Peak Periods	Restrictions
I-64 EB	I-265	I-264	1 Lane	[360] Prior to Substantial Completion [60] After Substantial Completion for approved Punch List work only.	Between 9:00 p.m. and 5:00 a.m. each night - Sunday night through Thursday night Between 10:00 p.m. and 6:00 a.m. Friday night Between 10:00 p.m. and 9:00 a.m. Saturday night
I-64 WB	I-265	I-264	1 Lane	[360] Prior to Substantial Completion [60] After Substantial Completion for approved Punch List work only.	Between 10:00 p.m. and 6:00 a.m. each night - Sunday night through Thursday night Between 10:00 p.m. and 9:00 a.m. Friday night Between 10:00 p.m. and 10:00 a.m. Saturday night

During the permitted off-peak closure(s) of I-64 EB or WB as defined in Table 12-1, Design Build Contractor shall close the following ramps:

- For the I-64 EB closure from the interchanges with I-265 to I-264: EB on ramp from West Spring Street and off ramp to I-264 East.
- For the I-64 WB closure from the interchanges with I-265 to I-264: WB on ramp from I-264 West and off ramp to West Elm Street



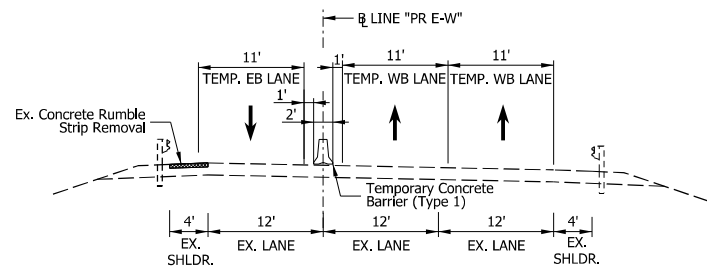
RECOMMENDED FOR APPROVAL	<i>Andrew W. Williams</i>	9/3/2021
DESIGNED:	DPS 4/20/21	DRAWN: DPS 4/20/21
CHECKED:	AWL 4/23/21	CHECKED: AWL 4/23/21

INDIANA DEPARTMENT OF TRANSPORTATION

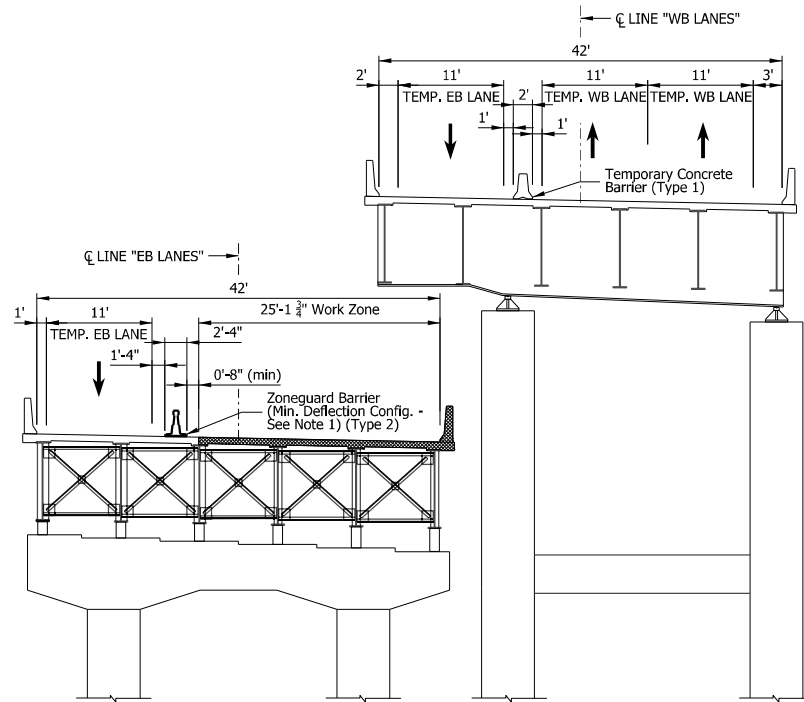
INDEX AND GENERAL NOTES

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION
	1702255
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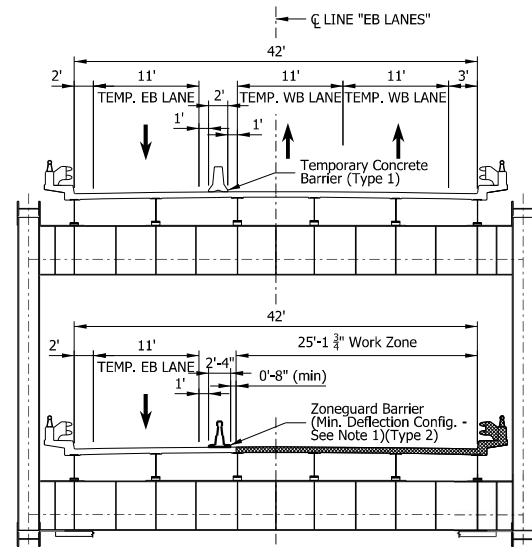
- (A) SHOULDER STRENGTHENING**
1.5" Mill; Overlay with 165 lbs/syd QC/QA-HMA, 4, 76, Surface, 9.5 mm
- (B) FULL DEPTH SHOULDER REPLACEMENT**
165 lbs/syd QC/QA-HMA, 4, 76, Surface, 9.5 mm on 275 lbs/syd QC/QA-HMA, 4, 76, Intermediate, 19.0 mm on 1100 lbs/syd QC/QA-HMA, 4, 64, Base, 25.0 mm on Subgrade Treatment, Type ID



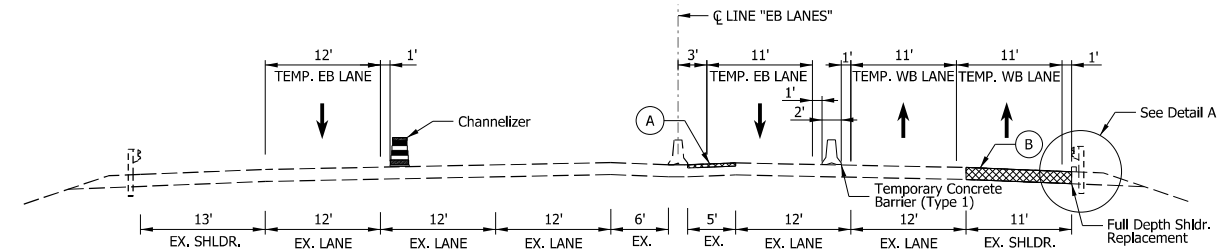
I-64 WB SECTION
STA. 361+00 "PR E-W"



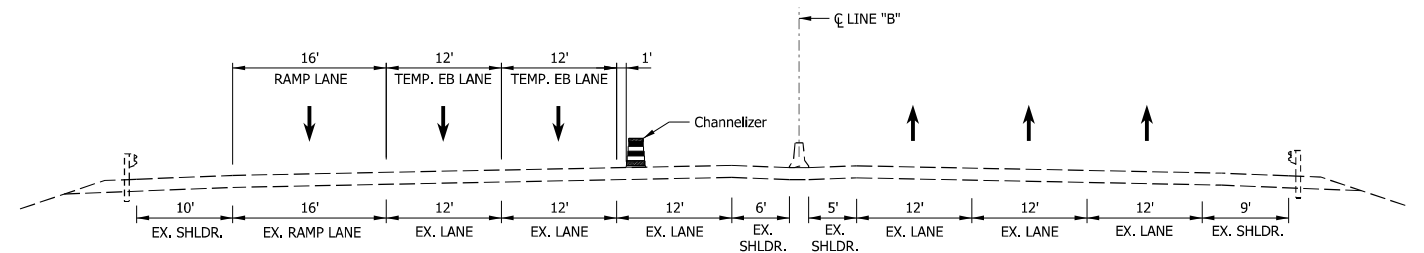
BRIDGE SECTION
STA. 78+00 "EB LANES"



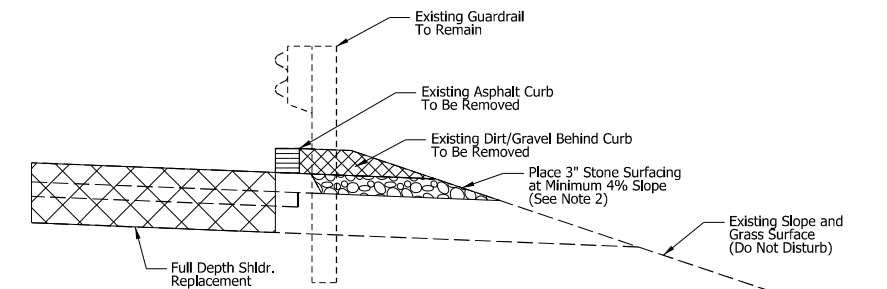
BRIDGE SECTION
STA. 90+00 "EB LANES"



I-64 SECTION
STA. 120+00 "EB LANES"



I-64 SECTION
STA. 134+00 "B"



DETAIL A
STA. 117+20 TO STA. 126+28 "EB LANES"

- Notes:**
- Zoneguard barrier shall be anchored per manufacturer's installation instructions for Minimum Deflection Configuration.
 - Stone surfacing shall be placed as soon as feasible following excavation of the top of the embankment in order to minimize the potential for erosion of exposed soils.

P10:19-AUG-2021 08:22 AM

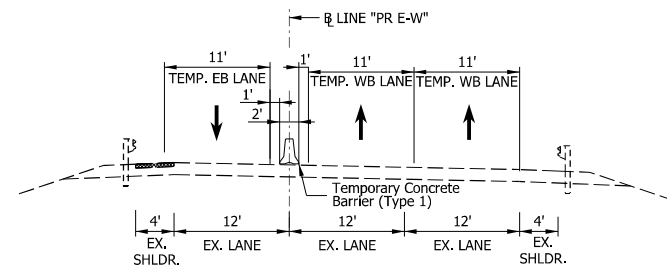


RECOMMENDED FOR APPROVAL	<i>Andrew M. Lepletter</i>	DESIGN ENGINEER	8/20/2021	DATE
DESIGNED:	DPS	3/22/21	DRAWN:	DPS 3/22/21
CHECKED:	AWL	3/26/21	CHECKED:	AWL 3/26/21

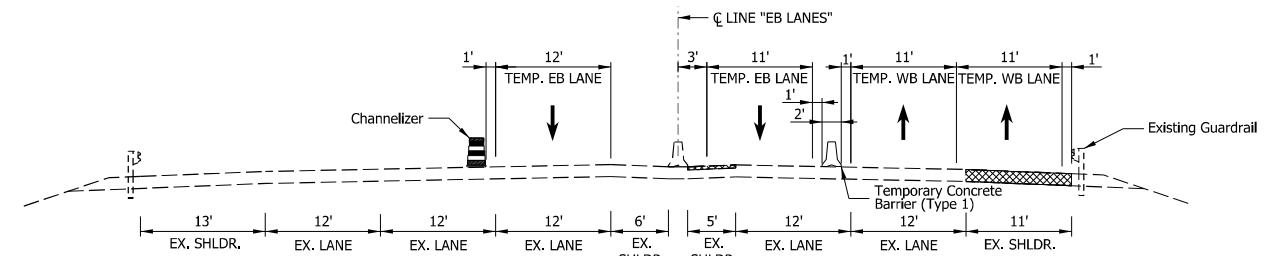
INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 1
TYPICAL SECTIONS

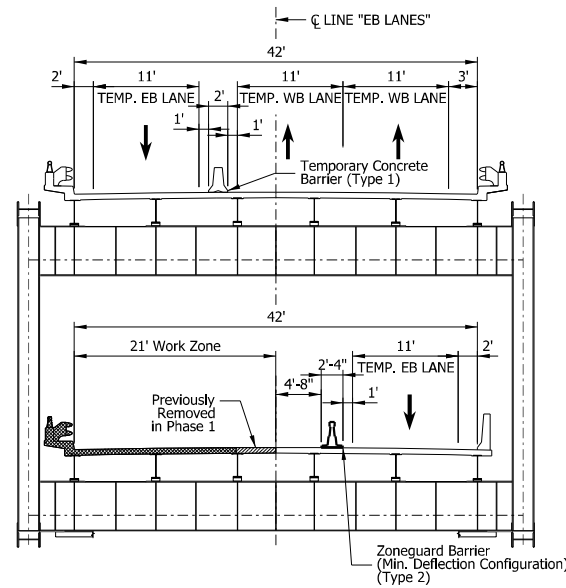
HORIZONTAL SCALE	BRIDGE FILE
1"=10'	
VERTICAL SCALE	DESIGNATION
	1702255
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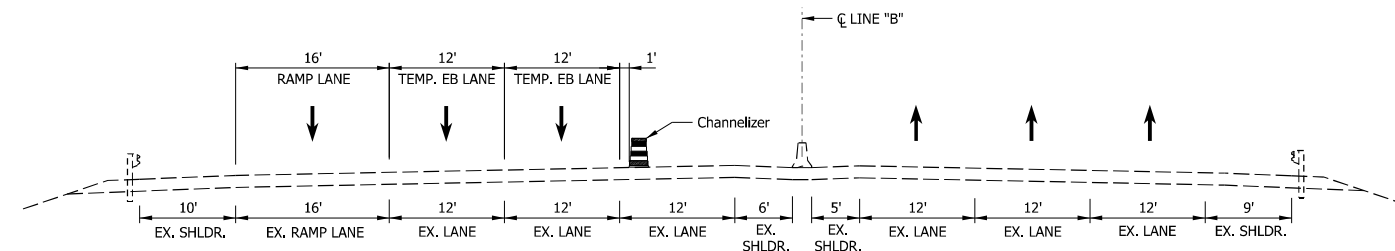
I-64 WB SECTION
STA. 361+00 "PR E-W"



I-64 SECTION
STA. 120+00 "EB LANES"



BRIDGE SECTION
STA. 90+00 "EB LANES"



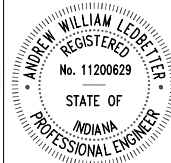
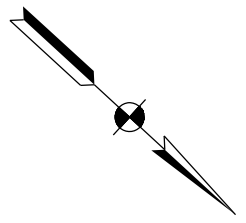
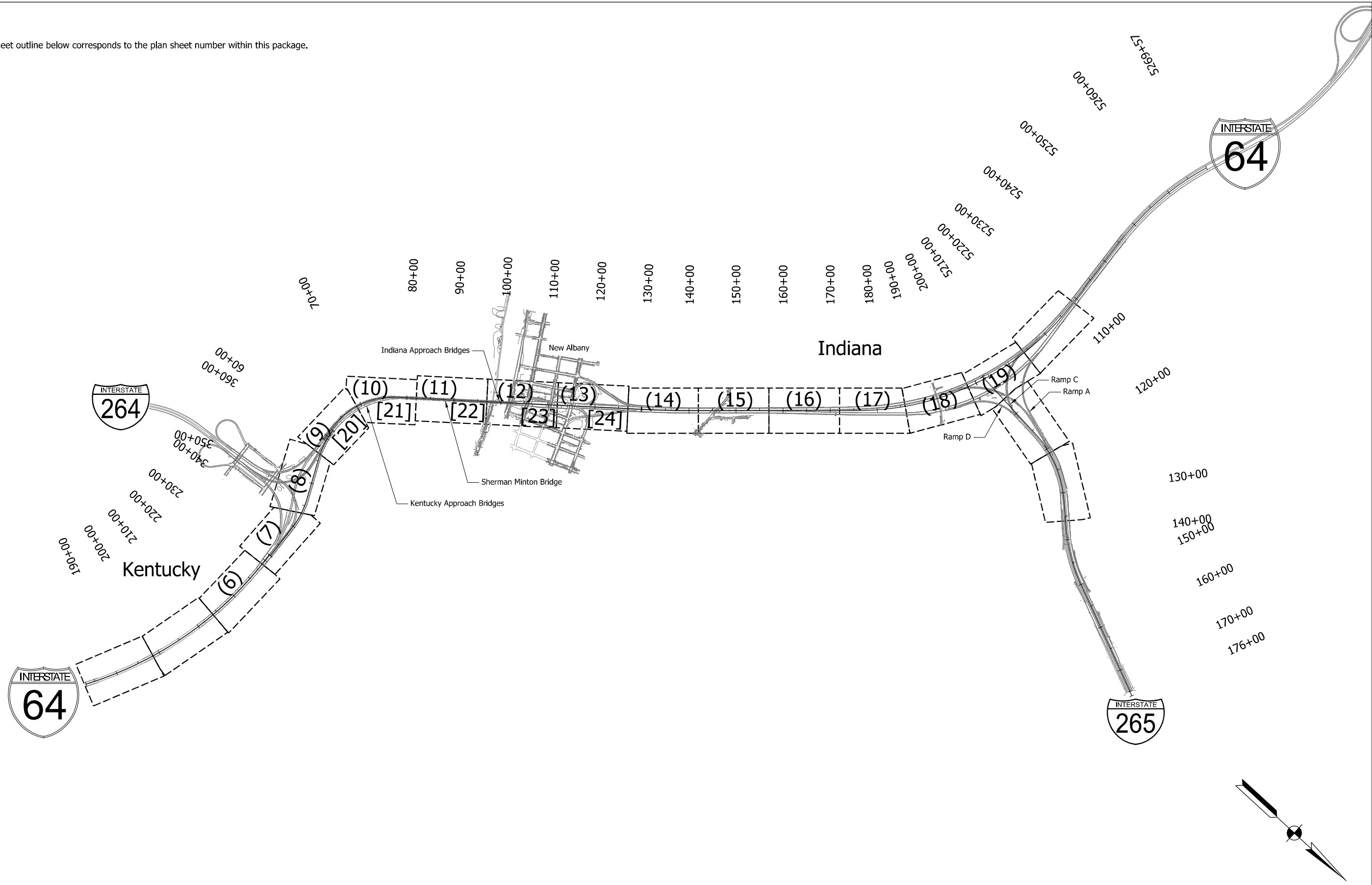
I-64 SECTION
STA. 134+00 "B"

Note:

1. The number within the sheet outline below corresponds to the plan sheet number within this package.

Legend

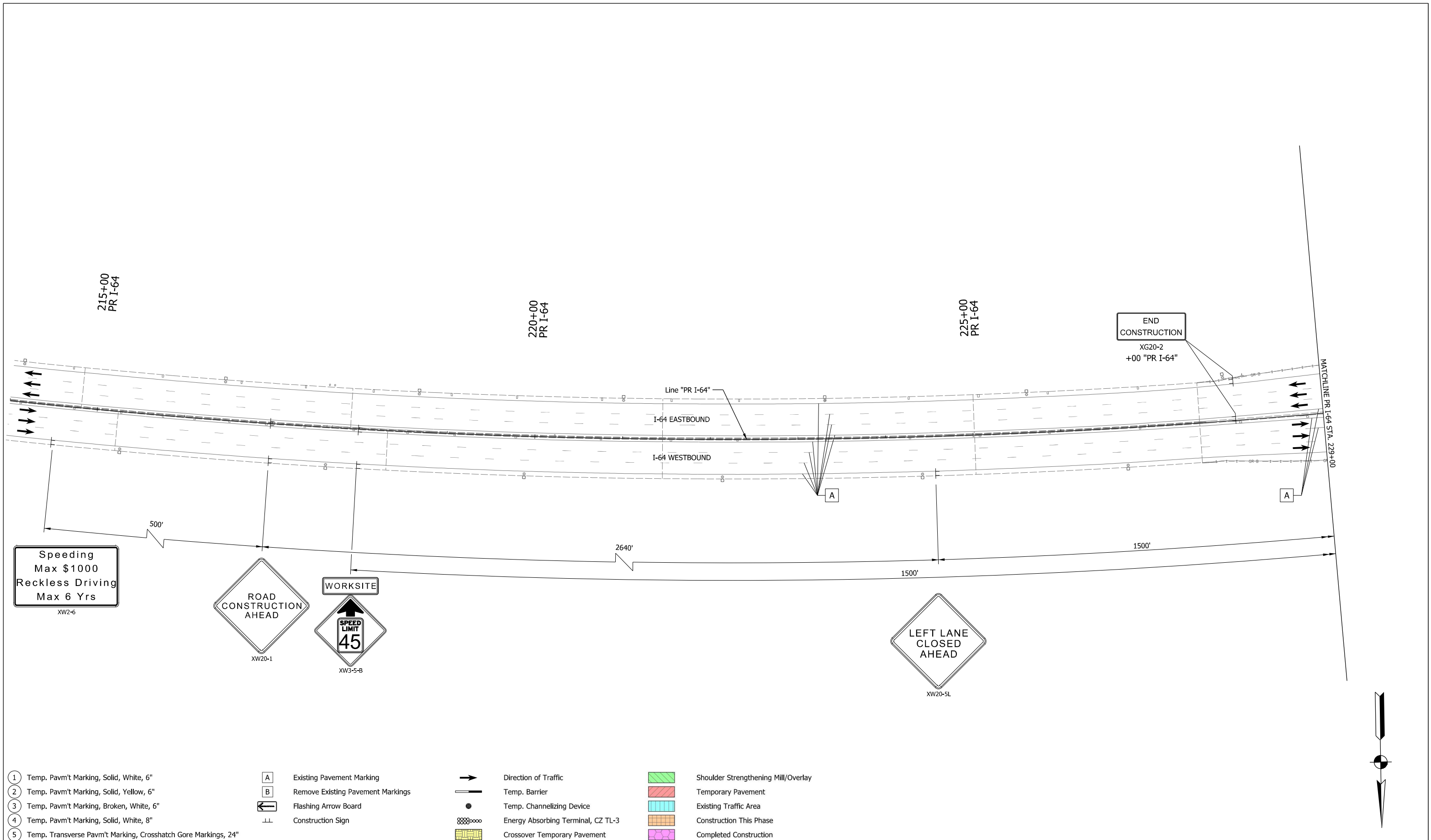
- (XX) Phase 1 Sheets
- [XX] Phase 2 Sheets



RECOMMENDED FOR APPROVAL	<i>Andrew M. Helber</i>	8/20/2021
DESIGNED: DPS	6/24/21	DRAWN: DPS 6/24/21
CHECKED: AWL	6/25/21	CHECKED: AWL 6/25/21

INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC KEY MAP

HORIZONTAL SCALE	BRIDGE FILE
1"=1000'	
VERTICAL SCALE	DESIGNATION
	1702255
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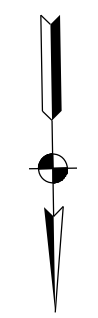


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ← Flashing Arrow Board
- LL Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- ▬ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction



Plot: 19-AUG-2021 08:24 AM

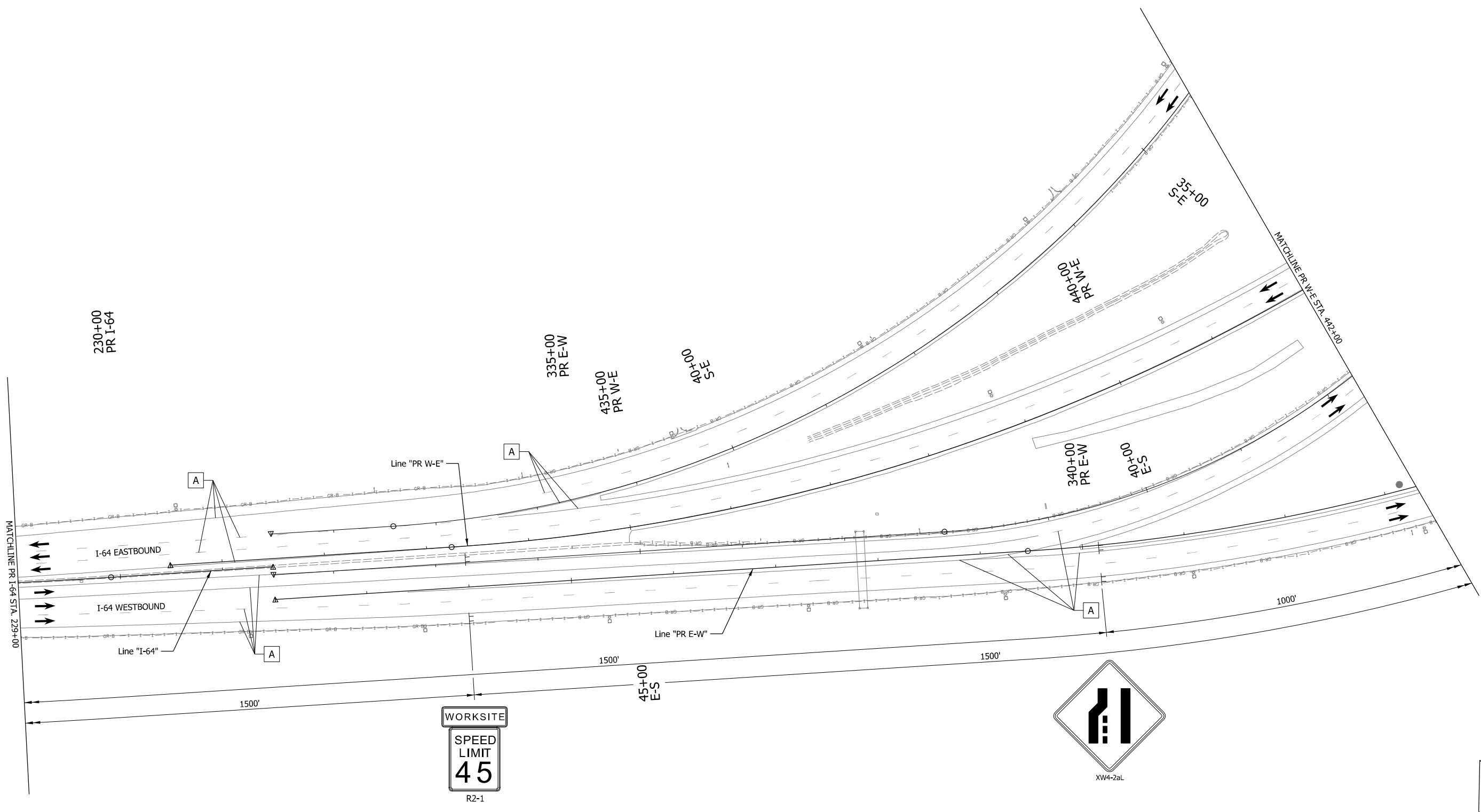


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: DPS	3/11/21	DRAWN: DPS 3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 215+00 TO STA. 229+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	6 of 59
CONTRACT	PROJECT
B-40719	1702255



- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



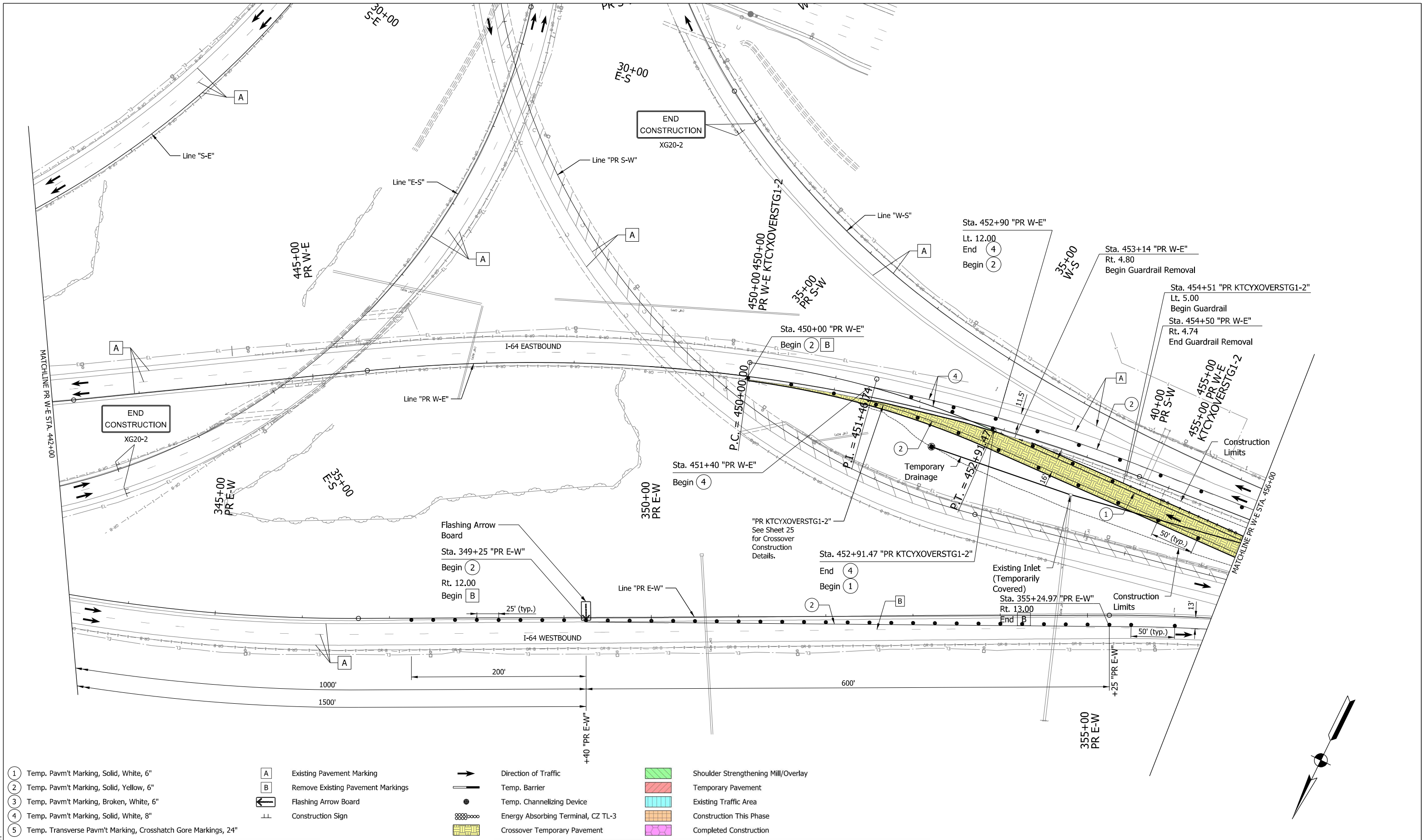
RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: DPS	3/11/21	DRAWN: DPS 3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 229+00 TO STA. 442+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	7 of 59
CONTRACT	PROJECT
B-40719	1702255

PLOT:19-AUG-2021 08:24 AM

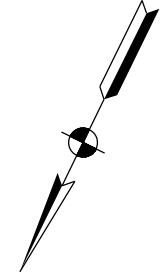


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



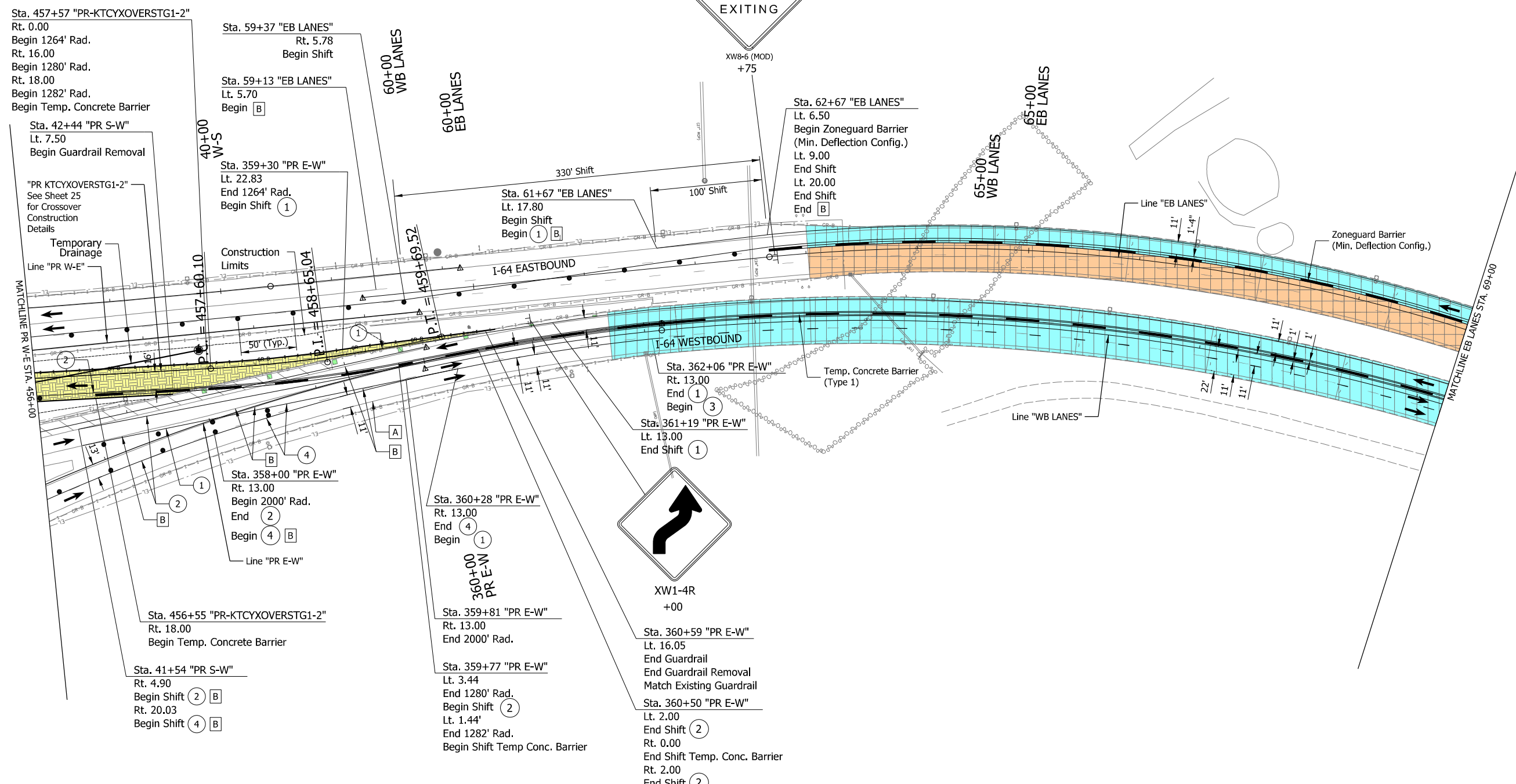
RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: DPS	3/11/21	DRAWN: DPS 3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 442+00 TO STA. 456+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	8 of 59
CONTRACT	PROJECT
B-40719	1702255

Plot: 19-AUG-2021 08:25 AM



- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ↔ Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ▨ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction



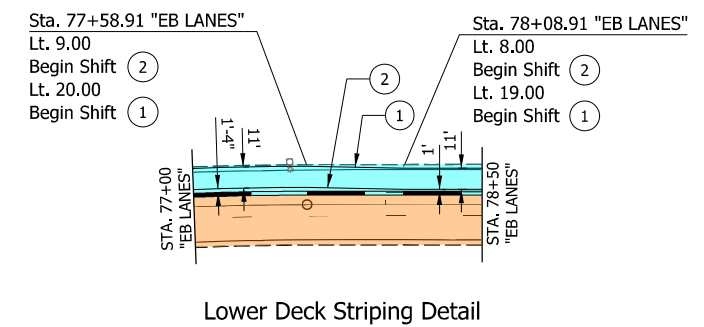
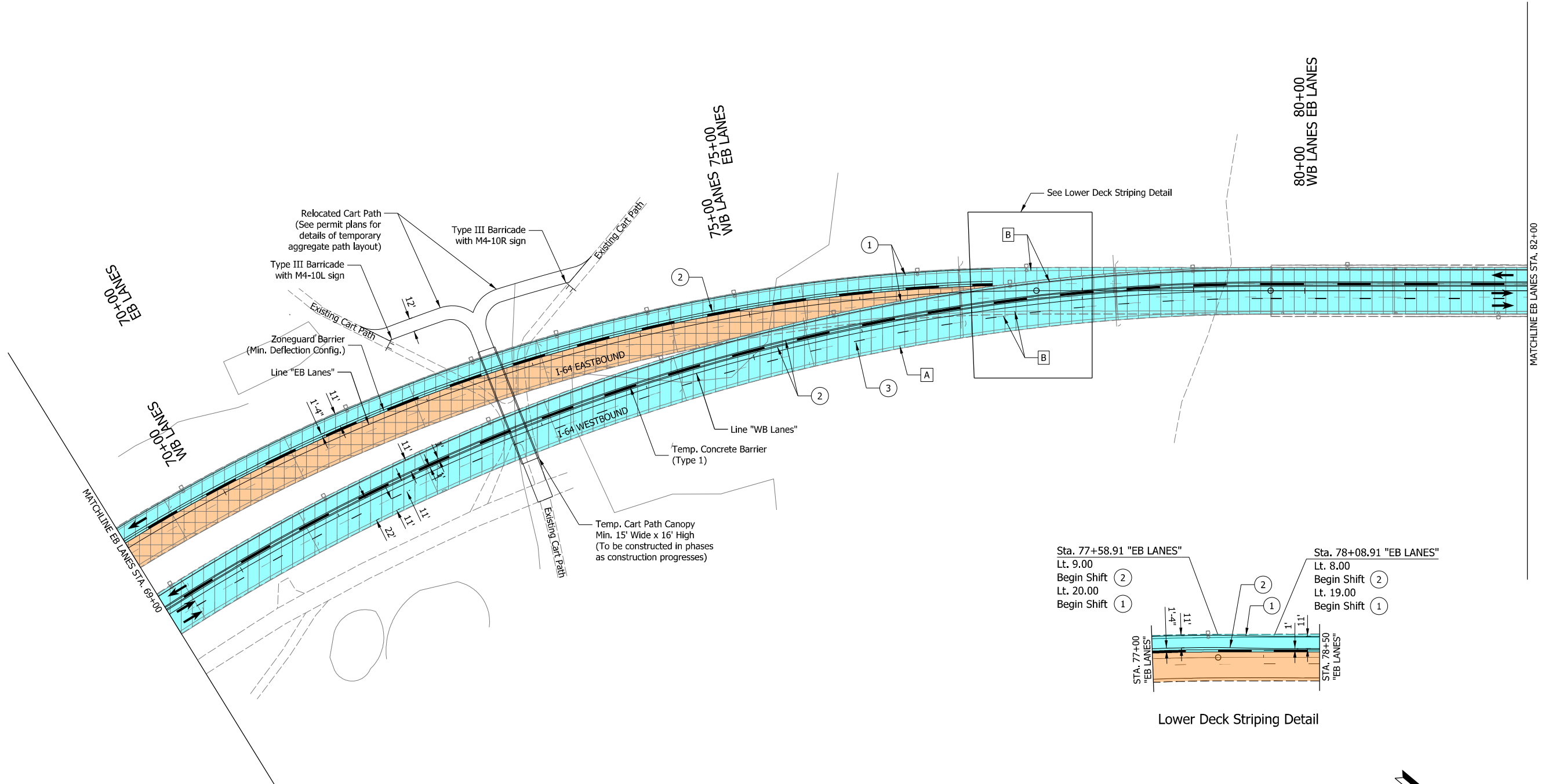
RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: DPS	3/11/21	DRAWN: DPS 3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 456+00 TO STA. 69+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	9 of 59
CONTRACT	PROJECT
B-40719	1702255

Plot: 19-AUG-2021 08:26 AM

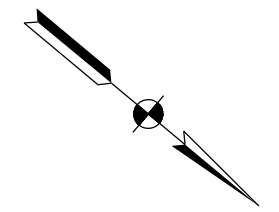


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction

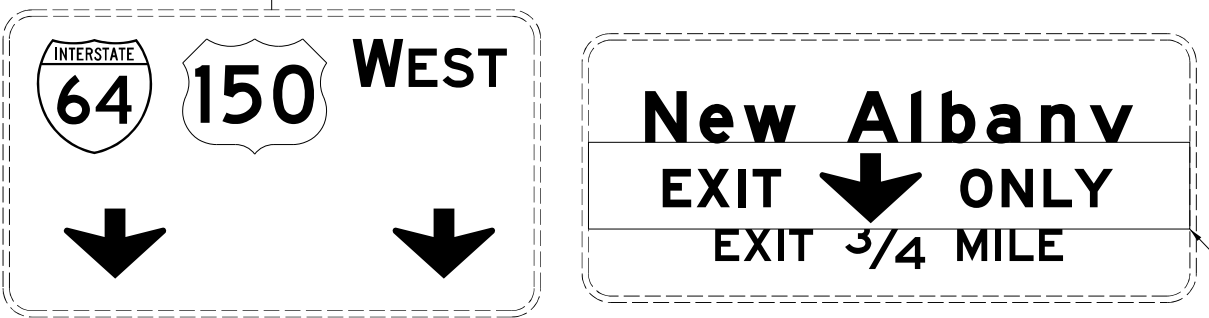
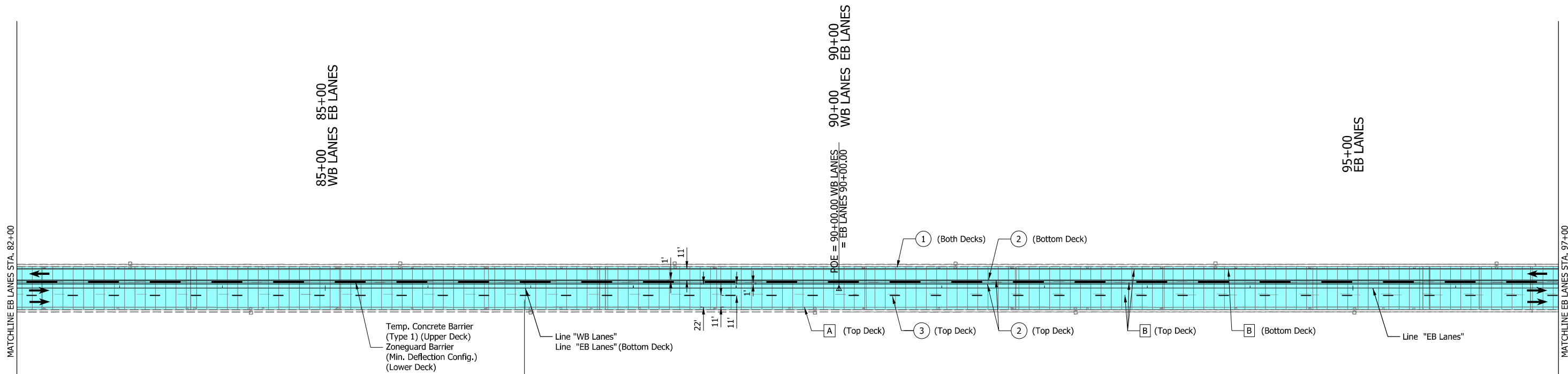


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: DPS	3/11/21	DRAWN: DPS 3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

INDIANA
DEPARTMENT OF TRANSPORTATION

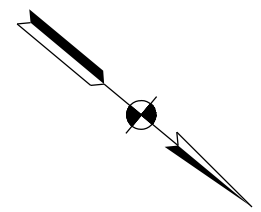
MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 69+00 TO STA. 82+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	10 of 59
CONTRACT	PROJECT
B-40719	1702255



- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign
- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement
- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



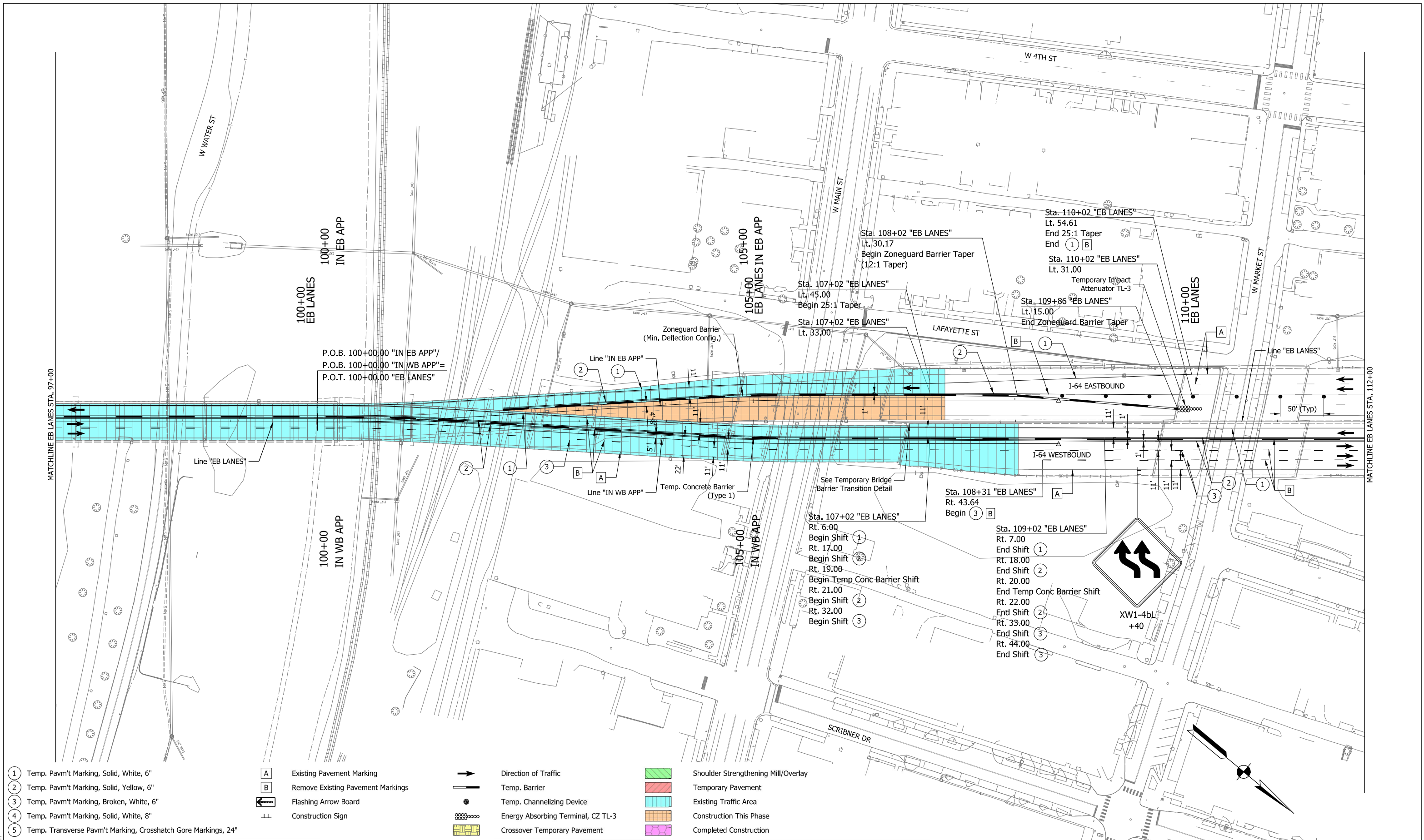
RECOMMENDED FOR APPROVAL	<i>Andrew W. Leffler</i>	8/20/2021
DESIGNED: DPS	3/11/21	DRAWN: DPS 3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 82+00 TO STA. 97+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	11 of 59
CONTRACT	PROJECT
B-40719	1702255

Plot: 19-AUG-2021 08:27 AM



- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
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- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ↔ Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ▨ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction



RECOMMENDED FOR APPROVAL	<i>Andrew W. Helbert</i>	DESIGN ENGINEER	8/20/2021	DATE
DESIGNED: DPS	3/11/21	DRAWN: DPS	3/11/21	
CHECKED: AWL	3/26/21	CHECKED: AWL	3/26/21	

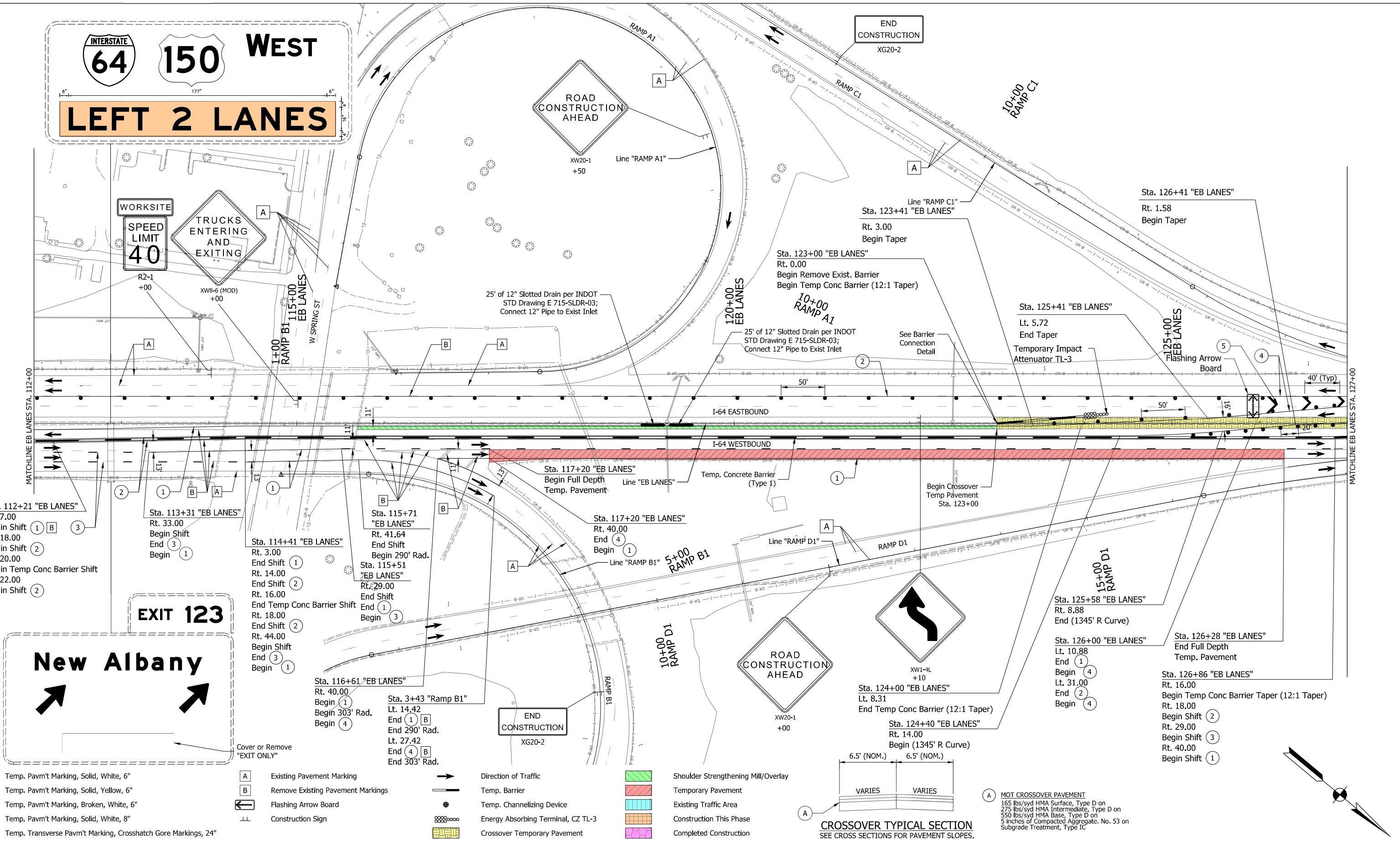
INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC MOT PHASE 1
STA. 97+00 TO STA. 112+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	12 of 59
CONTRACT	PROJECT
B-40719	1702255

P:\20-AUG-2021 08:48 AM

INTERSTATE 64 WEST
 150 WEST
LEFT 2 LANES

WORKSITE
 SPEED LIMIT 40
 TRUCKS ENTERING AND EXITING



- Sta. 112+21 "EB LANES"
Rt. 7.00
Begin Shift ① B ③
- Rt. 18.00
Begin Shift ②
- Rt. 20.00
Begin Shift ②
- Begin Temp Conc Barrier Shift
Rt. 22.00
Begin Shift ②

- Sta. 113+31 "EB LANES"
Rt. 33.00
Begin Shift ③
- End Shift ①
- Begin ①

- Sta. 114+41 "EB LANES"
Rt. 3.00
End Shift ①
- Rt. 14.00
End Shift ②
- Rt. 16.00
End Shift ②
- End Temp Conc Barrier Shift
Rt. 18.00
End Shift ②
- Rt. 44.00
Begin Shift ③
- End ③
- Begin ①

- Sta. 115+71 "EB LANES"
Rt. 41.64
End Shift ④
- Begin 290' Rad.
Rt. 29.00
End Shift ①
- End ①
- Begin ③

- Sta. 117+20 "EB LANES"
Rt. 40.00
End ④
- Begin ①

- Sta. 116+61 "EB LANES"
Rt. 40.00
Begin ①
- Begin 303' Rad.
End ④
- Begin ④

- Sta. 3+43 "Ramp B1"
Lt. 14.42
End ① B
- End 290' Rad.
Lt. 27.42
End ④ B
- End 303' Rad.

- Sta. 125+58 "EB LANES"
Rt. 8.88
End (1345' R Curve)
- Sta. 126+00 "EB LANES"
Lt. 10.88
End ①
- Begin ④
- Lt. 31.00
End ②
- Begin ④

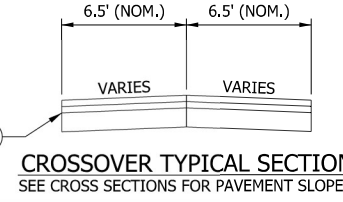
- Sta. 126+28 "EB LANES"
End Full Depth
Temp. Pavement
- Sta. 126+86 "EB LANES"
Rt. 16.00
Begin Temp Conc Barrier Taper (12:1 Taper)
Rt. 18.00
Begin Shift ②
- Rt. 29.00
Begin Shift ③
- Rt. 40.00
Begin Shift ①

- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



MOT CROSSOVER PAVEMENT
 165 lbs/syd HMA Surface, Type D on
 275 lbs/syd HMA Intermediate, Type D on
 550 lbs/syd HMA Base, Type D on
 5 inches of Compacted Aggregate. No. 53 on
 Subgrade Treatment, Type IC

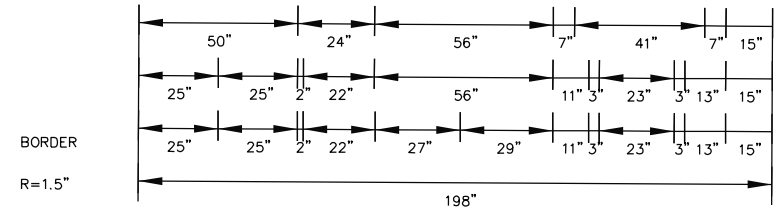
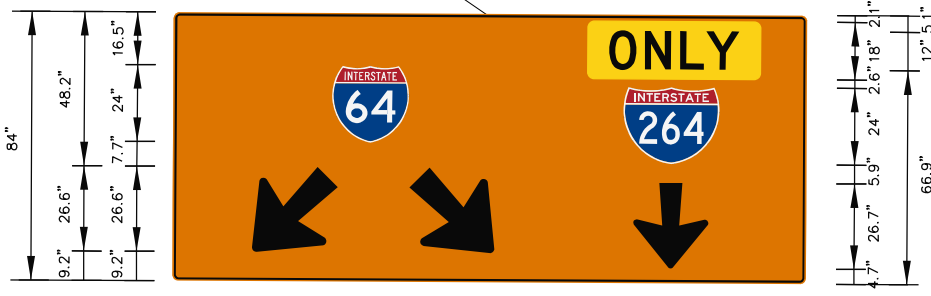
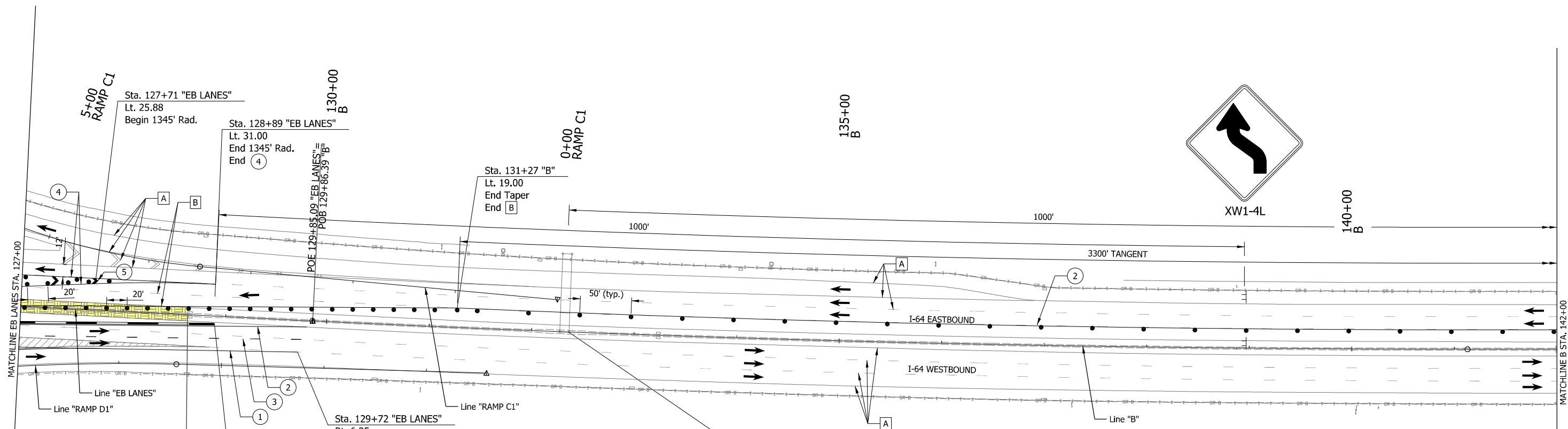


RECOMMENDED FOR APPROVAL	<i>Andrew M. Helbaker</i>	8/20/2021
DESIGNED	DPS	3/11/21
DRAWN	DPS	3/11/21
CHECKED	AWL	3/26/21
CHECKED	AWL	3/26/21

INDIANA DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC MOT PHASE 1
 STA. 112+00 TO STA. 127+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	13 of 59
CONTRACT	PROJECT
B-40719	1702255

P:\20-AUG-2021 08:49 AM



- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ← Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ▨ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction

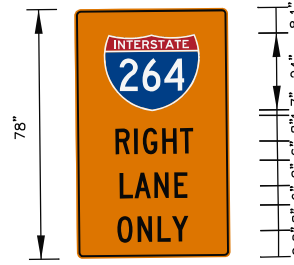
NOTE:
1. Cover existing EB I-64 guide sign with sign shown above.



RECOMMENDED FOR APPROVAL	<i>Andrew M. Lepletier</i>	8/20/2021
DESIGNED: DPS	3/11/21	DRAWN: DPS 3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC MOT PHASE 1
STA. 127+00 TO STA. 142+00

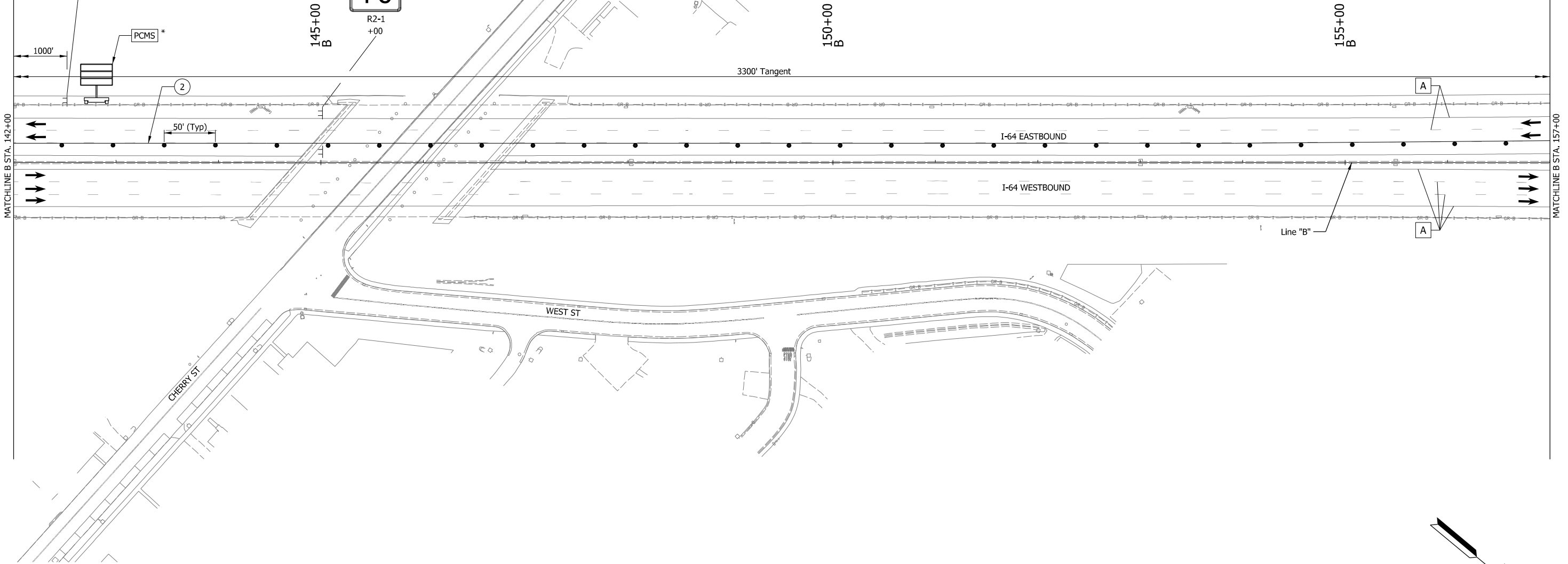
HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	14 of 59
CONTRACT	PROJECT
B-40719	1702255



RAMP TO I-264 EB CLOSED USE I-64 EB TO I-65 SB

* Cover "I-264 RIGHT LANE ONLY SIGN" and use PCMS during I-64 EB off-peak lane closure(s).

BORDER
R=1.5"
TH=0.63"
IN=0.47"
Panel Style: construction_guide.ssi
M.U.T.C.D.: 2009 Edition

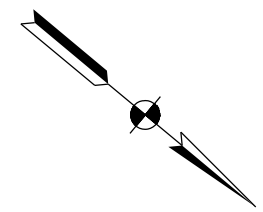


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

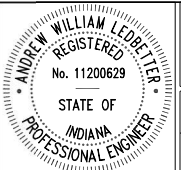
- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ← Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ▨ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction



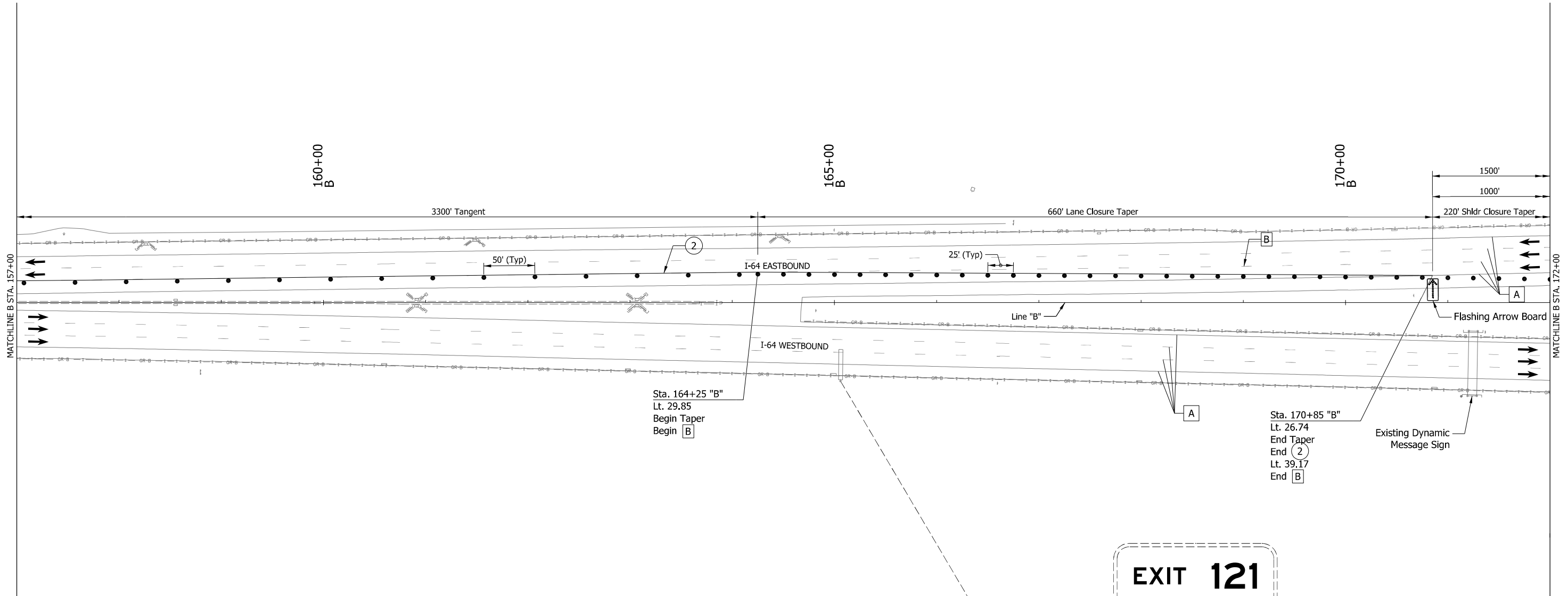
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RECOMMENDED FOR APPROVAL	<i>Andrew W. Lepletter</i>	9/3/2021
DESIGNED: DPS	3/11/21	DRAWN: DPS 3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

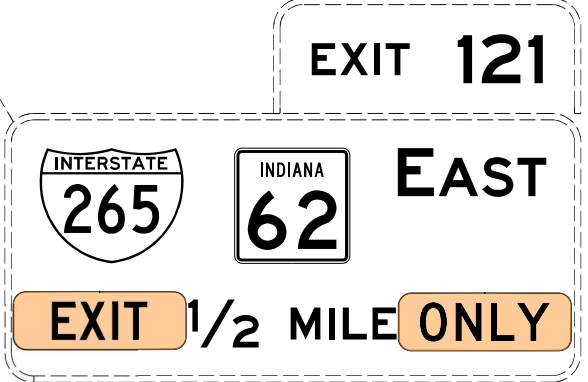
INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC MOT PHASE 1
STA. 142+00 TO STA. 157+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	15 of 59
CONTRACT	PROJECT
B-40719	1702255



Sta. 164+25 "B"
Lt. 29.85
Begin Taper
Begin [B]

Sta. 170+85 "B"
Lt. 26.74
End Taper
End [2]
Lt. 39.17
End [B]



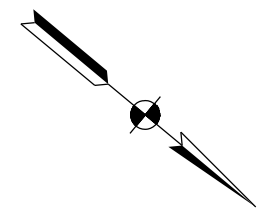
NOTE:
1. Sign modifications to remain from previous Phase.

- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- [A] Existing Pavement Marking
- [B] Remove Existing Pavement Markings
- ← Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ⊞ Crossover Temporary Pavement

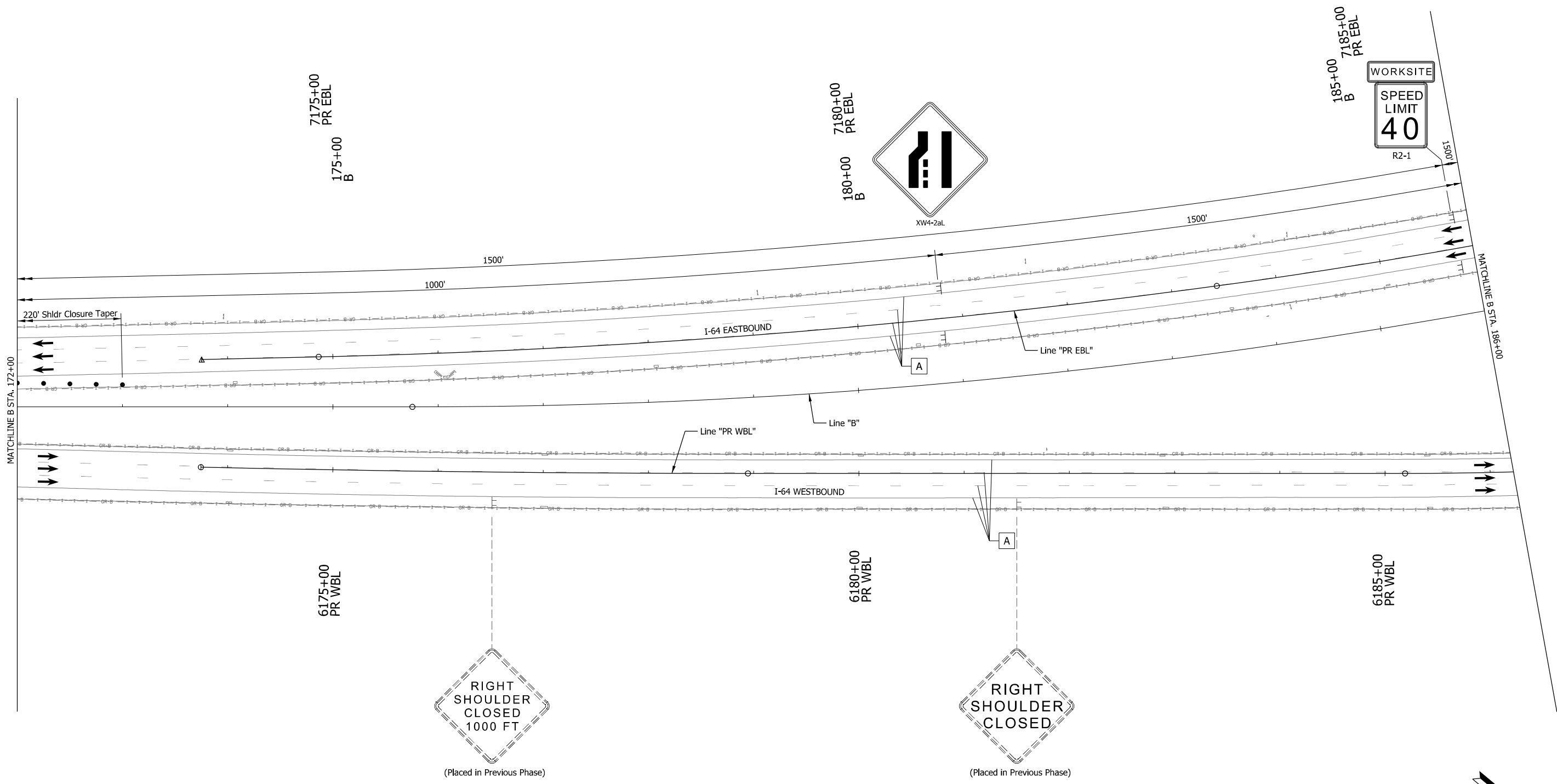
- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction



RECOMMENDED FOR APPROVAL	<i>Andrew W. Lepletter</i>	8/20/2021
DESIGNED: DPS	3/11/21	DRAWN: DPS 3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC MOT PHASE 1
 STA. 147+00 TO STA. 172+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	16 of 59
CONTRACT	PROJECT
B-40719	1702255

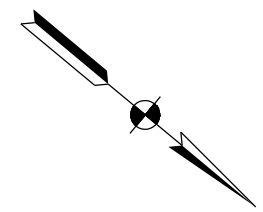


- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
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- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

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- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction

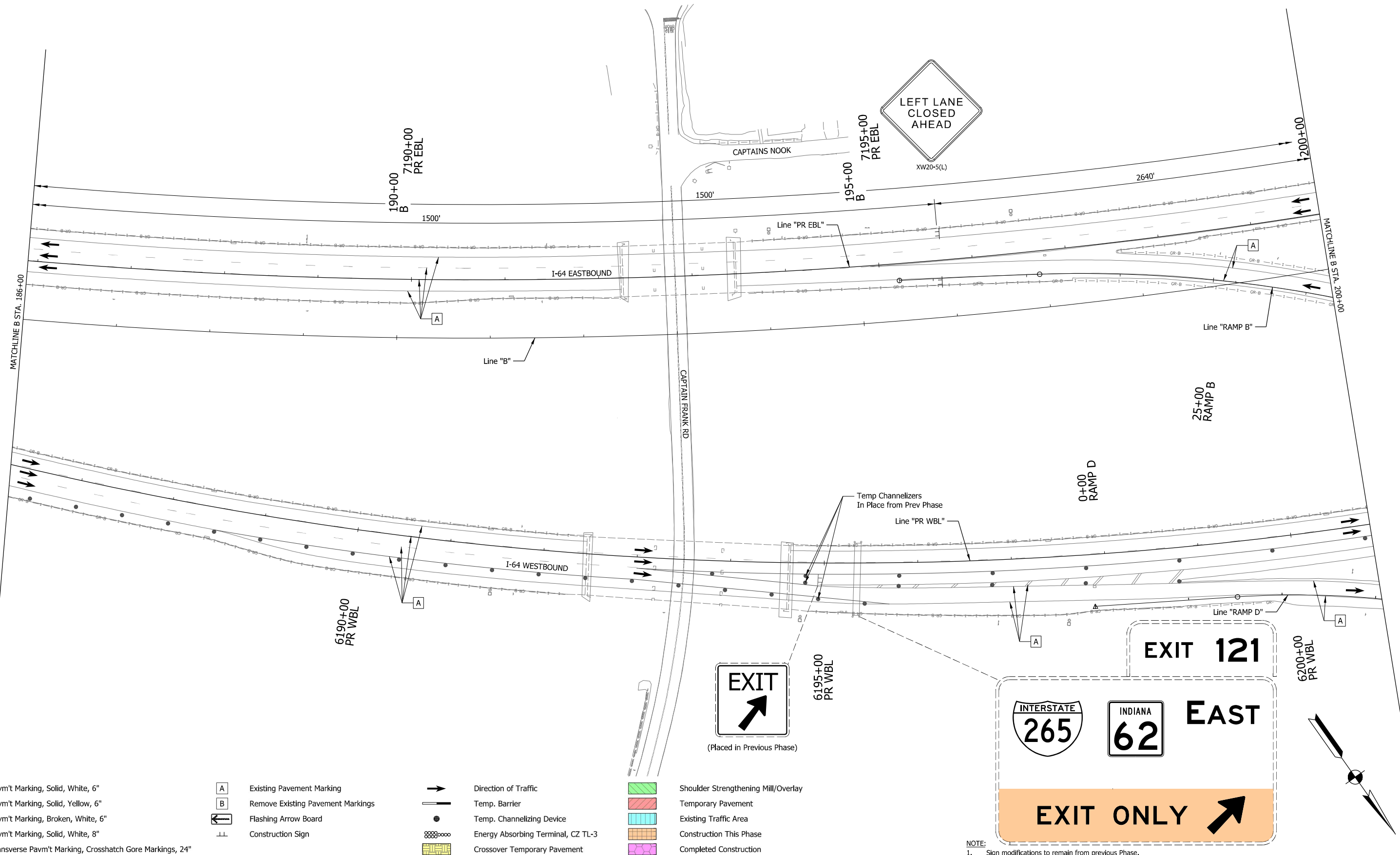


RECOMMENDED FOR APPROVAL	<i>Andrew M. Hultbert</i>	8/20/2021
DESIGNED: DPS	3/11/21	DRAWN: DPS 3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC MOT PHASE 1
 STA. 172+00 TO STA. 185+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	17 of 59
CONTRACT	PROJECT
B-40719	1702255

P:\19-AUG-2021 08:30 AM



- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction

NOTE:
1. Sign modifications to remain from previous Phase.



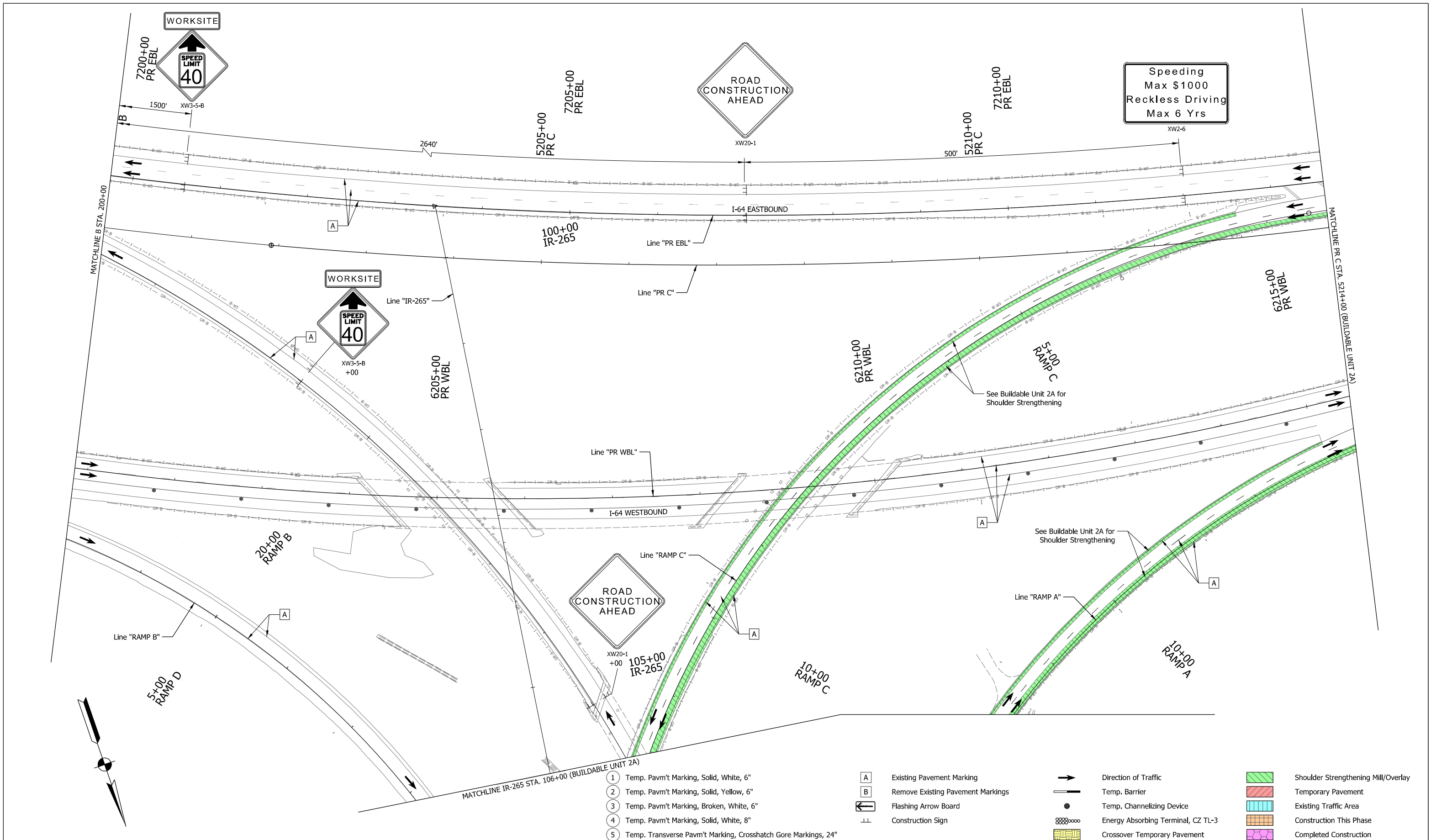
RECOMMENDED FOR APPROVAL	<i>Andrew M. Helber</i>	8/20/2021
DESIGNED: DPS	3/11/21	DRAWN: DPS 3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 185+00 TO STA. 200+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	18 of 59
CONTRACT	PROJECT
B-40719	1702255

Plot: 19-AUG-2021 08:30 AM



WORKSITE
SPEED LIMIT 40

ROAD CONSTRUCTION AHEAD

Speeding
Max \$1000
Reckless Driving
Max 6 Yrs

WORKSITE
SPEED LIMIT 40

ROAD CONSTRUCTION AHEAD

- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction

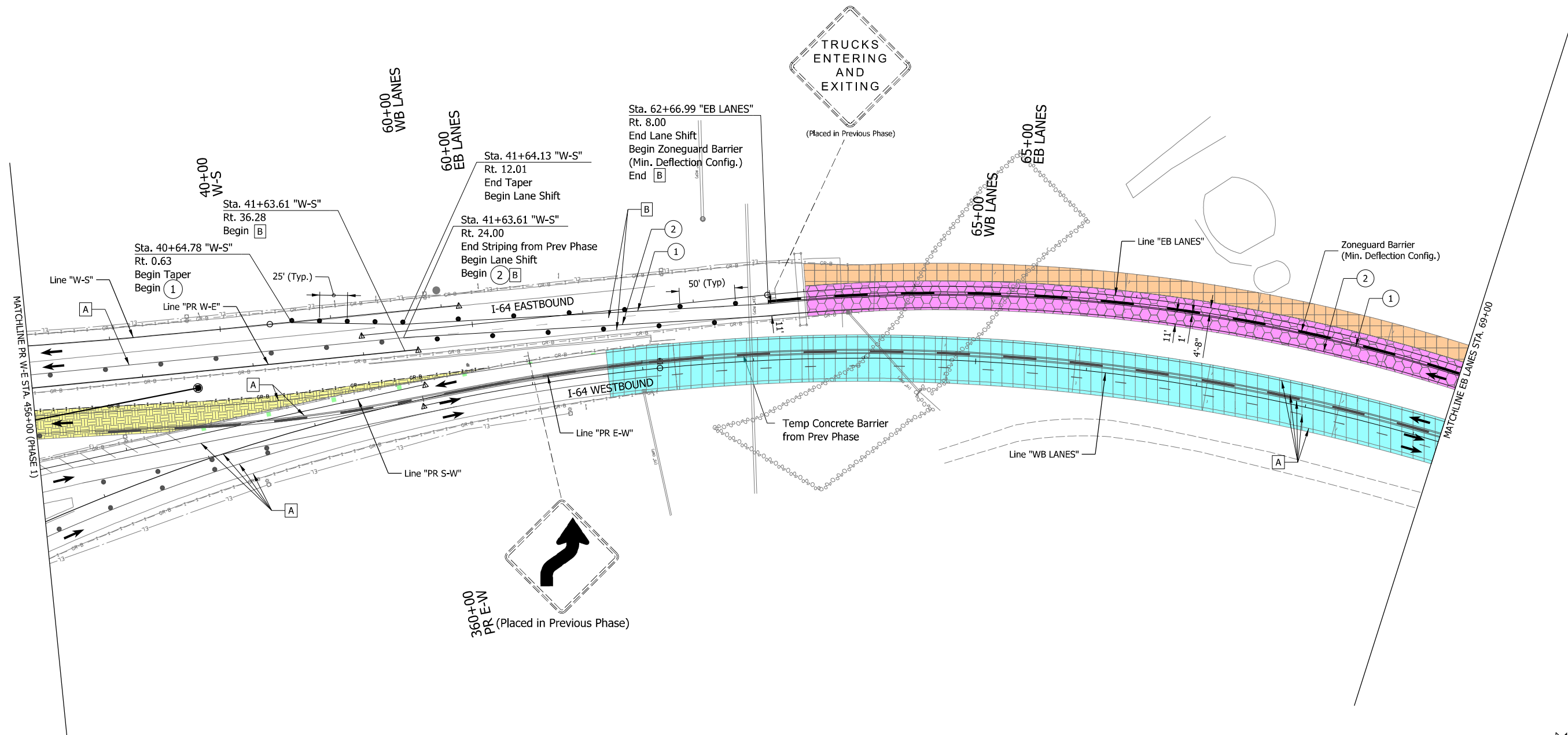
P19-AUG-2021 08:31 AM



RECOMMENDED FOR APPROVAL	<i>Andrew M. Helberter</i>	8/20/2021	DATE
DESIGNED: DPS	3/11/21	DRAWN: DPS	3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL	3/26/21

INDIANA
DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC
MOT PHASE 1
STA. 200+00 TO STA. 5214+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	19 of 59
CONTRACT	PROJECT
B-40719	1702255

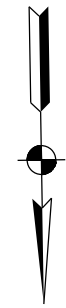


- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

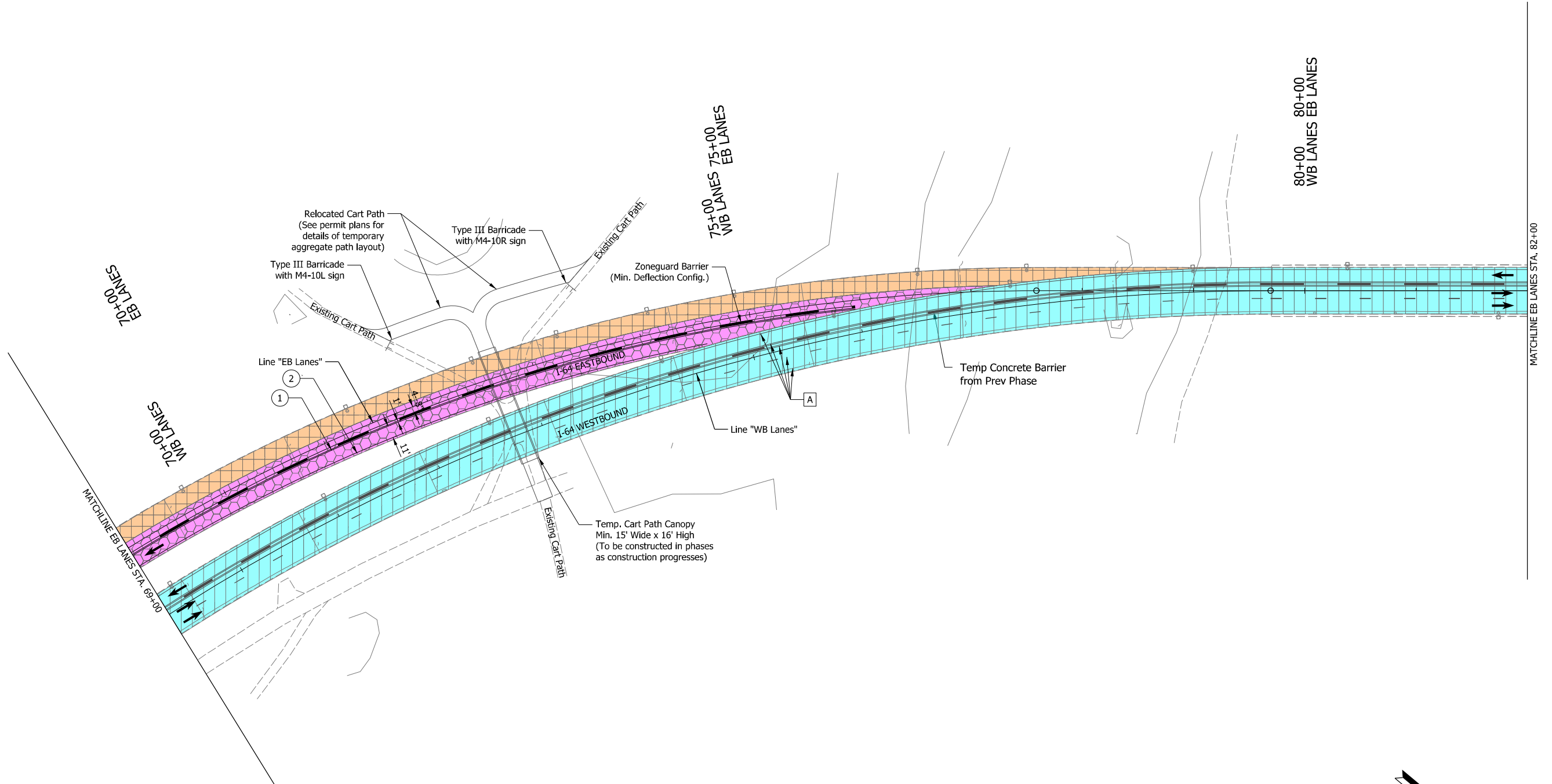
- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: DPS	4/20/21	DRAWN: DPS 4/20/21
CHECKED: AWL	4/23/21	CHECKED: AWL 4/23/21

INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC MOT PHASE 2
 STA. 456+00 TO STA. 69+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	20 of 59
CONTRACT	PROJECT
B-40719	1702255

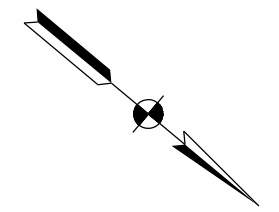


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ← Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ⊘ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction



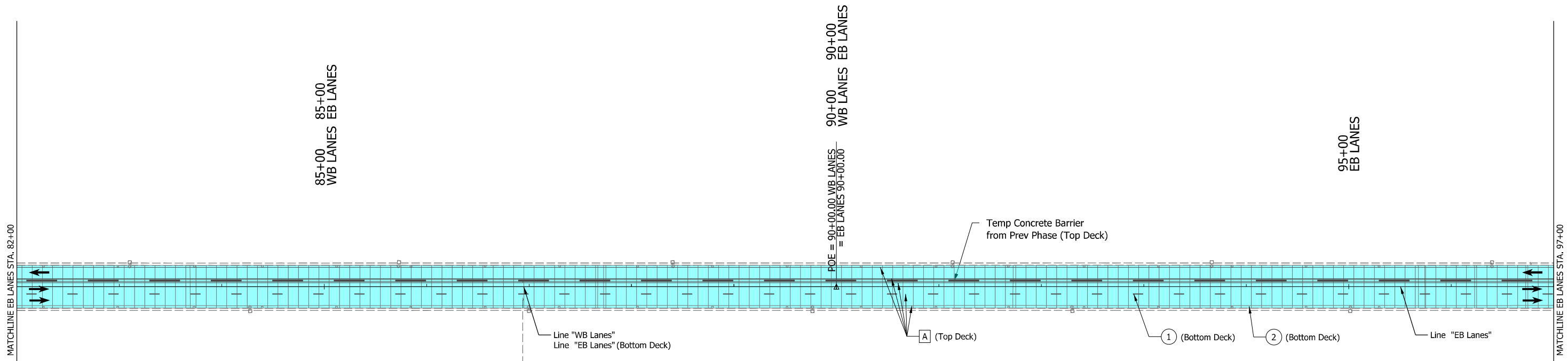
RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: DPS	4/20/21	DRAWN: DPS 4/20/21
CHECKED: AWL	4/23/21	CHECKED: AWL 4/23/21

INDIANA
DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC
MOT PHASE 2
STA. 69+00 TO STA. 82+00**

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	21 of 59
CONTRACT	PROJECT
B-40719	1702255

Plot: 19-AUG-2021 08:32 AM



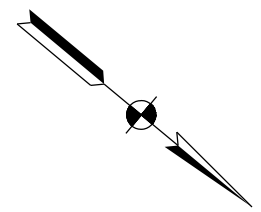
New Albany
EXIT 3/4 MILE
 (In place from Previous Phase)

- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ← Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ⊘ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction



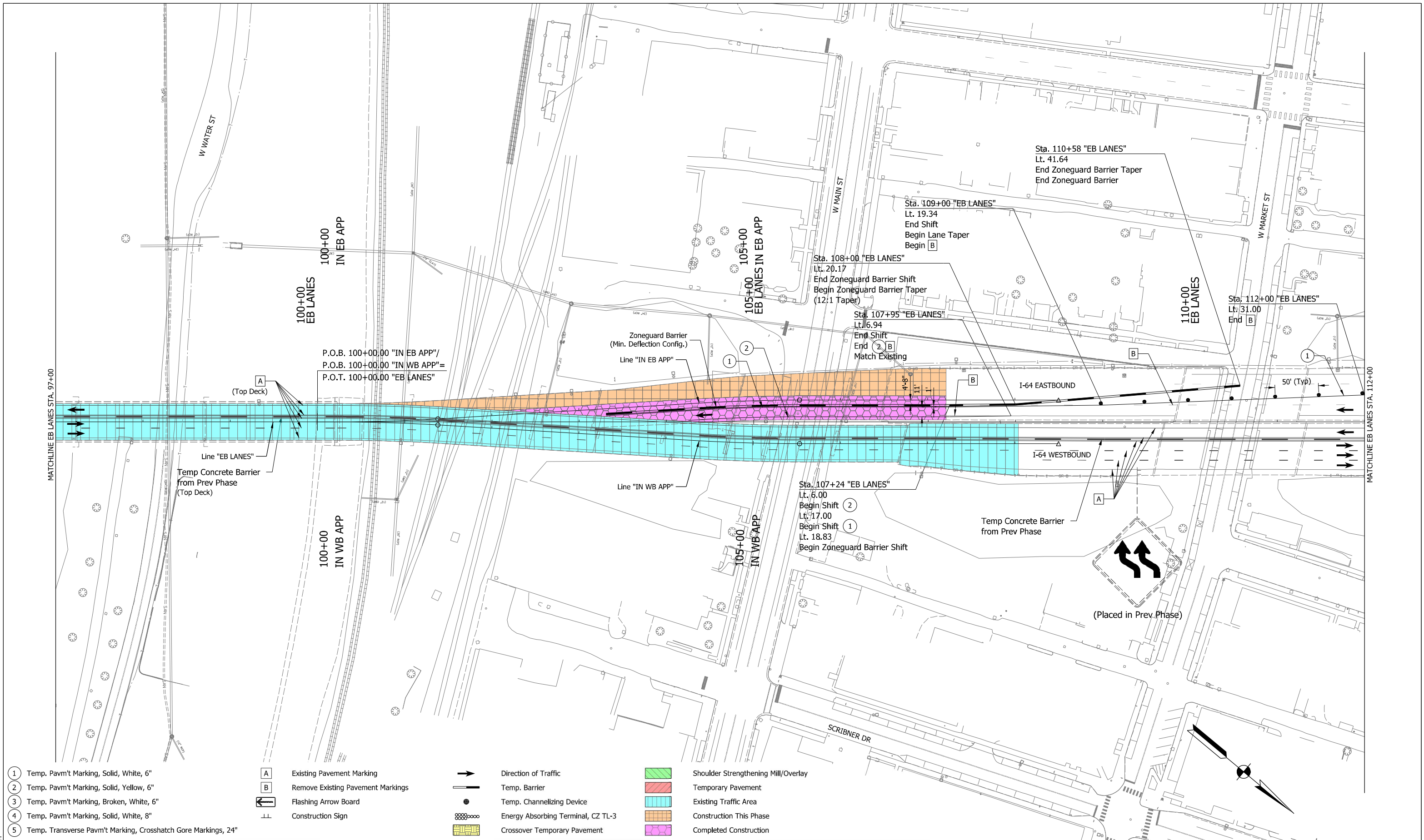
RECOMMENDED FOR APPROVAL	<i>Andrew M. Lepletier</i>	8/20/2021
DESIGNED: DPS	4/20/21	DRAWN: DPS 4/20/21
CHECKED: AWL	4/23/21	CHECKED: AWL 4/23/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 2
STA. 82+00 TO STA. 97+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	22 of 59
CONTRACT	PROJECT
B-40719	1702255

Plot: 19-AUG-2021 08:32 AM



- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



RECOMMENDED FOR APPROVAL	<i>Andrew M. Helbert</i>	DESIGN ENGINEER	8/20/2021	DATE
DESIGNED: DPS	4/20/21	DRAWN: DPS	4/20/21	
CHECKED: AWL	4/23/21	CHECKED: AWL	4/23/21	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 2
STA. 97+00 TO STA. 112+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	23 of 59
CONTRACT	PROJECT
B-40719	1702255

Plot: 19-AUG-2021 08:33 AM



WEST

LEFT 2 LANES

LANE ENDS MERGE LEFT

TRUCKS ENTERING AND EXITING

WORKSITE
SPEED LIMIT
40

EXIT 123

New Albany



NOTE:
1. Sign modifications to remain from previous Phase.

- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction

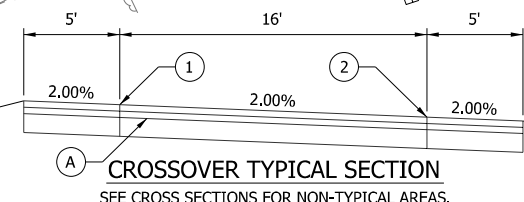
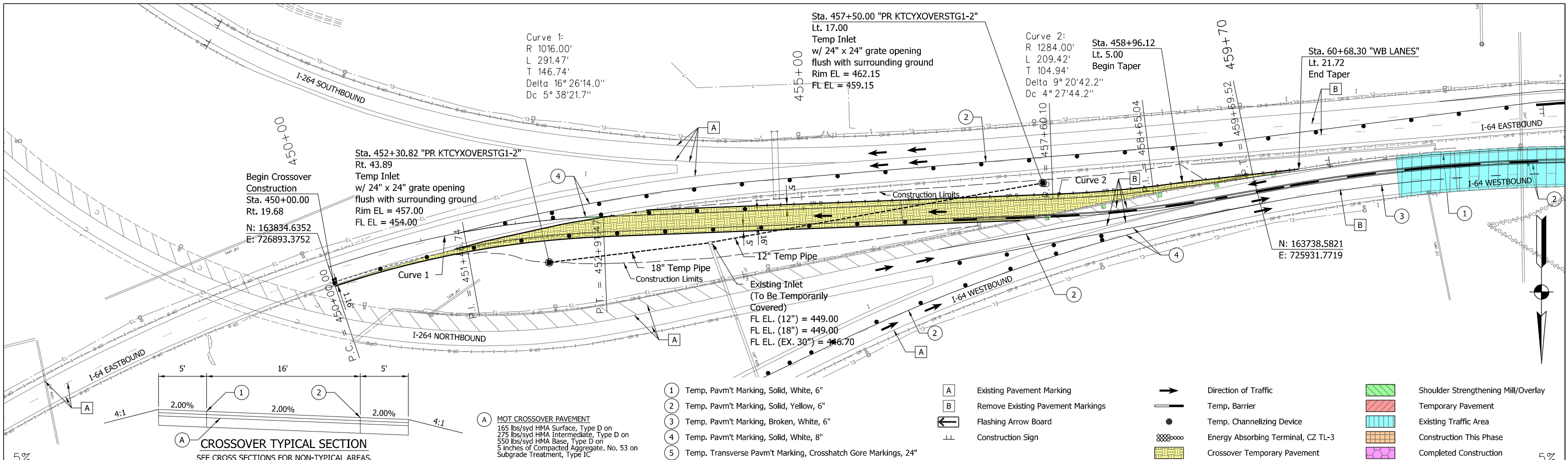


RECOMMENDED FOR APPROVAL	<i>Andrew W. Lepletter</i>	8/20/2021
DESIGNED: DPS	4/20/21	DRAWN: DPS 4/20/21
CHECKED: AWL	4/23/21	CHECKED: AWL 4/23/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 2
STA. 112+00 TO STA. 127+00

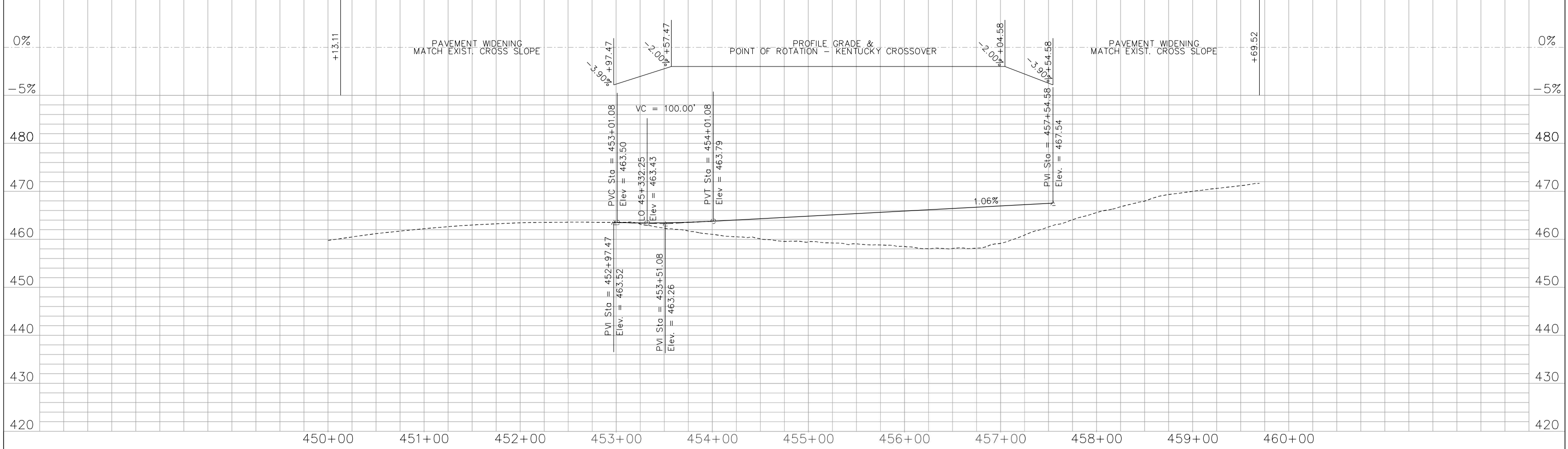
HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	24 of 59
CONTRACT	PROJECT
B-40719	1702255



MOT CROSSOVER PAVEMENT
165 lbs/syd HMA Surface, Type D on 275 lbs/syd HMA Intermediate, Type D on 550 lbs/syd HMA Base, Type D on 5 inches of Compacted Aggregate, No. 53 on Subgrade Treatment, Type IC

- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement
- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction

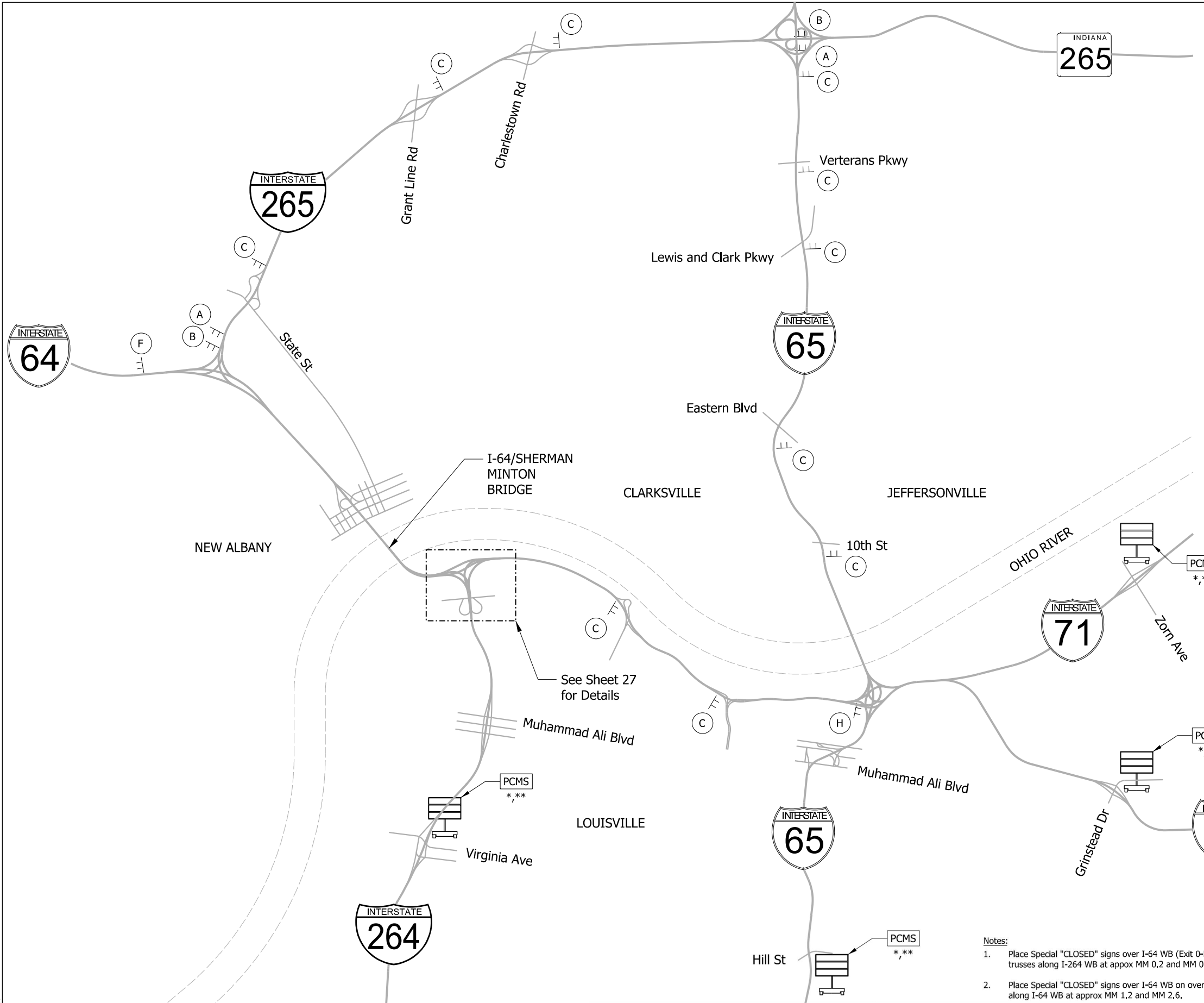


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: KLO	4/20/21	DRAWN: KLO 4/20/21
CHECKED: AWL	4/23/21	CHECKED: AWL 4/23/21

INDIANA DEPARTMENT OF TRANSPORTATION
KENTUCKY CROSSOVER PLAN & PROFILE SHEET

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
1"=10'	1702255
BUILDABLE UNIT	SHEETS
2B	25 of 59
CONTRACT	PROJECT
B-40719	1702255

PLOT: 19-AUG-2021 08:34 AM



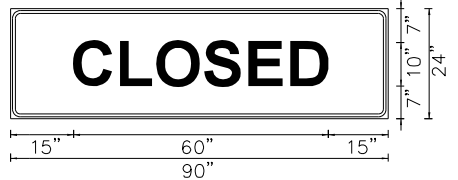
<p>(A)</p> <p>ALT</p> <p>WEST</p> <p>INTERSTATE</p> <p>64</p> <p>↗</p>	<p>M4-1</p> <p>M3-4</p> <p>M1-1</p> <p>M5-2R</p>	<p>(B)</p> <p>ALT</p> <p>WEST</p> <p>INTERSTATE</p> <p>64</p> <p>↗</p>	<p>M4-1</p> <p>M3-4</p> <p>M1-1</p> <p>M6-2R</p>	<p>(C)</p> <p>ALT</p> <p>WEST</p> <p>INTERSTATE</p> <p>64</p> <p>↑</p>	<p>M4-1</p> <p>M3-4</p> <p>M1-1</p> <p>M6-3</p>
<p>(D)</p> <p>ALT</p> <p>WEST</p> <p>INTERSTATE</p> <p>64</p> <p>←</p>	<p>M4-1</p> <p>M3-4</p> <p>M1-1</p> <p>M5-1L</p>	<p>(E)</p> <p>ALT</p> <p>WEST</p> <p>INTERSTATE</p> <p>64</p> <p>←</p>	<p>M4-1</p> <p>M3-4</p> <p>M1-1</p> <p>M6-1L</p>	<p>(F)</p> <p>END</p> <p>ALT</p> <p>WEST</p> <p>INTERSTATE</p> <p>64</p>	<p>M4-6</p> <p>M4-1</p> <p>M3-4</p> <p>M1-1</p>
<p>(G)</p> <p>ALT</p> <p>WEST</p> <p>INTERSTATE</p> <p>64</p> <p>↖</p>	<p>M4-1</p> <p>M3-4</p> <p>M1-1</p> <p>M5-2L</p>	<p>(H)</p> <p>ALT</p> <p>WEST</p> <p>INTERSTATE</p> <p>64</p> <p>↖</p>	<p>M4-1</p> <p>M3-4</p> <p>M1-1</p> <p>M6-2L</p>	<p>(I)</p> <p>ALT</p> <p>WEST</p> <p>INTERSTATE</p> <p>64</p> <p>→</p>	<p>M4-1</p> <p>M3-4</p> <p>M1-1</p> <p>M6-1R</p>

I-64 BRIDGE CLOSURE
 BEGINS XX XX XX

* Place "Sherman Minton Closure" PCMS 14 Calendar Days Prior to Closure or as Directed by the Engineer

I-64 BRIDGE CLOSED
 USE ALT. I-65 TO I-265

** For use during Closure



9.00" Radius, 2.00" Border, Black on Orange
SPECIAL "CLOSED" SIGN

- Notes:
- Place Special "CLOSED" signs over I-64 WB (Exit 0-B) on overhead trusses along I-264 WB at approx MM 0.2 and MM 0.7 and MM 0.9.
 - Place Special "CLOSED" signs over I-64 WB on overhead trusses along I-64 WB at approx MM 1.2 and MM 2.6.

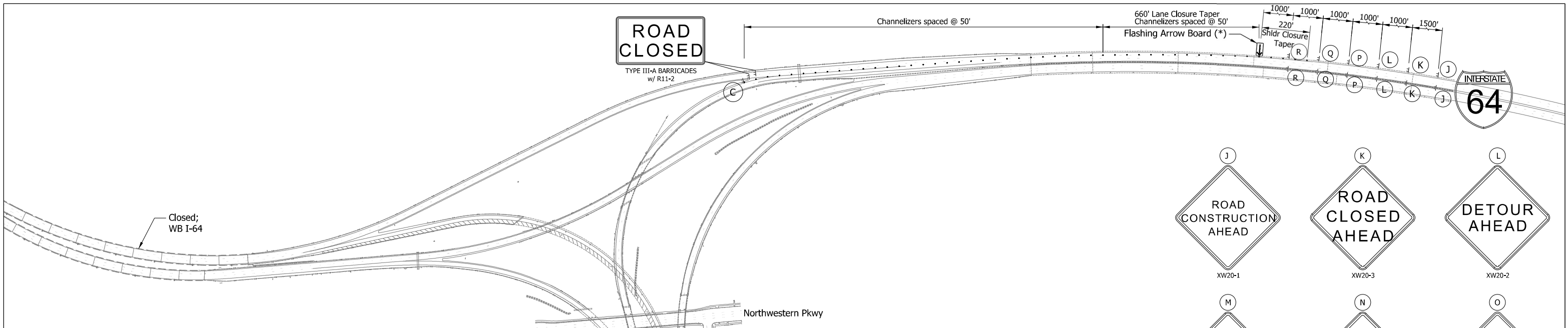


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021	DATE
DESIGNED: DPS	7/19/21	DRAWN: DPS	7/19/21
CHECKED: AWL	7/21/21	CHECKED: AWL	7/21/21

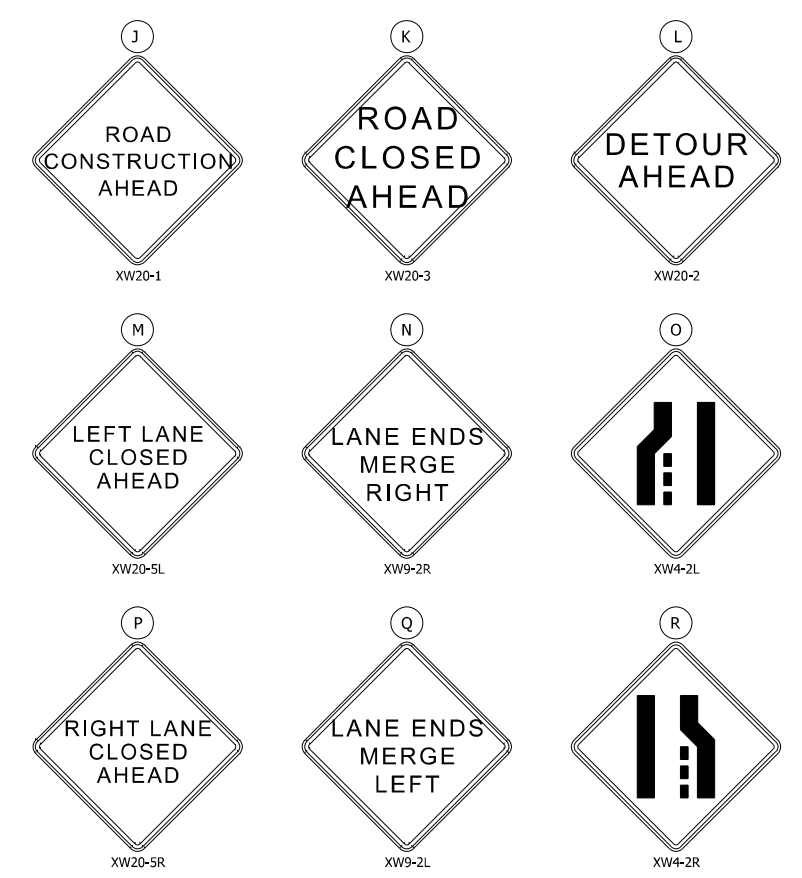
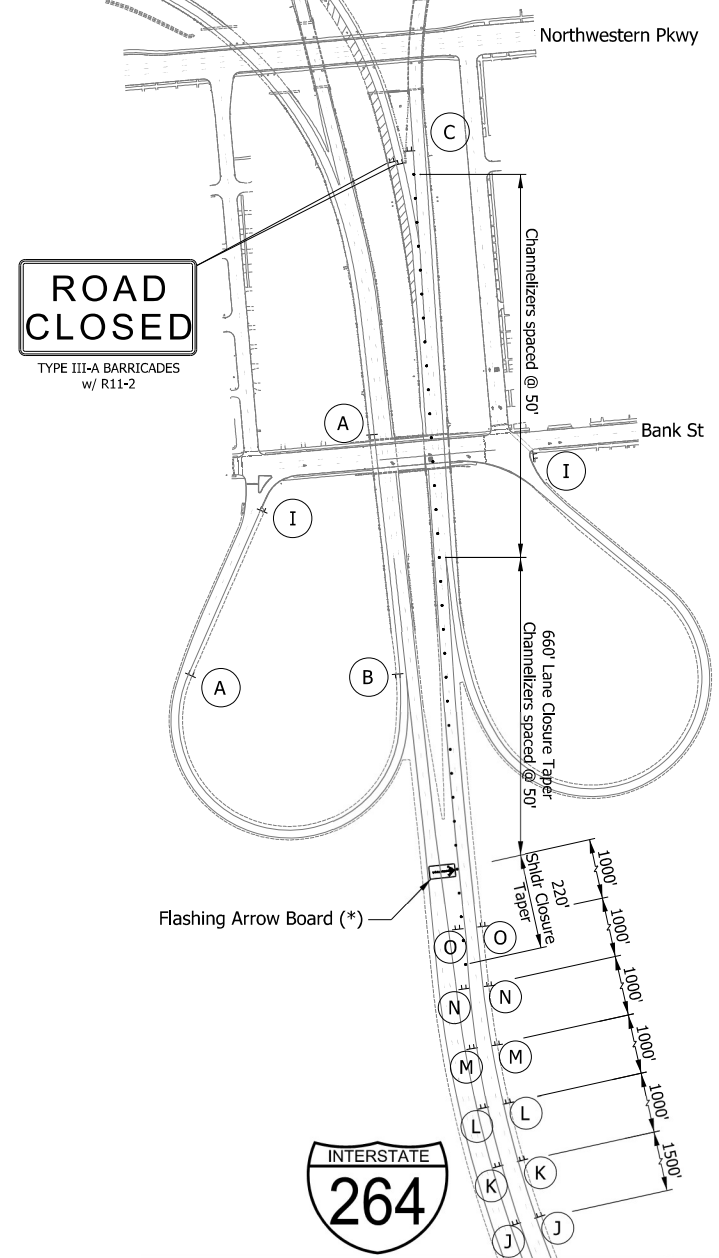
INDIANA DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC DETOUR
 WB I-64 CLOSURE

HORIZONTAL SCALE	BRIDGE FILE
1"=2500'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	26 of 59
CONTRACT	PROJECT
B-40719	1702255

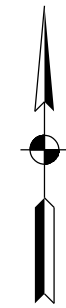
P:\19-AUG-2021_08:34 AM



<p>A</p> <p>ALT M4-1</p> <p>WEST M3-4</p> <p>INTERSTATE 64 M1-1</p> <p>↗ M5-2R</p>	<p>B</p> <p>ALT M4-1</p> <p>WEST M3-4</p> <p>INTERSTATE 64 M1-1</p> <p>↘ M6-2R</p>	<p>C</p> <p>ALT M4-1</p> <p>WEST M3-4</p> <p>INTERSTATE 64 M1-1</p> <p>↑ M6-3</p>
<p>D</p> <p>ALT M4-1</p> <p>WEST M3-4</p> <p>INTERSTATE 64 M1-1</p> <p>↖ M5-1L</p>	<p>E</p> <p>ALT M4-1</p> <p>WEST M3-4</p> <p>INTERSTATE 64 M1-1</p> <p>← M6-1L</p>	<p>F</p> <p>END M4-6</p> <p>ALT M4-1</p> <p>WEST M3-4</p> <p>INTERSTATE 64 M1-1</p>
<p>G</p> <p>ALT M4-1</p> <p>WEST M3-4</p> <p>INTERSTATE 64 M1-1</p> <p>↙ M5-2L</p>	<p>H</p> <p>ALT M4-1</p> <p>WEST M3-4</p> <p>INTERSTATE 64 M1-1</p> <p>↖ M6-2L</p>	<p>I</p> <p>ALT M4-1</p> <p>WEST M3-4</p> <p>INTERSTATE 64 M1-1</p> <p>→ M6-1R</p>



(*) Flashing Arrow Board to be placed within lane closure taper where space allows.



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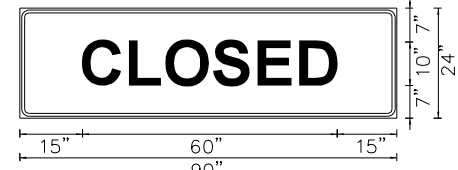
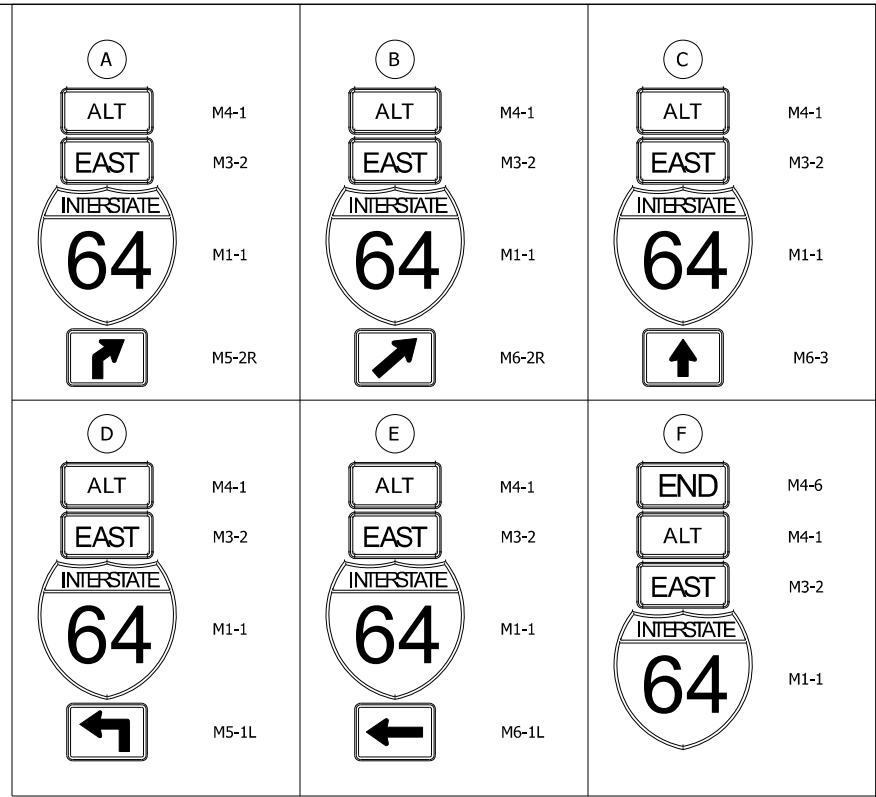
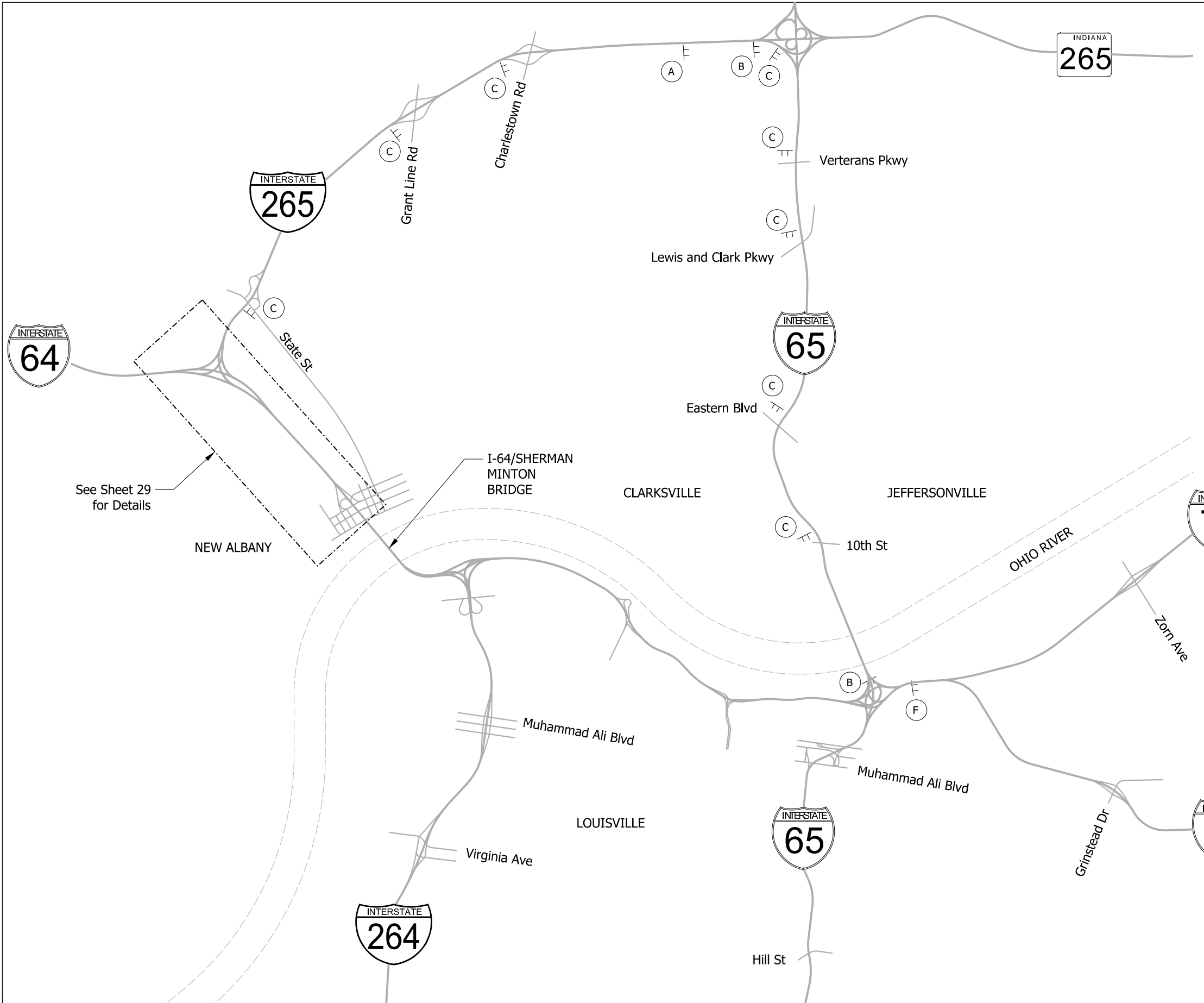


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: DPS	7/19/21	DRAWN: DPS 7/19/21
CHECKED: AWL	7/21/21	CHECKED: AWL 7/21/21

INDIANA DEPARTMENT OF TRANSPORTATION

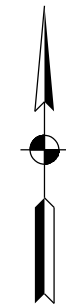
MAINTENANCE OF TRAFFIC DETOUR WB I-64 CLOSURE

HORIZONTAL SCALE	BRIDGE FILE
1"=200'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	27 of 59
CONTRACT	PROJECT
B-40719	1702255



9.00" Radius, 2.00" Border, Black on Orange
SPECIAL "CLOSED" SIGN

- Notes:**
- Place Special "CLOSED" sign over I-64 EB on overhead truss along I-64 EB at approx MM 121.5.
 - Place Special "CLOSED" signs over I-64 EB (Exit 0) on overhead trusses along I-265 WB at approx MM 0.0 and MM 0.7.

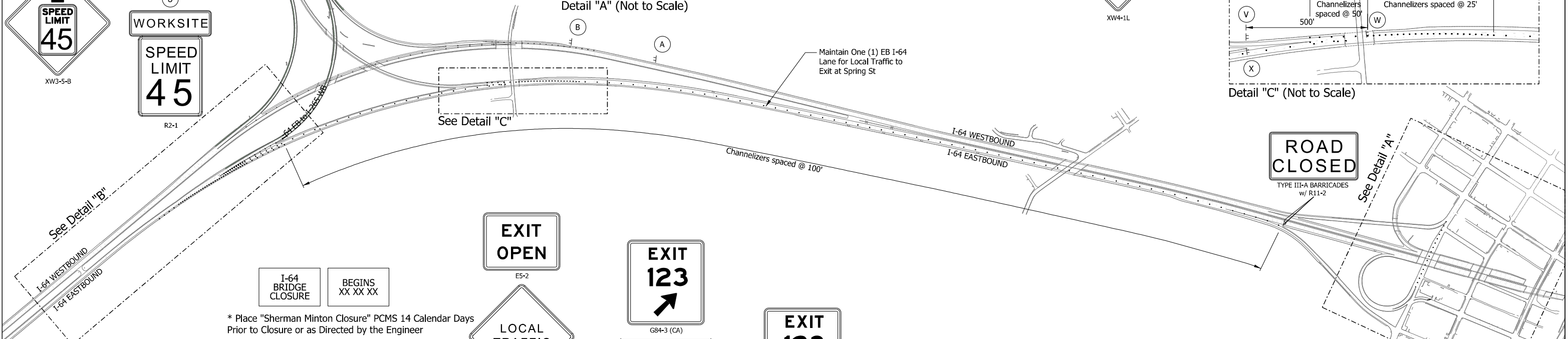
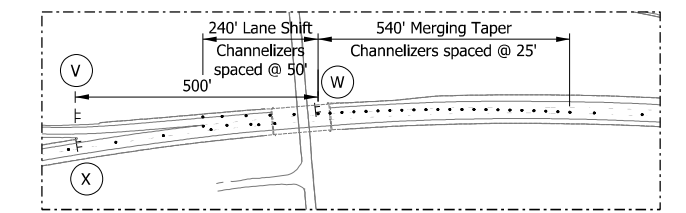
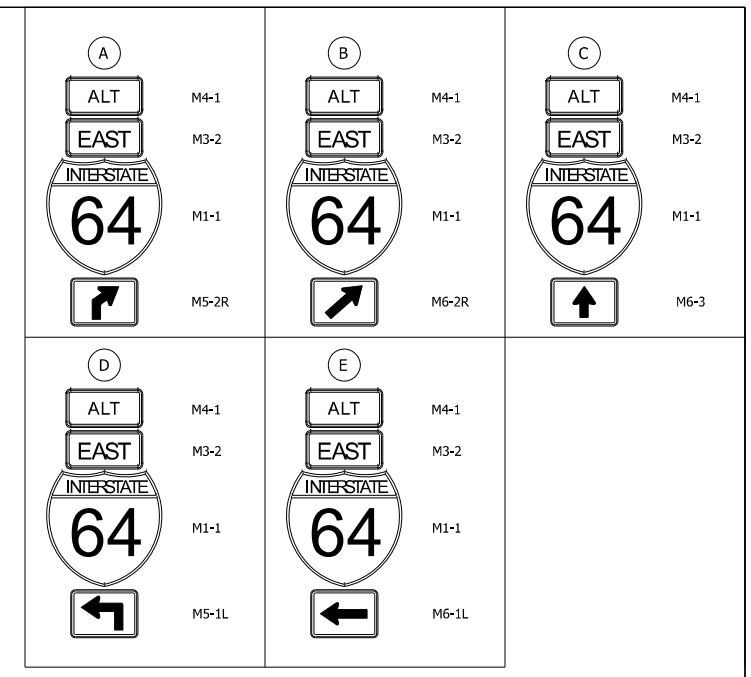
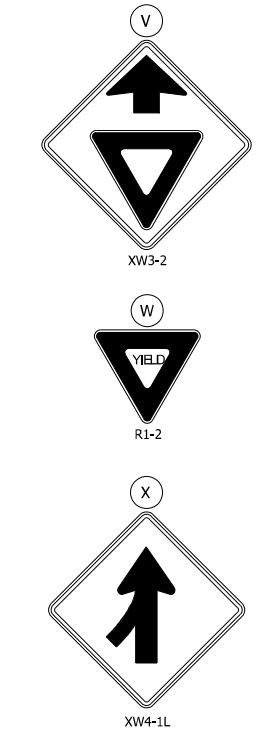
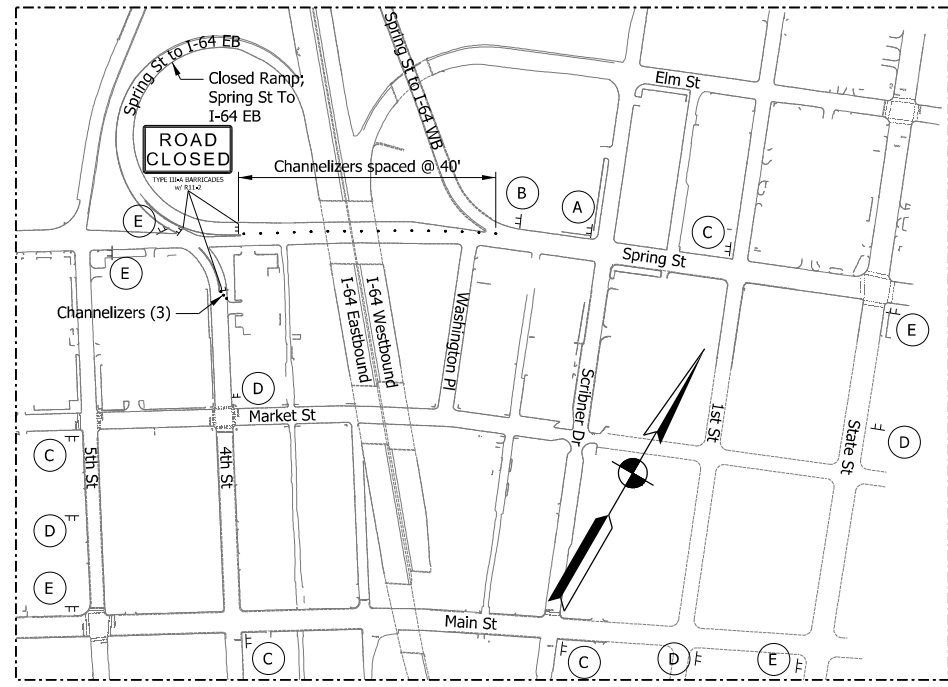
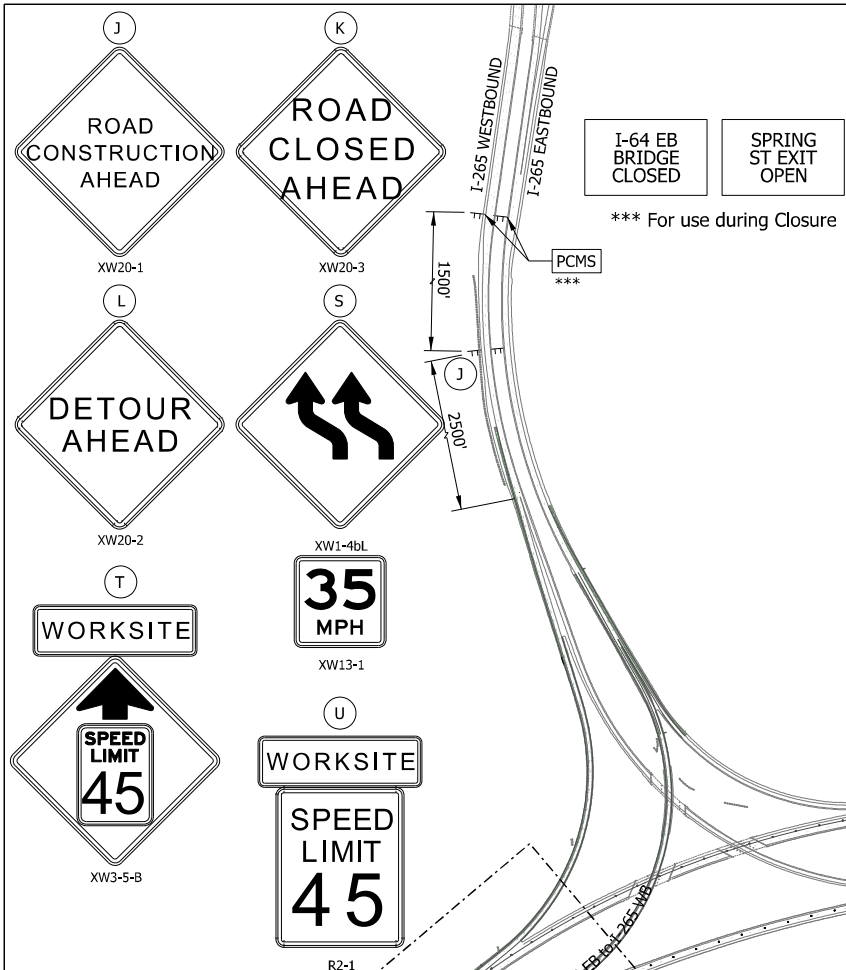


RECOMMENDED FOR APPROVAL	<i>Andrew M. Leibel</i>	8/20/2021
DESIGNED:	DPS	7/19/21
DRAWN:	DPS	7/19/21
CHECKED:	AWL	7/21/21
CHECKED:	AWL	7/21/21

INDIANA
 DEPARTMENT OF TRANSPORTATION

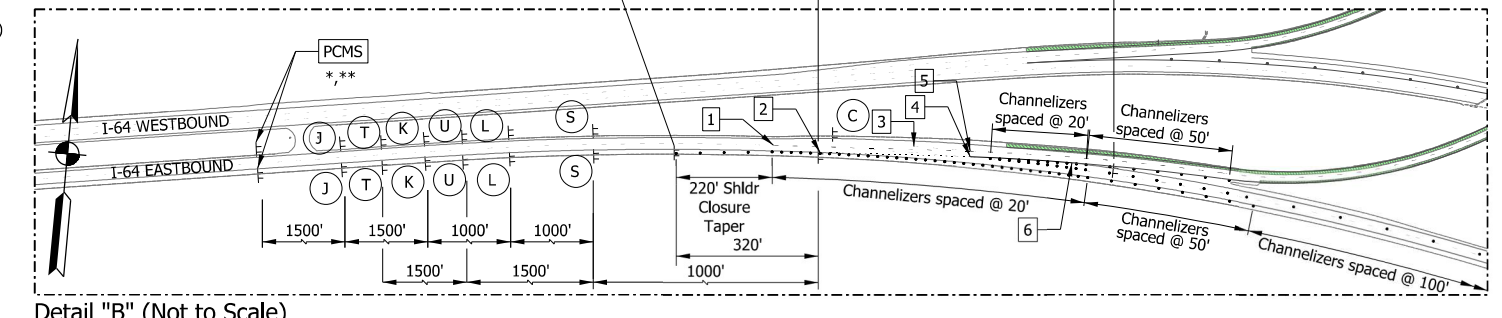
**MAINTENANCE OF TRAFFIC
 DETOUR
 EB I-64 CLOSURE**

HORIZONTAL SCALE	BRIDGE FILE
1"=2500'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	28 of 59
CONTRACT	PROJECT
B-40719	1702255



* Place "Sherman Minton Closure" PCMS 14 Calendar Days Prior to Closure or as Directed by the Engineer

** For use during Closure



Note:
 1. I-64 EB detour signs within New Albany to be used during I-64 EB peak and off-peak lane closures when EB on ramp from West Spring Street is closed.
 2. Utilize DMS at MM 1.9 on I-265 WB during I-64 EB closure. "I-64 EB Closed at River Bridge Exit to Spring St open to local traffic only"

- Sta. 7222+73 "PR EBL" Lt. 12.58
Begin Temp. Pav't Marking, Broken, White, 6" (10:30)
Begin Pav't Marking Removal
- Sta. 7221+75 "PR EBL" Lt. 24.00
Begin Temp. Pav't Marking, Broken, White, 6" (3:9)
Begin Pav't Marking Removal
- Sta. 7219+81 "PR EBL" Lt. 0.19
Begin Pav't Marking Removal
- Sta. 7218+62 "PR EBL" Lt. 11.76
End Temp. Pav't Marking, Broken, White, 6" (3:9)
End Pav't Marking Removal
Begin Temp. Pav't Marking, Solid, White, 8"
- Sta. 7218+63 "PR EBL" Lt. 0.28
End Temp. Pav't Marking, Broken, White, 6" (10:30)
End Pav't Marking Removal
- Sta. 7216+49 "PR EBL" Lt. 0.16
End Temp. Pav't Marking, Solid, White, 8"

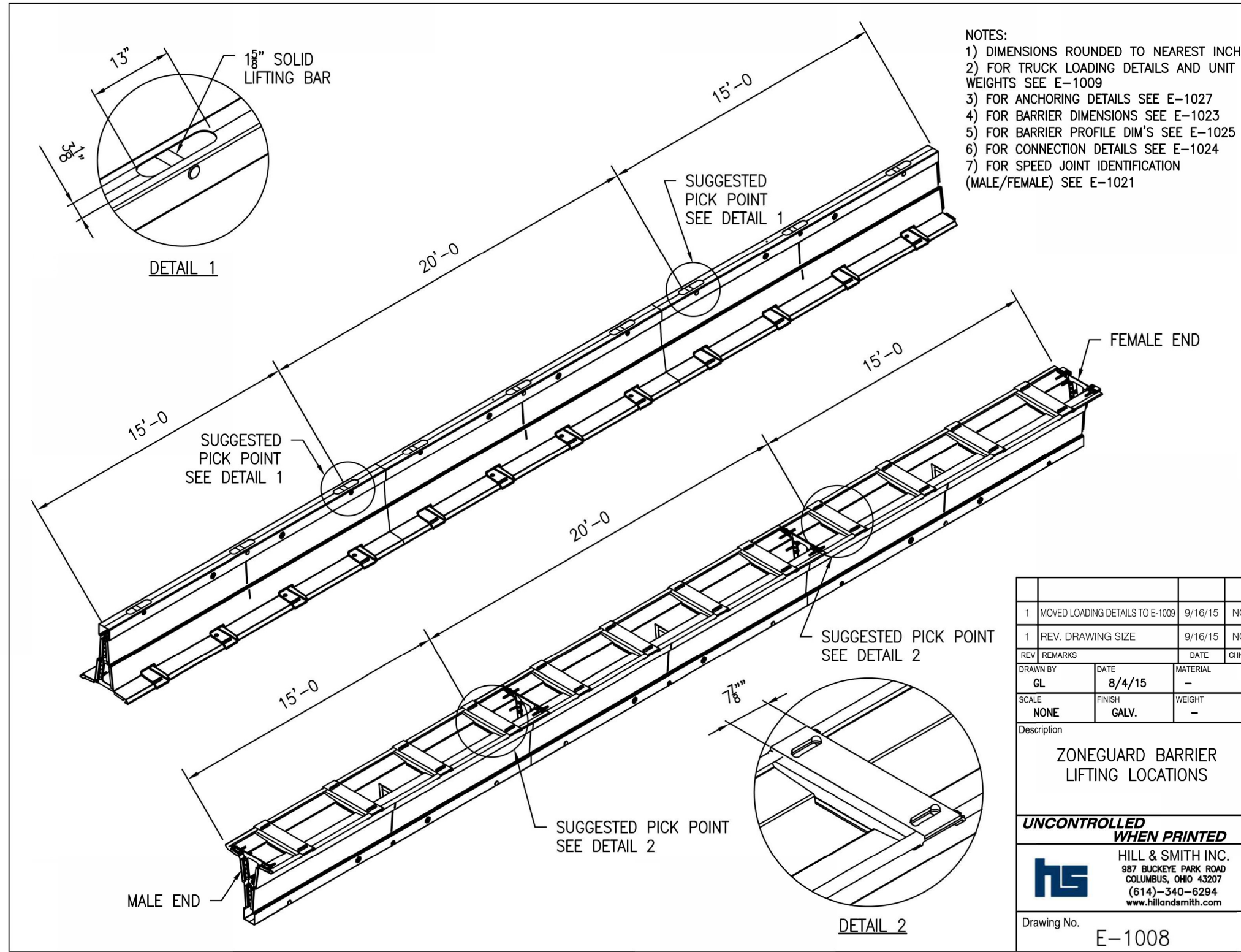
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RECOMMENDED FOR APPROVAL	<i>Andrew M. Leubert</i>	9/13/2021
DESIGNED: DPS	7/20/21	DRAWN: DPS 7/20/21
CHECKED: AWL	7/21/21	CHECKED: AWL 7/21/21

INDIANA DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC DETOUR
 EB I-64 CLOSURE

HORIZONTAL SCALE	BRIDGE FILE
1"=400'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	29 of 59
CONTRACT	PROJECT
B-40719	1702255



- NOTES:
- 1) DIMENSIONS ROUNDED TO NEAREST INCH
 - 2) FOR TRUCK LOADING DETAILS AND UNIT WEIGHTS SEE E-1009
 - 3) FOR ANCHORING DETAILS SEE E-1027
 - 4) FOR BARRIER DIMENSIONS SEE E-1023
 - 5) FOR BARRIER PROFILE DIM'S SEE E-1025
 - 6) FOR CONNECTION DETAILS SEE E-1024
 - 7) FOR SPEED JOINT IDENTIFICATION (MALE/FEMALE) SEE E-1021

REV	REMARKS	DATE	CHK
1	MOVED LOADING DETAILS TO E-1009	9/16/15	NC
1	REV. DRAWING SIZE	9/16/15	NC
DRAWN BY		DATE	MATERIAL
GL		8/4/15	-
SCALE		FINISH	WEIGHT
NONE		GALV.	-
Description			
ZONEGUARD BARRIER LIFTING LOCATIONS			
UNCONTROLLED WHEN PRINTED			
 HILL & SMITH INC. 987 BUCKEYE PARK ROAD COLUMBUS, OHIO 43207 (614)-340-6294 www.hillandsmith.com			
Drawing No. E-1008			

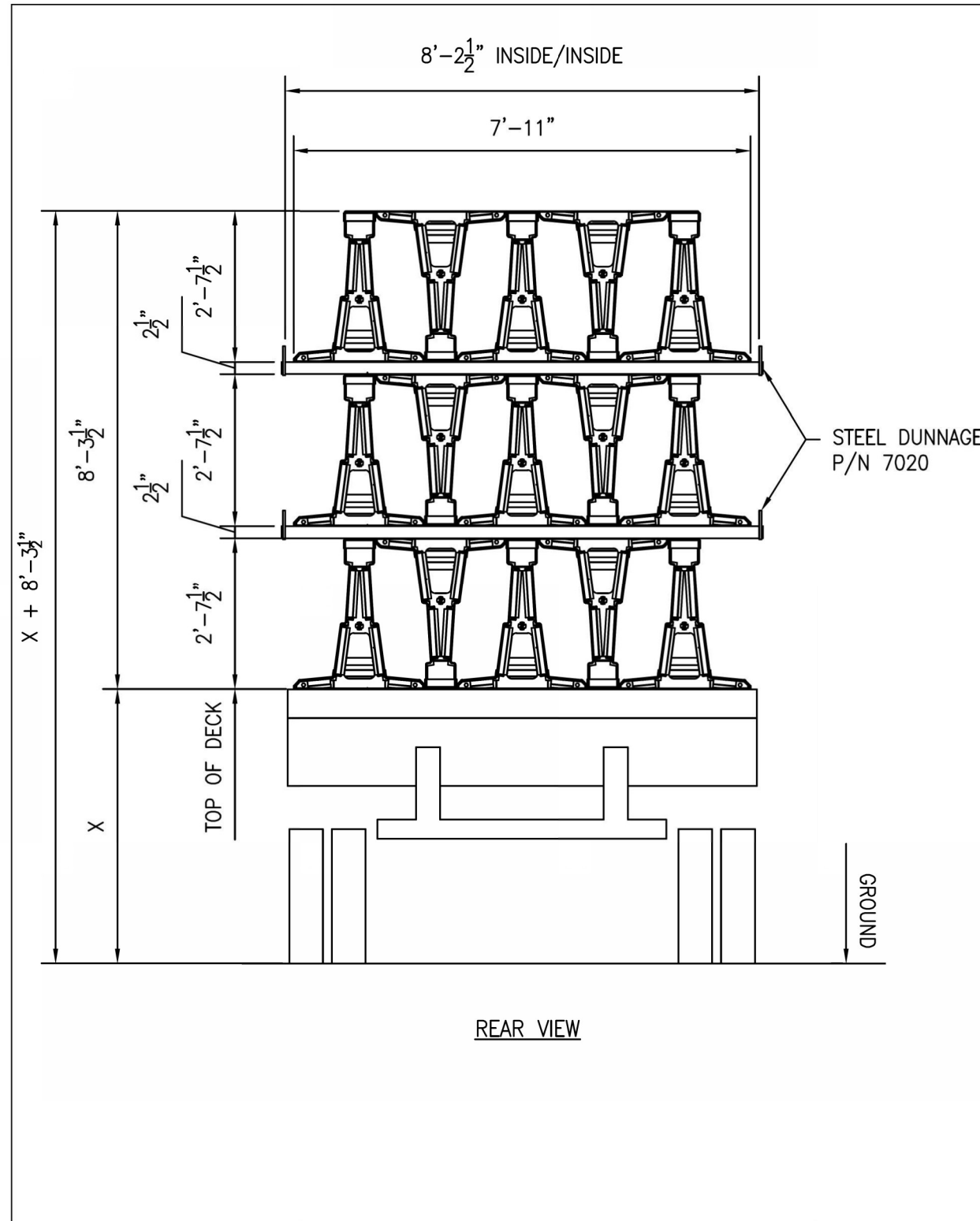
P01:19-AUG-2021 08:36 AM



RECOMMENDED FOR APPROVAL	<i>Andrew M. Lepletier</i>	8/20/2021
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CHECKED:	AWL	7/21/21


INDIANA DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC DETAIL ZONEGUARD BARRIER

HORIZONTAL SCALE	BRIDGE FILE
N/A	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	30 of 59
CONTRACT	PROJECT
B-40719	1702255



NOTES:

- 1) EACH 50'-0 LONG UNIT WEIGHS 3,097 LB.
- 2) 1 BUNDLE CONSISTS OF (5) UNITS= 15,485 LBS.
- 3) A TRUCKLOAD CONSISTS OF 3 BUNDLES = 46,455 LBS
- 4) EACH LAYER ON THE TRUCK SHOULD BE SECURED USING CHAINS OR STRAPS.
- 5) 2 PIECES OF STEEL DUNNAGE SHOULD BE USED ON EACH LAYER, BETWEEN THE TOP AND MIDDLE ROW, AND BETWEEN THE BOTTOM AND MIDDLE ROW. NO DUNNAGE REQUIRED BELOW THE BOTTOM ROW.
- 6) LOAD ALL FEMALE ENDS TOWARDS THE FRONT OF THE VEHICLE.
- 7) ENSURE ENGAGEMENT ASSEMBLIES ARE SECURE.
- 8) FOR ADDITIONAL LOADING, UNLOADING AND LIFTING DETAILS REFER TO THE DESIGN GUIDANCE & INSTALLATION MANUAL.
- 9) FOR ANCHORING DETAILS SEE E-1027
- 10) FOR BARRIER DIMENSIONS SEE E-1023
- 11) FOR BARRIER PROFILE DIM'S SEE E-1025
- 12) FOR CONNECTION DETAILS SEE E-1024
- 13) FOR SPEED JOINT IDENTIFICATION (MALE/FEMALE) SEE E-1021

REV	REMARKS	DATE	CHK
-	-	-	-
DRAWN BY	DATE	MATERIAL	
GL	8/4/15	-	
SCALE	FINISH	WEIGHT	
NONE	GALV.	-	
Description			
ZONEGUARD BARRIER TRUCK LOADING DETAILS			
UNCONTROLLED WHEN PRINTED			
 HILL & SMITH INC. 987 BUCKEYE PARK ROAD COLUMBUS, OHIO 43207 (614)-340-6294 www.hillandsmith.com			
Drawing No. E-1009			

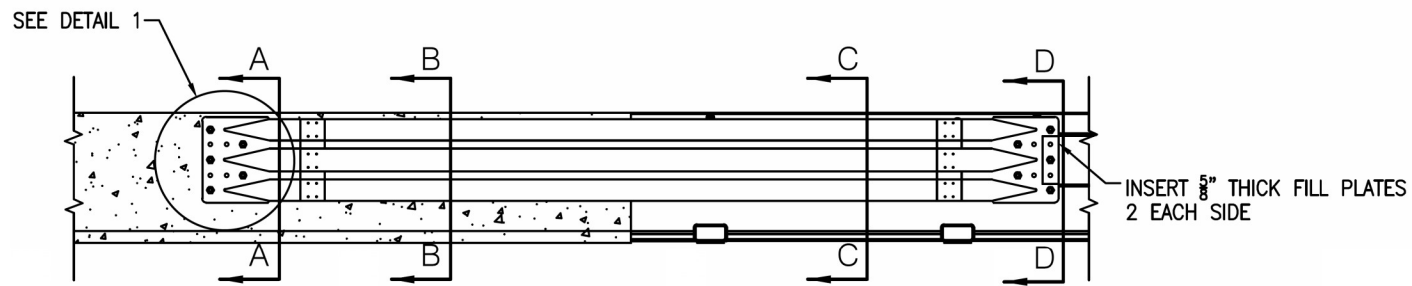


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CHECKED:	AWL	7/21/21
CHECKED:	AWL	7/21/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
DETAIL
ZONEGUARD BARRIER

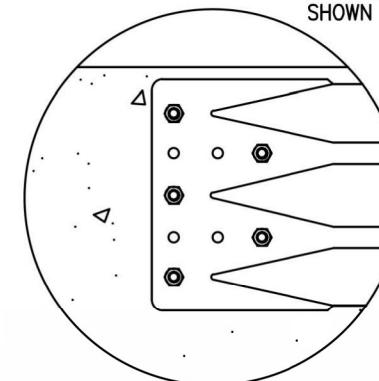
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N/A	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	31 of 59
CONTRACT	PROJECT
B-40719	1702255



ELEVATION VIEW

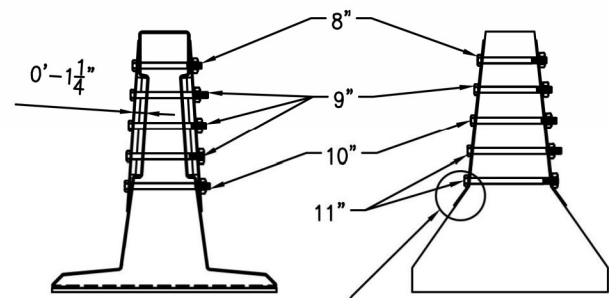
NOTES:

- 1) ALL BOLTS 3/4" DIAMETER A307 WITH NUT & WASHER
- 2)



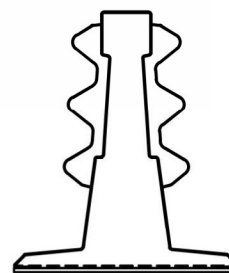
DETAIL 1

ONLY 5 BOLTS REQUIRED
IN EACH END SHOE
SHOWN THUS:

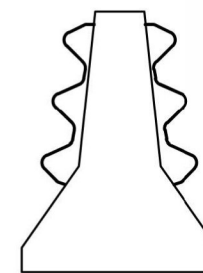


SECTION D-D

SECTION A-A



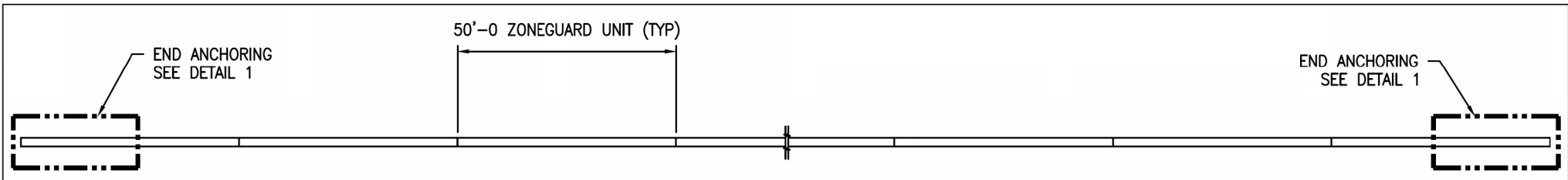
SECTION C-C



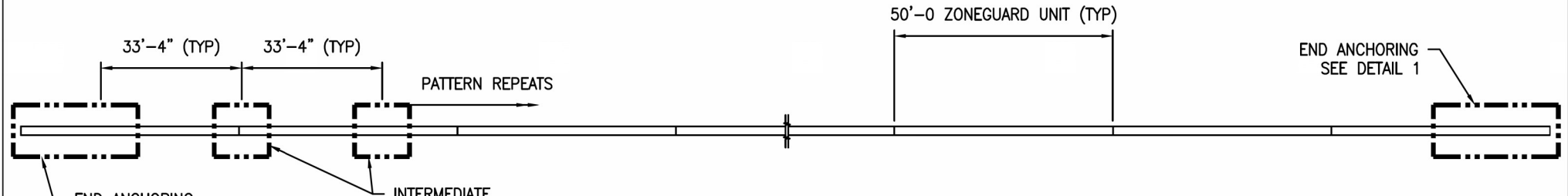
SECTION B-B

BEND END SHOE IN FIELD
BY UTILIZING BOLT TO COMPRESS

REV	REMARKS	ECR	DATE	CHK
DRAWN BY	DATE	MATERIAL		
GL	05/23/14	-		
SCALE	FINISH	WEIGHT		
NONE	-	-		
Description				
ZONEGUARD BARRIER DOUBLE SIDED TRANSITION TO CONCRETE BARRIER				
UNCONTROLLED WHEN PRINTED				
HILL & SMITH INC. 1000 BUCKEYE PARK ROAD COLUMBUS, OHIO 43207 (614)-340-6294 www.hillandsmith.com				
Drawing No. E-1014				

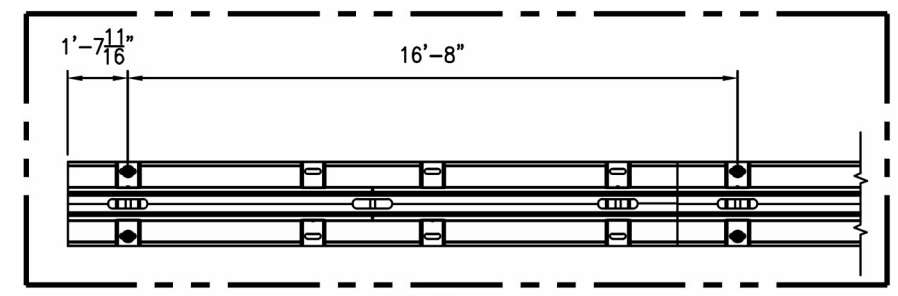


PLAN VIEW
TYPICAL STANDARD CONFIGURATION

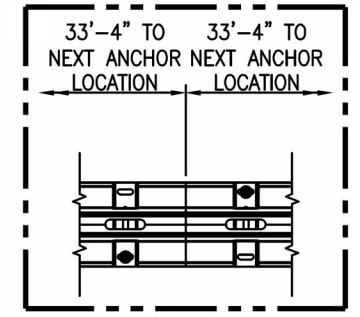


PLAN VIEW
TYPICAL MINIMUM DEFLECTION CONFIGURATION

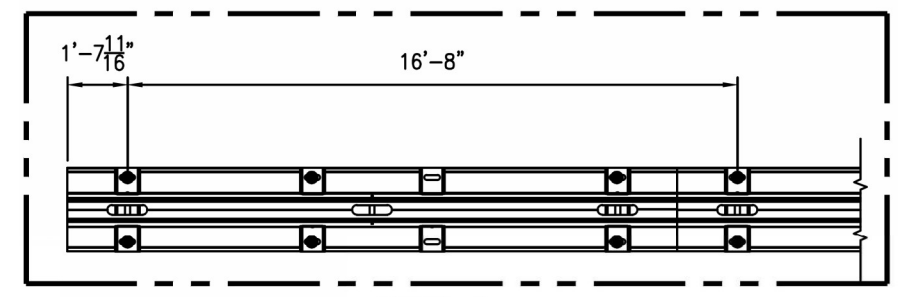
- NOTES:
- 1) ● DENOTES ANCHOR LOCATION
 - 2) FOR ANCHORING DETAILS SEE E-1027
 - 3) FOR BARRIER DIMENSIONS SEE E-1023
 - 4) FOR BARRIER PROFILE DIM'S SEE E-1025
 - 5) FOR CONNECTION DETAILS SEE E-1024



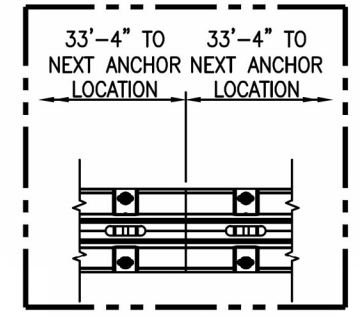
DETAIL 1
END ANCHORING WITH 1 1/2" ANCHORS




DETAIL 2
INTERMEDIATE ANCHORING WITH 1 1/2" ANCHORS



DETAIL 1
END ANCHORING WITH 7/8" ANCHORS ON BRIDGE DECKS



DETAIL 2
INTERMEDIATE ANCHORING WITH 7/8" ANCHORS ON BRIDGE DECKS

REV	REMARKS	DATE	CHK
-	-	-	-
DRAWN BY	GL	DATE	MATERIAL
SCALE	NONE	FINISH GALV.	WEIGHT
Description			
ZONEGUARD BARRIER ANCHORING LOCATIONS			
UNCONTROLLED WHEN PRINTED			
 HILL & SMITH INC. 987 BUCKEYE PARK ROAD COLUMBUS, OHIO 43207 (614)-340-6294 www.hillandsmith.com			
Drawing No.		E-1020	

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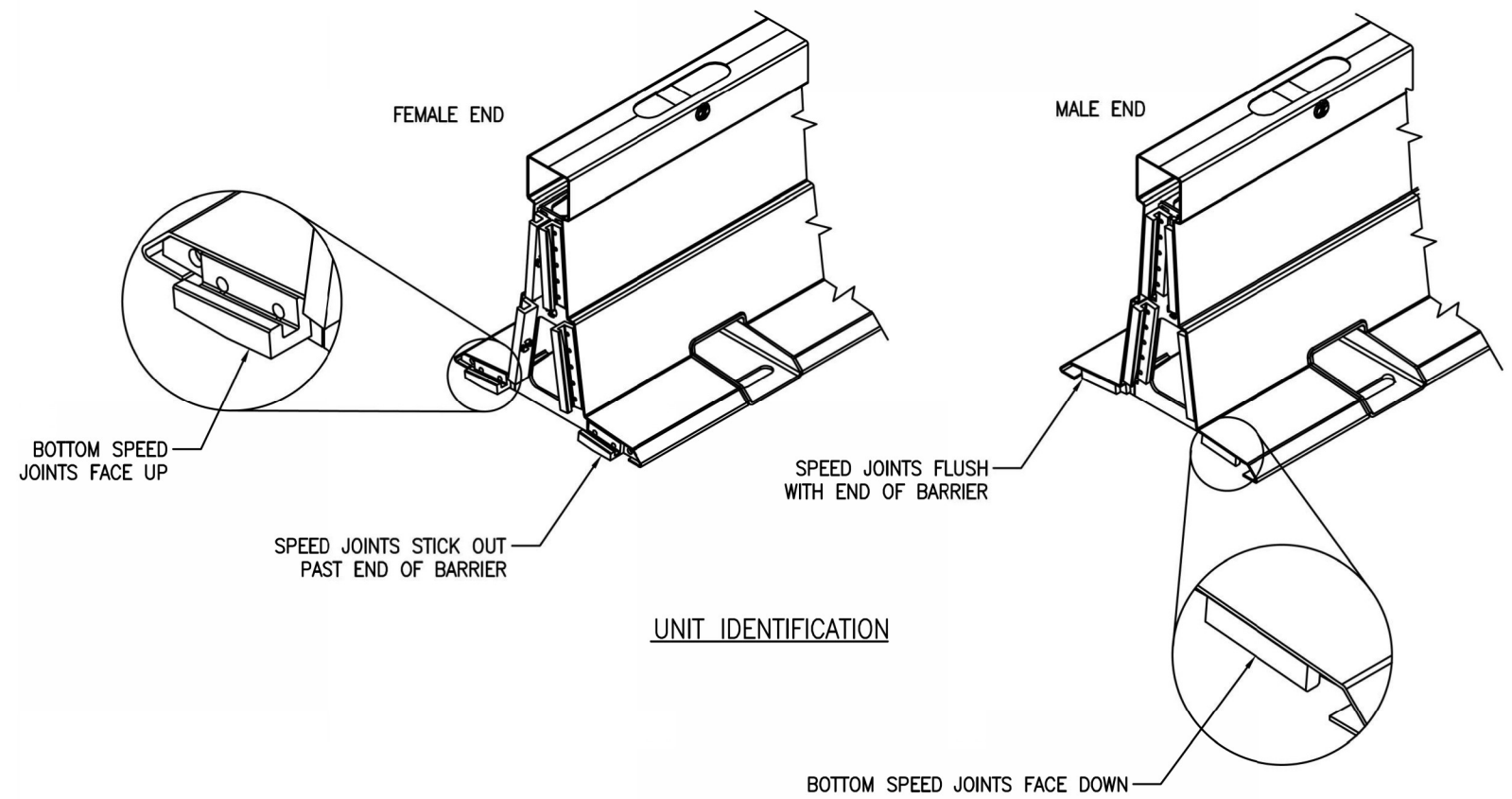



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CHECKED:	AWL	7/21/21
CHECKED:	AWL	7/21/21

INDIANA DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC DETAIL ZONEGUARD BARRIER

HORIZONTAL SCALE	BRIDGE FILE
N/A	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	33 of 59
CONTRACT	PROJECT
B-40719	1702255

- NOTES:
- 1) FOR ANCHORING DETAILS SEE E-1027
 - 2) FOR BARRIER DIMENSIONS SEE E-1023
 - 3) FOR BARRIER PROFILE DIM'S SEE E-1025
 - 4) FOR CONNECTION DETAILS SEE E-1024
 - 5) MOST BARRIER RUNS WILL BEGIN WITH THE UPSTREAM UNIT FIRST. PLACE THE UNIT IN LINE WITH THE FEMALE END FACING DOWNSTREAM.



REV	REMARKS	DATE	CHK
-	-	-	-
DRAWN BY	DATE	MATERIAL	
GL	-	-	
SCALE	FINISH	WEIGHT	
NONE	GALV.	-	
Description			
ZONEGUARD BARRIER IDENTIFICATION OF SPEED JOINTS			
UNCONTROLLED WHEN PRINTED			
 HILL & SMITH INC. 987 BUCKEYE PARK ROAD COLUMBUS, OHIO 43207 (614)-340-6294 www.hillandsmith.com			
Drawing No. E-1021			

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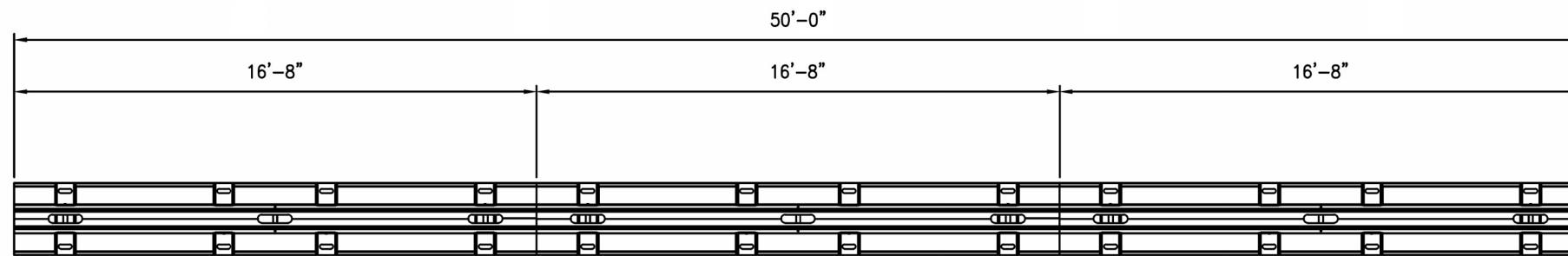


RECOMMENDED FOR APPROVAL	<i>Andrew M. Ledbetter</i>	8/20/2021
DESIGNED:	DPS	7/20/21
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CHECKED:	AWL	7/21/21
CHECKED:	AWL	7/21/21

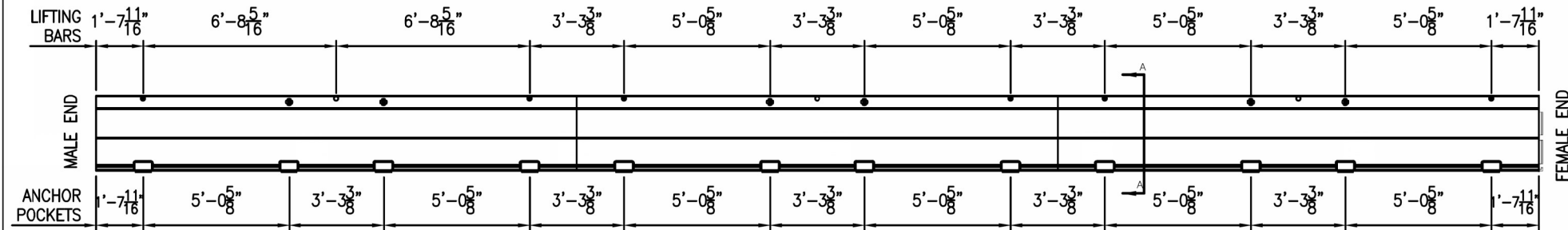
INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC DETAIL ZONEGUARD BARRIER

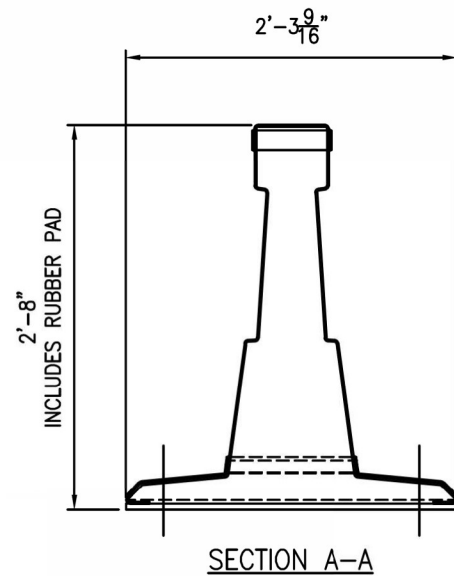
HORIZONTAL SCALE	BRIDGE FILE
N/A	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	34 of 59
CONTRACT	PROJECT
B-40719	1702255



PLAN VIEW

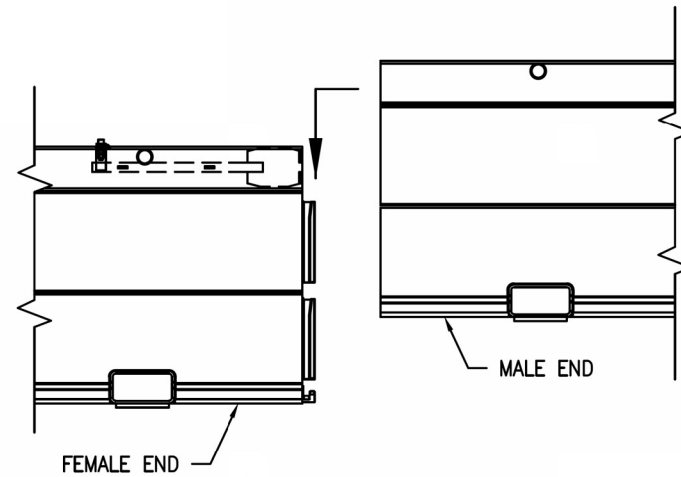


ELEVATION VIEW

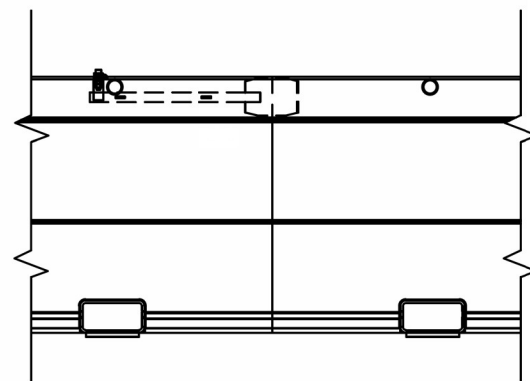


- NOTES:
- 1) FOR ANCHORING DETAILS SEE E-1027
 - 2) FOR BARRIER DIMENSIONS SEE E-1023
 - 3) FOR BARRIER PROFILE DIM'S SEE E-1025
 - 4) FOR CONNECTION DETAILS SEE E-1024
 - 5) FOR SPEED JOINT IDENTIFICATION (MALE/FEMALE) SEE E-1021

REV	REMARKS	DATE	CHK
-	-	-	-
DRAWN BY	DATE	MATERIAL	
GL	-	-	
SCALE	FINISH	WEIGHT	
NONE	GALV.	-	
Description			
ZONEGUARD BARRIER 50'-0 UNIT DIMENSIONS			
UNCONTROLLED WHEN PRINTED			
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Drawing No. E-1023			




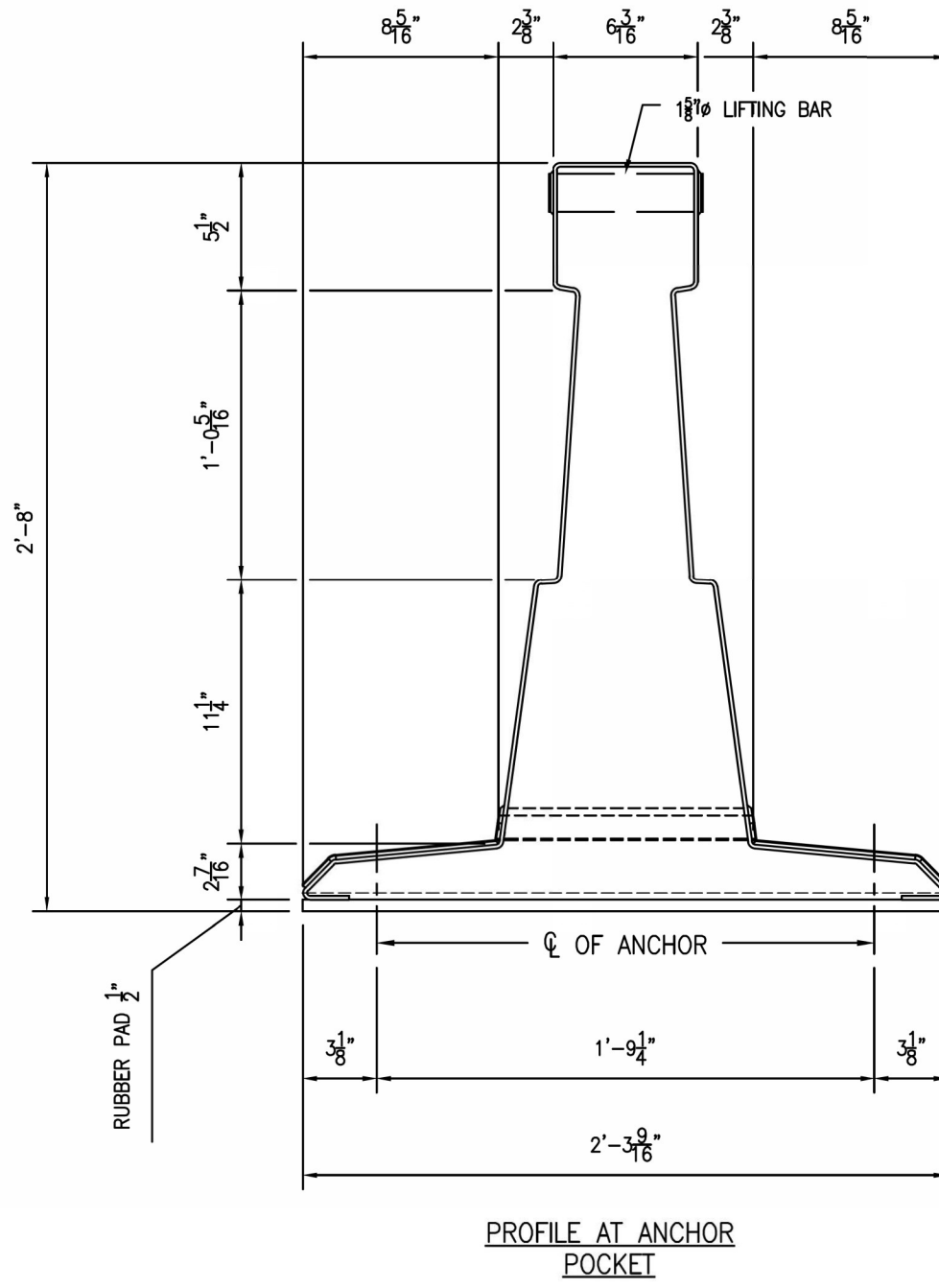
STEP 1
SLIDE MALE END ONTO FEMALE




STEP 2
REACH THROUGH OPENING IN TOP OF BARRIER, SLIDE ENGAGEMENT ASSEMBLY TO ENGAGE MALE END, AND TIGHTEN BOLT

- NOTES:
- 1) FOR ANCHORING DETAILS SEE E-1027
 - 2) FOR BARRIER DIMENSIONS SEE E-1023
 - 3) FOR BARRIER PROFILE DIM'S SEE E-1025
 - 4) FOR CONNECTION DETAILS SEE E-1024
 - 5) FOR SPEED JOINT IDENTIFICATION (MALE/FEMALE) SEE E-1021

REV	REMARKS	DATE	CHK
-	-	-	-
DRAWN BY	DATE	MATERIAL	
GL	-	-	
SCALE	FINISH	WEIGHT	
NONE	GALV.	-	
Description			
ZONEGUARD BARRIER CONNECTION DETAIL			
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Drawing No. E-1024			



- NOTES:
- 1) FOR ANCHORING DETAILS SEE E-1027
 - 2) FOR BARRIER DIMENSIONS SEE E-1023
 - 3) FOR BARRIER PROFILE DIM'S SEE E-1025
 - 4) FOR CONNECTION DETAILS SEE E-1024
 - 5) FOR SPEED JOINT IDENTIFICATION (MALE/FEMALE) SEE E-1021

REV	REMARKS	DATE	CHK
-	-	-	-
DRAWN BY	DATE	MATERIAL	
GL	-	-	
SCALE	FINISH	WEIGHT	
NONE	GALV.	-	
Description			
ZONEGUARD BARRIER PROFILE			
UNCONTROLLED WHEN PRINTED			
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Drawing No. E-1025			

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		7/21/21

INDIANA DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC DETAIL ZONEGUARD BARRIER

HORIZONTAL SCALE	BRIDGE FILE
N/A	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	37 of 59
CONTRACT	PROJECT
B-40719	1702255

1 DETERMINE ANCHOR TYPE

SURFACE	ANCHOR TYPE
CONCRETE BRIDGE DECK >6"	$\frac{7}{8}$ " ϕ ALL THREAD ANCHOR OR KELIANCHOR
CONCRETE BRIDGE DECK >8"	$1\frac{1}{4}$ " ϕ ALL THREAD ANCHOR OR KELIANCHOR
CONCRETE ROADWAY >6"	$1\frac{1}{4}$ " ϕ ANCHOR PIN x 12"
ASPHALT >3"	$1\frac{1}{4}$ " ϕ ANCHOR PIN x 20"

2 DETERMINE ZONEGUARD SYSTEM CONFIGURATION

ZONEGUARD HAS BEEN TESTED IN TWO CONFIGURATIONS, NAMELY STANDARD & MINIMUM DEFLECTION CONFIGURATION. THE ONLY DIFFERENCE IN THESE TWO IS THE QUANTITY OF ANCHORS USED. THE STANDARD CONFIGURATION IS ANCHORED AT THE ENDS OF THE RUN ONLY. THE MINIMUM DEFLECTION CONFIGURATION IS ANCHORED AT THE ENDS OF THE RUN AND EVERY 33'4" ALONG THE LENGTH OF THE RUN. REFER TO SHEET E-1020 FOR DETAILS.

IN GENERAL IF UNANCHORED CONCRETE BARRIER IS SPECIFIED THE STANDARD CONFIGURATION IS AN APPROPRIATE ALTERNATE. IF LESS DEFLECTION IS DESIRED THE MINIMUM DEFLECTION SYSTEMS SHOULD BE USED.

3 DETERMINE ANCHOR LOCATIONS

BASED UPON ZONEGUARD CONFIGURATION CHOSEN, DETERMINE ANCHOR LOCATIONS FROM E-1020

KELIANCHOR INSTALLATION INSTRUCTIONS

KELIANCHOR NOTES

- 1) THE LEFT HAND KELIANCHOR IS MANUFACTURED BY KELKEN CONSTRUCTION SYSTEMS.
- 2) MATERIAL SPECIFICATION IS ASTM F1554 GRADE 55 GALVANIZED TO ASTM A153. EACH ANCHOR INCLUDES ONE A563 GRADE DH NUT AND ONE F436 FLAT WASHER.
- 3) ANCHOR IS SHIPPED PRECOATED WITH KELISLIP, WHICH IS A PAINT LIKE PRODUCT THAT ALLOWS FOR THE REMOVAL OF THE ANCHORS

FIRST INSTALLATION PROCEDURE

- 1) DETERMINE IF BOLT HAS BEEN SHIPPED FROM THE FACTORY OR IF IT HAS ALREADY BEEN INSTALLED. IF IT HAS BEEN SHIPPED FROM THE FACTORY IT WILL HAVE BEEN PRECOATED WITH KELISLIP, PROCEED TO STEP 2. IF IT HAS BEEN INSTALLED PREVIOUSLY PROCEED TO "SUBSEQUENT INSTALLATION PROCEDURE BELOW.
- 2) DRILL A 1" ϕ HOLE FOR THE $\frac{7}{8}$ " ANCHOR OR A $1\frac{1}{8}$ " HOLE FOR THE $1\frac{1}{4}$ " ANCHOR, FOR THE EMBEDMENT DEPTHS AS SHOWN, AND BLOW HOLE CLEAN.
- 3) PRIOR TO INJECTING KELIGROUT INTO HOLE, ADJUST THE NUT SO THAT PROPER EMBEDMENT DEPTH CAN BE ACHIEVED.
- 4) INJECT THE RESIN INTO THE HOLE, AND PUSH ANCHOR AND NUT ASSEMBLY THROUGH ZONEGUARD FOOT SECTION AND INTO THE HOLE. EMBEDDING THE ANCHOR PAST THE LEFT-HAND THREADS MAY RESULT IN ANCHOR NOT BEING ABLE TO BE REMOVED.
- 5) DO NOT DISTURB THE ANCHOR FOR 1 HOUR, ALLOW 24 HOURS FOR FULL CURE.
- 6) FOR FURTHER INSTRUCTIONS REGARDING KELIGROUT, SUCH AS SHELF LIFE, ETC. SEE TECHNICAL INFORMATION INCLUDED WITH EVERY SHIPMENT OF KELIGROUT.

ANCHOR REMOVAL PROCEDURE

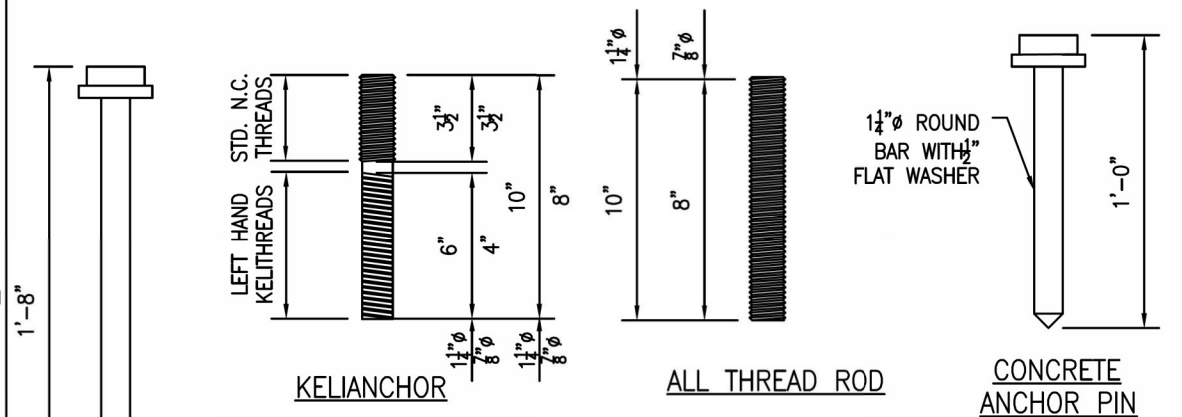
PLACE AN ADDITIONAL NUT ONTO THE TOP OF THE EXISTING NUT, AND USING A SOCKET DEEP ENOUGH TO COVER BOTH NUTS, THE ANCHOR WILL UNSCREW ITSELF FROM THE HOLE.

SUBSEQUENT INSTALLATION PROCEDURE

IF ANCHOR HAS BEEN USED PREVIOUSLY, IT MUST BE RECOATED WITH KELISLIP PRIOR TO REUSE. DIP THE LEFT HAND THREAD ANCHORS SECTION INTO THE QUART OF KELISLIP, OR BRUSH IT ON. ALLOW THE EXCESS TO DRIP BACK INTO THE CAN OR ANOTHER CAN. ALLOW MINIMUM 1 HOUR DRY TIME BEFORE PROCEEDING TO STEP 2 ABOVE

MATERIAL USAGE

	APPROXIMATE NUMBER OF ANCHORS PER 32 OZ KELIGROUT 101-P	APPROXIMATE RECOATINGS PER QUART OF KELISLIP
$\frac{7}{8}$ " ANCHOR	40	60
$1\frac{1}{4}$ " ANCHOR	12	40



REV	REMARKS	DATE	CHK
-	-	-	-
DRAWN BY	DATE	MATERIAL	
GL	-	-	
SCALE	FINISH	WEIGHT	
NONE	GALV.	-	

Description

ANCHOR TYPES AND INSTALLATION DETAILS

UNCONTROLLED WHEN PRINTED

HILL & SMITH INC.
987 BUCKEYE PARK ROAD
COLUMBUS, OHIO 43207
(614)-340-6294
www.hillandsmith.com

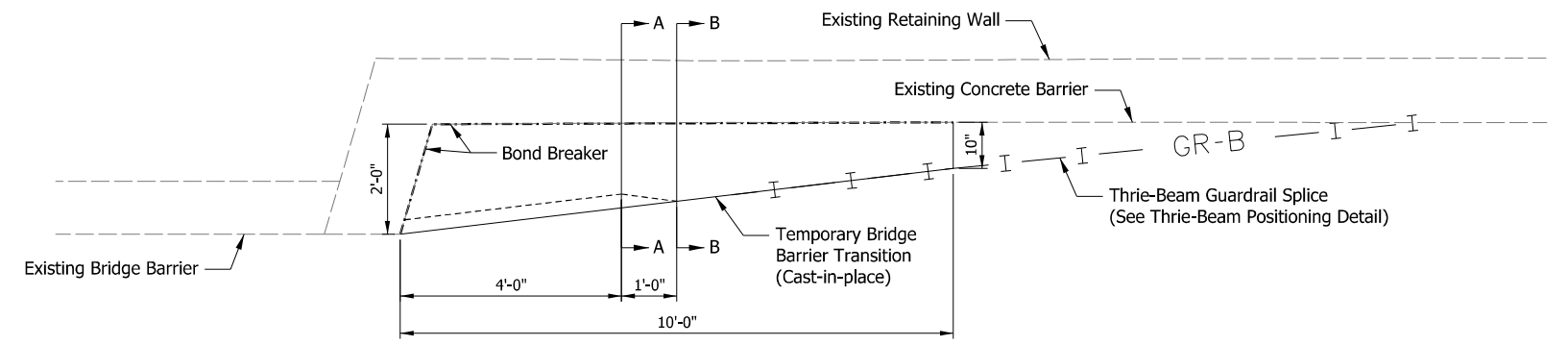
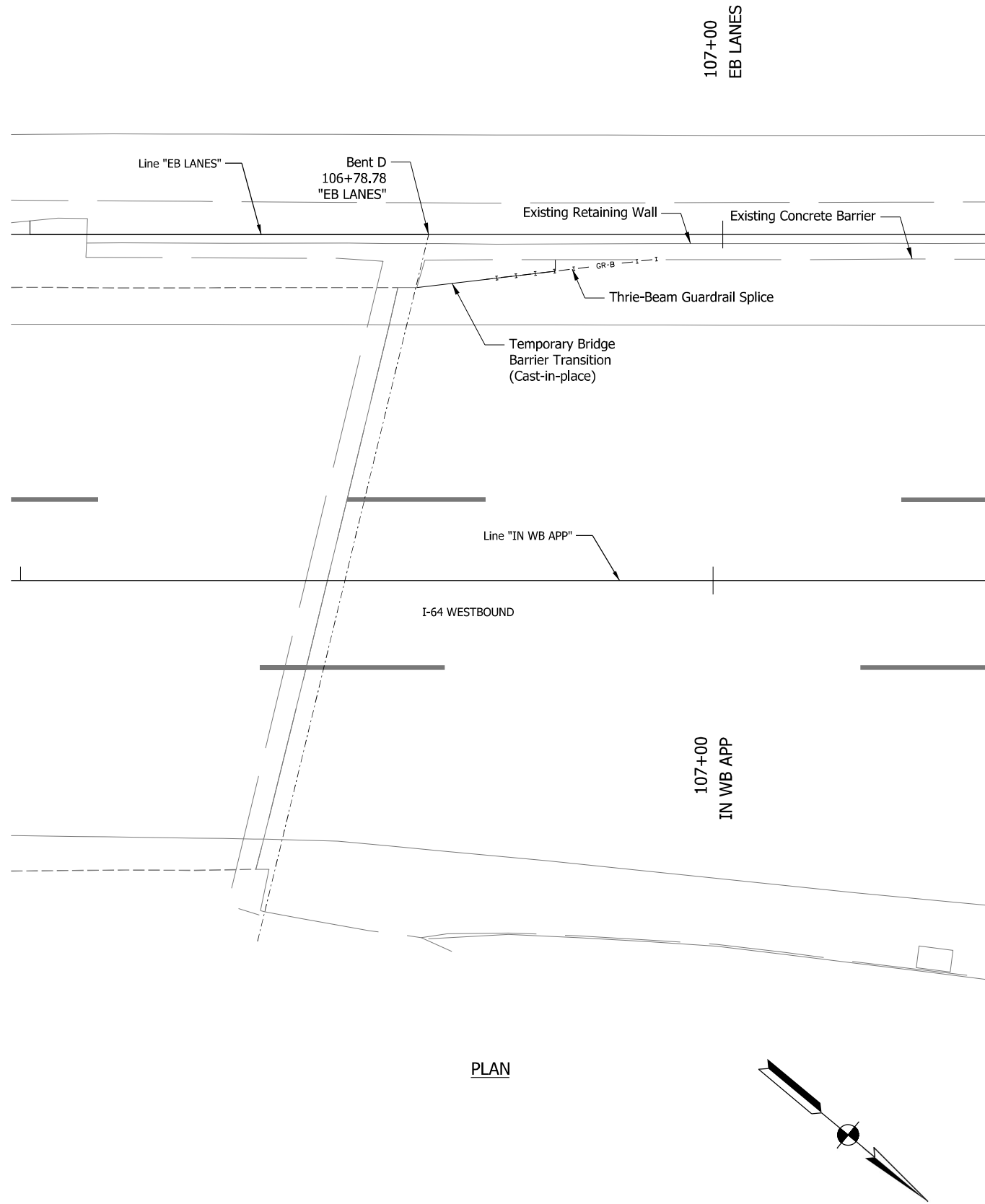
Drawing No. E-1027

NOTES:

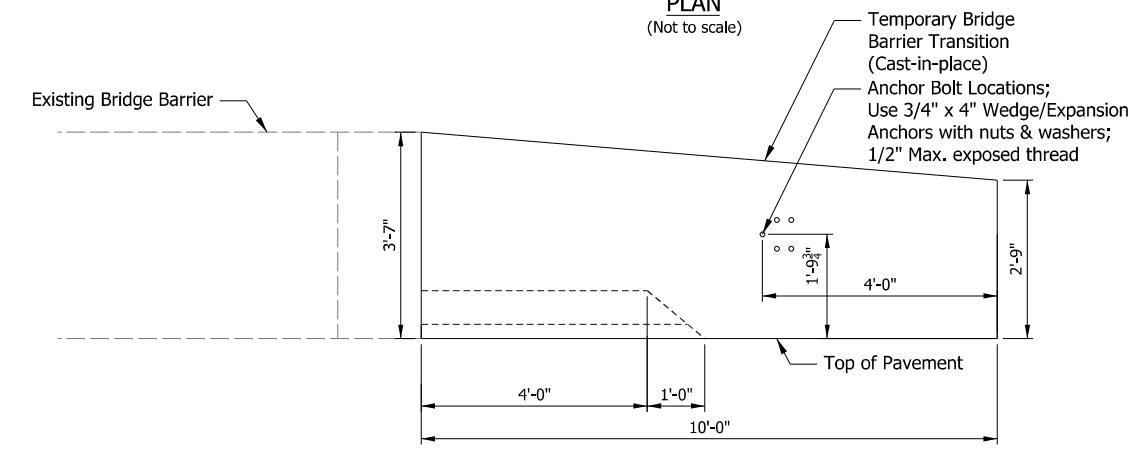
1) ZONEGUARD SHALL NOT BE ANCHORED INTO AN EARTH OR DIRT SURFACE. FOR INSTALLATIONS ON SURFACES OTHER THAN CONCRETE OR ASPHALT CONTACT THE MANUFACTURER.

ANCHOR HOLE PATCHING PROCEDURE IN NEW PAVEMENT

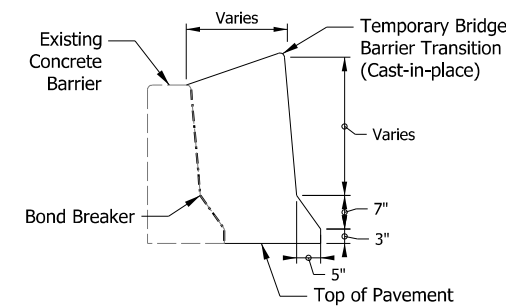
- 1) CLEAN HOLE AFTER ANCHOR BOLT REMOVAL OF ALL DEBRIS AND KELISLIP RELEASE AGENT WITH COMPRESSED AIR.
- 2) FILL CLEANED HOLE WITH EPOXY FROM INDOT APPROVED MATERIALS LIST.



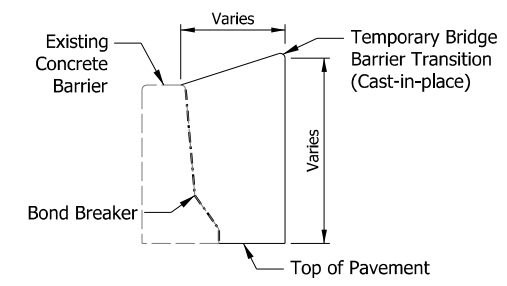
PLAN
(Not to scale)



ELEVATION
(Not to scale)



SECTION A-A
(Not to scale)



SECTION B-B
(Not to scale)

- NOTE:
- Use Class C concrete for Temporary Bridge Barrier Transition.
 - Temporary Bridge Barrier Transition to be removed during final phase of bridge construction.
 - Cover existing barriers and pavement with polyethylene as a bond breaker.

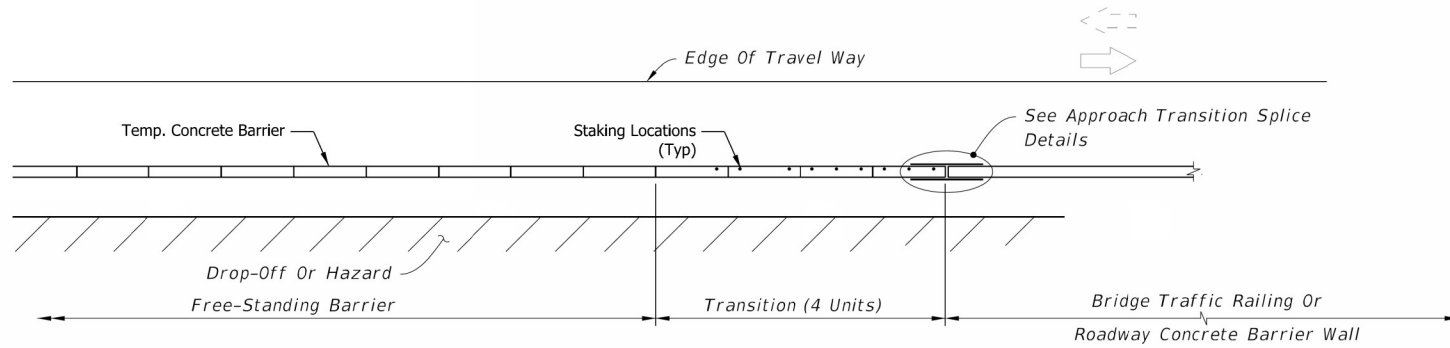


RECOMMENDED FOR APPROVAL	<i>Andrew M. Helter</i>	8/20/2021
DESIGNED: DPS	8/18/21	DRAWN: DPS 8/18/21
CHECKED: AWL	8/19/21	CHECKED: AWL 8/19/21

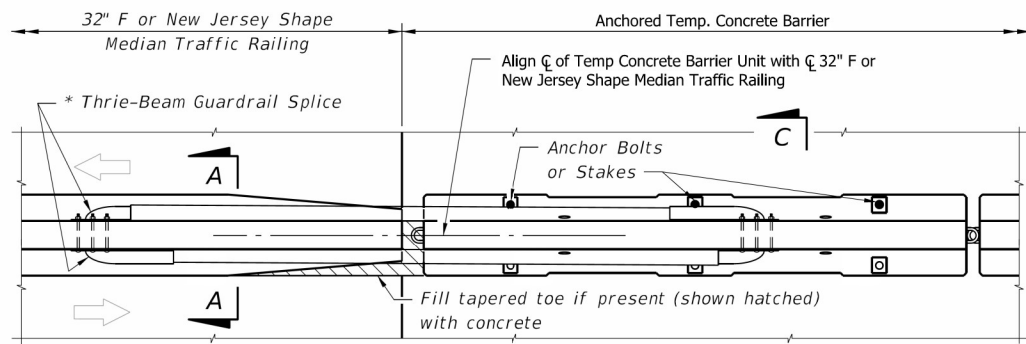
INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
DETAIL
TEMPORARY BRIDGE BARRIER TRANSITION

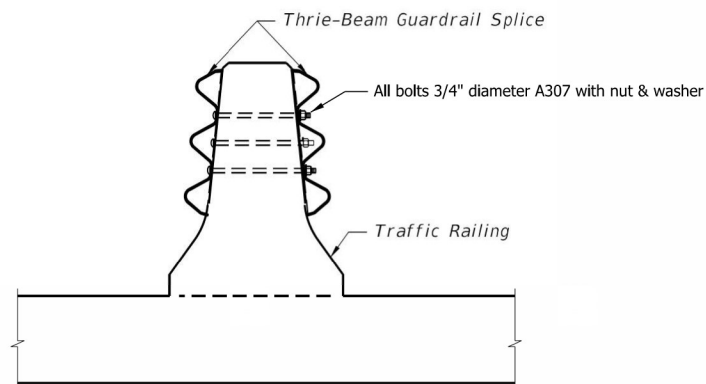
HORIZONTAL SCALE	BRIDGE FILE
1"=5'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
2B	39 of 59
CONTRACT	PROJECT
B-40719	1702255



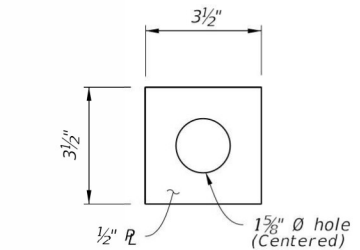
Transition from Free Standing Temp Concrete Barrier to Roadway Median Concrete Barrier Wall



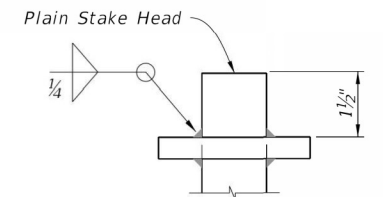
PARTIAL PLAN VIEW AT MEDIAN TRAFFIC RAILING



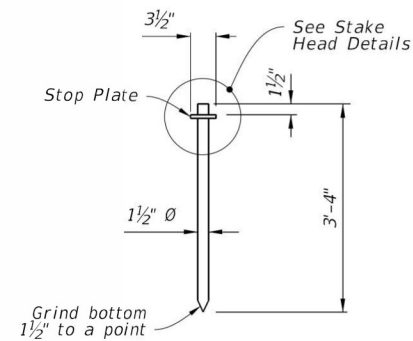
SECTION A-A
32" F Shape Median Traffic Railing (shown),
Median Concrete Barrier Wall (similar)



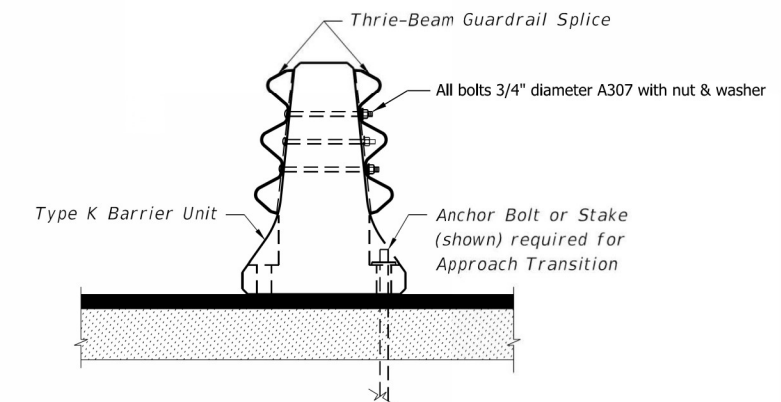
STOP PLATE DETAIL



PLAIN STAKE HEAD DETAIL

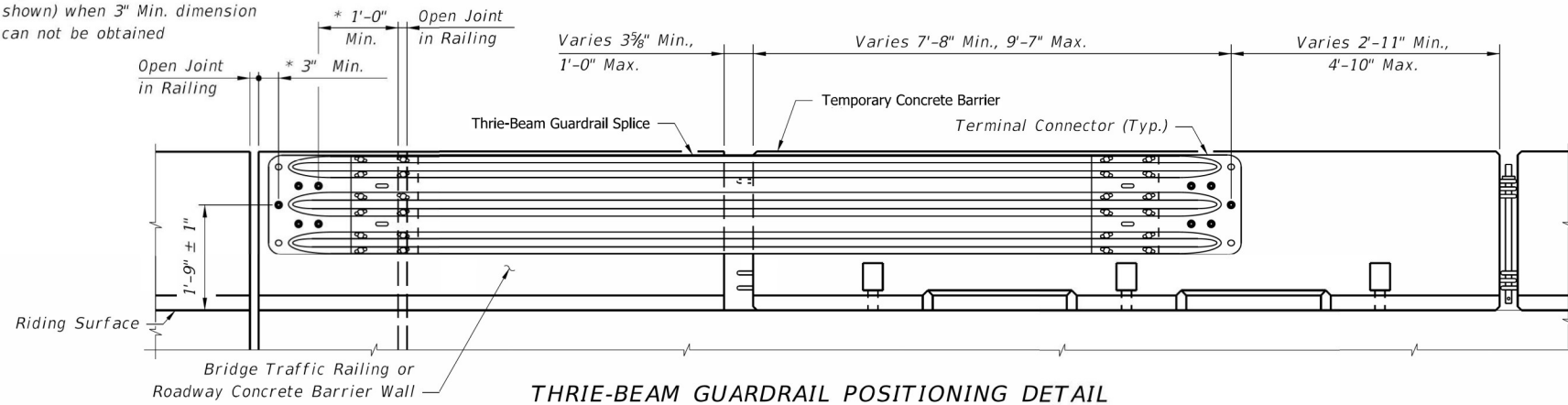


STAKE DETAIL



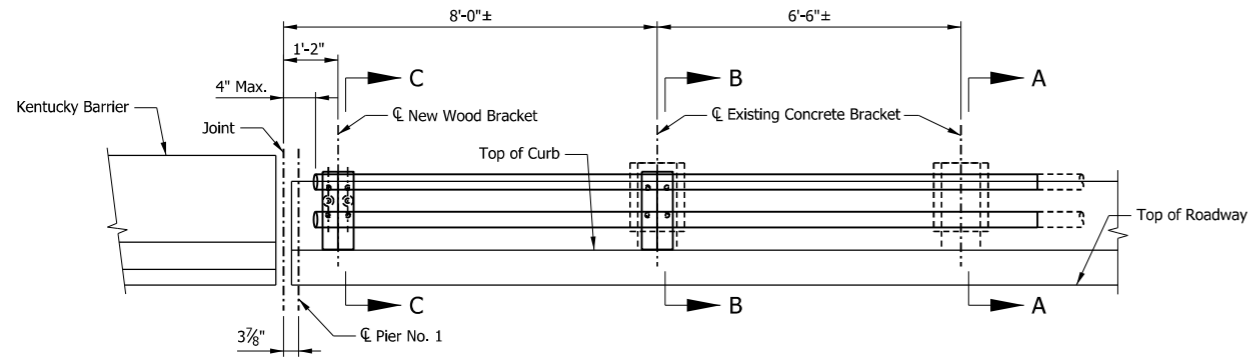
SECTION C-C
Adjacent to 32" F or New Jersey Shape
Median Traffic Railing or
Median Concrete Barrier Wall

* Shift Thrie-Beam Guardrail Splice beyond Open Joint 1'-0" Min. (as shown) when 3" Min. dimension can not be obtained

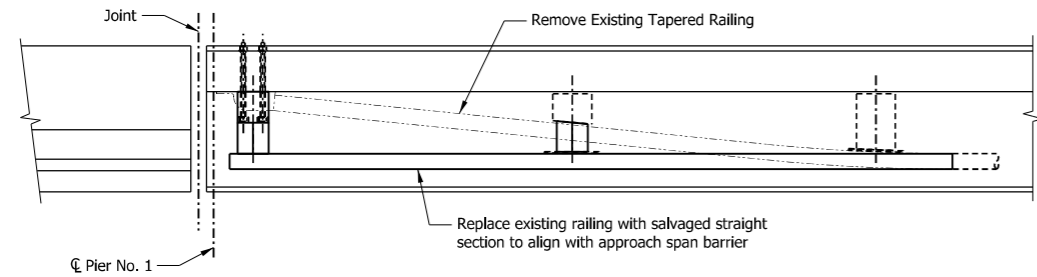


THRIE-BEAM GUARDRAIL POSITIONING DETAIL

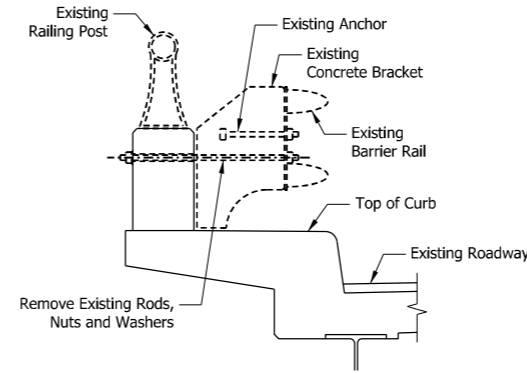
STAKES: Provide steel for Stake assemblies in accordance with ASTM A 36 or ASTM A 709 Grade 36.



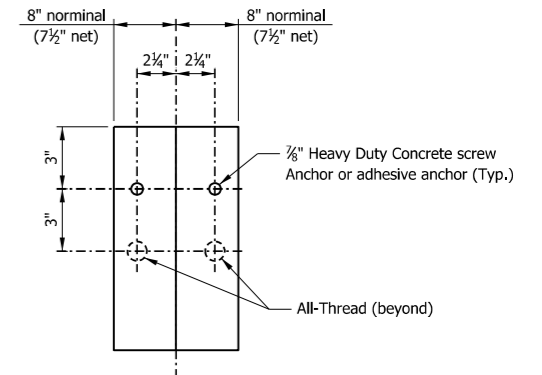
LEFT RAILING ELEVATION
Scale: No Scale



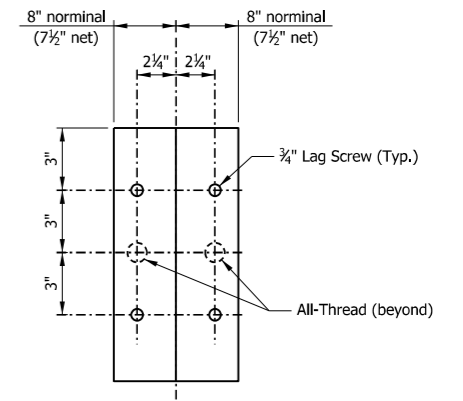
LEFT RAILING PLAN
Scale: No Scale



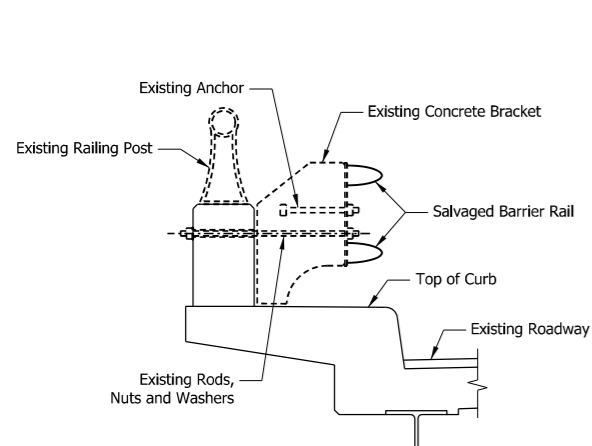
SECTION SHOWING EXISTING RAILING
Scale: No Scale



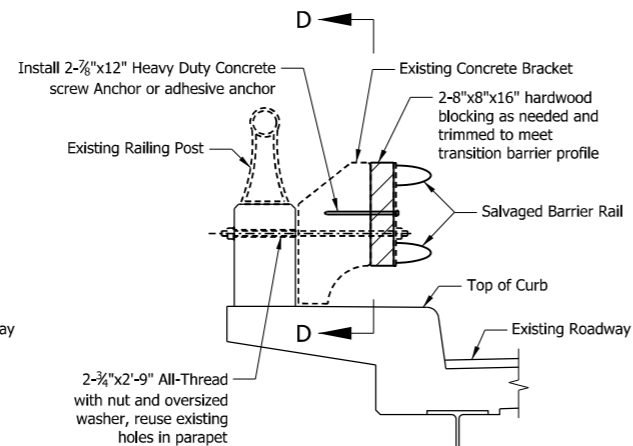
SECTION D-D
Scale: No Scale



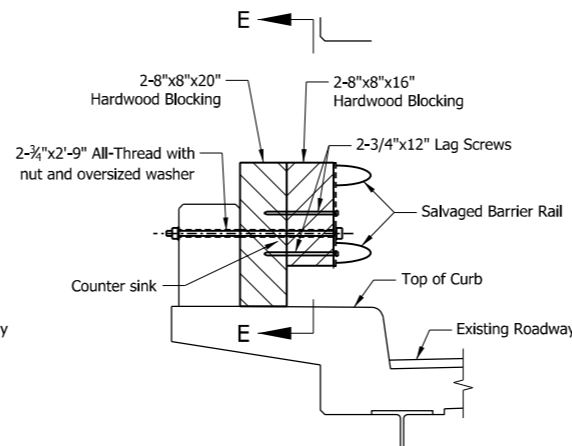
SECTION E-E
Scale: No Scale



SECTION A-A
Scale: No Scale



SECTION B-B
Scale: No Scale

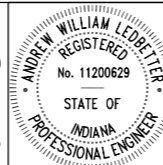


SECTION C-C
Scale: No Scale

NOTES

1. All thread shall be replaced as noted. Material shall be A325 (Fy = 120 ksi), or better.
2. Heavy Duty Concrete screw Anchor or adhesive anchor shall be A325 (Fy = 120 ksi), or better.
3. Lag screws shall be A305, Grade A (Fy = 60 ksi), or better.

REV.	DATE	DESCRIPTION
1	12/16/2021	SHEET ADDED

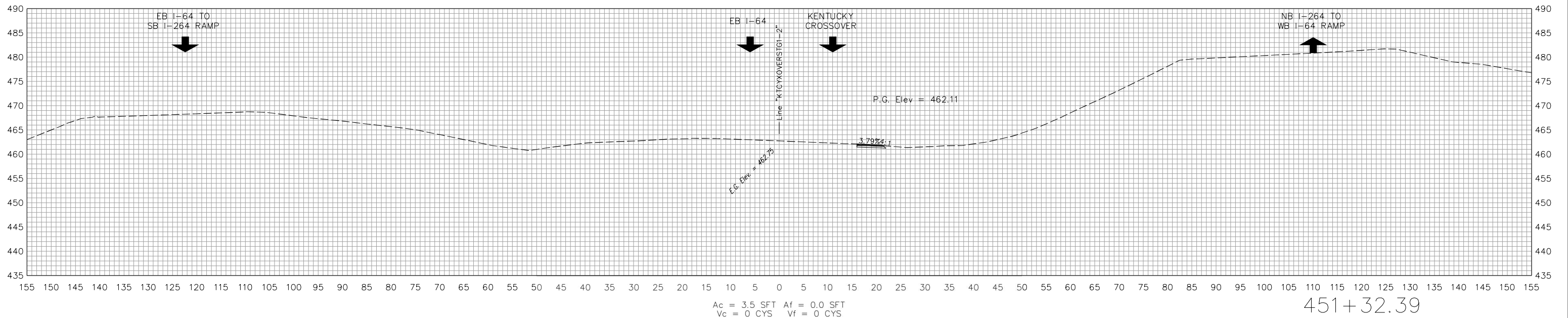
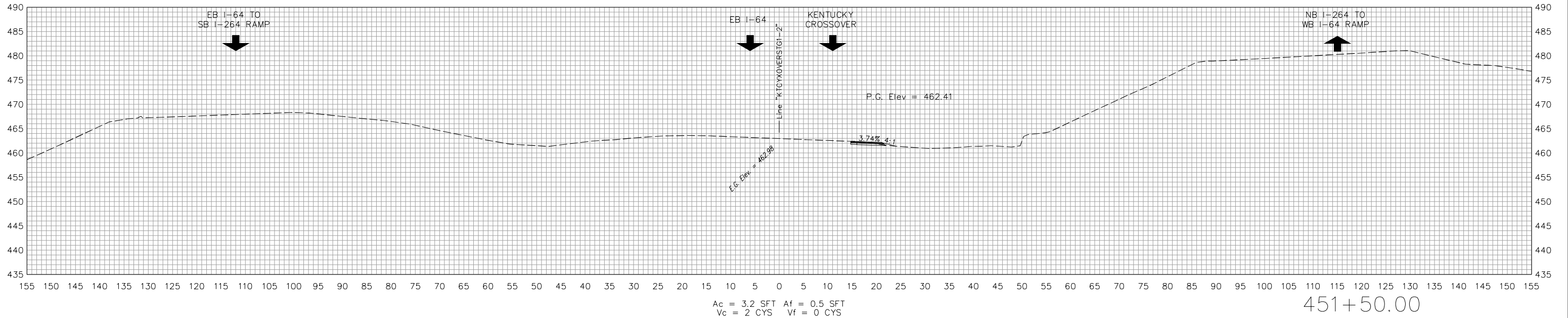


RECOMMENDED FOR APPROVAL	<i>Andrew W. Ledbetter</i>	12/16/2021
DESIGNED:	MC	DRAWN: EK
CHECKED:	AK	CHECKED: MC

INDIANA
DEPARTMENT OF TRANSPORTATION

PIER NO.1 TEMPORARY
GUARDRAIL TRANSITION

HORIZONTAL SCALE	BRIDGE FILE
NO SCALE	164 123 04691 E
VERTICAL SCALE	DESIGNATION
NO SCALE	1702255, 1592187
BUILDABLE UNIT	SHEETS
2B	40-1 OF 59
CONTRACT	PROJECT
B-40719	1702255



STA. 451+32.39 TO STA. 451+50.00

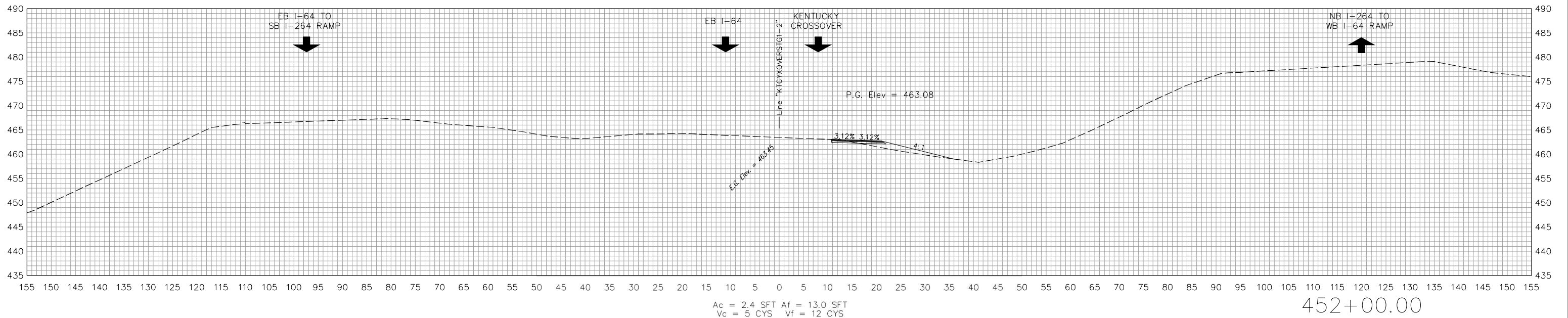
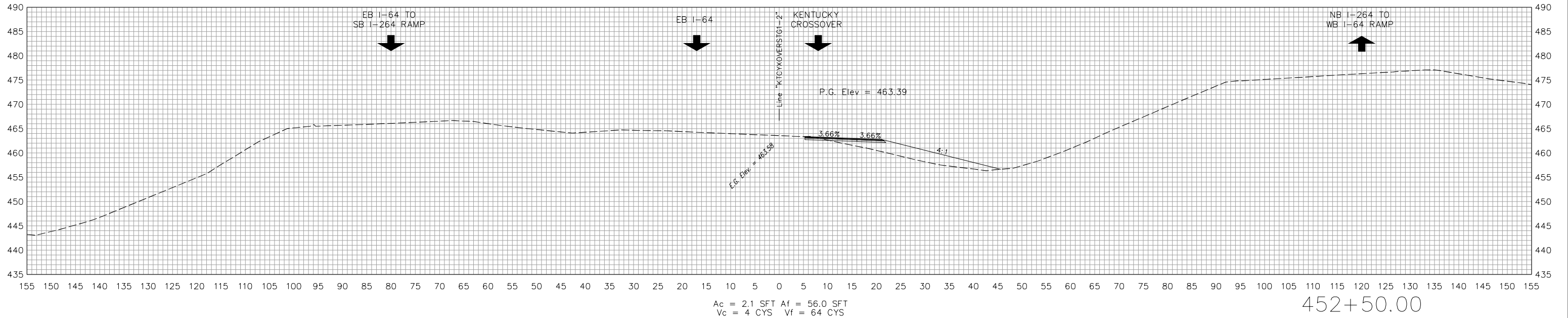


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: KLO	06/01/21	DRAWN: KLO 06/01/21
CHECKED: AWL		CHECKED: AWL

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
KENTUCKY CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	41 of 59
CONTRACT	PROJECT
R-40719	1702255



STA. 452+00.00 TO STA. 452+50.00

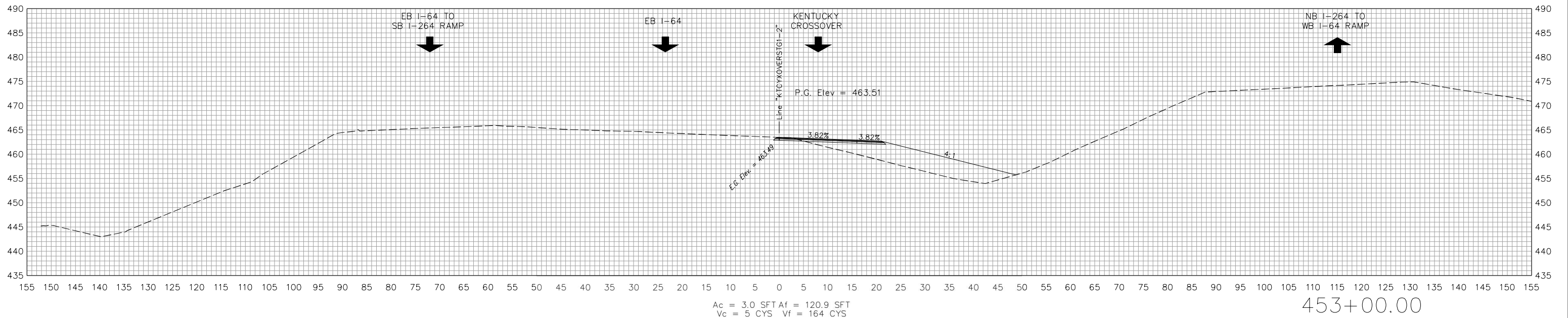
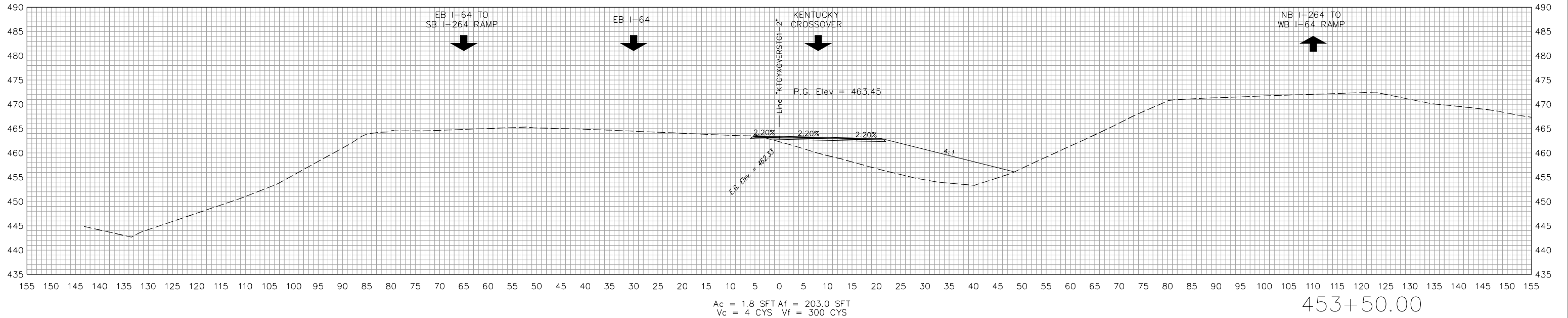


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: KLO	06/01/21	DRAWN: KLO 06/01/21
CHECKED: AWL		CHECKED: AWL

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
KENTUCKY CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	42 of 59
CONTRACT	PROJECT
R-40719	1702255



STA. 453+00.00 TO STA. 453+50.00



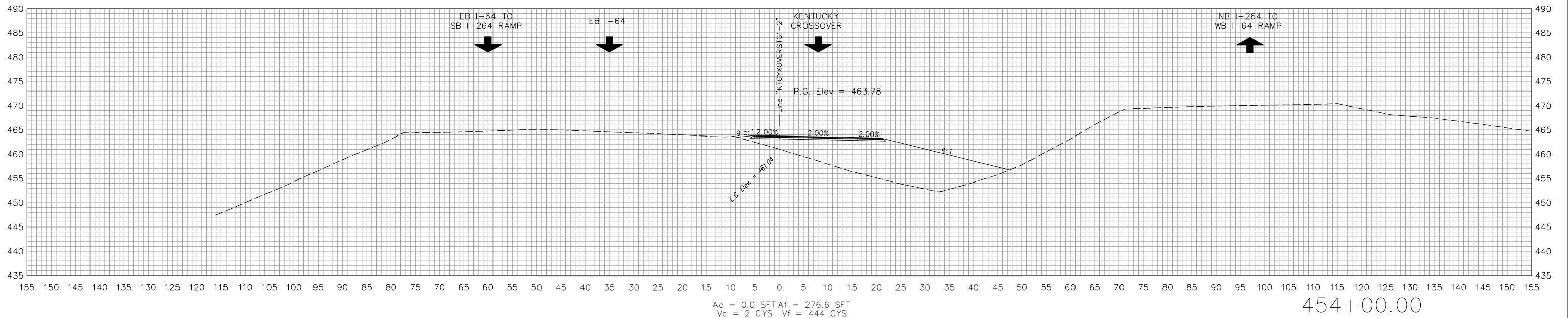
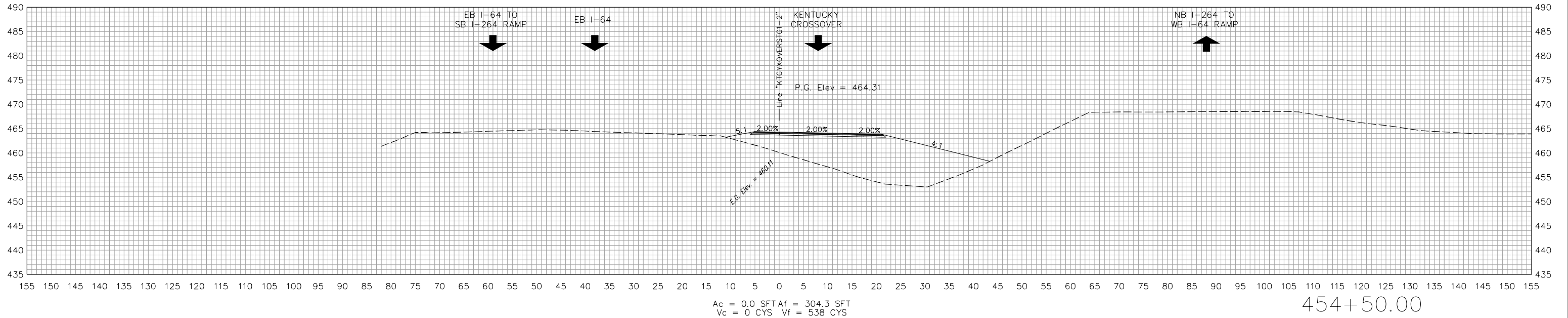
RECOMMENDED FOR APPROVAL		8/20/2021
DESIGNED: KLO	06/01/21	DRAWN: KLO 06/01/21
CHECKED: AWL		CHECKED: AWL

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
KENTUCKY CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	43 of 59
CONTRACT	PROJECT
R-40719	1702255

Plot: 19-AUG-2021 08:42 AM



STA. 454+00.00 TO STA. 454+50.00



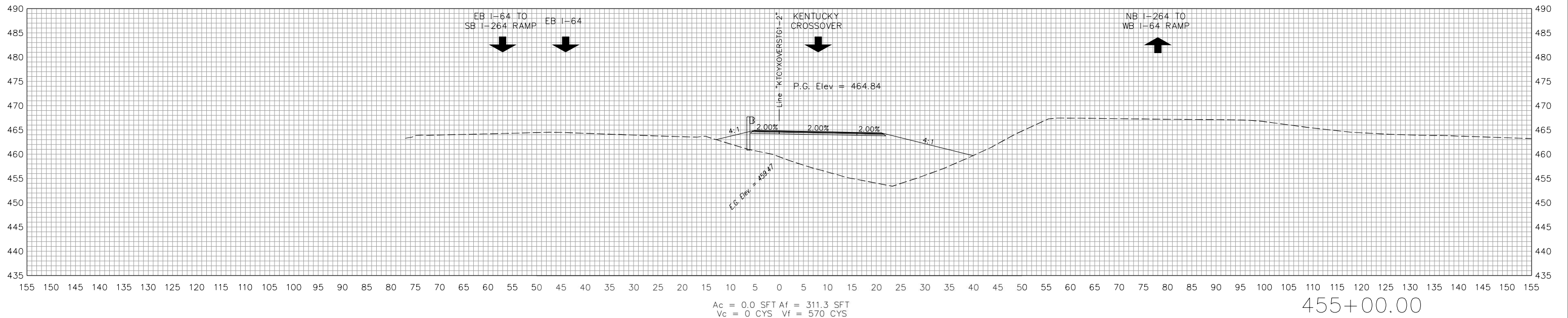
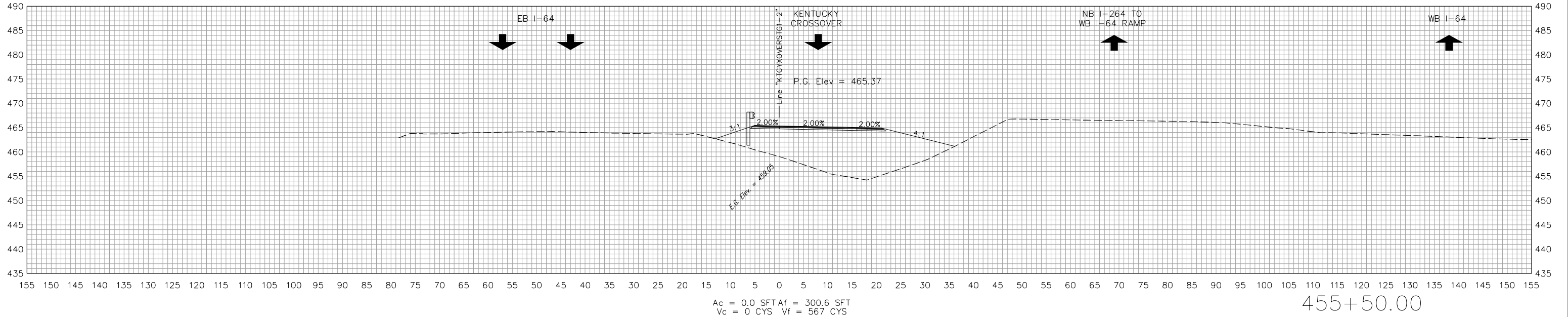
RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: KLO	06/01/21	DRAWN: KLO 06/01/21
CHECKED: AWL		CHECKED: AWL

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
KENTUCKY CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	44 of 59
CONTRACT	PROJECT
R-40719	1702255

Plot: 19-AUG-2021 08:42 AM



STA. 455+00.00 TO STA. 455+50.00

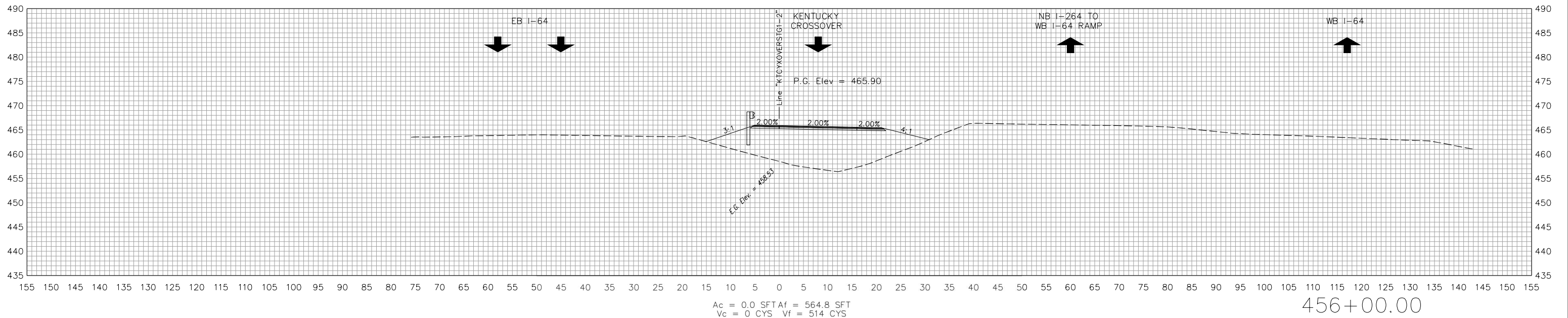
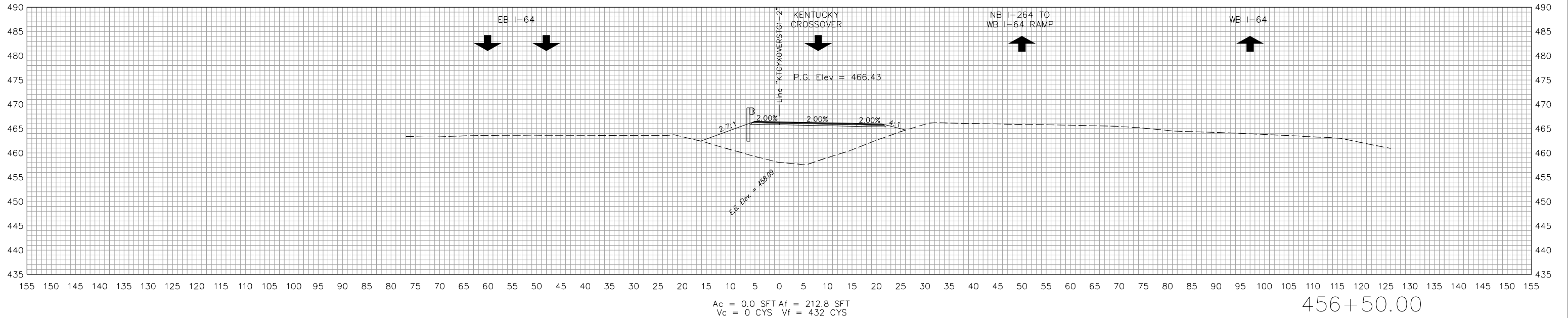


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: KLO	06/01/21	DRAWN: KLO 06/01/21
CHECKED: AWL		CHECKED: AWL

INDIANA DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS KENTUCKY CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	45 of 59
CONTRACT	PROJECT
R-40719	1702255



STA. 456+00.00 TO STA. 456+50.00

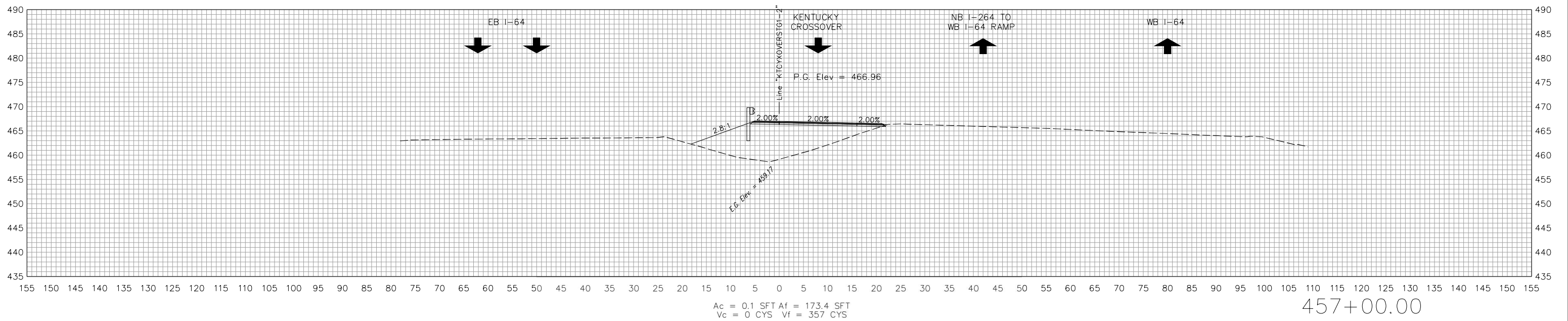
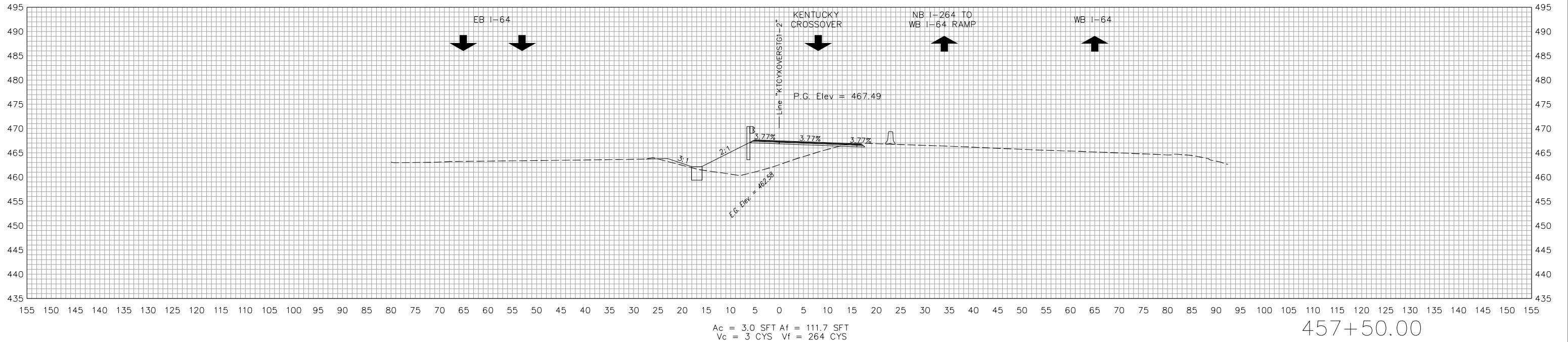


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021	DATE
DESIGNED: KLO	06/01/21	DRAWN: KLO	06/01/21
CHECKED: AWL		CHECKED: AWL	

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
KENTUCKY CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	46 of 59
CONTRACT	PROJECT
R-40719	1702255



STA. 457+00.00 TO STA. 457+50.00



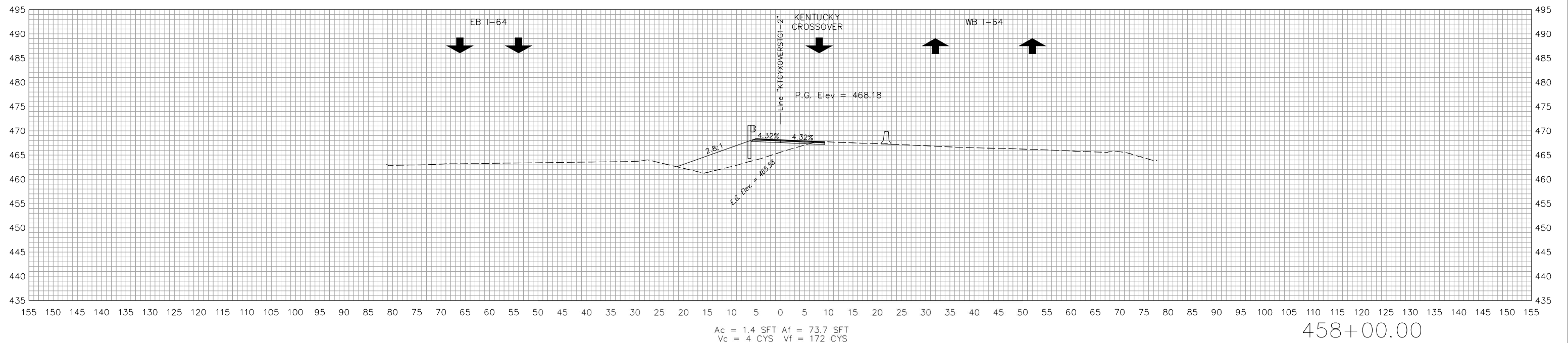
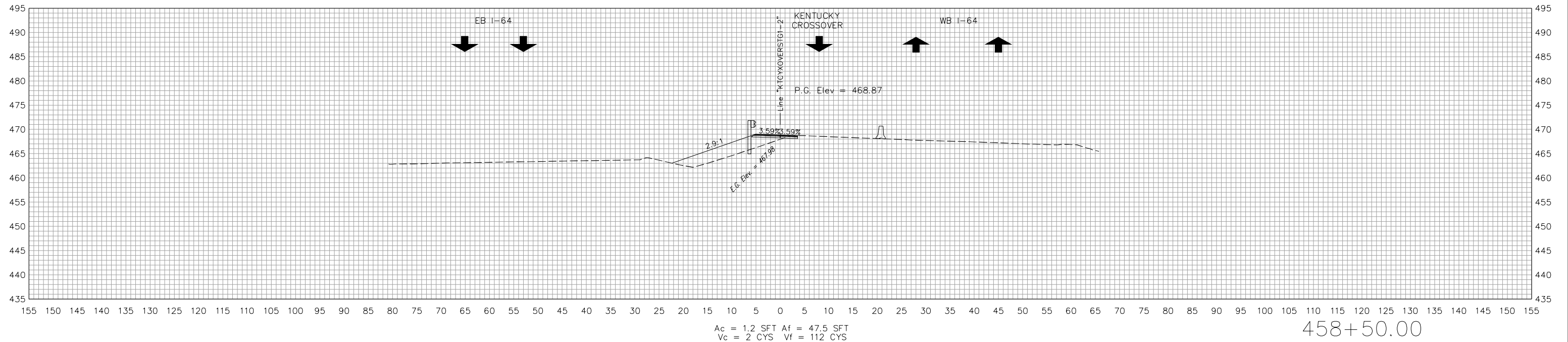
RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: KLO	06/01/21	DRAWN: KLO 06/01/21
CHECKED: AWL		CHECKED: AWL

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
KENTUCKY CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	47 of 59
CONTRACT	PROJECT
R-40719	1702255

Plot: 19-AUG-2021 08:42 AM

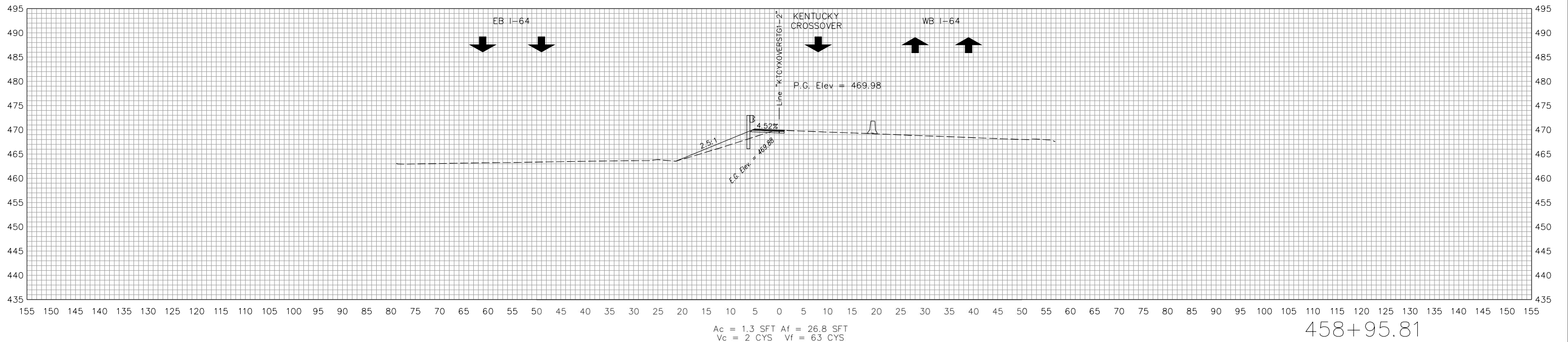


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
DESIGNED: KLO	06/01/21	DRAWN: KLO 06/01/21
CHECKED: AWL		CHECKED: AWL

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
KENTUCKY CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	48 of 59
CONTRACT	PROJECT
R-40719	1702255



Ac = 1.3 SFT Af = 26.8 SFT
 Vc = 2 CYS Vf = 63 CYS

458+95.81

Plot: 19-AUG-2021 08:42 AM



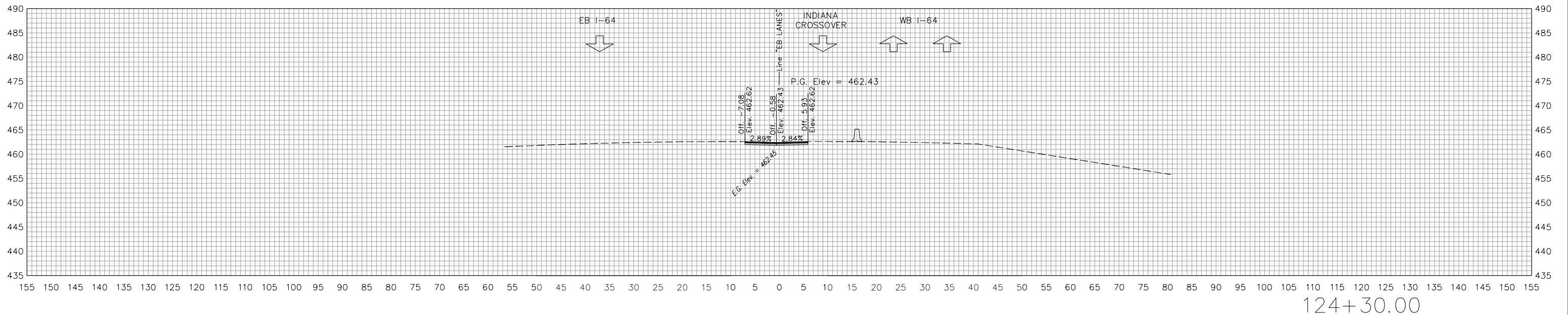
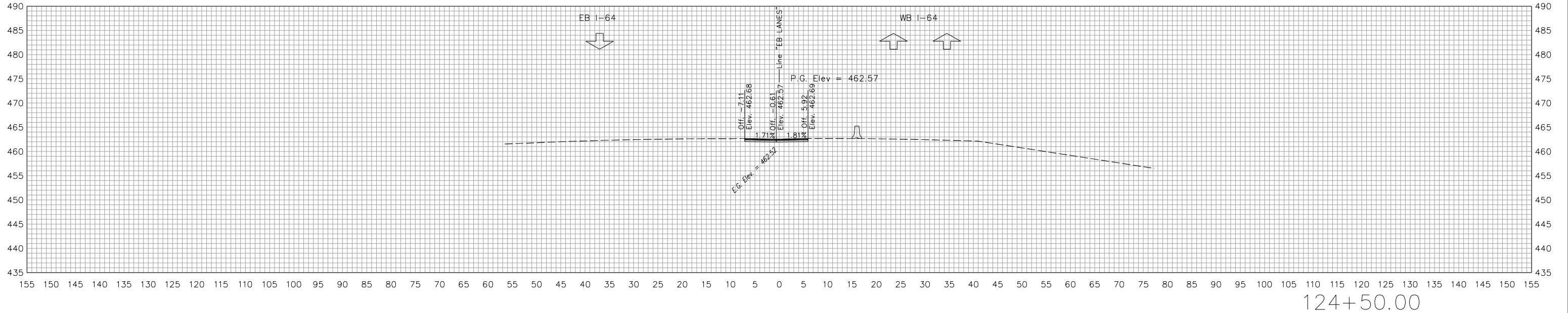
RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	8/20/2021
	DESIGN ENGINEER	DATE
DESIGNED: KLO	06/01/21	DRAWN: KLO 06/01/21
CHECKED: AWL		CHECKED: AWL

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
KENTUCKY CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	49 of 59
CONTRACT	PROJECT
R-40719	1702255

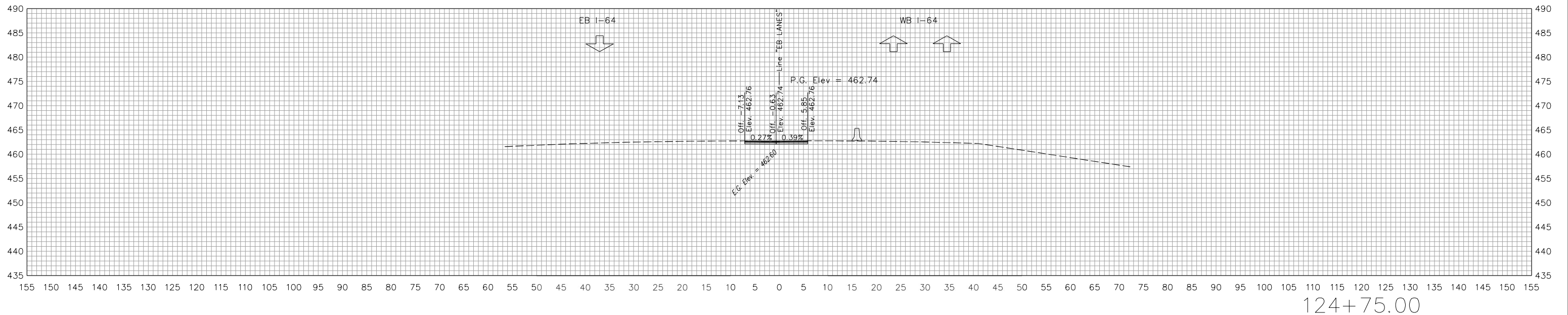
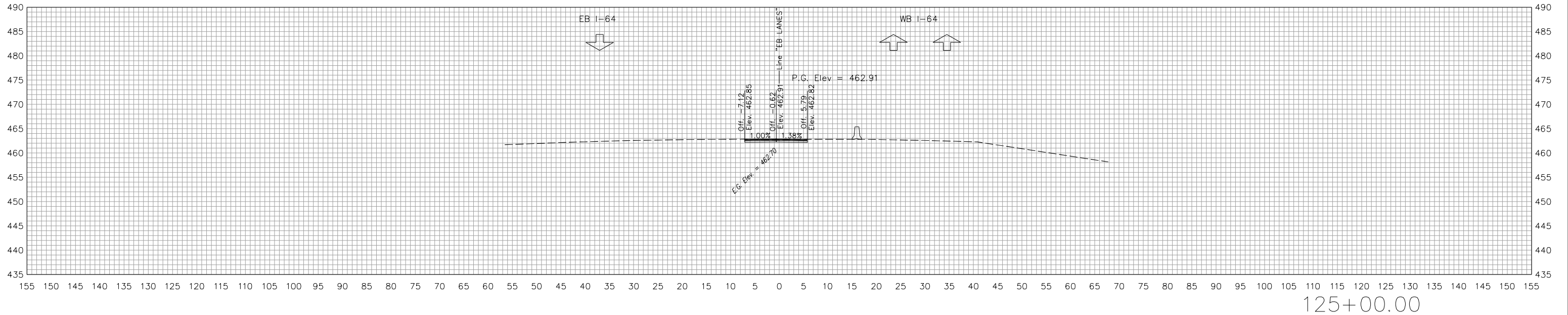
STA. 458+95.81 TO STA. 458+95.81



RECOMMENDED FOR APPROVAL	<i>Andrew M. Helber</i>	8/20/2021
DESIGNED:	KLO	05/24/21
DRAWN:	KLO	05/24/21
CHECKED:	AWL	-

INDIANA DEPARTMENT OF TRANSPORTATION	
CROSS SECTIONS	
INDIANA CROSSOVER	

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	50 of 59
CONTRACT	PROJECT
B-40719	1702255



STA. 124+75.00 TO STA. 125+00.00

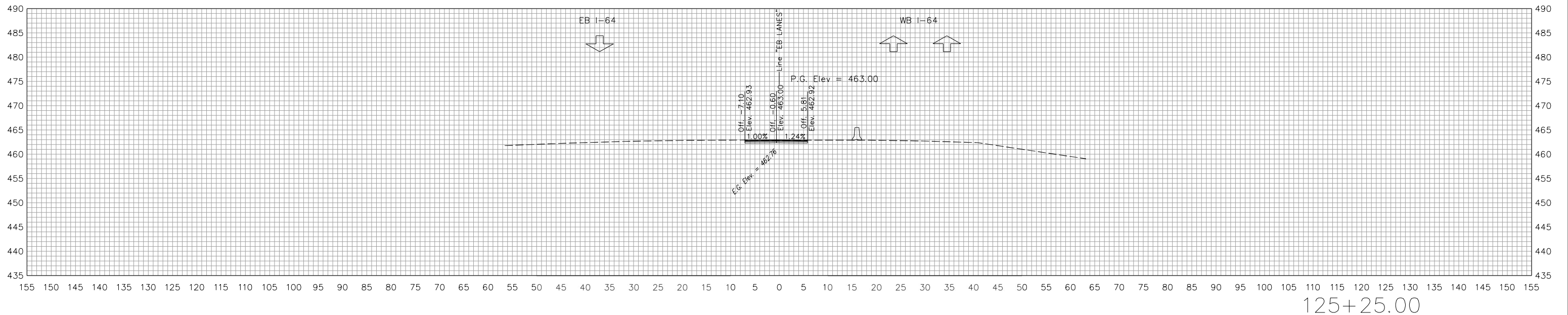
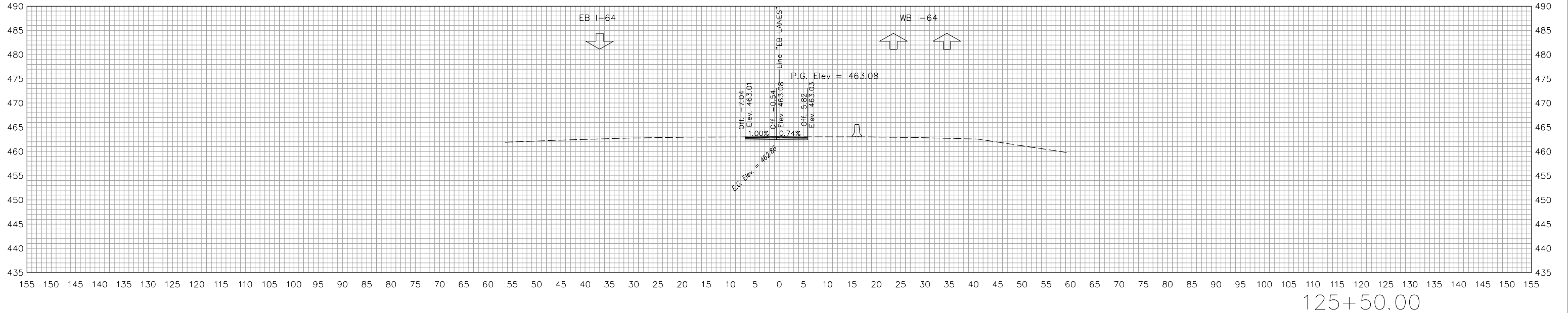


RECOMMENDED FOR APPROVAL	<i>Andrew W. Leichter</i>	8/20/2021
DESIGNED: KLO	05/24/21	DRAWN: KLO 05/24/21
CHECKED: AWL	-	CHECKED: AWL -

INDIANA DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS INDIANA CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	51 of 59
CONTRACT	PROJECT
B-40719	1702255

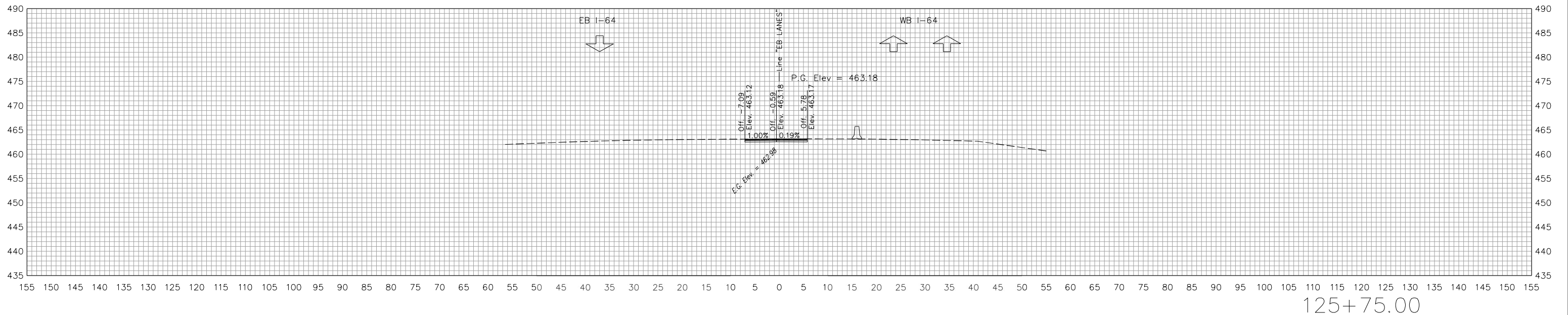
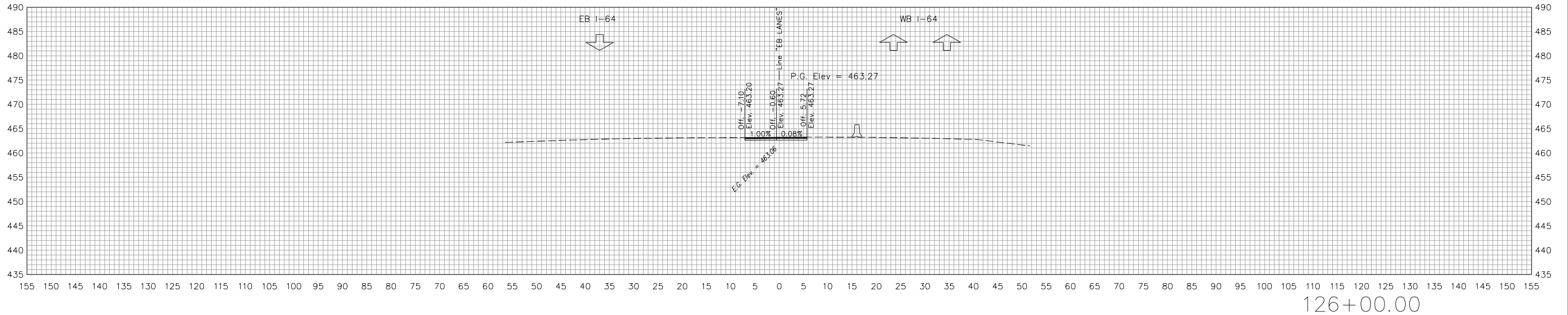


RECOMMENDED FOR APPROVAL	<i>Andrew W. Leichter</i>	8/20/2021
DESIGNED:	KLO	05/24/21
DRAWN:	KLO	05/24/21
CHECKED:	AWL	-

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
INDIANA CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	52 of 59
CONTRACT	PROJECT
B-40719	1702255

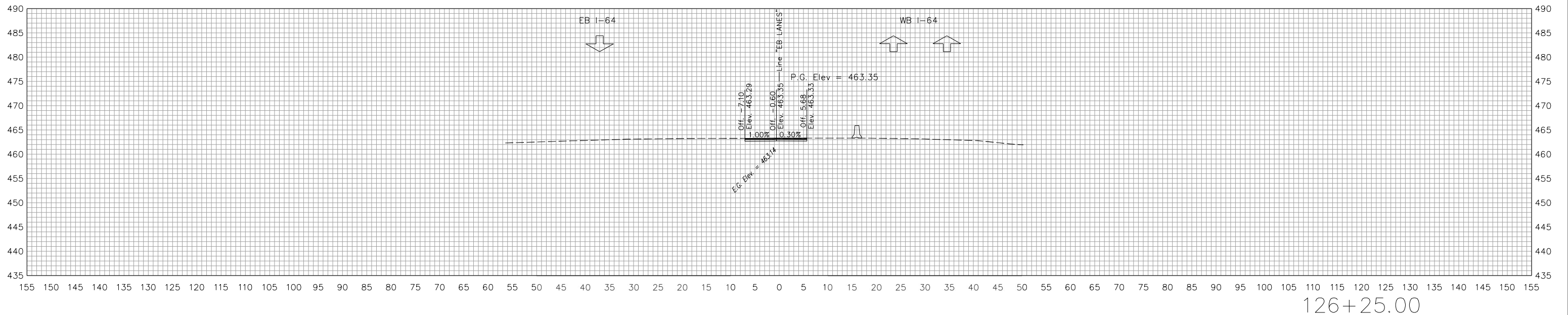
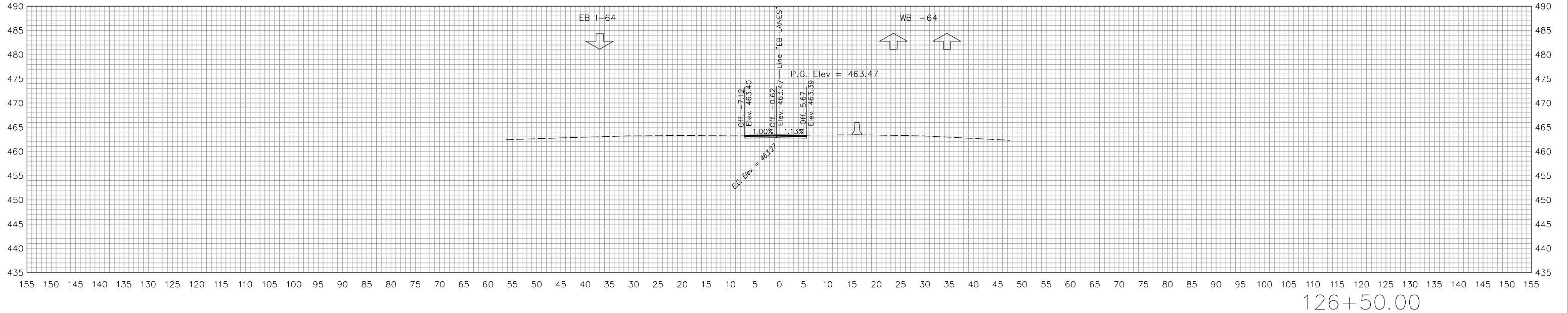


RECOMMENDED FOR APPROVAL	<i>Andrew W. Leichter</i>	8/20/2021
DESIGNED: KLO	05/24/21	DRAWN: KLO 05/24/21
CHECKED: AWL	-	CHECKED: AWL -

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
INDIANA CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	53 of 59
CONTRACT	PROJECT
B-40719	1702255

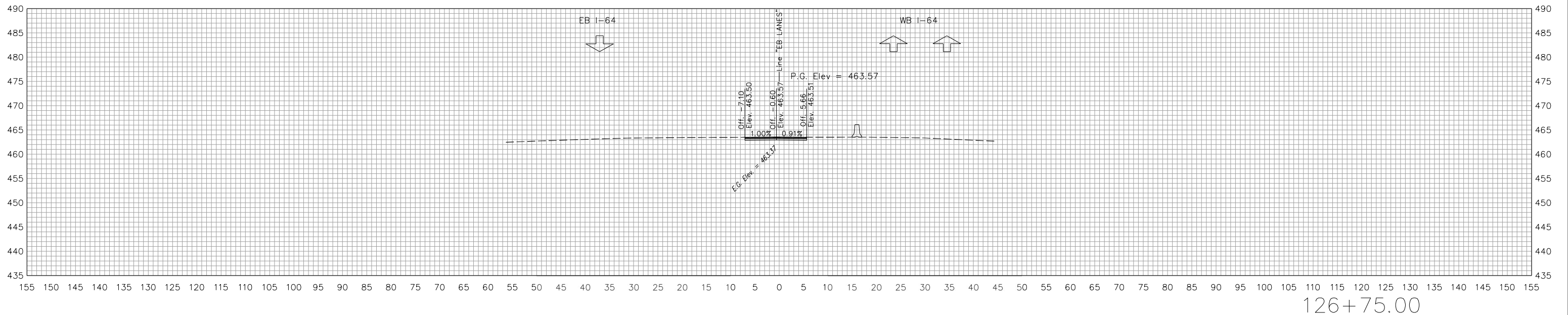
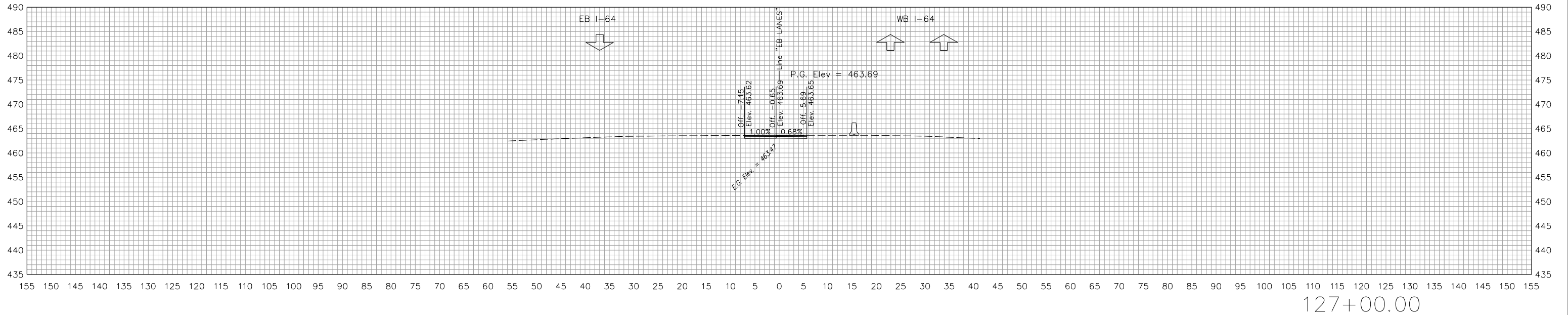


RECOMMENDED FOR APPROVAL	<i>Andrew W. Leibel</i>	8/20/2021
DESIGNED: KLO	05/24/21	DRAWN: KLO 05/24/21
CHECKED: AWL	-	CHECKED: AWL -

INDIANA DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS INDIANA CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	54 of 59
CONTRACT	PROJECT
B-40719	1702255

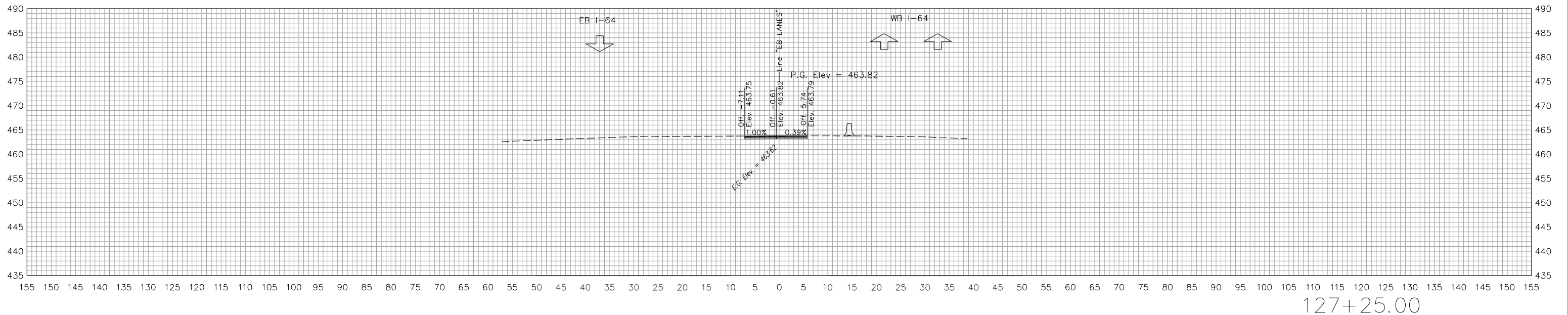
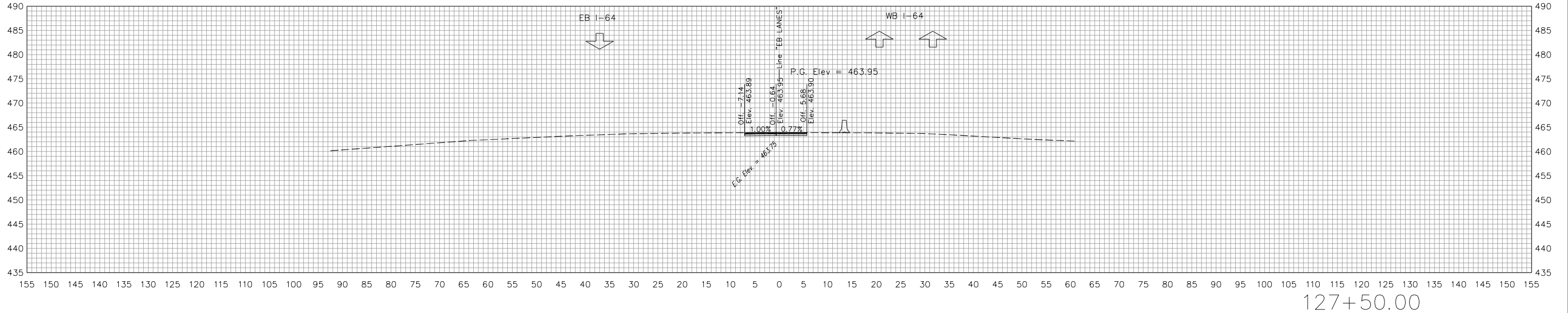


RECOMMENDED FOR APPROVAL	<i>Andrew W. Leibel</i>	8/20/2021
DESIGNED: KLO	05/24/21	DRAWN: KLO 05/24/21
CHECKED: AWL	-	CHECKED: AWL -

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
INDIANA CROSSOVER

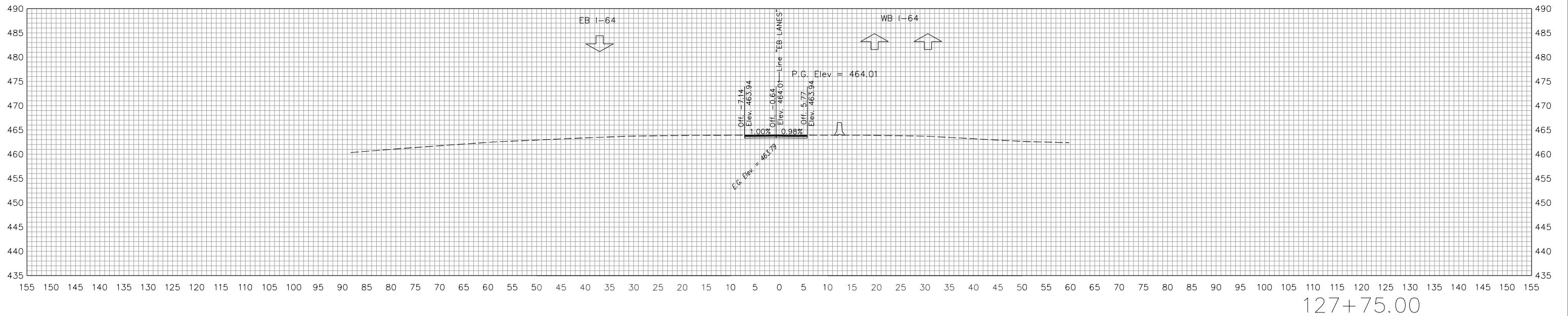
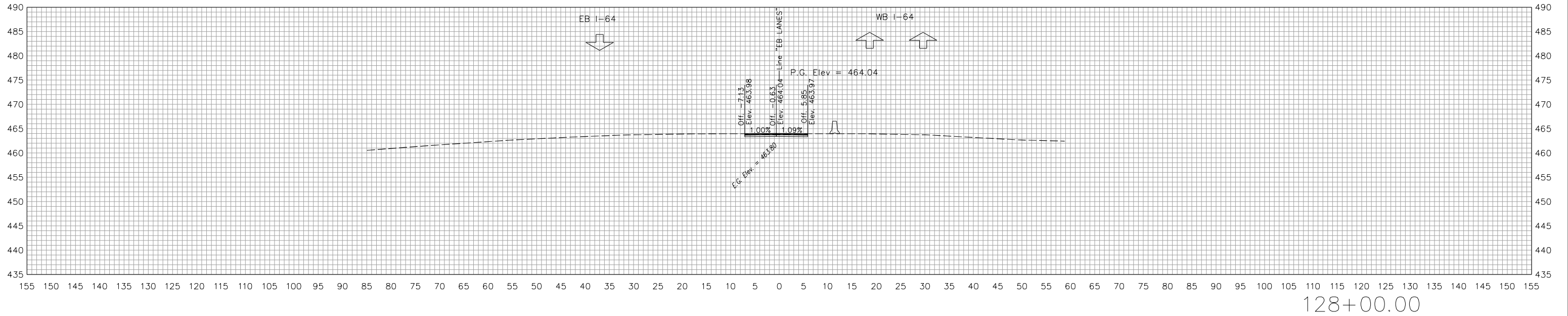
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VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	55 of 59
CONTRACT	PROJECT
B-40719	1702255



RECOMMENDED FOR APPROVAL	<i>Andrew W. Leibel</i>	8/20/2021
DESIGNED: KLO	05/24/21	DRAWN: KLO 05/24/21
CHECKED: AWL	-	CHECKED: AWL -

INDIANA DEPARTMENT OF TRANSPORTATION	
CROSS SECTIONS INDIANA CROSSOVER	

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	56 of 59
CONTRACT	PROJECT
B-40719	1702255

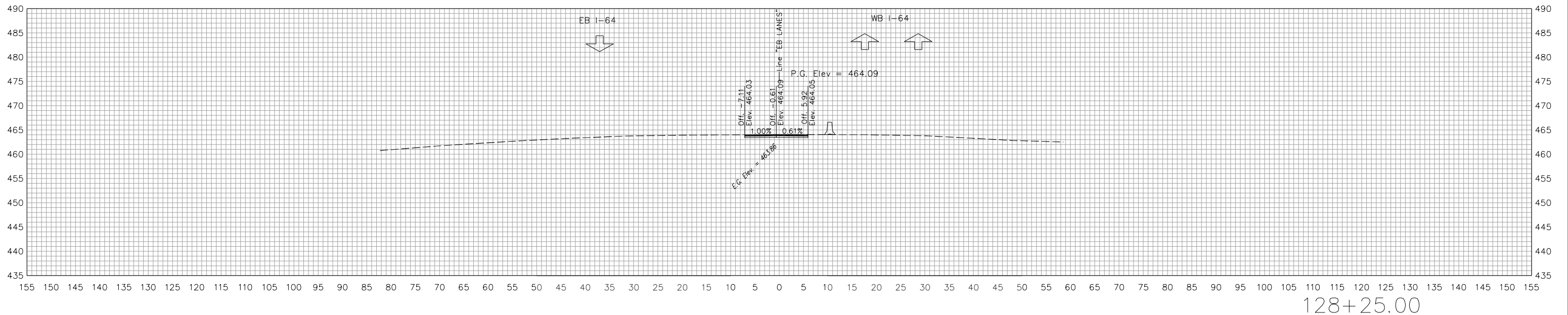
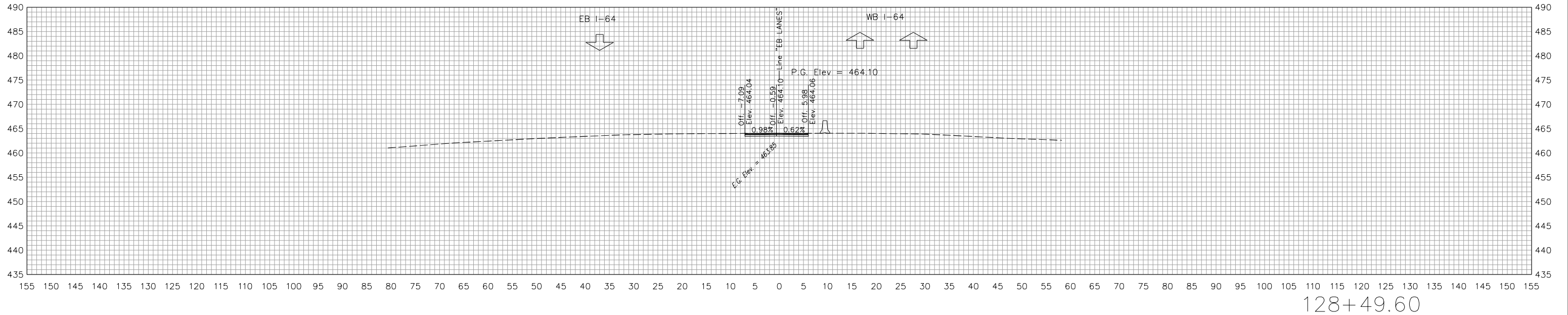


RECOMMENDED FOR APPROVAL	<i>Andrew W. Leibel</i>	8/20/2021
DESIGNED: KLO	05/24/21	DRAWN: KLO 05/24/21
CHECKED: AWL	-	CHECKED: AWL -

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
INDIANA CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	57 of 59
CONTRACT	PROJECT
B-40719	1702255

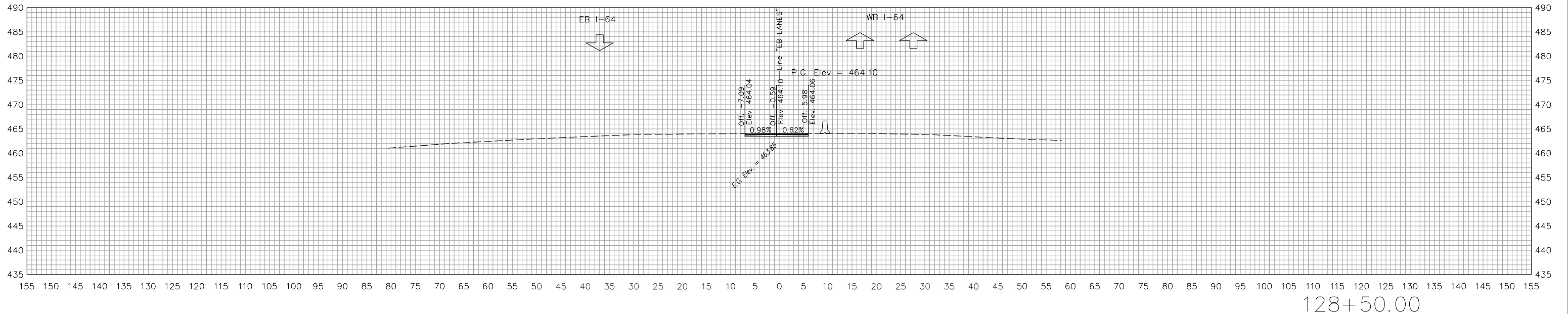
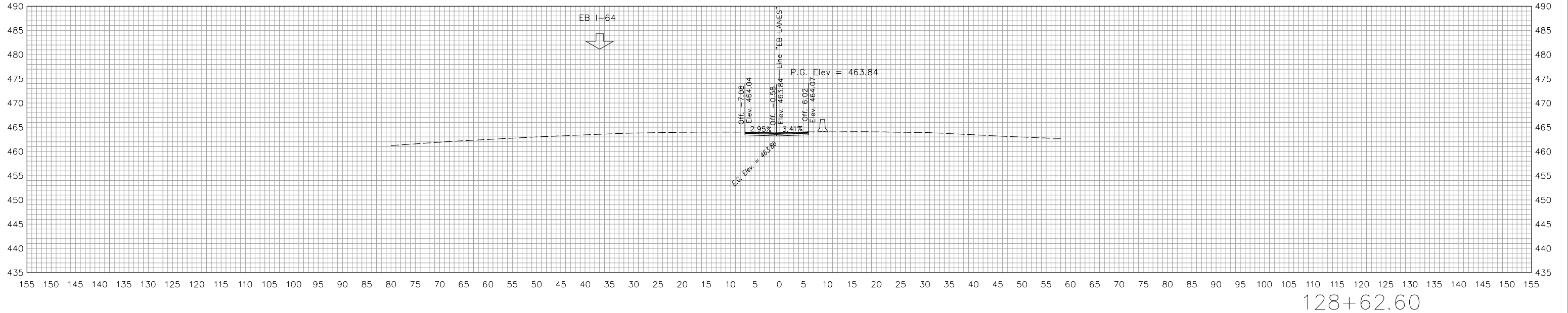


RECOMMENDED FOR APPROVAL	<i>Andrew W. Leibel</i>	8/20/2021
DESIGNED: KLO	05/24/21	DRAWN: KLO 05/24/21
CHECKED: AWL	-	CHECKED: AWL -

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
INDIANA CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	58 of 59
CONTRACT	PROJECT
B-40719	1702255



RECOMMENDED FOR APPROVAL	<i>Andrew W. Leibel</i>	8/20/2021
DESIGNED: KLO	05/24/21	DRAWN: KLO 05/24/21
CHECKED: AWL	-	CHECKED: AWL -

INDIANA DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS INDIANA CROSSOVER

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
2B	59 of 59
CONTRACT	PROJECT
B-40719	1702255

PROJECT	DESIGNATION
1702255	1702255
CONTRACT	
B-40719	

INDIANA DEPARTMENT OF TRANSPORTATION



MAINTENANCE OF TRAFFIC PLANS BUILDABLE UNIT 14

ROUTE: I-64 AT: SHERMAN MINTON BRIDGE
PROJECT NO. 1702255 P.E.

NO ADDITIONAL RIGHT OF WAY
REQUIRED FOR THIS PROJECT

R/W
1702255 CONST.

Project Description: Phases 3 and 4 Maintenance of Traffic Plans for the Rehabilitation of Sherman Minton Bridge over Ohio River and associated Indiana and Kentucky Approach Bridges Section 2, T-3-S, R-6-E, New Albany Township, Floyd County, Indiana



SHERMAN MINTON BRIDGE
REHABILITATION

A.A.D.T.	(2018)	45,200 V.P.D. (EB)	44,800 V.P.D. (WB)
A.A.D.T.	(2031)	45,490 V.P.D. (EB)	42,500 V.P.D. (WB)
D.H.V	(2031)	4,090 V.P.H. (EB)	3,830 V.P.H. (WB)
DIRECTIONAL DISTRIBUTION		50.3 % EB	
TRUCKS		11 % A.A.D.T. 6 % D.H.V.	

DESIGN SPEED	SEE GENERAL NOTES AND INDEX SHEET
PROJECT DESIGN CRITERIA	3R (FREEWAY)
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL
RURAL/URBAN	URBAN
TERRAIN	LEVEL
ACCESS CONTROL	FULL



PROJECT LOCATION SHOWN BY
FLOYD COUNTY

LATITUDE: 38°16'43" N LONGITUDE: 85°49'20" W



SCALE: 1" = 2000'

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS



PLANS PREPARED BY: Jacobs Engineering (314) 335-4000
PHONE NUMBER
CERTIFIED BY: Andrew M. Ledbetter 4/15/2022
DATE
RECOMMENDED FOR LETTING: _____
INDIANA DEPARTMENT OF TRANSPORTATION DATE

DESIGNATION	
1702255	
BUILDABLE UNIT	SHEETS
14	1 of 42
CONTRACT	PROJECT
B-40719	1702255

UTILITIES

Kentucky Utilities

<u>Communication</u> AT&T - Distribution Scott Roche 3719 Bardstown Rd Louisville, KY 40218 (502) 348-4528 SR8832@att.com KYTC KYTC Lighting Lonny Hamblin (502) 338-5273 lonny.hamblin@ky.gov	<u>Gas and Electric</u> Louisville Gas and Electric Caroline Justice 820 W. Broadway Louisville, KY 40202 (502) 627-3708 Caroline.Justice@LGE-KU.com KYTC Traffic Signals (Outside of Jefferson Co.) David Huber david.huber@ky.gov	<u>Water and Sewer</u> Louisville Water Eric Walls 550 S. Third Street Louisville, KY 40202 (502) 333-4858 ewalls@lwcky.com MSD David Mulloy/Josh Dickerson 700 W. Liberty Street Louisville, KY 40203 (502) 540-6615 david.mulloy@louisvillemtd.org Local Louisville Metro Traffic Signals Stacy Keith stacy.keith@louisvilleky.gov Jason Yeager jason.yeager@louisvilleky.gov (502) 794-6928	<u>TRIMARC</u> Todd Hood (207) 307-7456
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Indiana Utilities

<u>Gas and Electric</u> Centerpoint Energy Distribution Timmy Langston (812) 348-6703 timothy.langston@centerpointenergy.com Centerpoint Energy Transmission Raj Brahmhatt, PE 28100 Torch Pkwy Warrenville, IL 60555 (630) 225-6086 rbrahmhatt@enengineering.com Duke Energy Distribution Quentin Knight quentin.knight@duke-energy.com (812) 277-3112 Duke Energy Transmission Zach Boston dei-tline-coord@duke-energy.com (317) 838-1053 Duke Energy Telecom Chris Gere (980) 373-6842 Chris.Gere@duke-energy.com	<u>Water</u> Ed Bergsieker ed.bergsieker@amwater.com (317) 626-8560 <u>Communication</u> Centurylink (Lumen) Rob Smith 1409 E. Grace Street Rensselaer, IN 47978 relocations@Lumen.com Nationalrelo@centurylink.com MCI/Verizon Rick Pickens rick.pickens@tcscomm.com (502) 791-2725 Zayo Waylon Higgins 9209 Castlegate Drive Indianapolis, IN 46256 INKY_relo@zayo.com Windstream Jeremy Smiddie jeremy.smiddie@windstream.com (618) 383-4032	<u>Local</u> Housing Authority - New Albany Dwayne Gadd (812) 948-2319 dgadd@nahain.com City of New Albany Scott Wilkinson (812) 948-5320 swilkinson@cityofnewalbany.com <u>INDOT</u> INDOT ITS Drew Sorenson 8620 E. 21st Street Indianapolis, IN 46219 ITS_Locates@indot.in.gov <u>INDOT Traffic Signal</u> Steve Gann (812) 524-3777 sgann@indot.in.gov
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INDIANA UNDERGROUND
1-800-382-5544 OR CALL 811
24 HOURS A DAY 7 DAYS A WEEK

1.800.752.6007

Not all utilities listed are members of the Indiana and Kentucky Underground (811) locate system. Contractor is to verify all utilities have been contacted before excavations can proceed.

- Phase 3**
Construction Activities
- Remove concrete median barrier between EB and WB I-64 Sta. 230+25 "PR I-64" to Sta. 335+59 "PR E-W" on the Kentucky side of the Sherman Minton Bridge and construct crossover.
 - Construct crossover on the Kentucky side of the Sherman Minton Bridge between EB and WB I-64.
 - Complete Sherman Minton Bridge repairs and associated I-64 roadway work on the 2 closed right lanes of WB I-64 (Upper Deck).
- Traffic Revisions
- Close 1 lane of EB I-64 to allow for 1 lane of WB I-64 in the EB I-64 lanes.
 - Utilize crossovers and shift 1 lane of WB I-64 to EB I-64.
 - Close 2 right lanes of WB I-64 across the Sherman Minton Bridge.
 - Maintain left lane closure on Indiana side of Sherman Minton Bridge from Phases 1 and 2.
 - Remove Dynamic Late Merge System from WB I-64 in Kentucky.
- Phase 3A**
Construction Activities and Traffic Revisions
- Close EB I-64 during 9 day allowable closure and detour single WB lane from I-264 into existing EV lanes across SM Bridge.
 - Remove saw cut deck panels from existing SM Bridge for Phase 3 demolition.
- Phase 4**
Construction Activities
- Maintain crossovers constructed in Phase 3.
 - Complete Sherman Minton Bridge repairs and associated I-64 roadway work on the remaining left lane of WB I-64 (Upper Deck).
- Traffic Revisions
- Maintain 1 lane of WB I-64 traffic in the EB direction from Phase 3.
 - Shift 1 lane of WB I-64 traffic across the Sherman Minton Bridge to the newly constructed WB lanes from Phase 3.
 - Close 2 left lanes of WB I-64 across the Sherman Minton Bridge.
 - Maintain left lane closure on Indiana side of Sherman Minton Bridge from Phases 1, 2 and 3.

GENERAL NOTES

- Traffic shall be maintained in accordance with the Indiana Manual on Uniform Traffic Control Devices (2011 Edition), the InDOT Construction Standards and Project Technical Provisions. Daytime and nighttime shoulder closures, lane closures, and other temporary operations not detailed in the traffic control plans shall follow the standard details found in the InDOT Manual on Uniform Traffic Control Devices (2011 Edition).
- Along I-64 a 15-mile-per-hour speed reduction within the work zone shall be posted when a single 11 foot lane is maintained in any direction. A 10 mile-per-hour speed reduction within the work zone shall be posted in all other instances along I-64.
- The minimum MOT lane width shall be 11 feet on I-64, I-264, and I-265.
- The minimum MOT lane width for multiple lane ramps shall be 11.5 feet.
- The minimum MOT lane width for single lane ramps shall be 16 feet.
- The minimum MOT lane width on local streets shall match existing lane widths up to 11 feet.
- The minimum MOT lane width within multi-lane shifts on interstate mainline and ramps shall be 13 feet.
- A minimum horizontal clearance of 1 foot between barrier and edge of travel lane shall be provided.
- Opposing traffic lanes of I-64, I-264, or I-265 traffic shall be separated by Type 1 traffic barrier if the existing open median width is reduced or otherwise cannot be provided.
- For TTB used to separate two directions of traffic in a crossover, glare screens shall be used to eliminate headlight glare.
- Design-Build Contractor shall maintain positive drainage on all active travel lanes during Construction Work.
- Barriers shall not impede snow removal operations outside the limits of the Project. To facilitate snow removal operations by INDOT and KYTC, from November 1 through March 31 each year, Design-Build Contractor shall maintain open areas along interstate routes and ramps matching existing shoulder widths. Within the limits of the Project, shoulders may be reduced to the widths shown in the Design-Build Contractor's approved MOT plans, while work is progressing.
- Design-Build Contractor shall remove all existing rumble strips prior to moving any traffic onto shoulders. Vehicles cannot straddle rumble strips at any point of construction. Before substantial project completion, all rumble strips shall be replaced in their existing location.
- Existing striping which conflicts with temporary markings within 12-feet of construction shall be water blasted and removed prior to any traffic shifts. All areas shown in the Design-Build Contractor's approved MOT plans shall be returned to their original configuration prior to Final Acceptance.
- All existing raised pavement markers within 12-feet of construction shall be disabled prior to any traffic shifts.
- Temporary minimum vertical clearance shall be 14'-6" for I-64 during construction.
- Access to all businesses and residences shall be maintained at all times.
- Design-Build Contractor shall maintain existing or detoured pedestrian and/or bicycle access on all sidewalks, trails, transit facilities and at all intersections that are open to traffic. Design-Build Contractor shall also maintain safe access and passage for all pedestrian facilities.
- Design-Build Contractor shall provide an alternate accessible pedestrian route that complies with the MUTCD, PROWAG, and ADA requirements where accessible pedestrian and/or bicycle routes are allowed to be closed by Design-Build Contractor during construction.
- Design-Build Contractor shall not park motor vehicles or construction vehicles, locate construction signs, or place equipment on a pedestrian sidewalk or path open to pedestrian traffic; or use the pedestrian sidewalk or path for loading operations, stockpiling of materials, or allowing demolished or spoil materials to be deposited on the surface of a pedestrian and/or bicycle sidewalk or path.
- Design-Build Contractor shall provide temporary access for the Shawnee Golf Course through the Work area under the Kentucky Approach Bridge at all times. If Work areas encroach upon the golf course access, a minimum clear width of 15 feet wide by 16 feet high shall be provided.
- All temporary golf course access points shall be coordinated with the management of the Shawnee Golf Course a minimum of 14 days prior to implementation of temporary access.
- Design Build Contractor shall always maintain two I-64 traffic lanes in each direction on the Indiana Approach Bridges, the Sherman Minton Bridge, and the Kentucky Approach Bridge. However, I-64 may be reduced to one lane in each direction during off-peak hours per Table 12-1 of the Project Technical Provisions shown on this sheet.
- Use InDOT Type B borrow material per 211.02 and compacted per 211.04 to construct temporary crossover in Kentucky.

INDEX

SHEET NO.	DRAWINGS INDEX
1	TITLE
2	INDEX AND GENERAL NOTES
3-4	MAINTENANCE OF TRAFFIC - TYPICALS - PHASE 3 & PHASE 3A
5	MAINTENANCE OF TRAFFIC - TYPICALS - PHASE 4
6	MAINTENANCE OF TRAFFIC - KEY MAP
7 - 17	MAINTENANCE OF TRAFFIC - I-64 PLAN SHEETS - PHASE 3
18 - 23	MAINTENANCE OF TRAFFIC - I-64 PLAN SHEETS - PHASE 3A
24 - 29	MAINTENANCE OF TRAFFIC - I-64 PLAN SHEETS - PHASE 4
30	MAINTENANCE OF TRAFFIC - KENTUCKY CROSSOVER I-264 - PLAN AND PROFILE
31	MAINTENANCE OF TRAFFIC - KENTUCKY CROSSOVER I-64 - PLAN AND PROFILE
32 - 36	MAINTENANCE OF TRAFFIC - CROSS SECTIONS - KENTUCKY CROSSOVER - I-264
37 - 42	MAINTENANCE OF TRAFFIC - CROSS SECTIONS - KENTUCKY CROSSOVER - I-64

DESIGN SPEEDS

Roadway	Posted Speed	Construction Speed
I-64 (Indiana)	55 MPH	45 MPH*
I-64 (Kentucky)	50 MPH	40 MPH
I-65	65 MPH	65 MPH
I-264	55 MPH	55 MPH
I-265	65 MPH	65 MPH
Ramp A (I-64/I-265)	40 MPH	30 MPH
Ramp C (I-64/I-265)	40 MPH	30 MPH
Ramp EB-R-FUT/ SW-R (I-265/I-65)	45 MPH	35 MPH
Ramp A1	25 MPH	25 MPH
Ramp B1	25 MPH	25 MPH
Ramp C1	40 MPH	40 MPH
Ramp D1	45 MPH	45 MPH
Ramp W-S (I-64/I-264)	50 MPH	50 MPH
Ramp PR S-W (I-264/I-64)	45 MPH	40 MPH
Ramp E-S (I-64/I-264)	45 MPH	45 MPH
Ramp S-E (I-264/I-64)	45 MPH	45 MPH
Local Streets	25 MPH	25 MPH

*See General Note 2.

INDOT STANDARD DRAWINGS

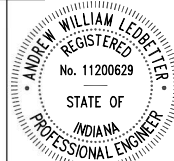
INDOT Standard Drawings (2020) used:
 E 720-EDCA Earth Ditch Casting Type 7
 E 720-INST Inlet Standard
 E 801-TCCO Temporary Construction Crossover
 E 801-TCDV Traffic Control Devices
 E 801-TCLC Traffic Control Lane Closures
 E 801-TCLG Traffic Control Legend
 E 801-TCSC Traffic Control Shoulder Closure
 E 801-TCSN Traffic Control Signs
 E 808-DLIM Dotted Line Markings

Table 12-1 Permitted Mainline Interstate Off-Peak Movement Closures

Roadway	From	To	Minimum Number of Lanes Maintained on the Bridges	Number of Off-Peak Periods	Restrictions
I-64 EB	I-265	I-264	1 Lane	[360] Prior to Substantial Completion [60] After Substantial Completion for approved Punch List work only.	Between 9:00 p.m. and 5:00 a.m. each night - Sunday night through Thursday night Between 10:00 p.m. and 6:00 a.m. Friday night Between 10:00 p.m. and 9:00 a.m. Saturday night
I-64 WB	I-265	I-264	1 Lane	[360] Prior to Substantial Completion [60] After Substantial Completion for approved Punch List work only.	Between 10:00 p.m. and 6:00 a.m. each night - Sunday night through Thursday night Between 10:00 p.m. and 9:00 a.m. Friday night Between 10:00 p.m. and 10:00 a.m. Saturday night

During the permitted off-peak closure(s) of I-64 EB or WB as defined in Table 12-1, Design Build Contractor shall close the following ramps:

- For the I-64 EB closure from the interchanges with I-265 to I-264: EB on ramp from West Spring Street and off ramp to I-264 East.
- For the I-64 WB closure from the interchanges with I-265 to I-264: WB on ramp from I-264 West and off ramp to West Elm Street



RECOMMENDED FOR APPROVAL	<i>Andrew M. Hultbelle</i>	4/15/2022
DESIGNED:	DPS 7/30/21	DRAWN: DPS 7/30/21
CHECKED:	AWL 8/13/21	CHECKED: AWL 8/13/21

INDIANA DEPARTMENT OF TRANSPORTATION

INDEX AND GENERAL NOTES

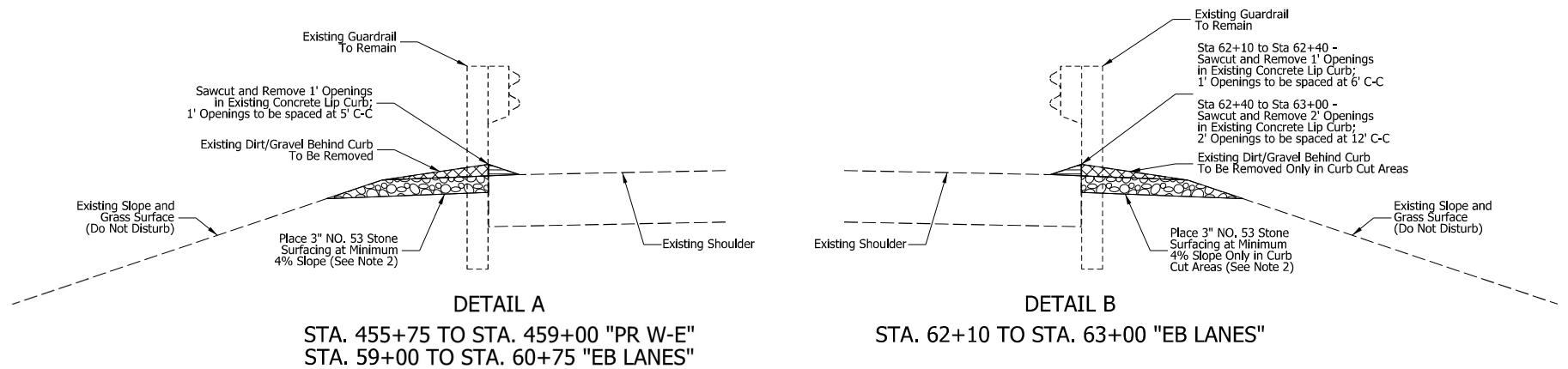
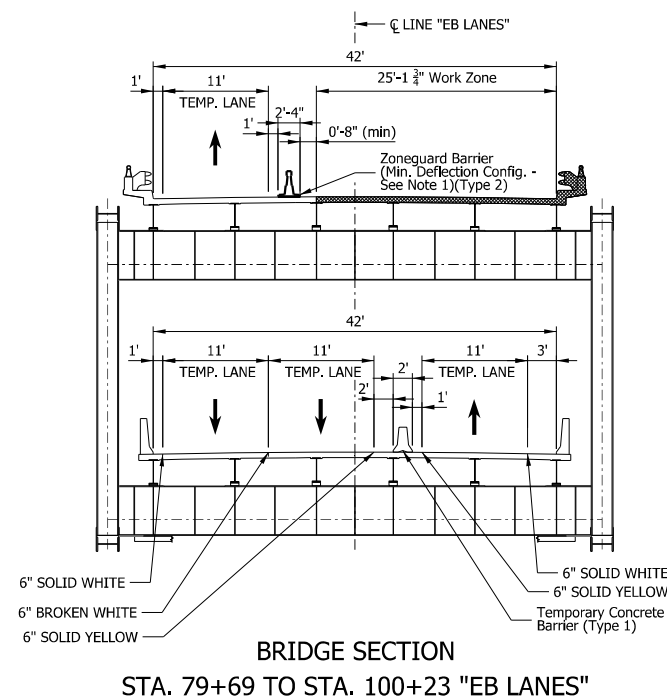
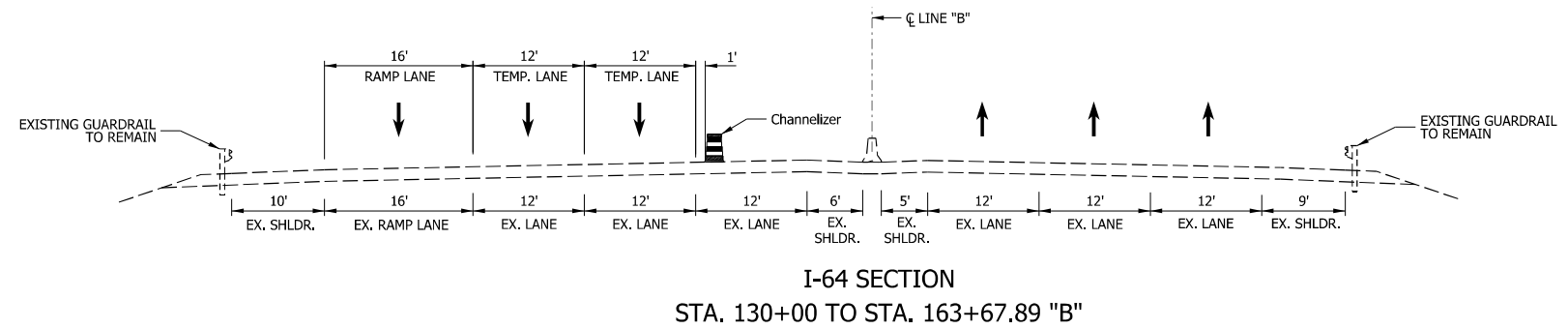
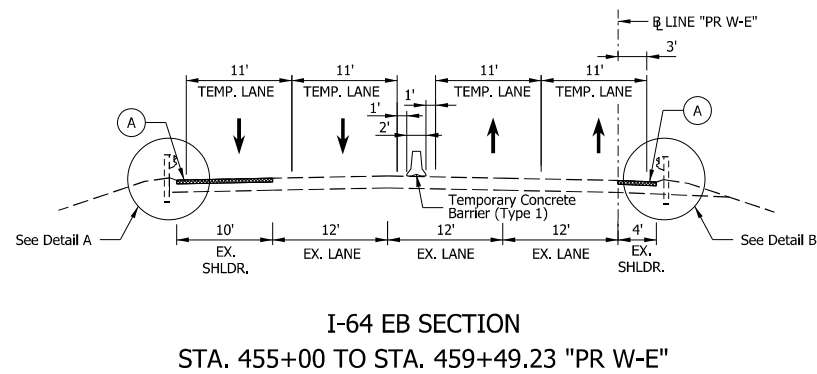
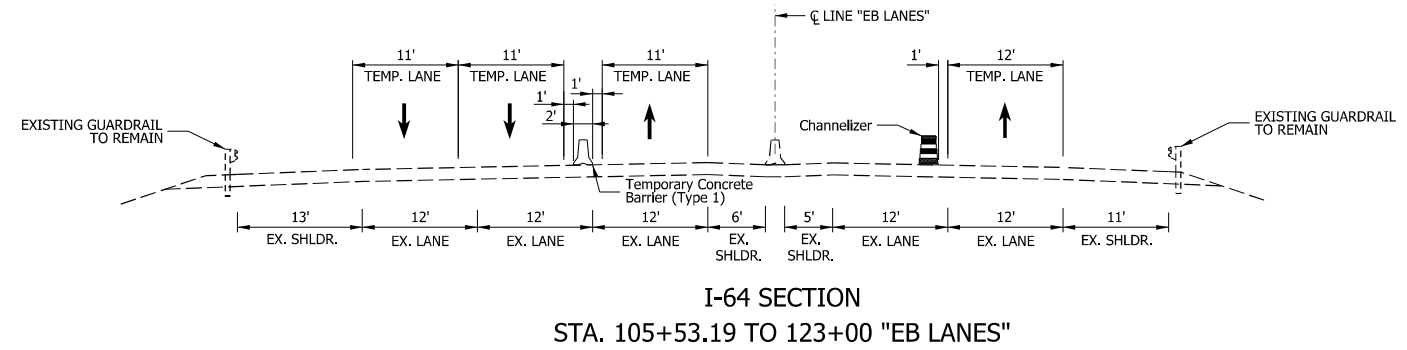
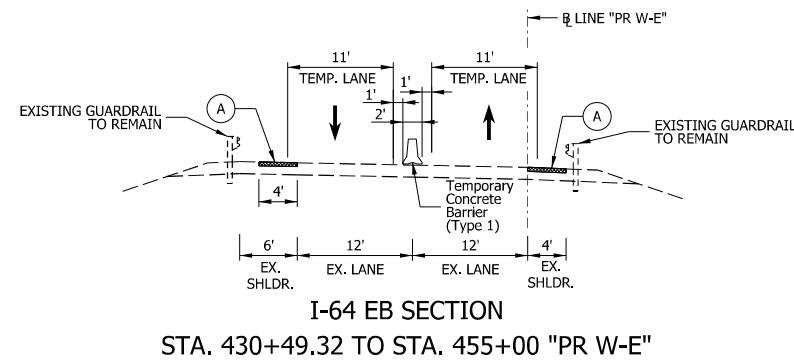
HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION
BUILDABLE UNIT	SHEETS
14	2 of 42
CONTRACT	PROJECT
B-40719	1702255

A CONCRETE RUMBLE STRIP REMOVAL

Existing rumble strips to be perimeter saw-cut a minimum of 2" and chipped out to a uniform minimum 2" depth. Removed rumble strip to be patched with material from InDOT QPL or a pre-approved ready mix.

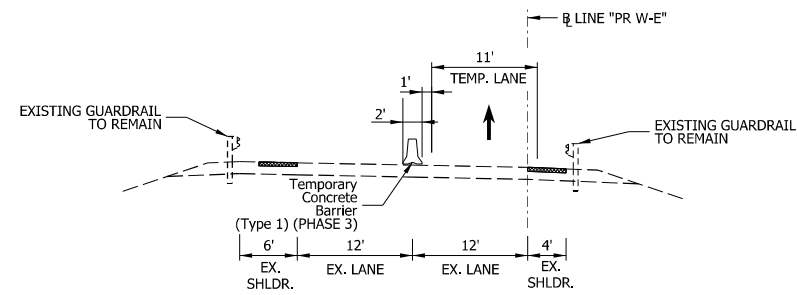
Notes:

1. Zoneguard barrier shall be anchored per manufacturer's installation instructions for Minimum Deflection Configuration. Center Zoneguard expansion sections in "neutral" position over existing finger joints in Sherman Minton Bridge.
2. Stone surfacing shall be placed as soon as feasible following excavation of the top of the embankment in order to minimize the potential for erosion of exposed soils.
3. See Sheet 30 for Crossover Typical Section.

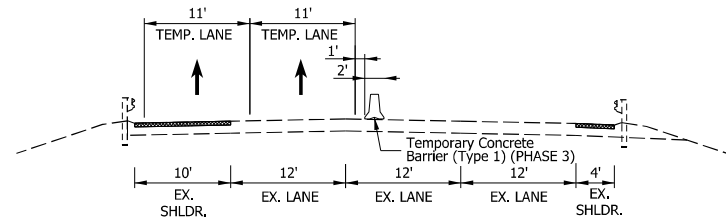


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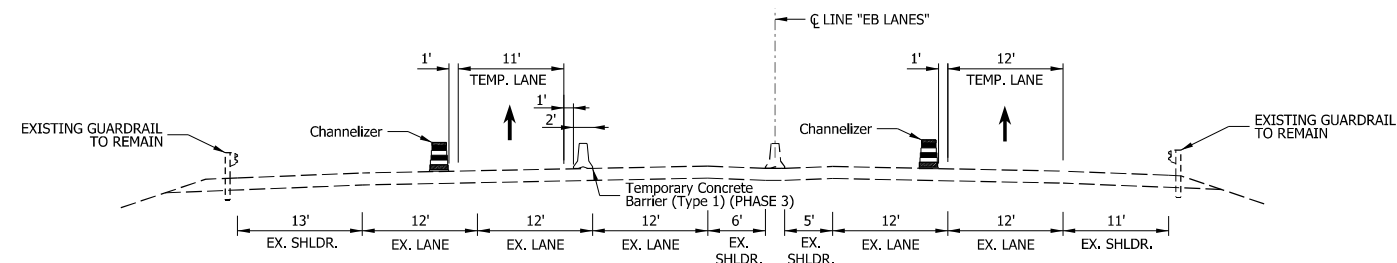
1. Zoneguard barrier shall be anchored per manufacturer's installation instructions for Minimum Deflection Configuration. Center Zoneguard expansion sections in "neutral" position over existing finger joints in Sherman Minton Bridge.



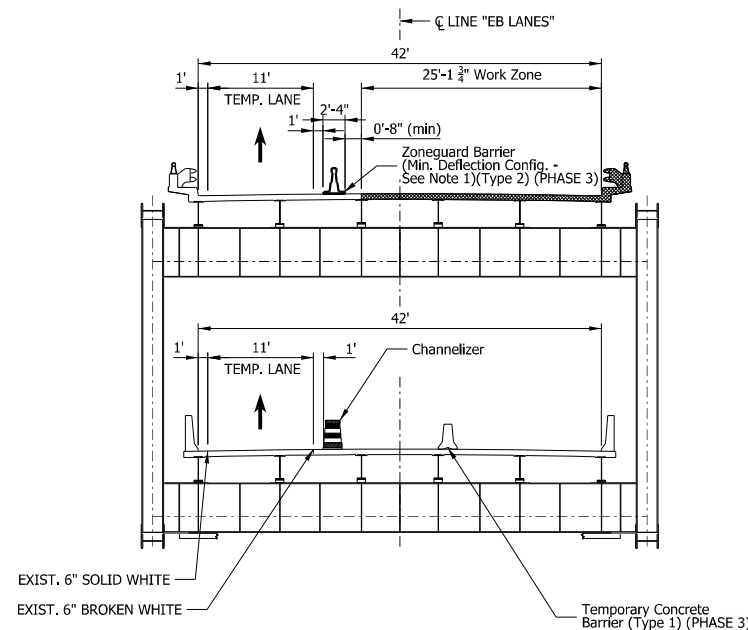
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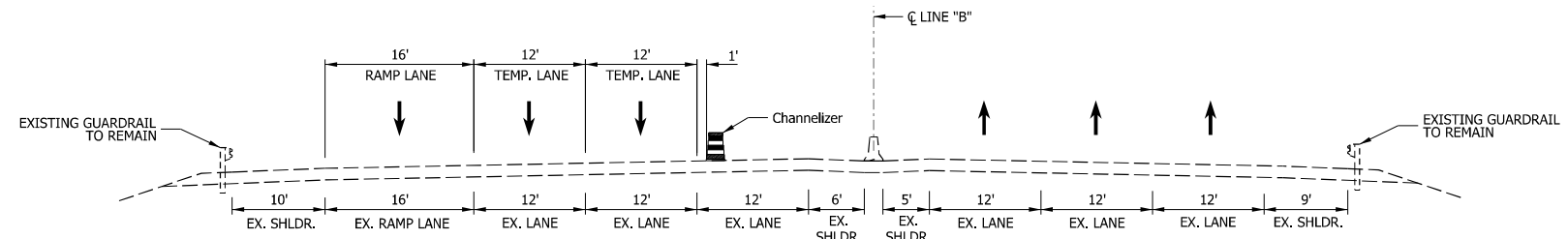
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STA. 59+52 TO STA. 62+52 "EB LANES"



I-64 SECTION
STA. 108+00 TO STA. 121+57 "EB LANES"



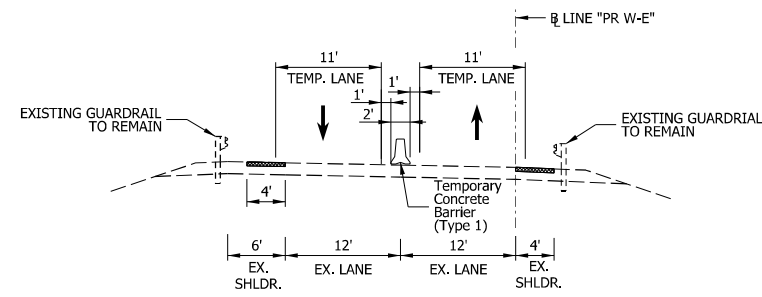
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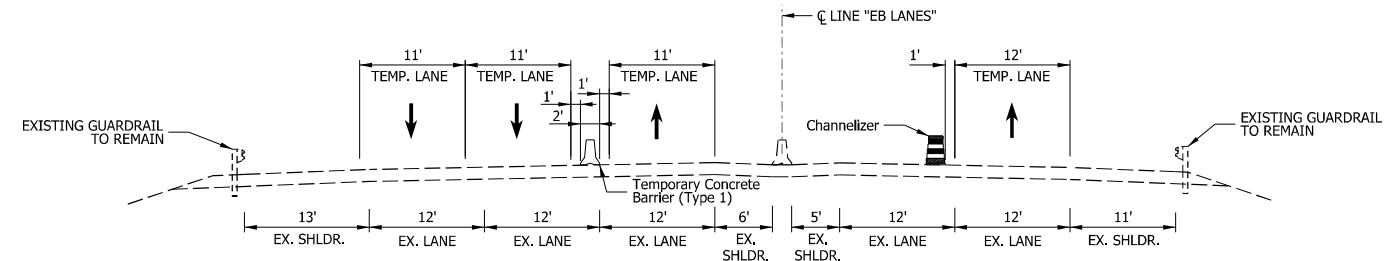
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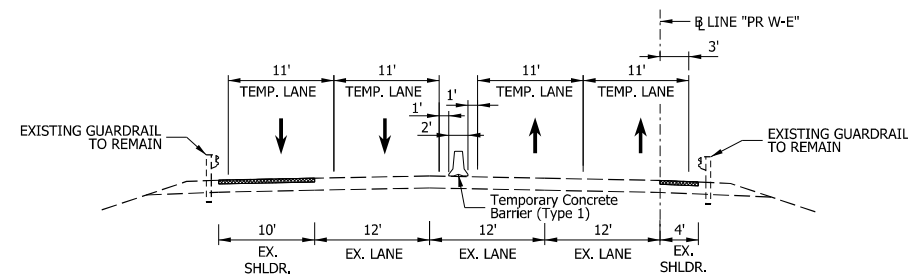
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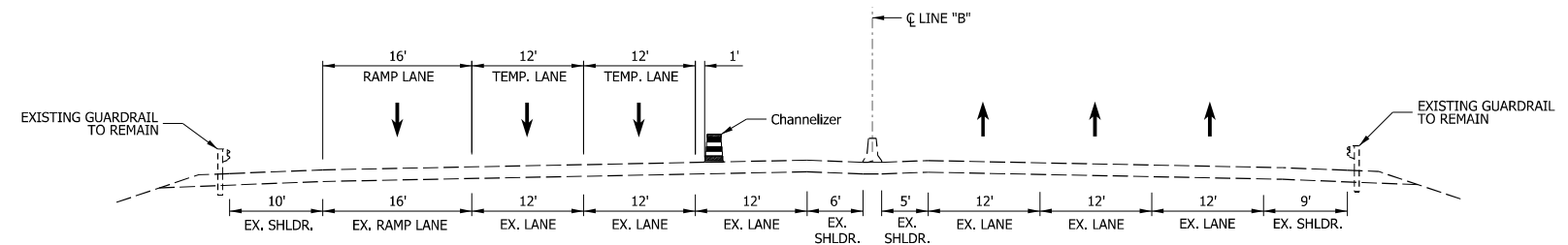
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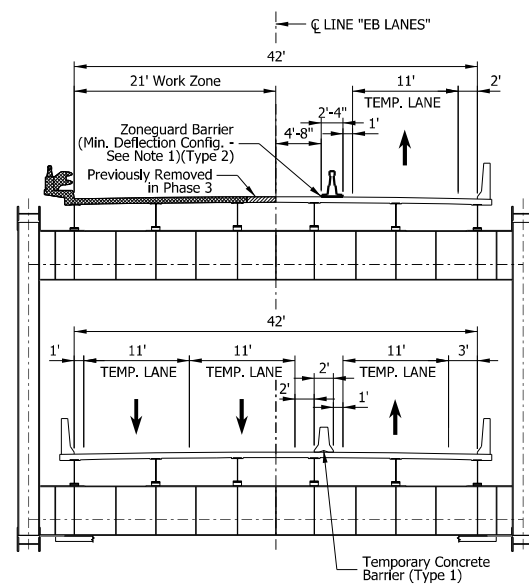
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I-64 EB SECTION
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I-64 SECTION
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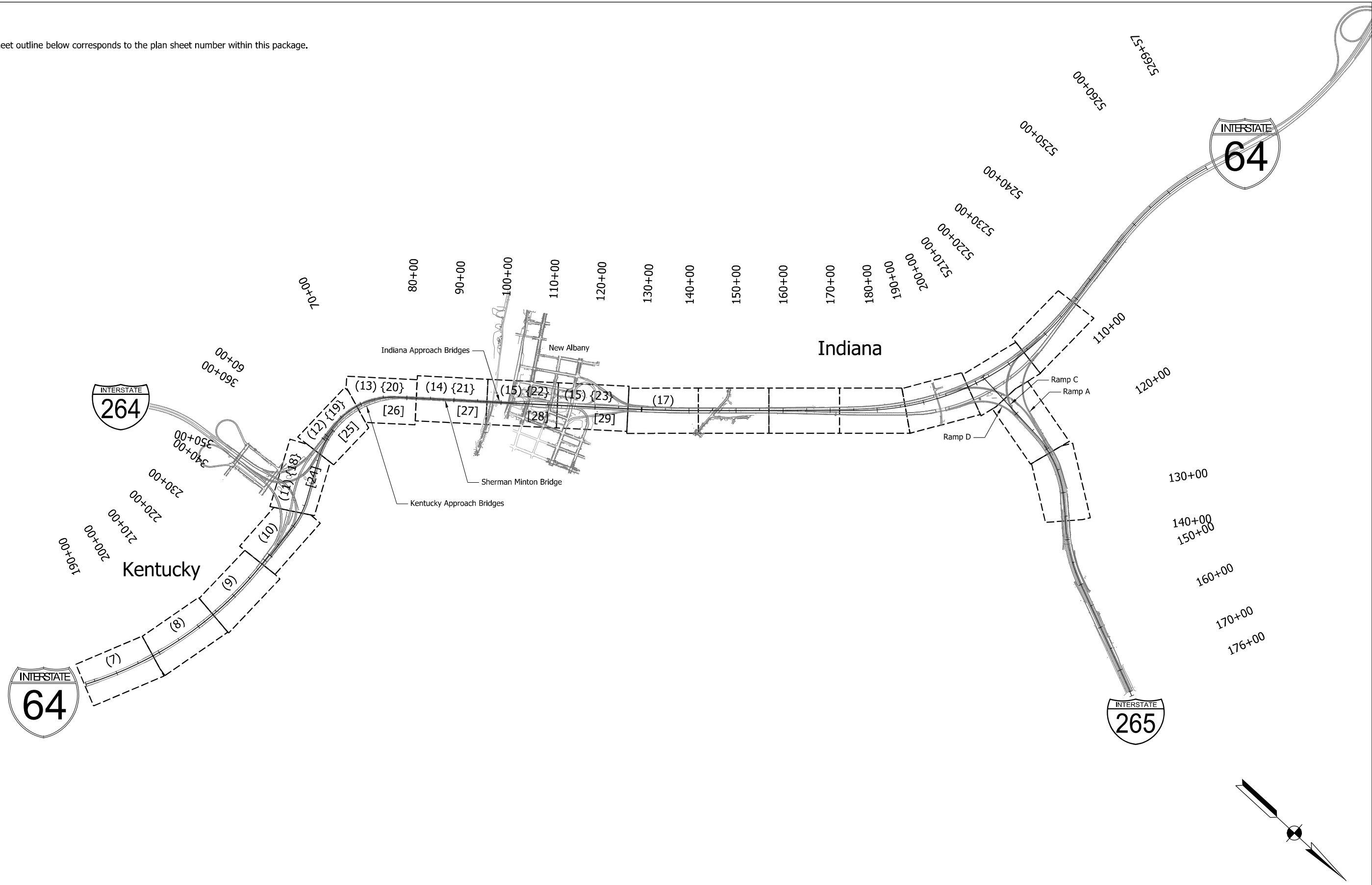
BRIDGE SECTION
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Note:

1. The number within the sheet outline below corresponds to the plan sheet number within this package.

Legend

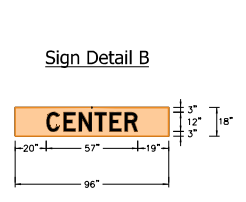
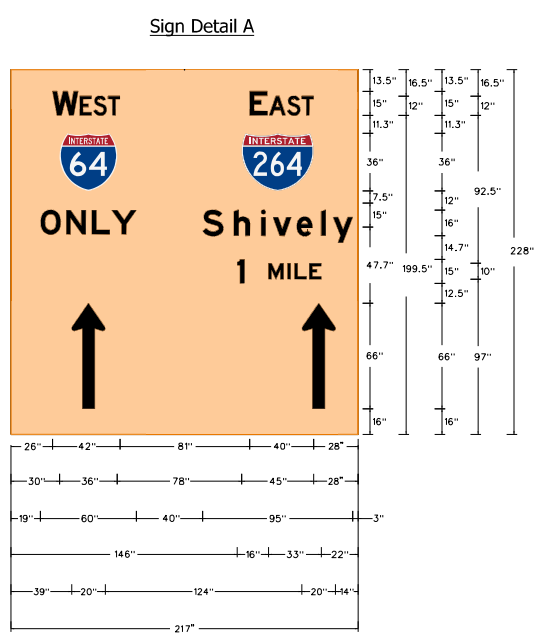
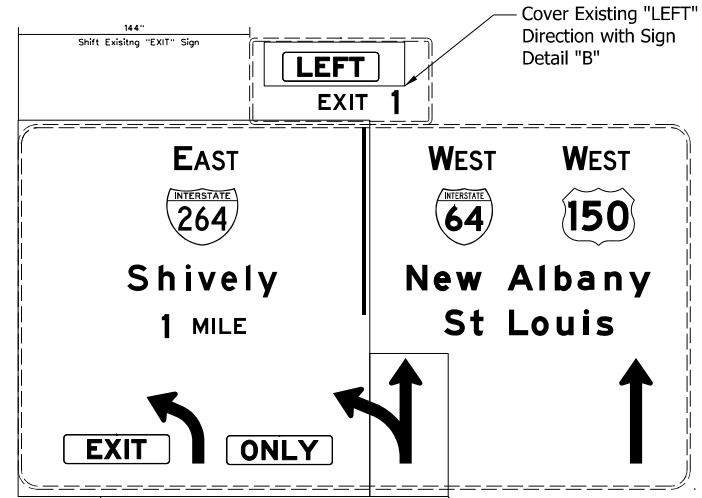
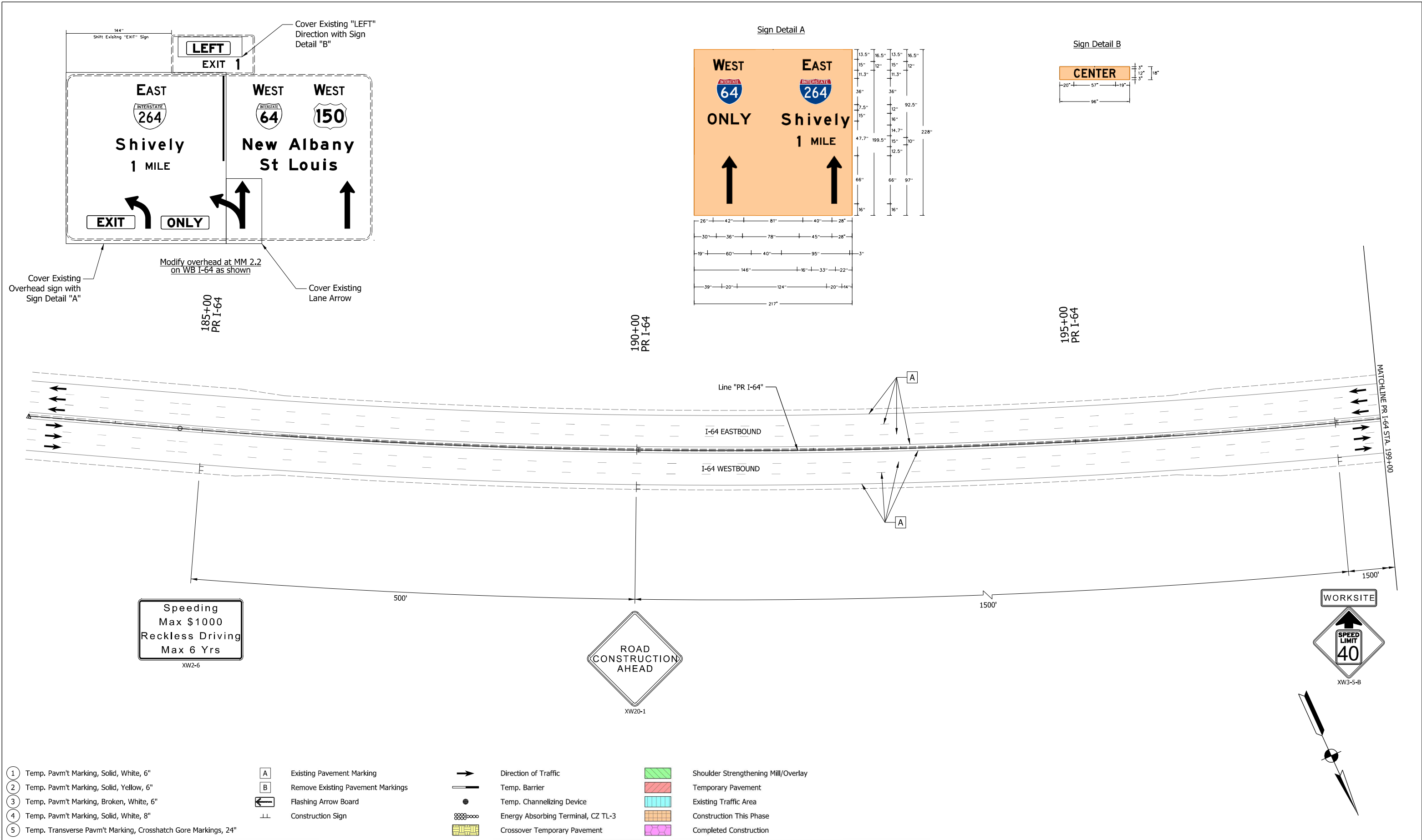
(XX) Phase 3 Sheets
 {XX} Phase 3A Sheets
 [XX] Phase 4 Sheets



RECOMMENDED FOR APPROVAL	<i>Andrew W. Leffler</i>	4/15/2022
DESIGNED:	DPS	6/24/21
DRAWN:	DPS	6/24/21
CHECKED:	AWL	6/25/21
DESIGN ENGINEER		DATE

INDIANA DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC KEY MAP

HORIZONTAL SCALE	BRIDGE FILE
1"=1000'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	6 of 42
CONTRACT	PROJECT
B-40719	1702255



- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



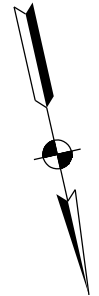
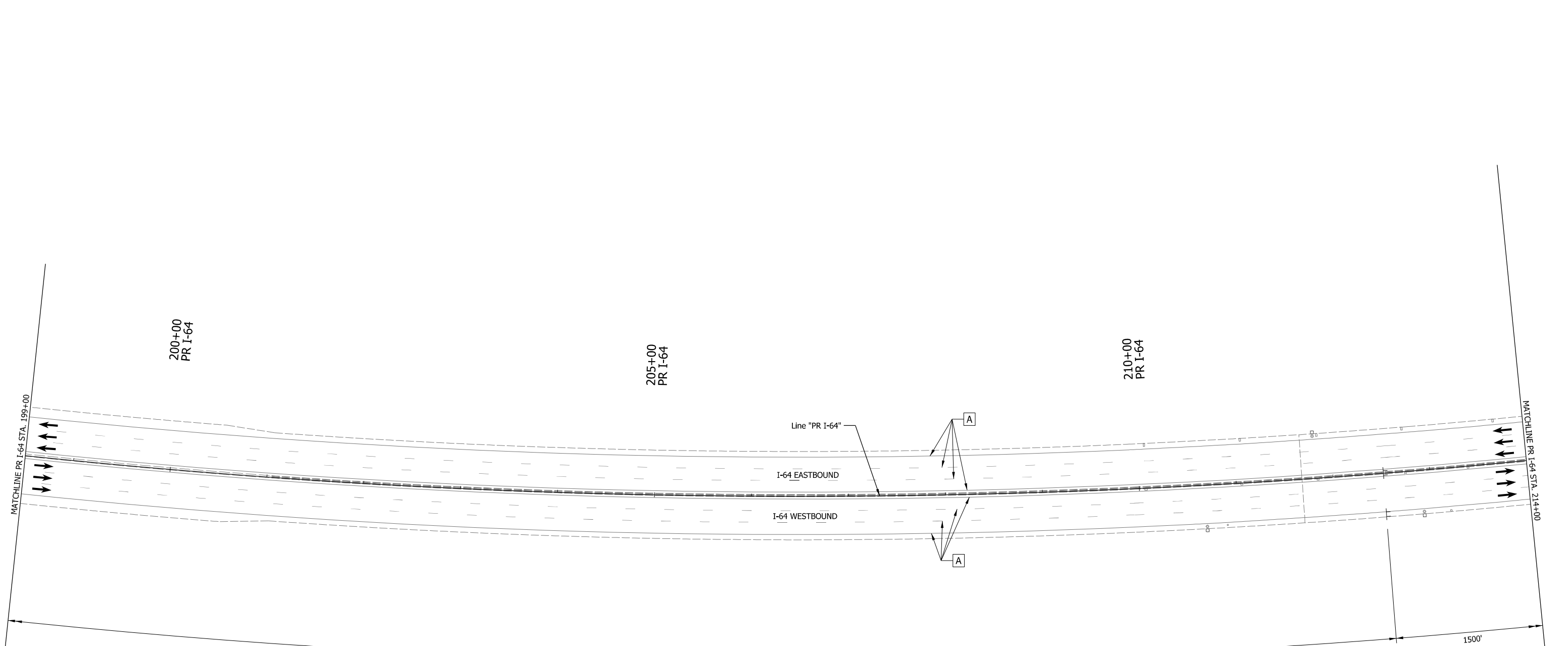
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DESIGNED: DPS	7/28/21	DRAWN: DPS 7/28/21
CHECKED: AWL	8/13/21	CHECKED: AWL 8/13/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 3
STA. 183+00 TO STA. 199+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	7 of 42
CONTRACT	PROJECT
B-40719	1702255

P:\0303-MAR-2022_14:23 PM



- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ← Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- ▬ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction



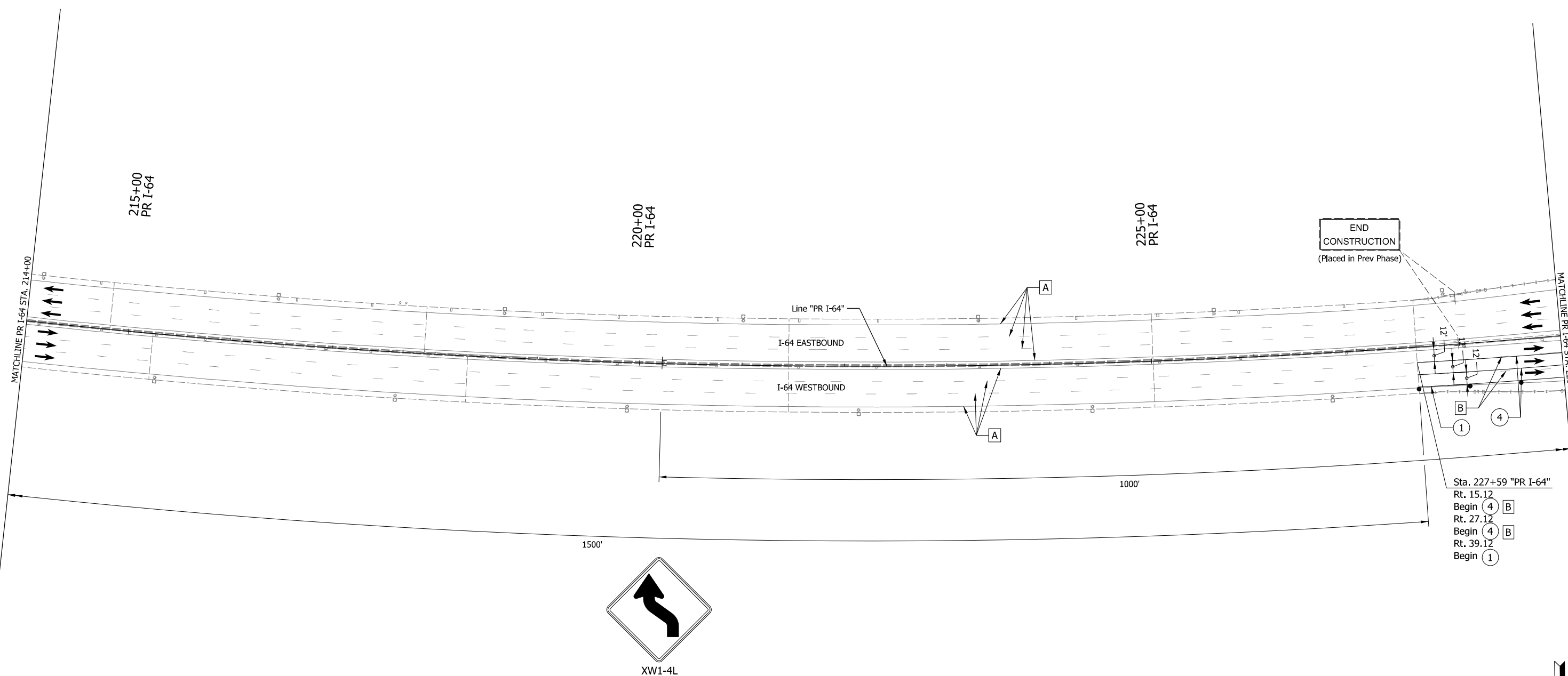
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DESIGNED: DPS	7/28/21	DRAWN: DPS 7/28/21
CHECKED: AWL	8/13/21	CHECKED: AWL 8/13/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 3
STA. 199+00 TO STA. 214+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
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BUILDABLE UNIT	SHEETS
14	8 of 42
CONTRACT	PROJECT
B-40719	1702255

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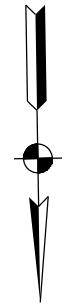


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ← Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ⊘ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction



RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
DESIGNED: DPS	3/11/21	DRAWN: DPS 3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

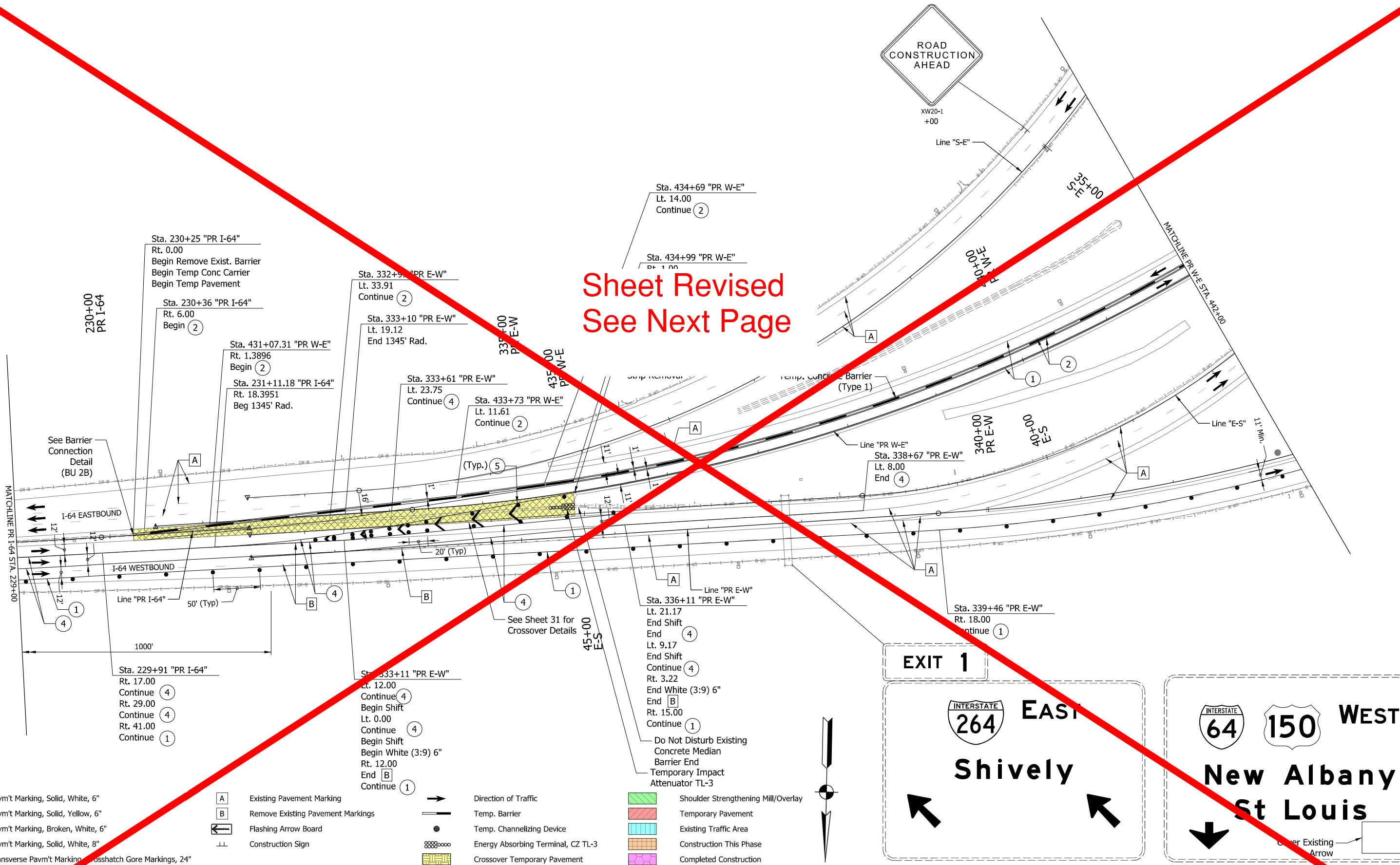
INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 3
STA. 214+00 TO STA. 229+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	9 of 42
CONTRACT	PROJECT
B-40719	1702255

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Sheet Revised
See Next Page



- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A** Existing Pavement Marking
- B** Remove Existing Pavement Markings
- ↔ Flashing Arrow Board
- ⊥ Construction Sign
- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ▨ Crossover Temporary Pavement
- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction
- Do Not Disturb Existing Concrete Median Barrier End Temporary Impact Attenuator TL-3

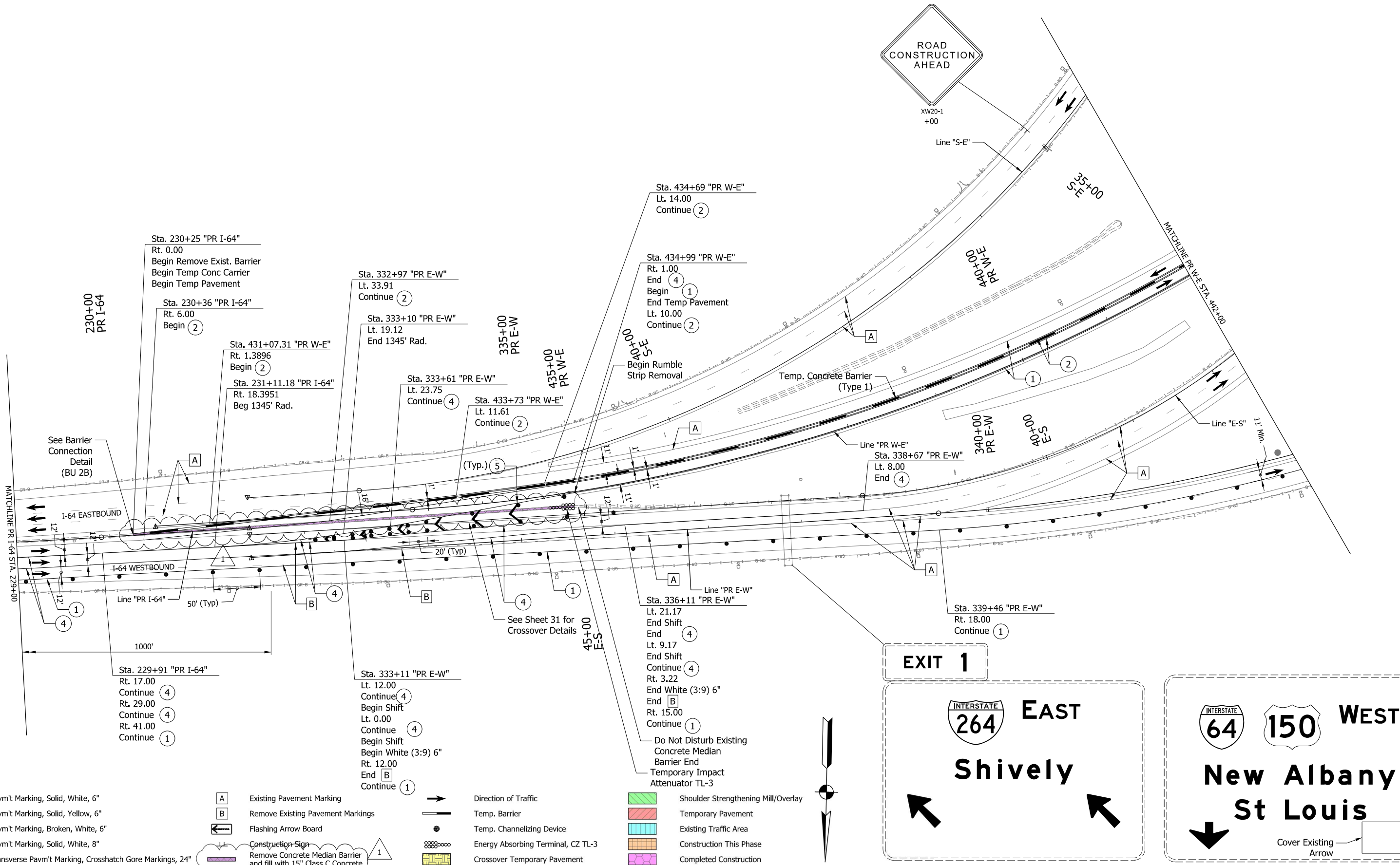


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CHECKED: AWL	8/13/21	CHECKED: AWL 8/13/21

INDIANA
DEPARTMENT OF TRANSPORTATION

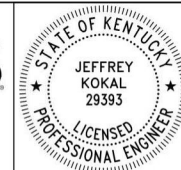
MAINTENANCE OF TRAFFIC
MOT PHASE 3
STA. 229+00 TO STA. 442+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	10 of 42
CONTRACT	PROJECT
B-40719	1702255



REV	DATE	DESCRIPTION
1	11/8/22	Concrete Median Barrier Removal

Page 153 of 186



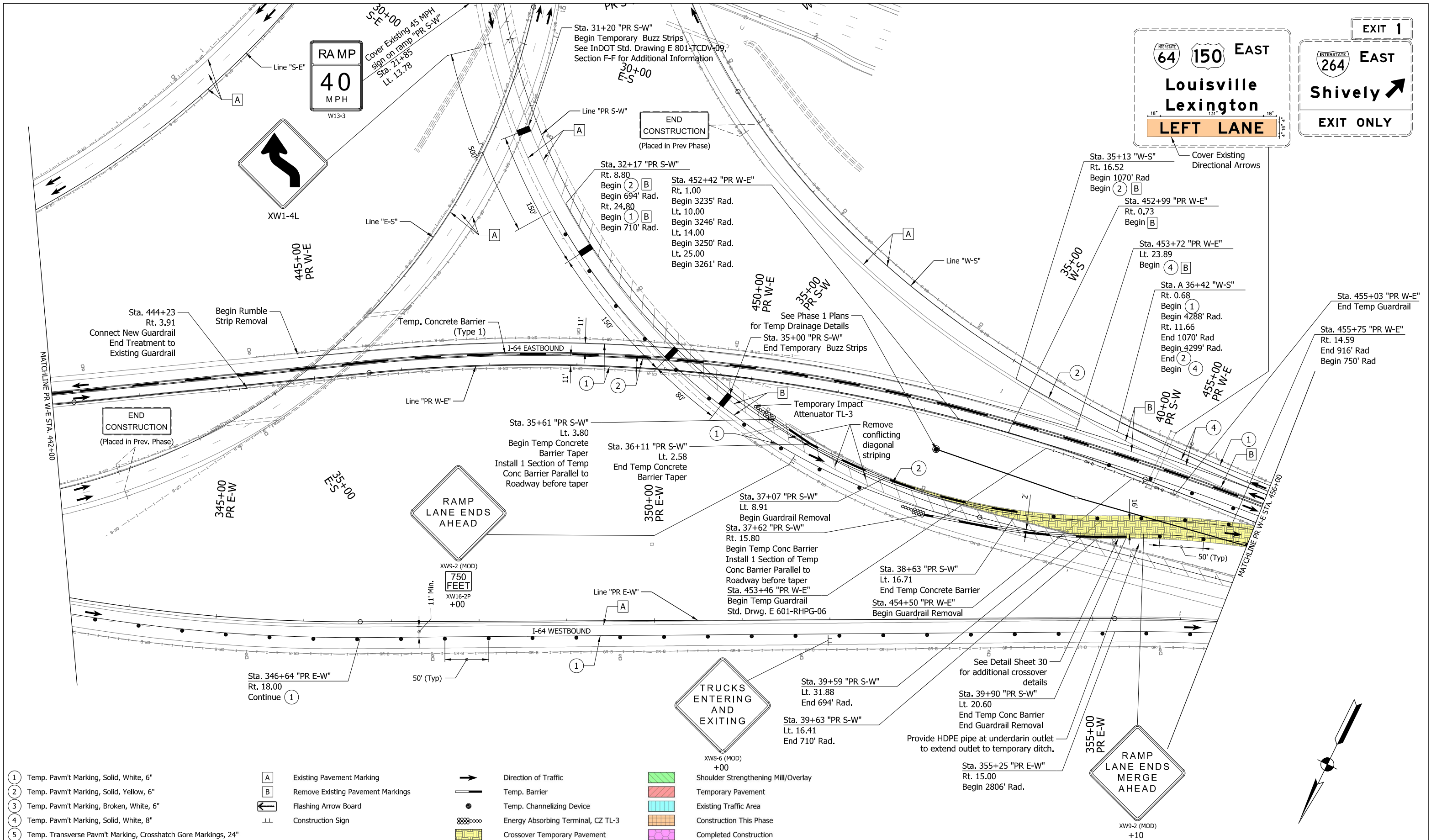
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INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 3
STA. 229+00 TO STA. 442+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	10 of 42
CONTRACT	PROJECT
B-40719	1702255

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- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



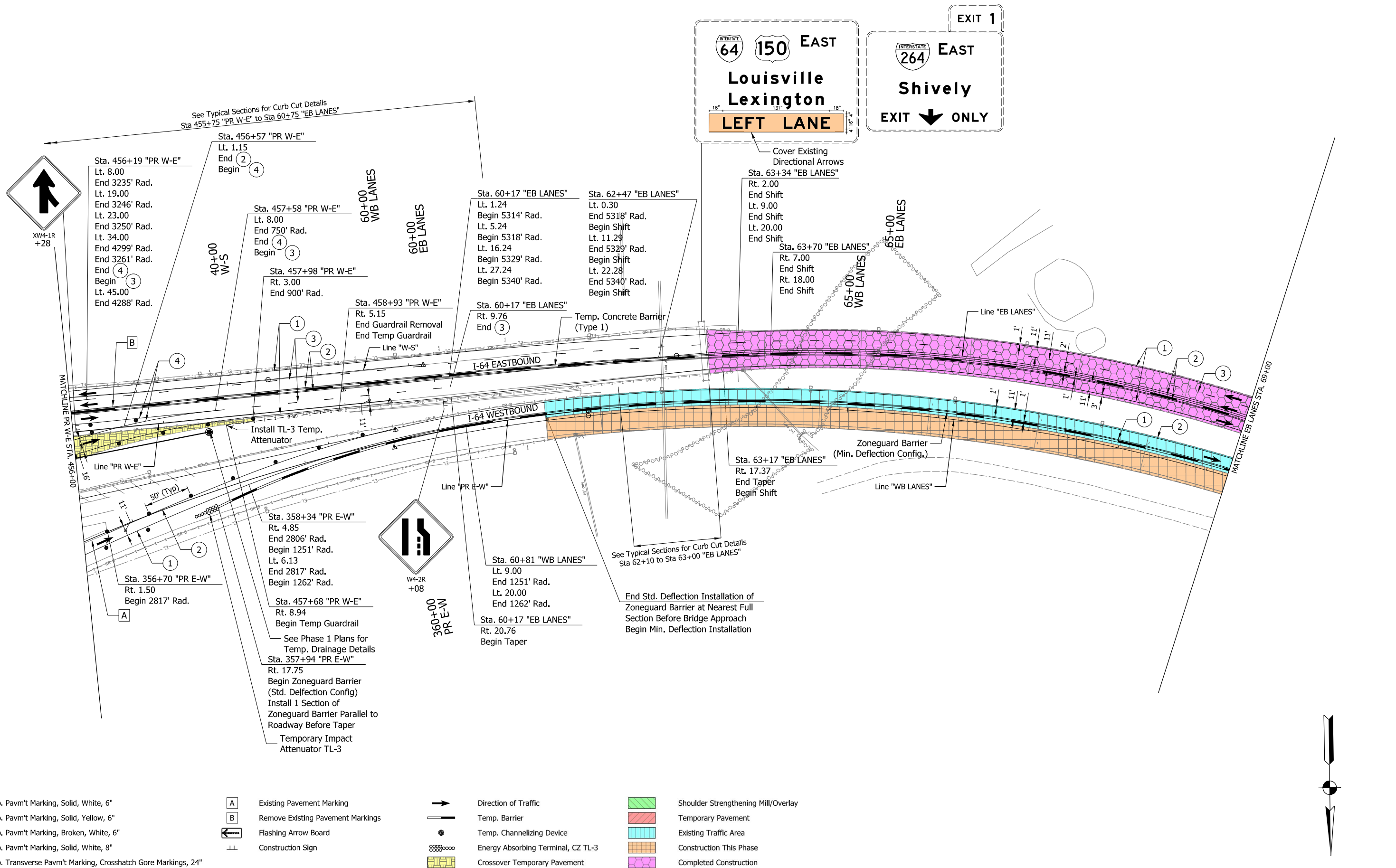
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CHECKED	AWL 8/13/21	CHECKED: AWL 8/13/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 3
STA. 442+00 TO STA. 456+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	11 of 42
CONTRACT	PROJECT
B-40719	1702255

Plot: 15-APR-2022 09:17 AM



- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



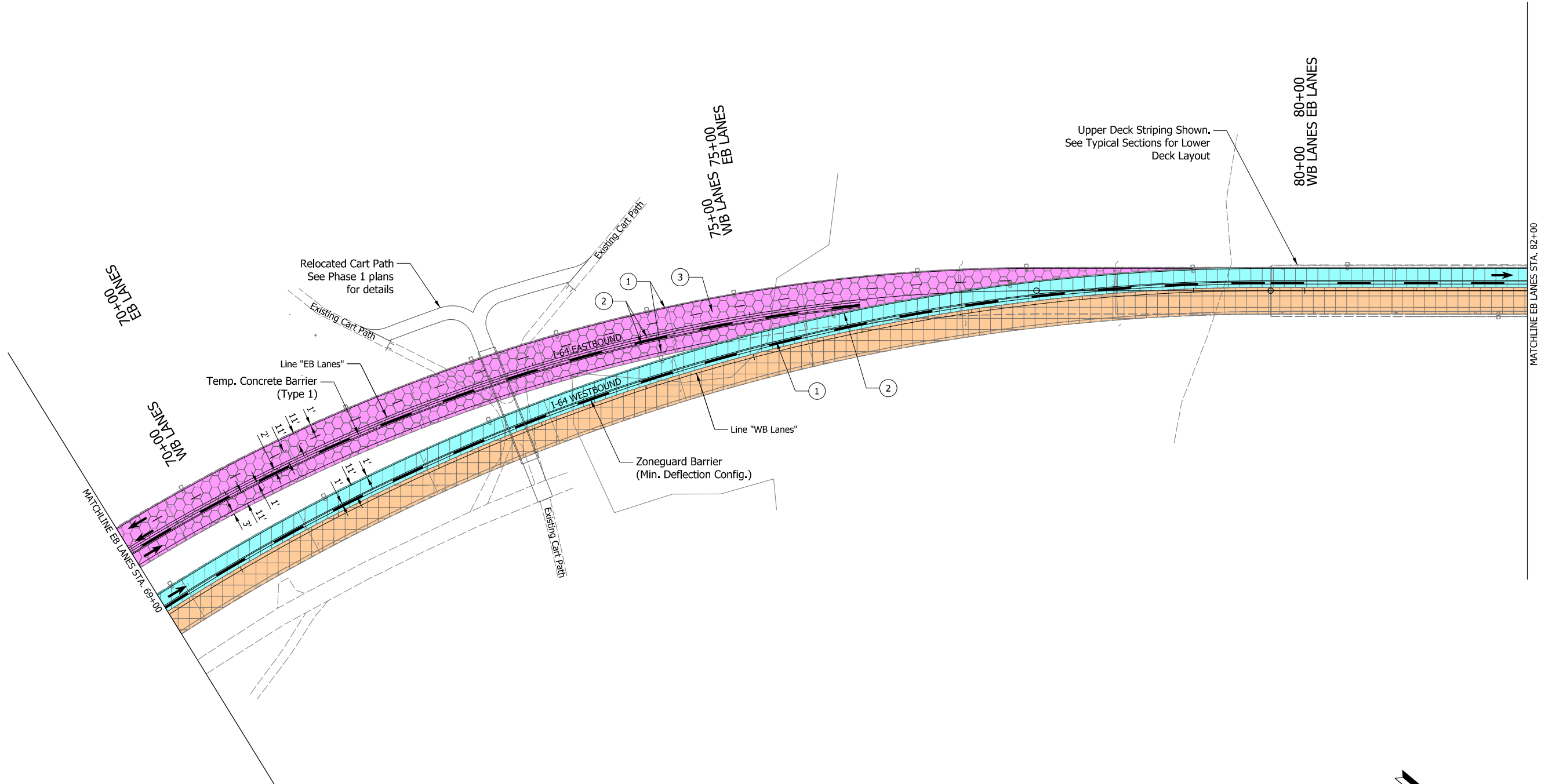
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CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 3
STA. 456+00 TO STA. 69+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
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BUILDABLE UNIT	SHEETS
14	12 of 42
CONTRACT	PROJECT
B-40719	1702255

Plot: 15-APR-2022 09:17 AM

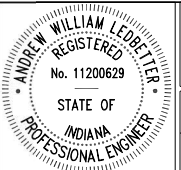
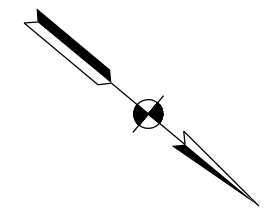


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ← Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ⊘ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction



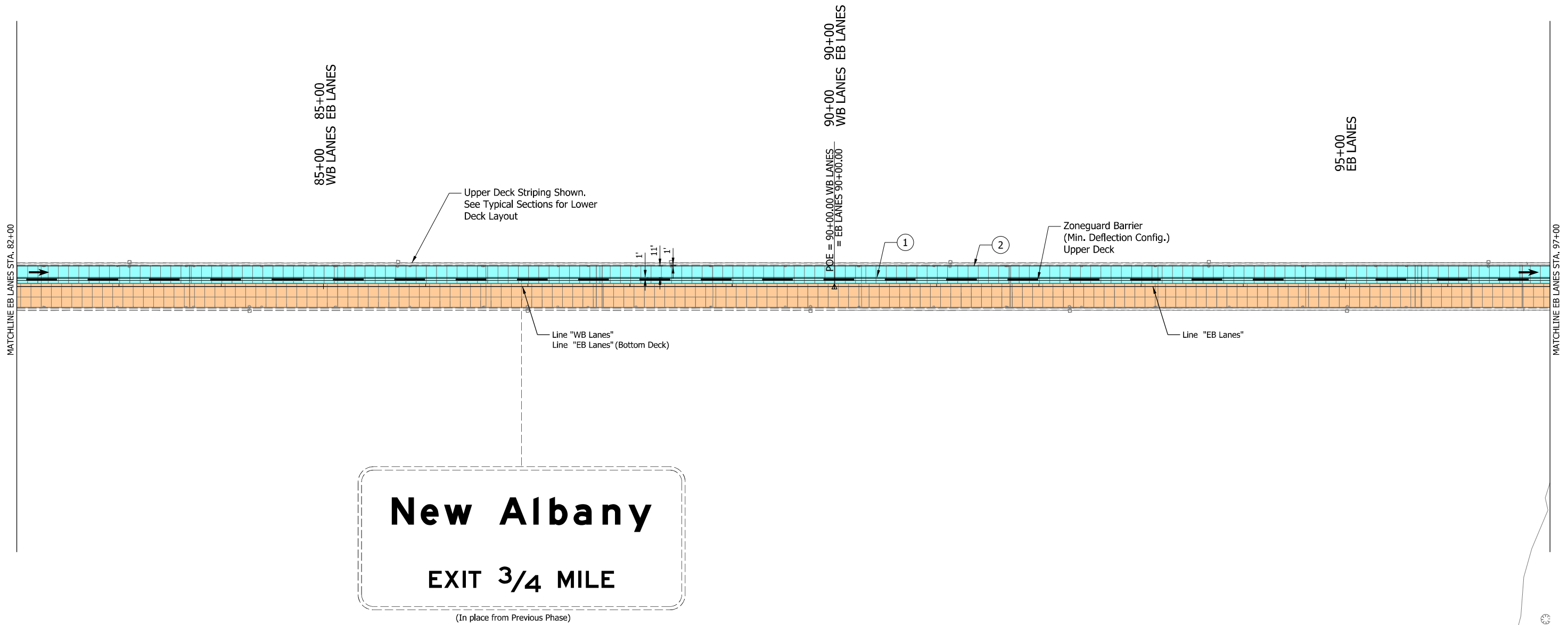
RECOMMENDED FOR APPROVAL	<i>Andrew W. Williams</i>	4/15/2022
DESIGNED:	DPS	7/30/21
DRAWN:	DPS	7/30/21
CHECKED:	AWL	8/13/21
	AWL	8/13/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 3
STA. 69+00 TO STA. 82+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
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BUILDABLE UNIT	SHEETS
14	13 of 42
CONTRACT	PROJECT
B-40719	1702255

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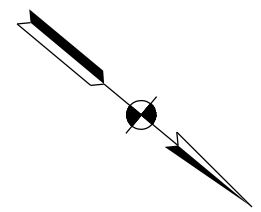


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- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ← Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ⊘ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction

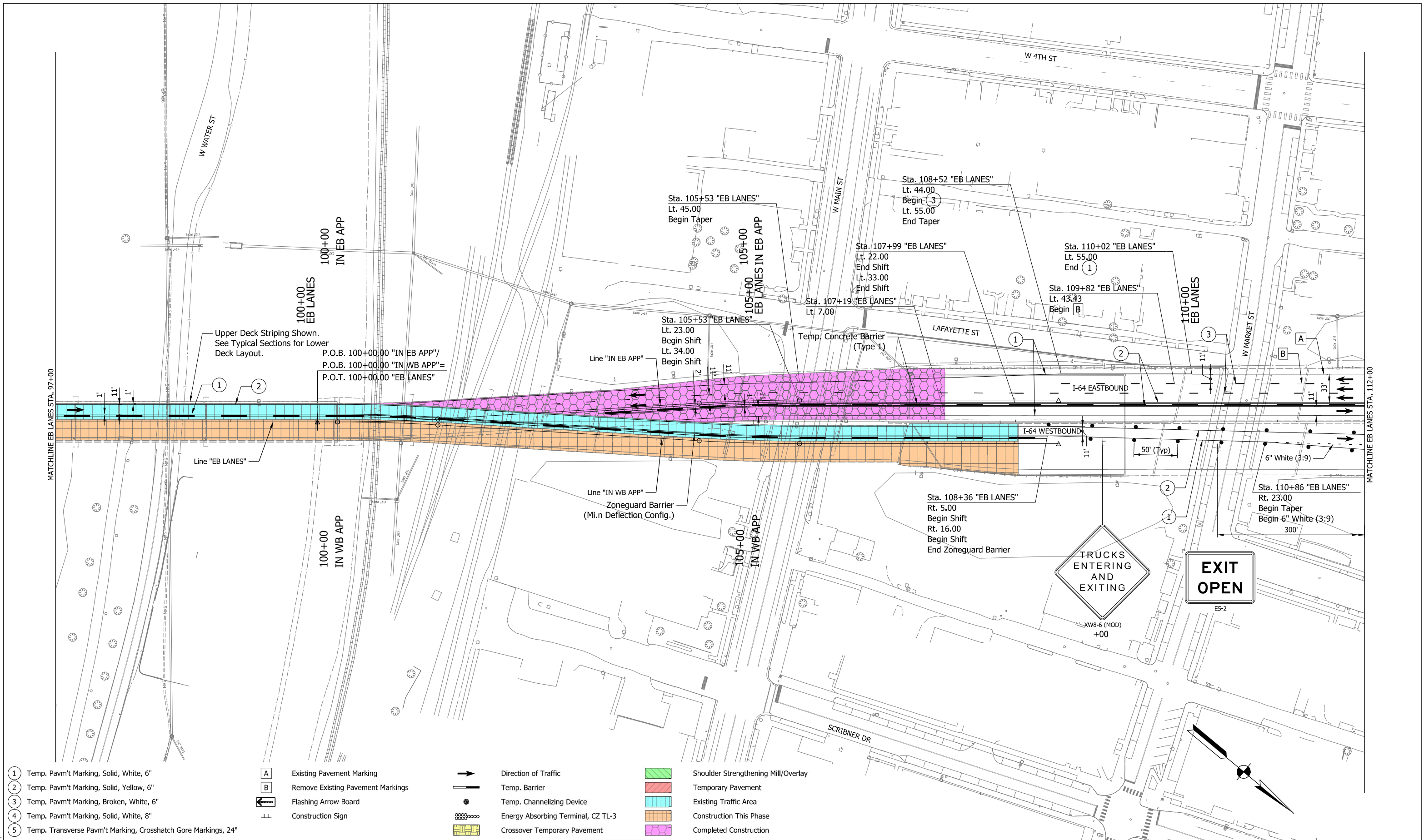


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CHECKED: AWL	8/13/21	CHECKED: AWL 8/13/21

INDIANA DEPARTMENT OF TRANSPORTATION	
MAINTENANCE OF TRAFFIC MOT PHASE 3 STA. 82+00 TO STA. 97+00	

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	14 of 42
CONTRACT	PROJECT
B-40719	1702255

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- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



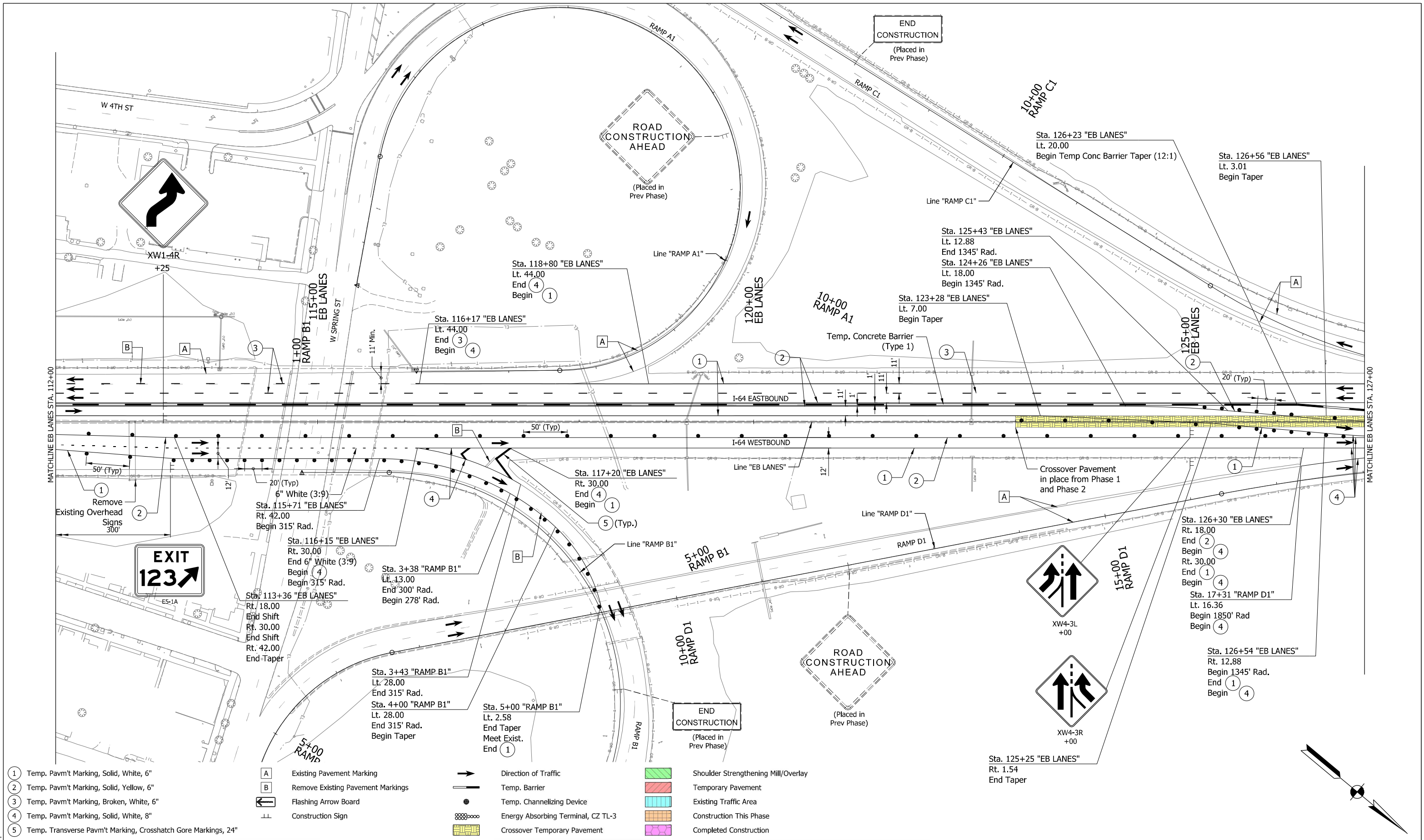
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				7/30/21
CHECKED:	AWL	8/13/21	CHECKED:	AWL
				8/13/21

INDIANA
DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC
MOT PHASE 3
STA. 97+00 TO STA. 112+00**

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	15 of 42
CONTRACT	PROJECT
B-40719	1702255

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- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channeled Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction

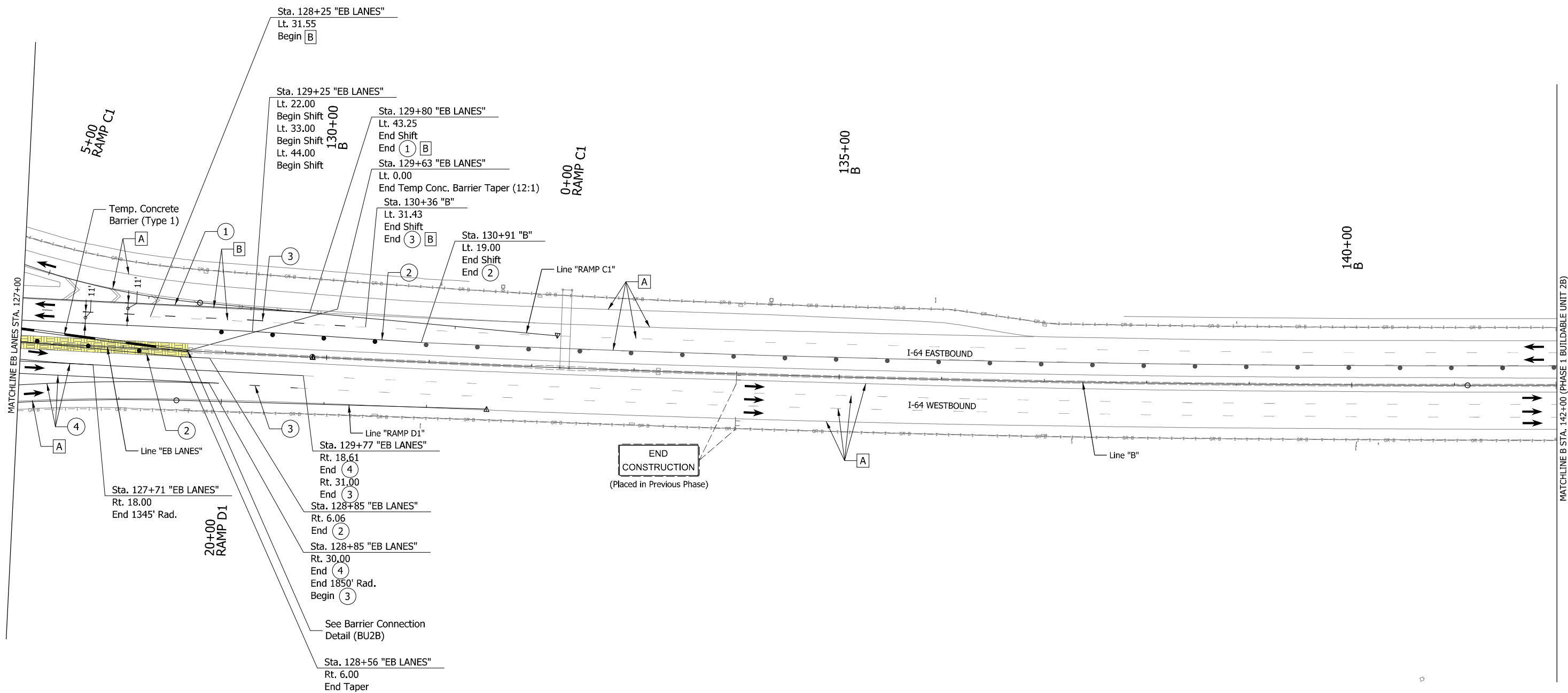


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DESIGNED:	DPS	7/30/21
DRAWN:	DPS	7/30/21
CHECKED:	AWL	8/13/21
	AWL	8/13/21

INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC MOT PHASE 3
STA. 112+00 TO STA. 127+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	16 of 42
CONTRACT	PROJECT
B-40719	1702255

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- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

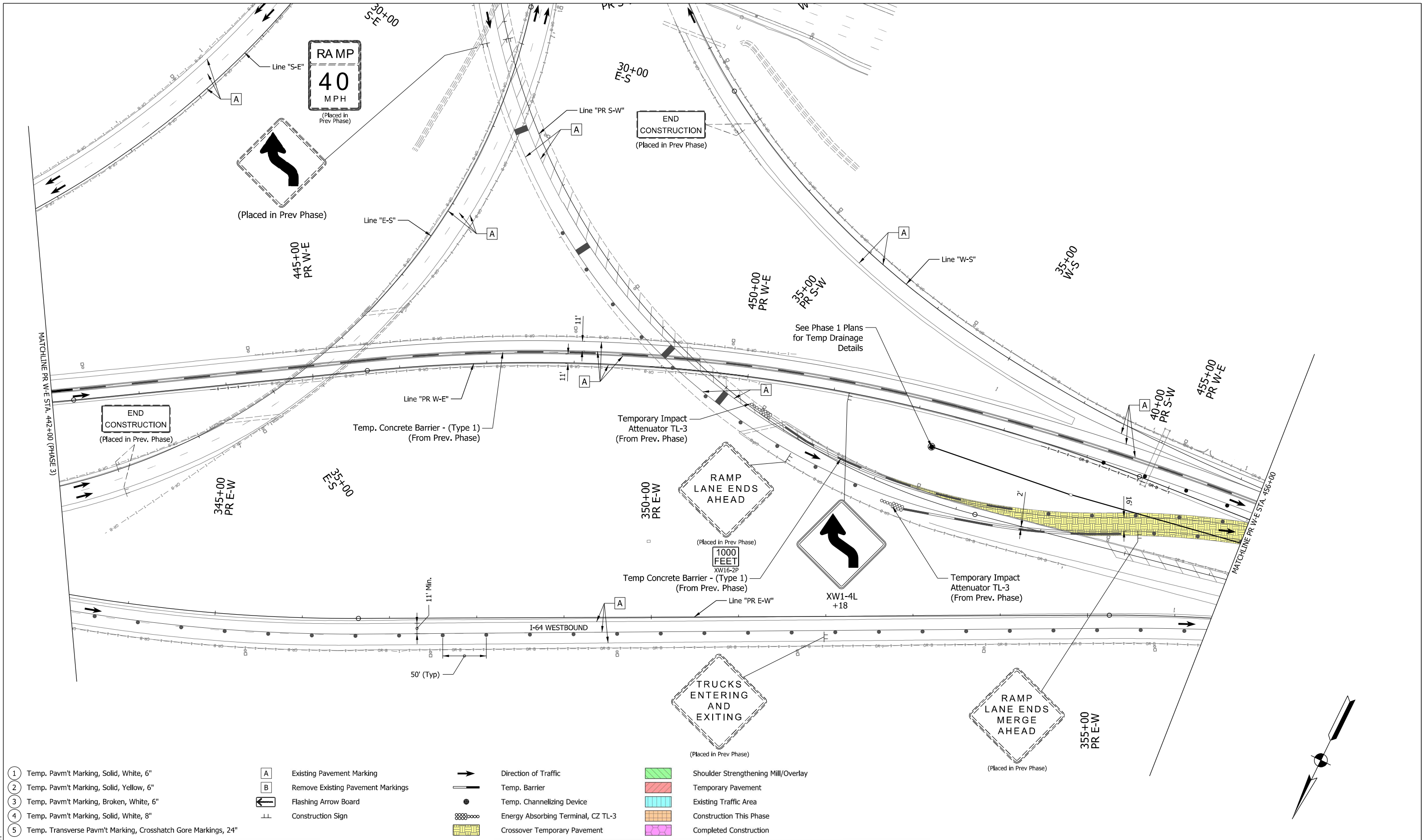
- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



RECOMMENDED FOR APPROVAL	<i>Andrew W. Ledbetter</i>	4/15/2022
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CHECKED: AWL	8/13/21	CHECKED: AWL 8/13/21

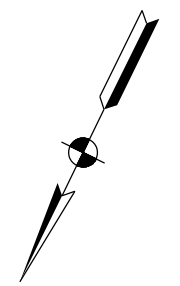
INDIANA DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC MOT PHASE 3
 STA. 127+00 TO STA. 142+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	17 of 42
CONTRACT	PROJECT
B-40719	1702255



- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- | | | |
|---|--|---|
| <ul style="list-style-type: none"> A Existing Pavement Marking B Remove Existing Pavement Markings Flashing Arrow Board Construction Sign | <ul style="list-style-type: none"> Direction of Traffic Temp. Barrier Temp. Channelizing Device Energy Absorbing Terminal, CZ TL-3 Crossover Temporary Pavement | <ul style="list-style-type: none"> Shoulder Strengthening Mill/Overlay Temporary Pavement Existing Traffic Area Construction This Phase Completed Construction |
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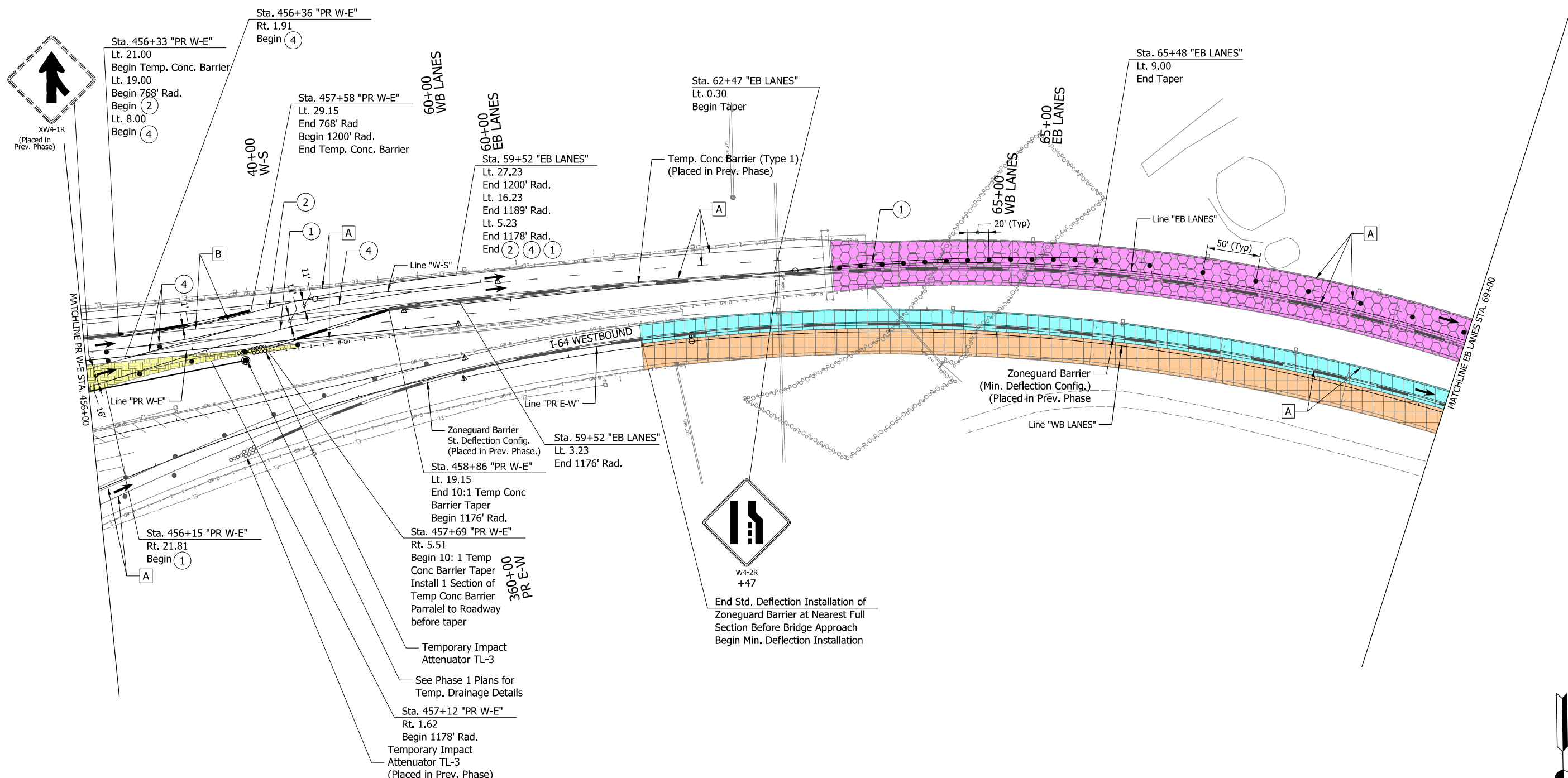
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INDIANA
DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC
MOT PHASE 3A
STA. 442+00 TO STA. 456+00**

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	18 of 42
CONTRACT	PROJECT
B-40719	1702255

PLOT: 15-APR-2022 09:18 AM



- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign
- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement
- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



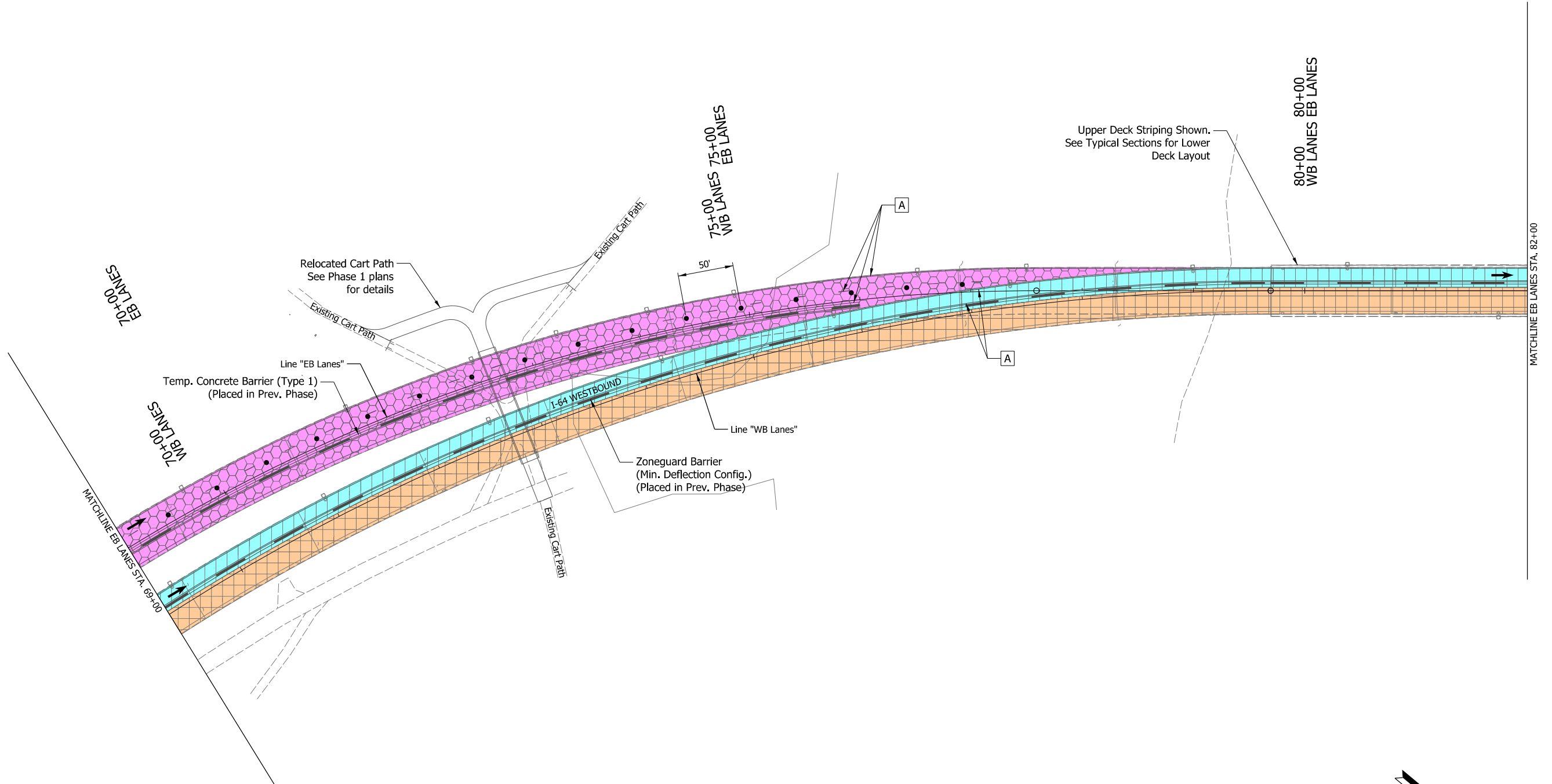
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DESIGNED: DPS	3/11/21	DRAWN: DPS 3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 3A
STA. 456+00 TO STA. 69+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
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BUILDABLE UNIT	SHEETS
14	19 of 42
CONTRACT	PROJECT
B-40719	1702255

Plot: 15-APR-2022 09:18 AM



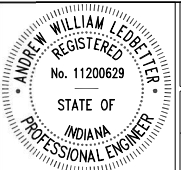
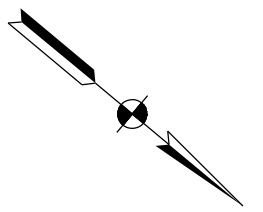
Upper Deck Striping Shown.
See Typical Sections for Lower Deck Layout

- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ← Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ⊘ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction

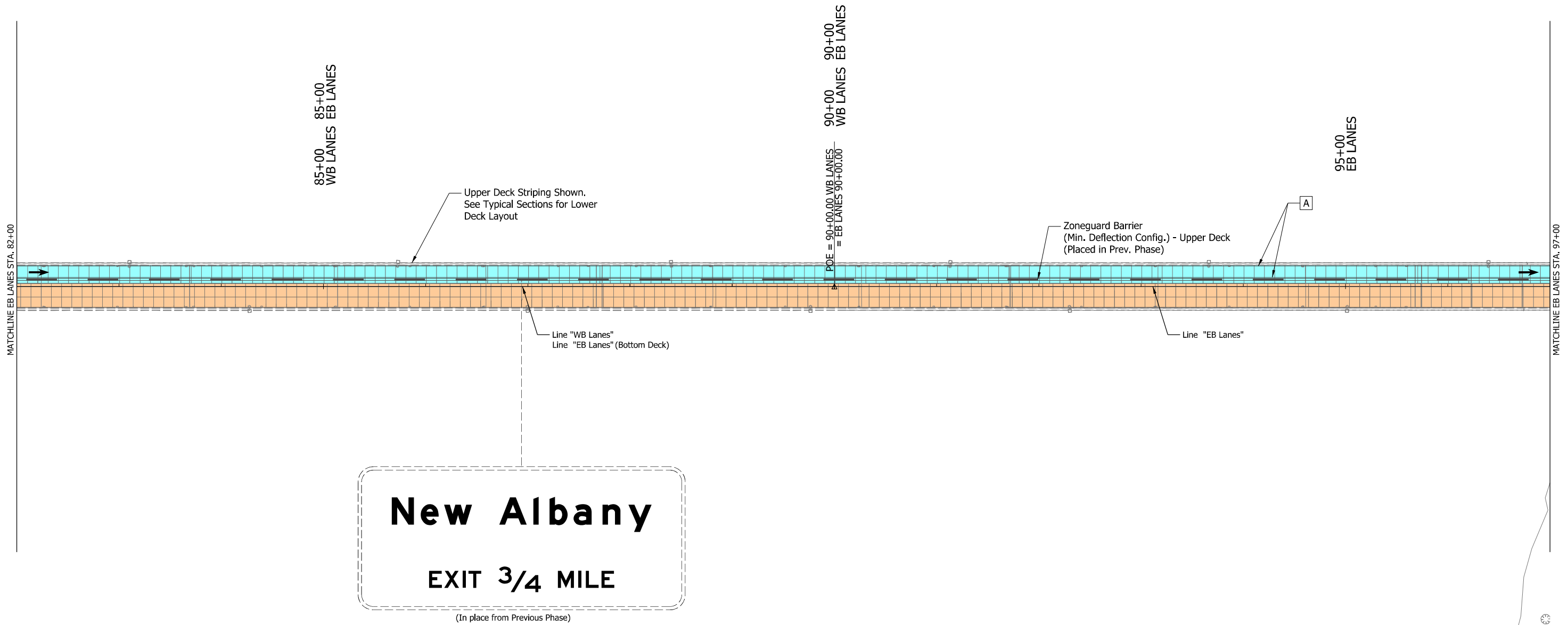


RECOMMENDED FOR APPROVAL	<i>Andrew W. Lepletter</i>	4/15/2022
	DESIGN ENGINEER	DATE
DESIGNED: DPS	7/30/21	DRAWN: DPS 7/30/21
CHECKED: AWL	8/13/21	CHECKED: AWL 8/13/21

INDIANA DEPARTMENT OF TRANSPORTATION	
MAINTENANCE OF TRAFFIC MOT PHASE 3A STA. 69+00 TO STA. 82+00	

HORIZONTAL SCALE	BRIDGE FILE	
1"=50'		
VERTICAL SCALE	DESIGNATION	
	1702255	
BUILDABLE UNIT	SHEETS	
14	20	of 42
CONTRACT	PROJECT	
B-40719	1702255	

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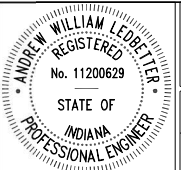
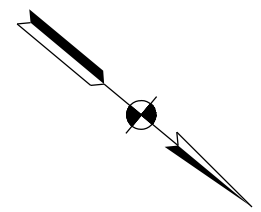


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ← Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ⊘ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction

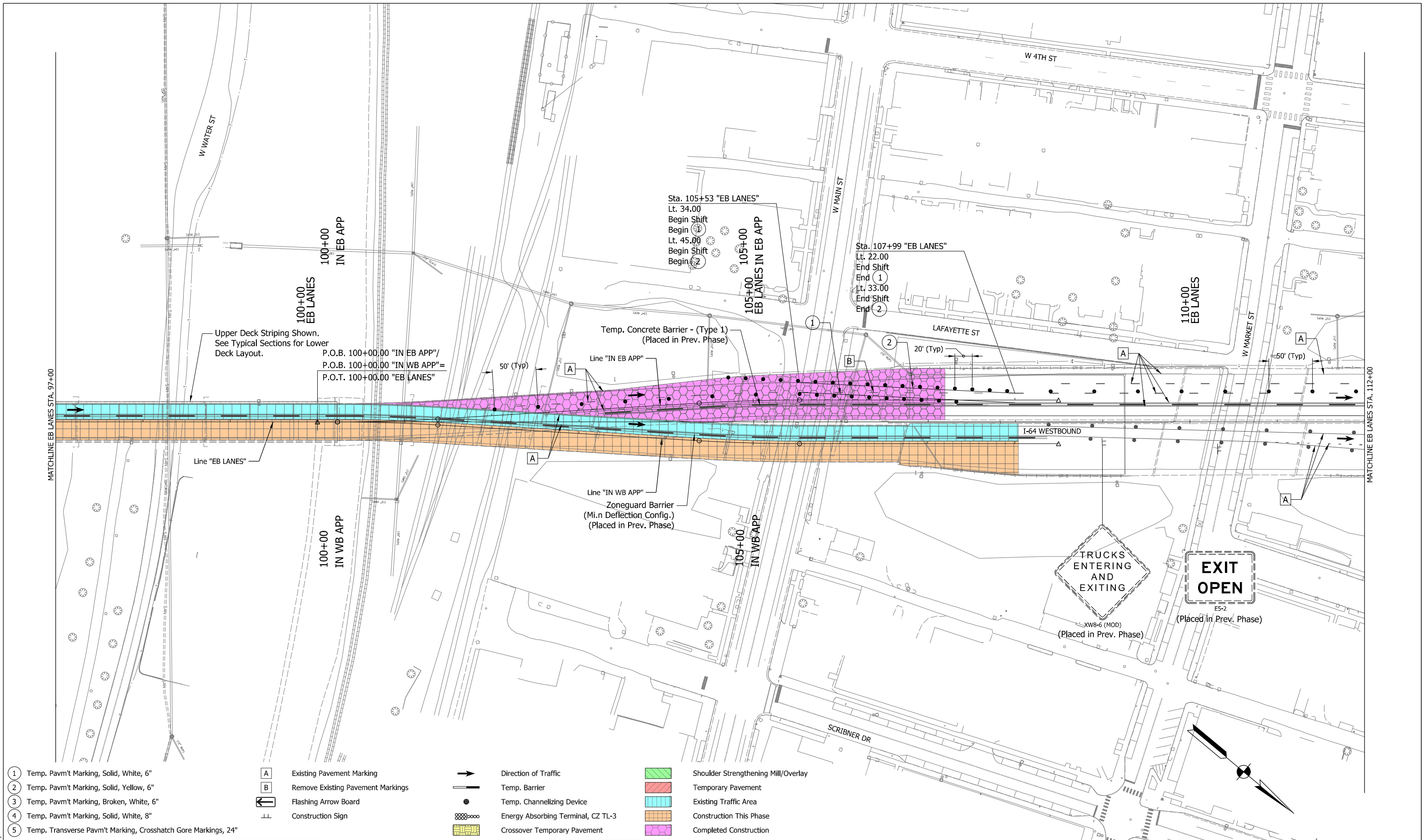


RECOMMENDED FOR APPROVAL	<i>Andrew W. Leppert</i>	4/15/2022
DESIGNED: DPS	7/30/21	DRAWN: DPS 7/30/21
CHECKED: AWL	8/13/21	CHECKED: AWL 8/13/21

INDIANA DEPARTMENT OF TRANSPORTATION	
MAINTENANCE OF TRAFFIC MOT PHASE 3A STA. 82+00 TO STA. 97+00	

HORIZONTAL SCALE 1"=50'	BRIDGE FILE
VERTICAL SCALE	DESIGNATION 1702255
BUILDABLE UNIT 14	SHEETS 21 of 42
CONTRACT B-40719	PROJECT 1702255

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- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



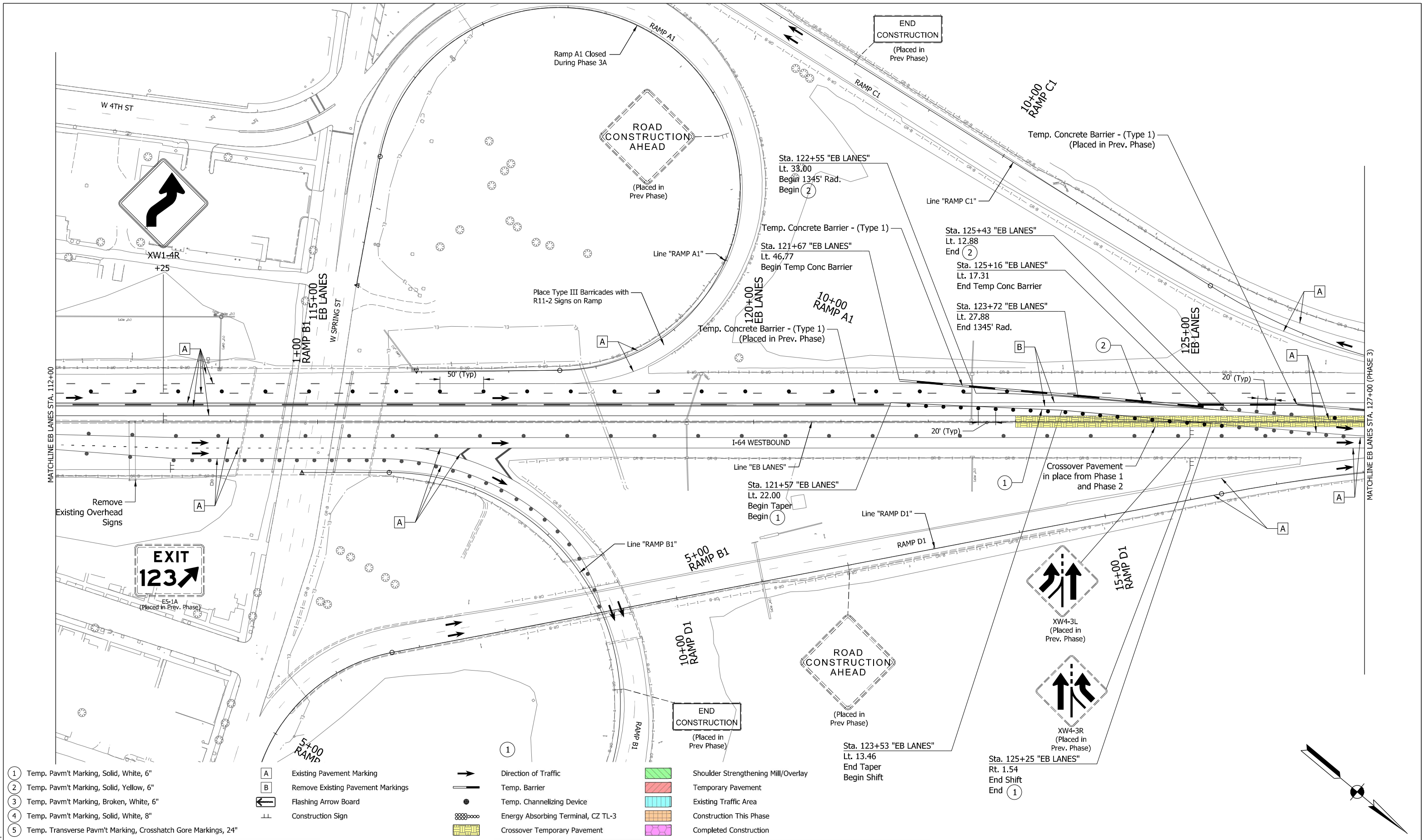
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DESIGNED: DPS	7/30/21	DRAWN: DPS	7/30/21	
CHECKED: AWL	8/13/21	CHECKED: AWL	8/13/21	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 3A
STA. 97+00 TO STA. 112+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	22 of 42
CONTRACT	PROJECT
B-40719	1702255

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- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



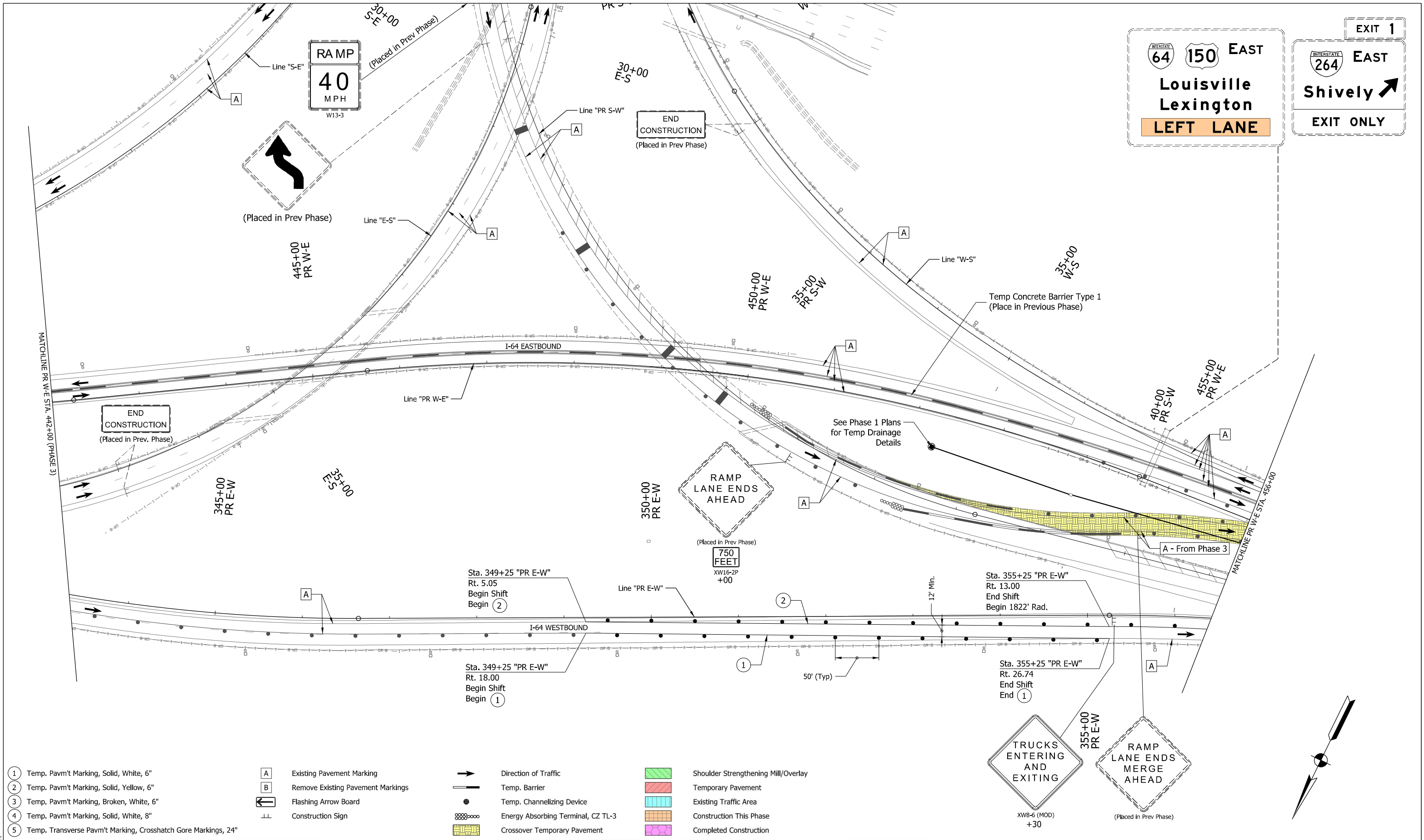
RECOMMENDED FOR APPROVAL	<i>Andrew W. Lebetter</i>	4/15/2022
DESIGNED:	DPS	7/30/21
DRAWN:	DPS	7/30/21
CHECKED:	AWL	8/13/21
	AWL	8/13/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 3A
STA. 112+00 TO STA. 127+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	23 of 42
CONTRACT	PROJECT
B-40719	1702255

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64 150 EAST
Louisville Lexington
LEFT LANE

EXIT 1
264 EAST
Shively ↗
EXIT ONLY

- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
- ⑤ Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A** Existing Pavement Marking
- B** Remove Existing Pavement Markings
- ↔ Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ▨ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction

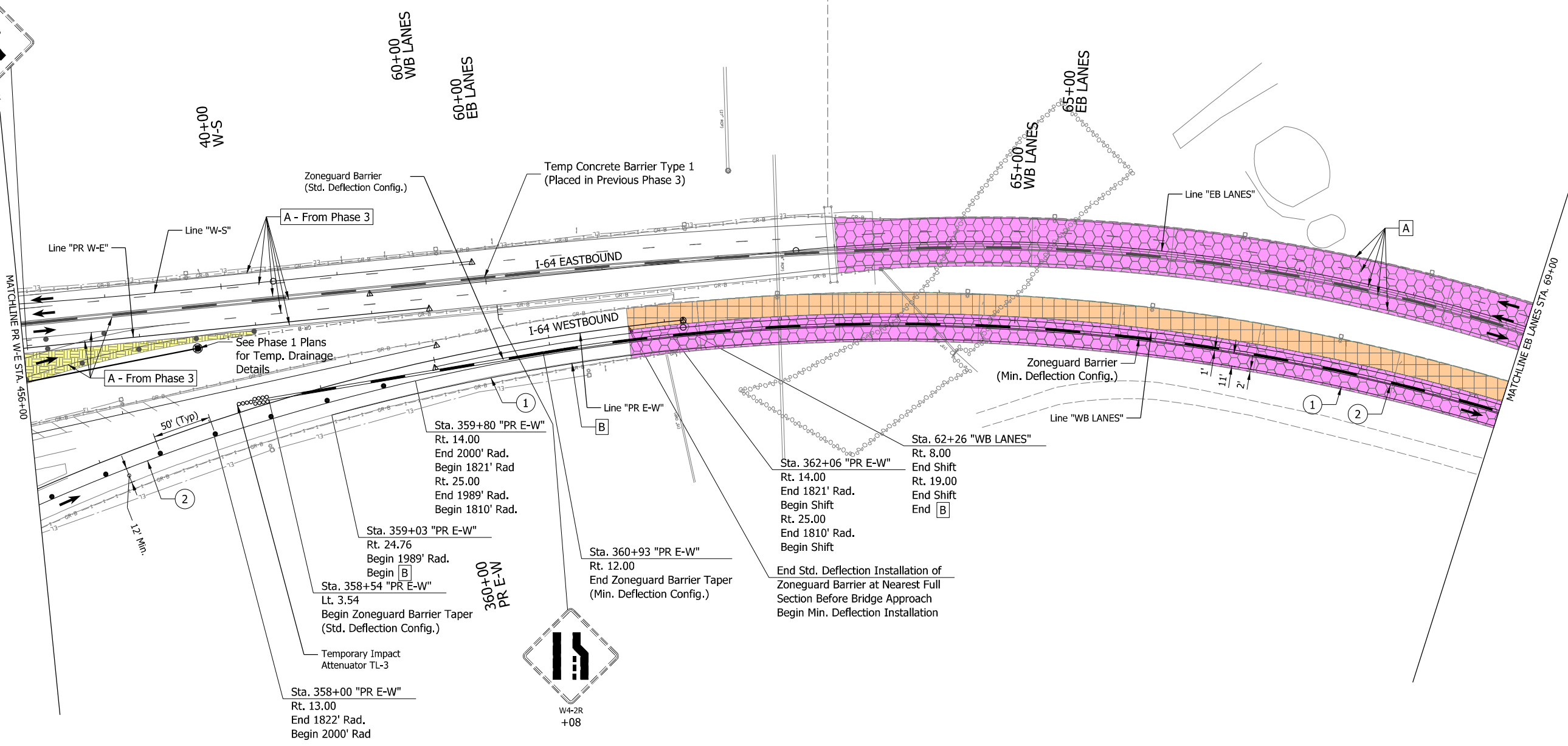
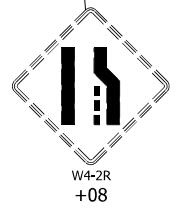
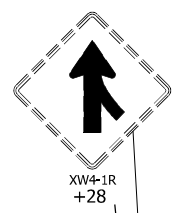
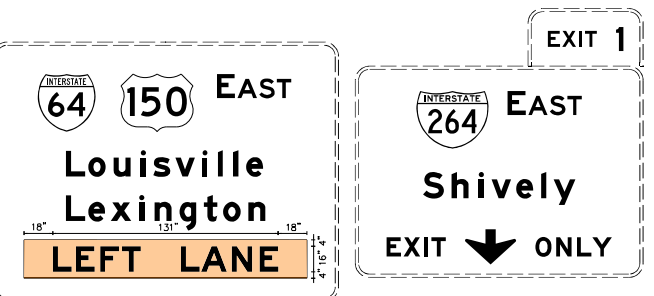


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DESIGNED: DPS	7/29/21	DRAWN: DPS 7/29/21
CHECKED: AWL	8/13/21	CHECKED: AWL 8/13/21

INDIANA
 DEPARTMENT OF TRANSPORTATION
**MAINTENANCE OF TRAFFIC
 MOT PHASE 4
 STA. 442+00 TO STA. 456+00**

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	24 of 42
CONTRACT	PROJECT
B-40719	1702255

Plot: 15-APR-2022 09:18 AM



- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction

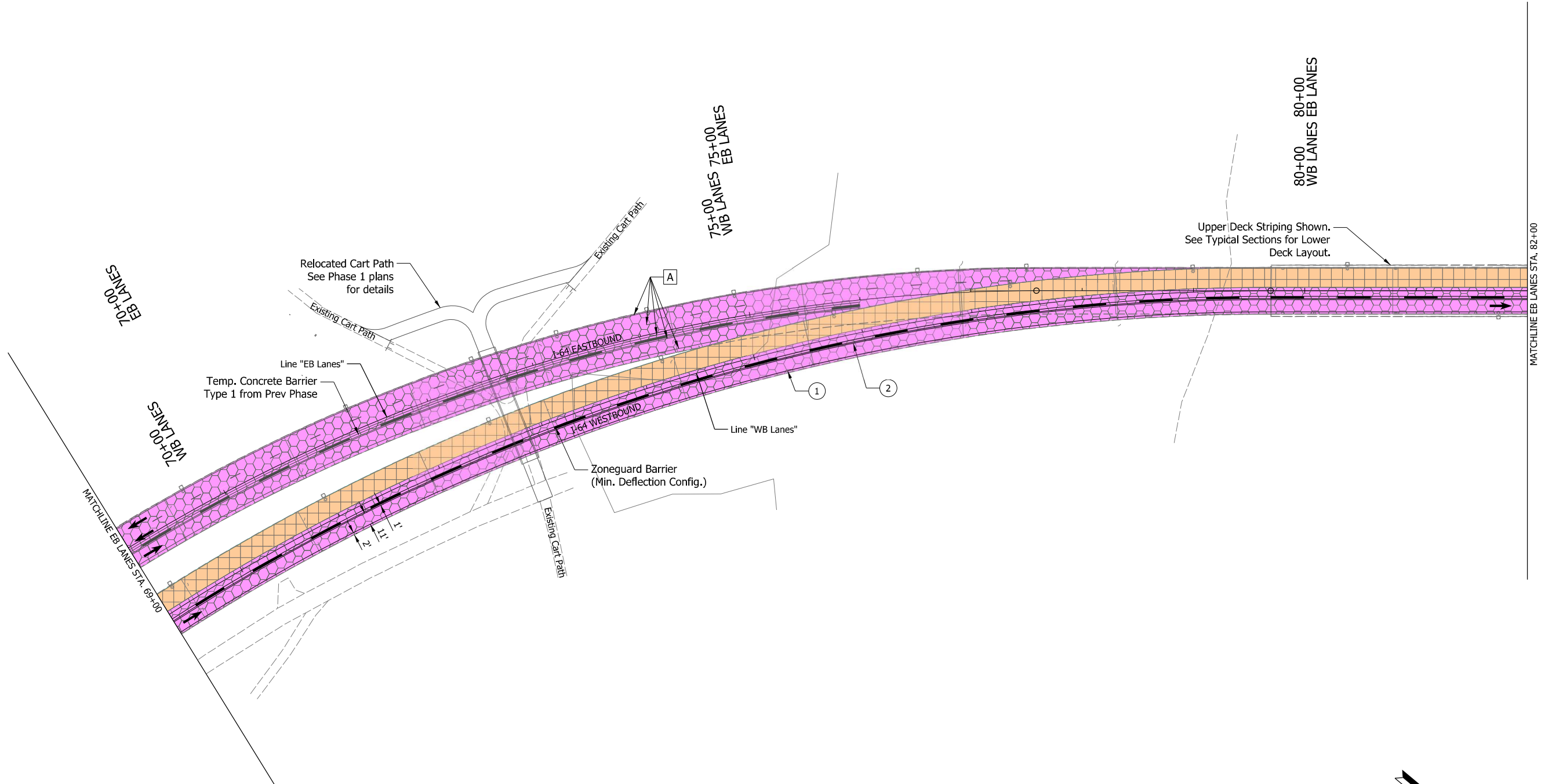


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
DESIGNED: DPS	3/11/21	DRAWN: DPS 3/11/21
CHECKED: AWL	3/26/21	CHECKED: AWL 3/26/21

INDIANA DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC MOT PHASE 4
 STA. 456+00 TO STA. 69+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	25 of 42
CONTRACT	PROJECT
B-40719	1702255

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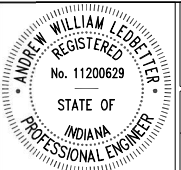
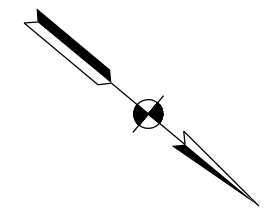


- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
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- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ← Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ⊘ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction



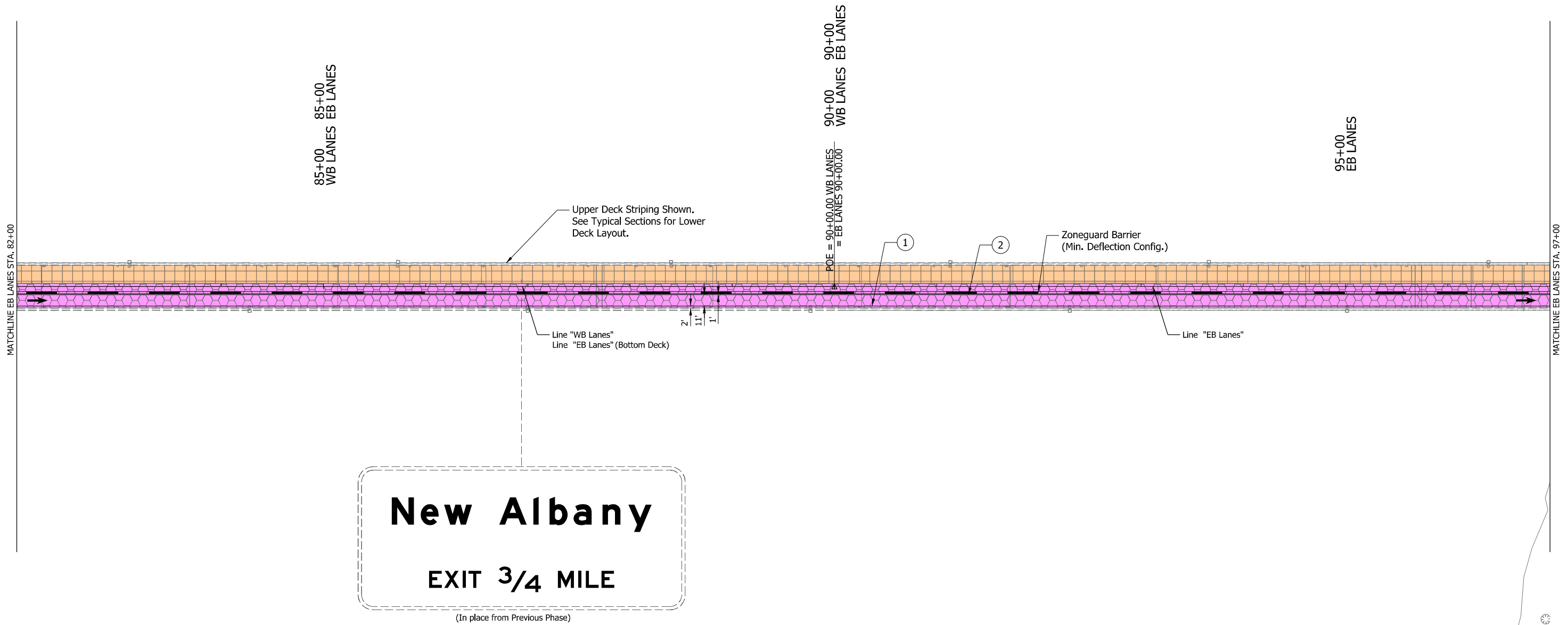
RECOMMENDED FOR APPROVAL	<i>Andrew W. William</i>	4/15/2022
DESIGNED: DPS	7/30/21	DRAWN: DPS 7/30/21
CHECKED: AWL	8/13/21	CHECKED: AWL 8/13/21

INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC MOT PHASE 4 STA. 69+00 TO STA. 82+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
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BUILDABLE UNIT	SHEETS
14	26 of 42
CONTRACT	PROJECT
B-40719	1702255

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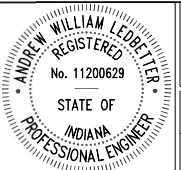
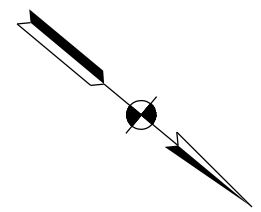


- ① Temp. Pavm't Marking, Solid, White, 6"
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- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- ← Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ⊘ Energy Absorbing Terminal, CZ TL-3
- ⊘ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction

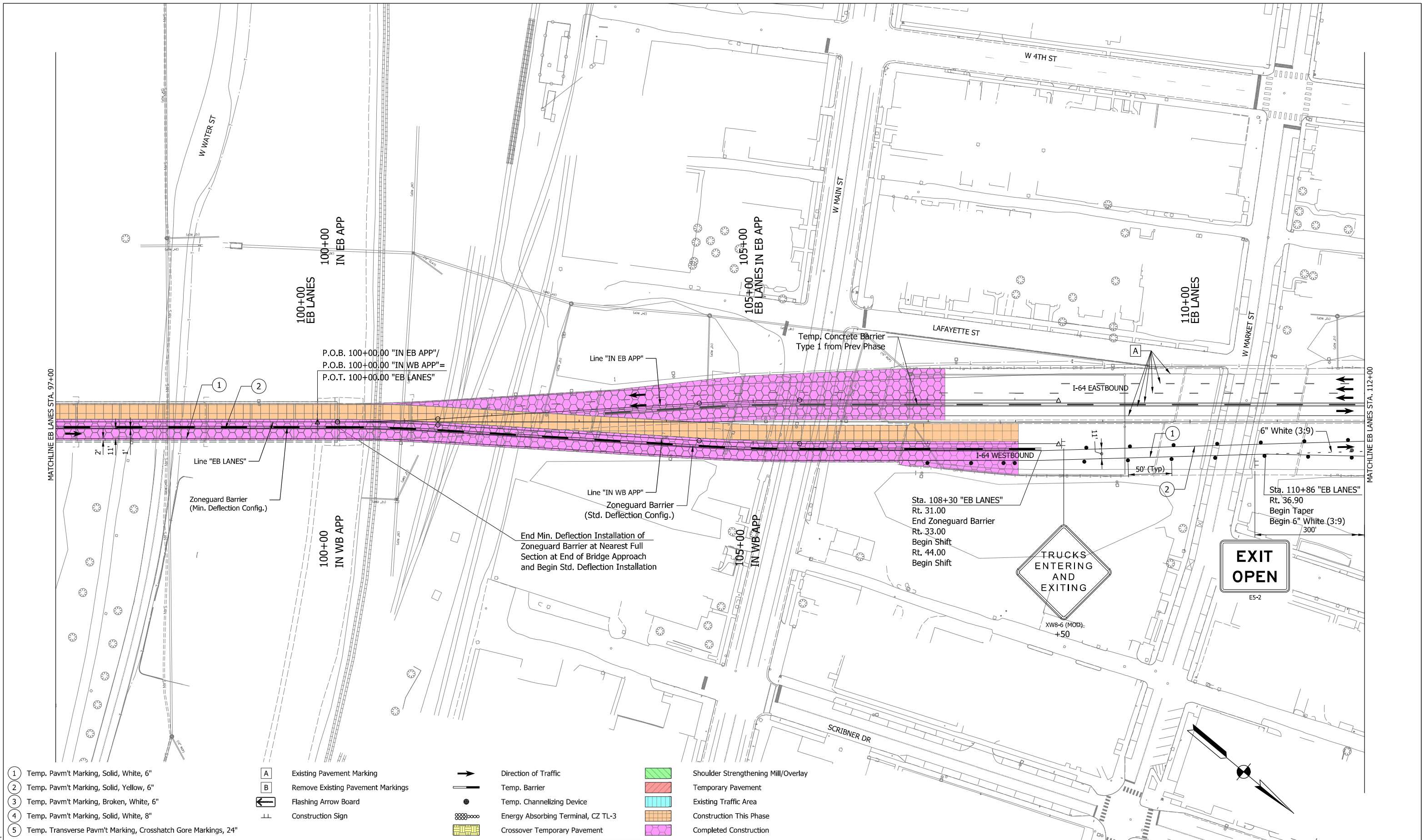


RECOMMENDED FOR APPROVAL	<i>Andrew W. Leppert</i>	4/15/2022
DESIGNED: DPS	7/30/21	DRAWN: DPS 7/30/21
CHECKED: AWL	8/13/21	CHECKED: AWL 8/13/21

INDIANA DEPARTMENT OF TRANSPORTATION	
MAINTENANCE OF TRAFFIC MOT PHASE 4 STA. 82+00 TO STA. 97+00	

HORIZONTAL SCALE 1"=50'	BRIDGE FILE
VERTICAL SCALE	DESIGNATION 1702255
BUILDABLE UNIT 14	SHEETS 27 of 42
CONTRACT B-40719	PROJECT 1702255

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- ① Temp. Pavm't Marking, Solid, White, 6"
- ② Temp. Pavm't Marking, Solid, Yellow, 6"
- ③ Temp. Pavm't Marking, Broken, White, 6"
- ④ Temp. Pavm't Marking, Solid, White, 8"
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- B Remove Existing Pavement Markings
- ↔ Flashing Arrow Board
- ⊥ Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- ▨ Energy Absorbing Terminal, CZ TL-3
- ▨ Crossover Temporary Pavement

- ▨ Shoulder Strengthening Mill/Overlay
- ▨ Temporary Pavement
- ▨ Existing Traffic Area
- ▨ Construction This Phase
- ▨ Completed Construction



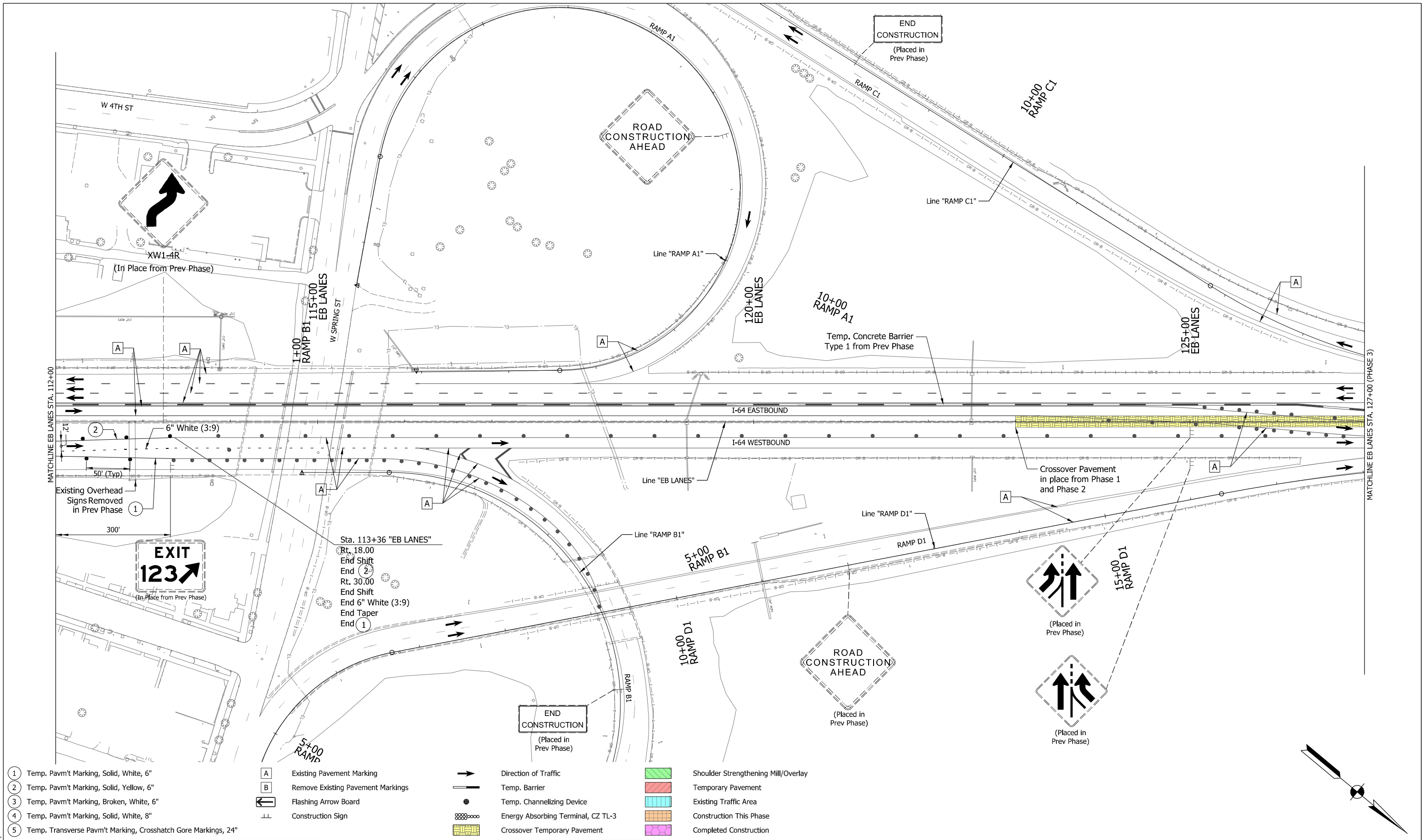
RECOMMENDED FOR APPROVAL	<i>Andrew W. Leffler</i>	4/15/2022
DESIGNED: DPS	7/30/21	DRAWN: DPS 7/30/21
CHECKED: AWL	8/13/21	CHECKED: AWL 8/13/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 4
STA. 97+00 TO STA. 112+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	28 of 42
CONTRACT	PROJECT
B-40719	1702255

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- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
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- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



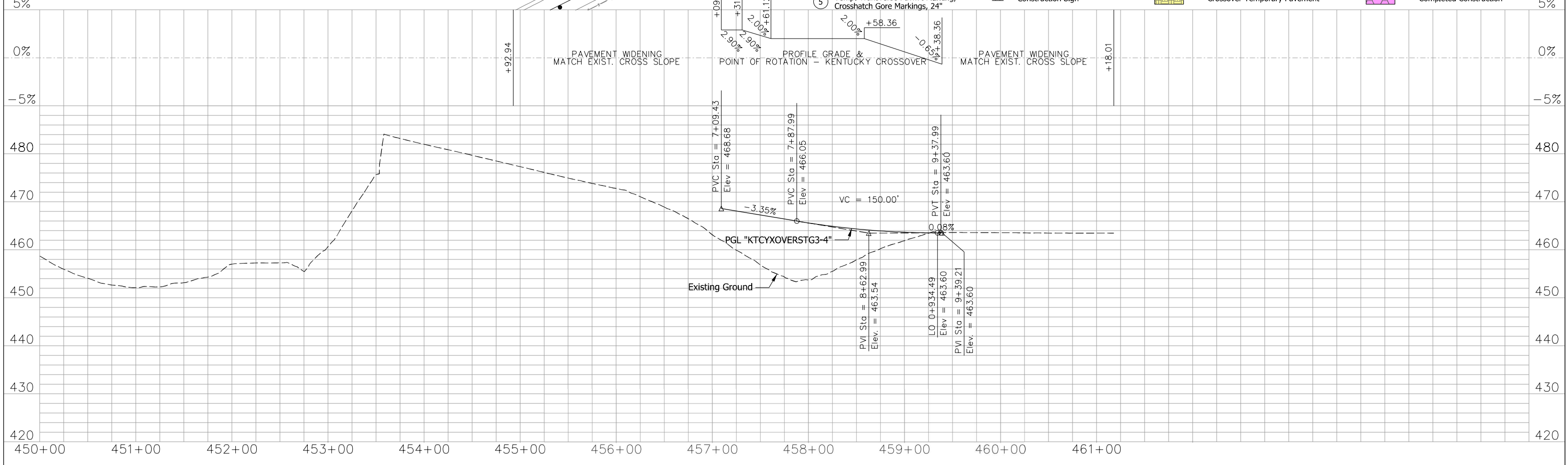
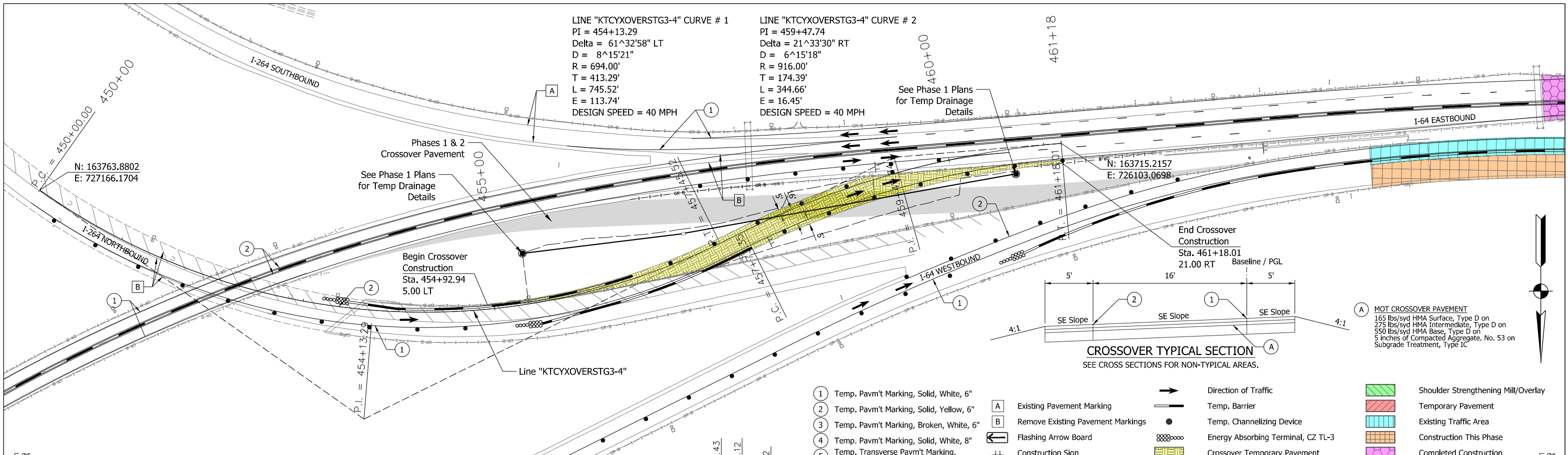
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DESIGNED: DPS	7/30/21	DRAWN: DPS 7/30/21
CHECKED: AWL	8/13/21	CHECKED: AWL 8/13/21

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
MOT PHASE 4
STA. 112+00 TO STA. 127+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
	1702255
BUILDABLE UNIT	SHEETS
14	29 of 42
CONTRACT	PROJECT
B-40719	1702255

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RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
DESIGNED: KLO	8/9/21	DRAWN: KLO 8/9/21
CHECKED: AWL	8/9/21	CHECKED: AWL 8/9/21

INDIANA
 DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
 KENTUCKY CROSSOVER I-264
 PLAN & PROFILE

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
1"=10'	1702255
BUILDABLE UNIT	SHEETS
14	30 of 42
CONTRACT	PROJECT
B-40719	1702255

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225+00
PR I-64

230+00
PR I-64

335+00
PR E-W

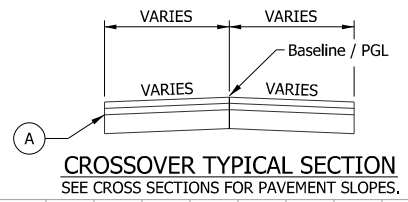
435+00
PR W-E

40+00
S-E

340+00
PR E-W

40+00
S-E

Sheet Revised
See Next Page

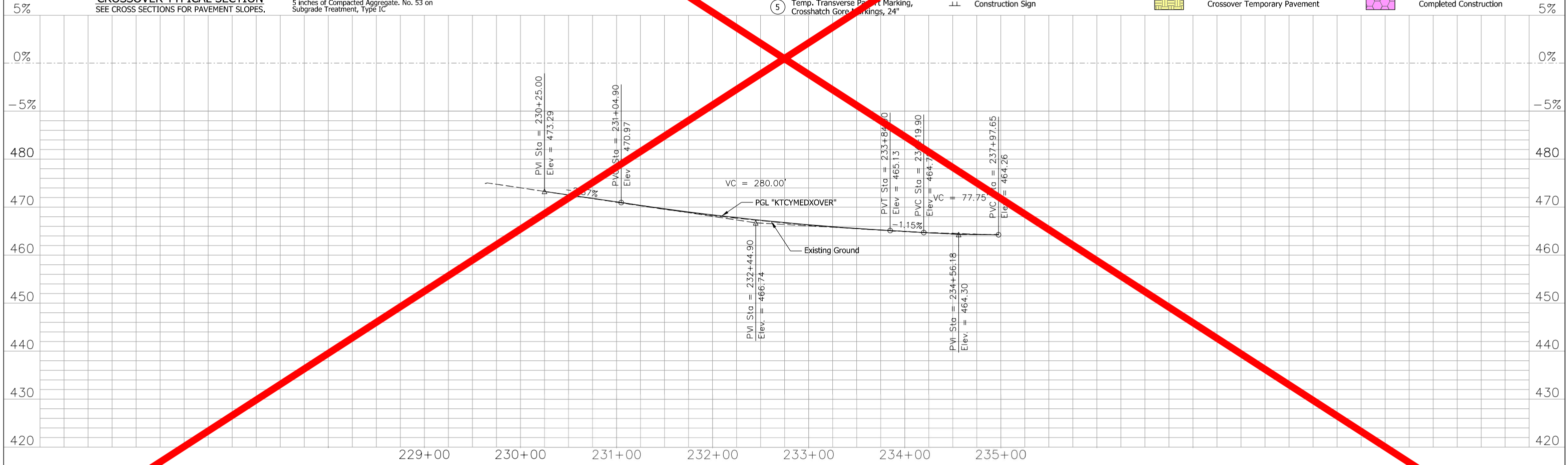


MOT CROSSOVER PAVEMENT
165 lbs/syd HMA Surface, Type D on
275 lbs/syd HMA Intermediate, Type D on
550 lbs/syd HMA Base, Type D on
5 inches of Compacted Aggregate, No. 53 on
Subgrade Treatment, Type 1C

- White, 6"
- Yellow, 6"
- en, White, 6"
- Temp. Pav't Marking, Solid, White, 8"
- Temp. Transverse Pav't Marking, Crosshatch Gore Markings, 24"
- Existing Pavement Marking
- Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction



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RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
DESIGNED: KLO	9/23/21	DRAWN: KLO 9/23/21
CHECKED: AWL	10/22/21	CHECKED: AWL 10/22/21

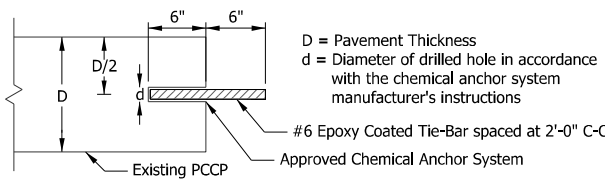
INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
KENTUCKY CROSSOVER I-64
PLAN & PROFILE

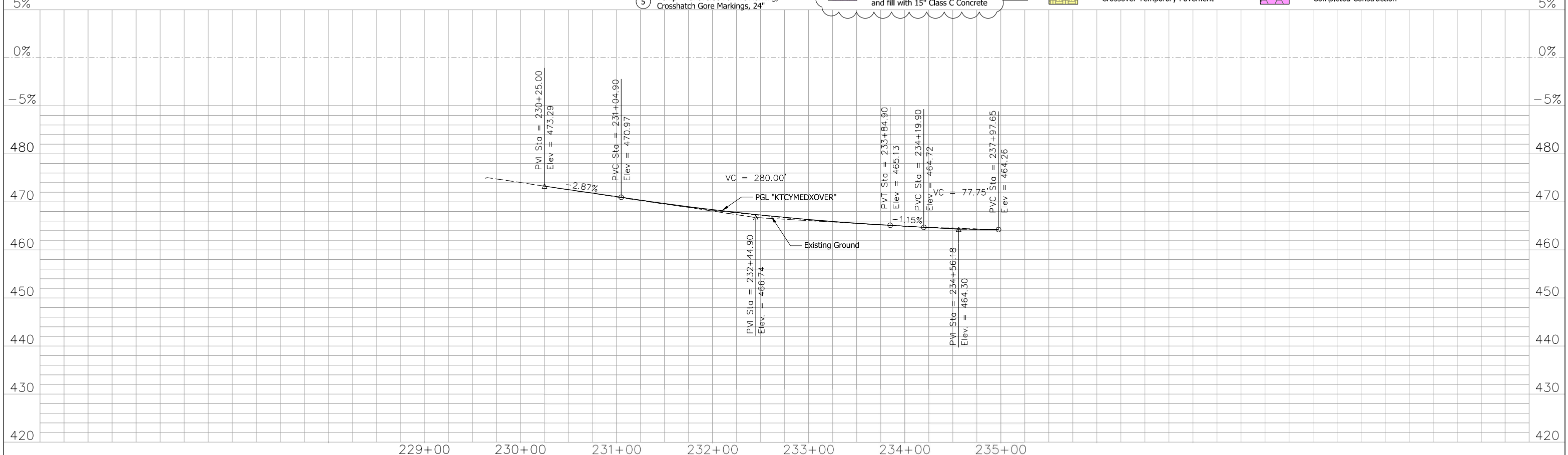
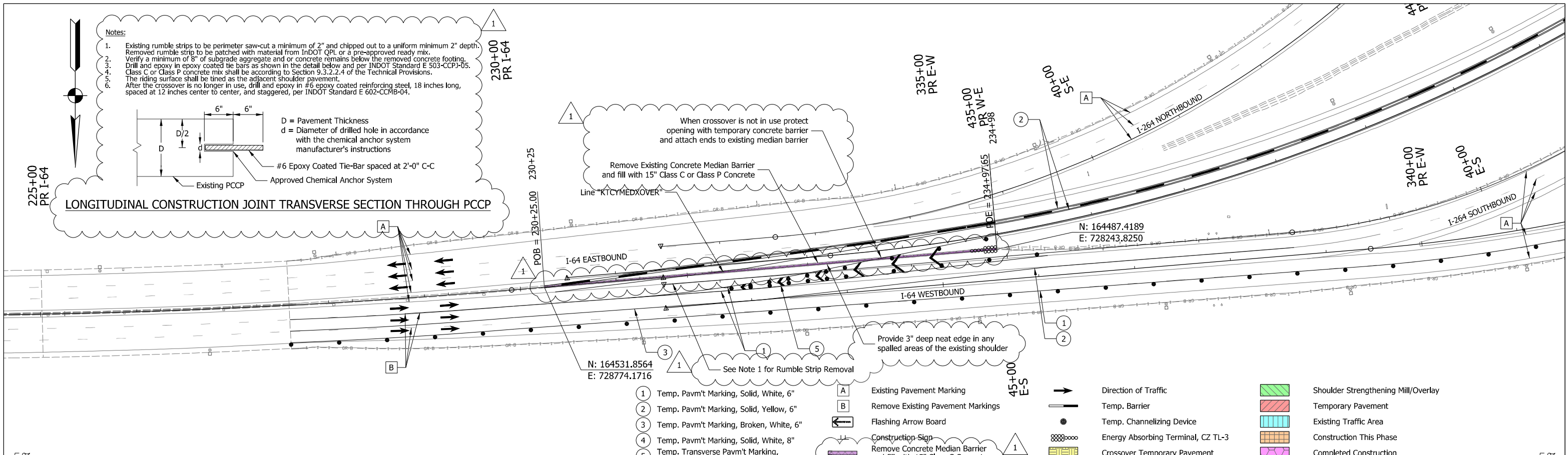
HORIZONTAL SCALE 1"=50'	BRIDGE FILE
VERTICAL SCALE 1"=10'	DESIGNATION 1702255
BUILDABLE UNIT 14	SHEETS 31 of 42
CONTRACT B-40719	PROJECT 1702255

Notes:

- Existing rumble strips to be perimeter saw-cut a minimum of 2" and chipped out to a uniform minimum 2" depth. Removed rumble strip to be patched with material from InDOT QPL or a pre-approved ready mix.
- Verify a minimum of 8" of subgrade aggregate and or concrete remains below the removed concrete footing.
- Drill and epoxy in epoxy coated tie bars as shown in the detail below and per INDOT Standard E 503-CCPJ-05. Class C or Class P concrete mix shall be according to Section 9.3.2.2.4 of the Technical Provisions.
- The riding surface shall be tined as the adjacent shoulder pavement.
- After the crossover is no longer in use, drill and epoxy in #6 epoxy coated reinforcing steel, 18 inches long, spaced at 12 inches center to center, and staggered, per INDOT Standard E 602-CCMB-04.



LONGITUDINAL CONSTRUCTION JOINT TRANSVERSE SECTION THROUGH PCCP



- 1 Temp. Pavm't Marking, Solid, White, 6"
- 2 Temp. Pavm't Marking, Solid, Yellow, 6"
- 3 Temp. Pavm't Marking, Broken, White, 6"
- 4 Temp. Pavm't Marking, Solid, White, 8"
- 5 Temp. Transverse Pavm't Marking, Crosshatch Gore Markings, 24"

- A Existing Pavement Marking
- B Remove Existing Pavement Markings
- Flashing Arrow Board
- Construction Sign
- Remove Concrete Median Barrier and fill with 15" Class C Concrete

- Direction of Traffic
- Temp. Barrier
- Temp. Channelizing Device
- Energy Absorbing Terminal, CZ TL-3
- Crossover Temporary Pavement

- Shoulder Strengthening Mill/Overlay
- Temporary Pavement
- Existing Traffic Area
- Construction This Phase
- Completed Construction

REV	DATE	DESCRIPTION
1	11/8/22	Concrete Median Barrier Removal

Page 175 of 186

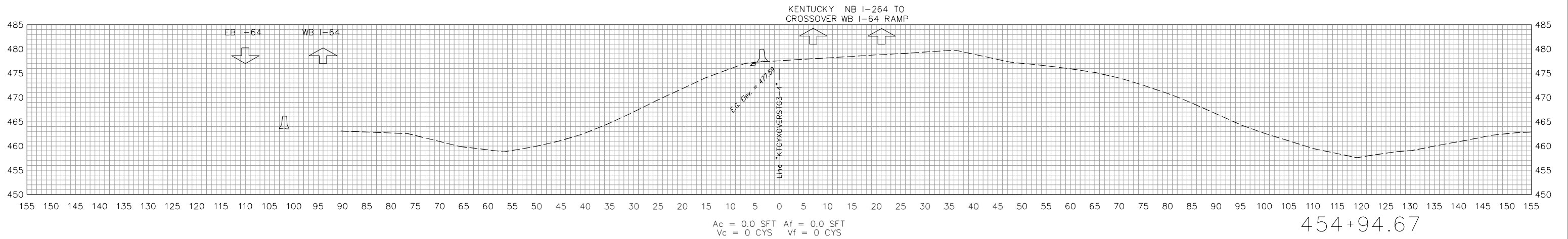
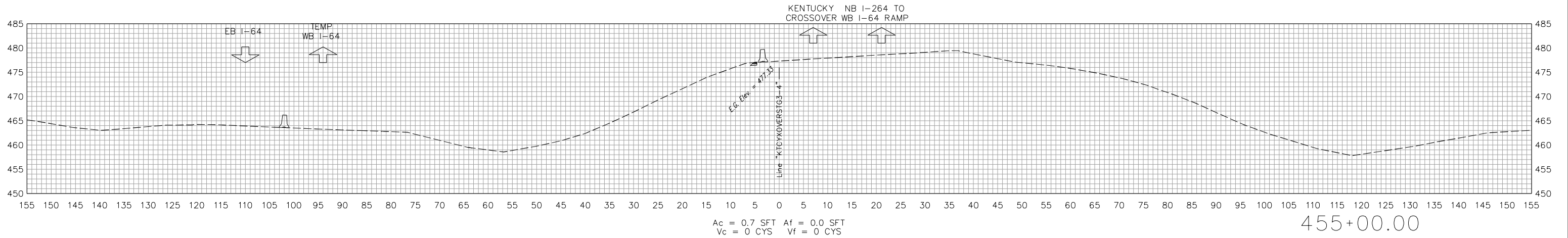
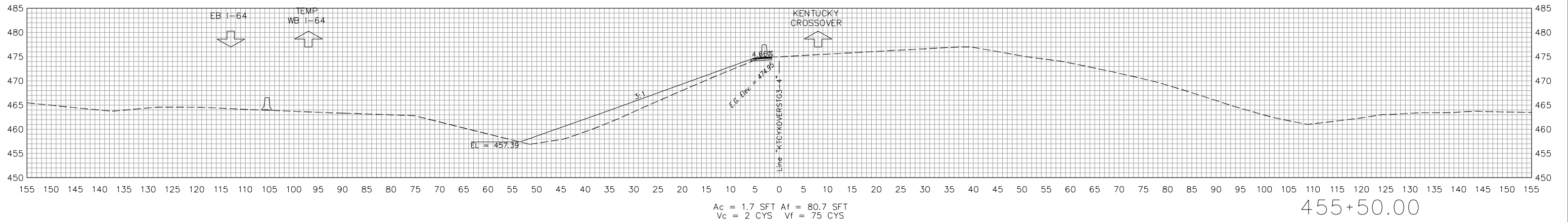


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DESIGNED: KLO	9/23/21	DRAWN: KLO 9/23/21
CHECKED: AWL	10/22/21	CHECKED: AWL 10/22/21

INDIANA DEPARTMENT OF TRANSPORTATION
 MAINTENANCE OF TRAFFIC
 KENTUCKY CROSSOVER I-64
 PLAN & PROFILE

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
1"=10'	1702255
BUILDABLE UNIT	SHEETS
14	31 of 42
CONTRACT	PROJECT
B-40719	1702255

P:\10-NOV-2022_13:12 PM



STA. 454+94.67 TO STA. 455+50.00

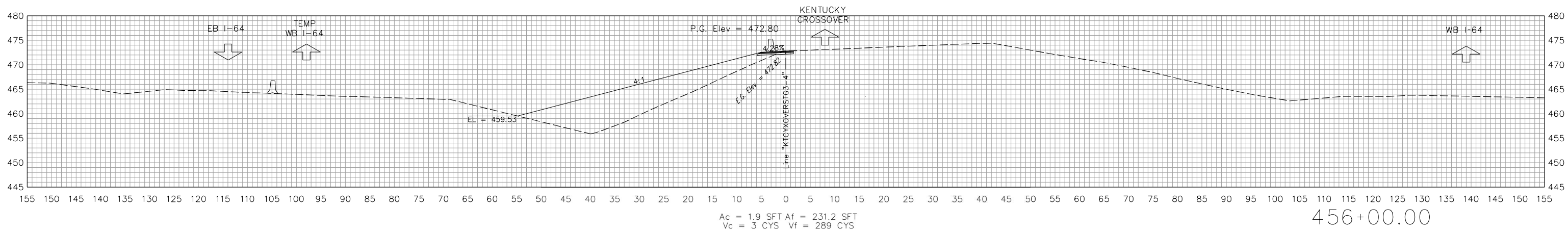
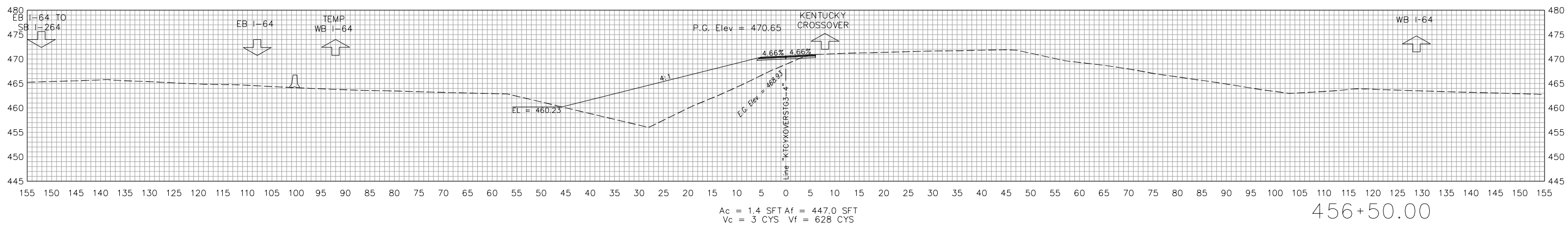
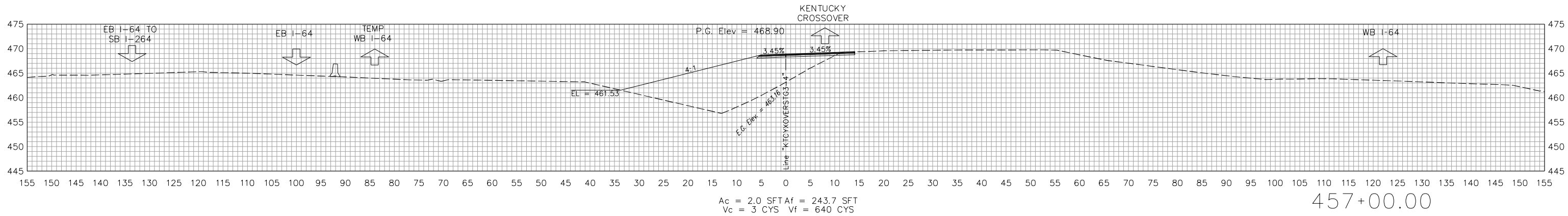


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
DESIGNED: KLO	10/15/21	DRAWN: KLO 10/15/21
CHECKED: AWL	10/21/21	CHECKED: AWL 10/21/21

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
KENTUCKY CROSSOVER - I-264

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
14	32 of 42
CONTRACT	PROJECT
B-40719	1702255



STA. 456+00.00 TO STA. 457+00.00



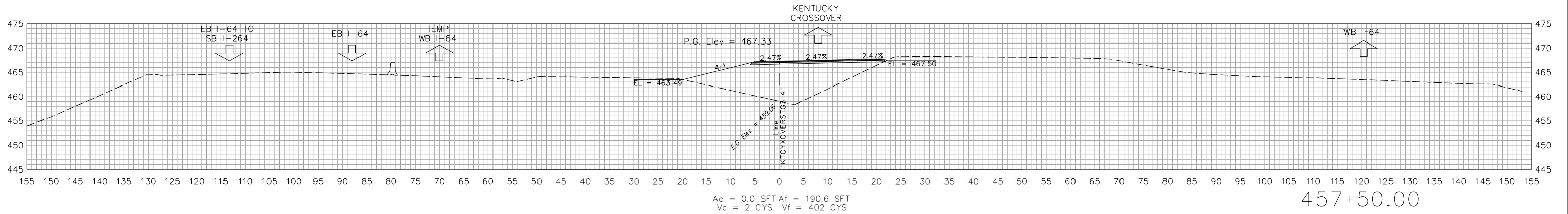
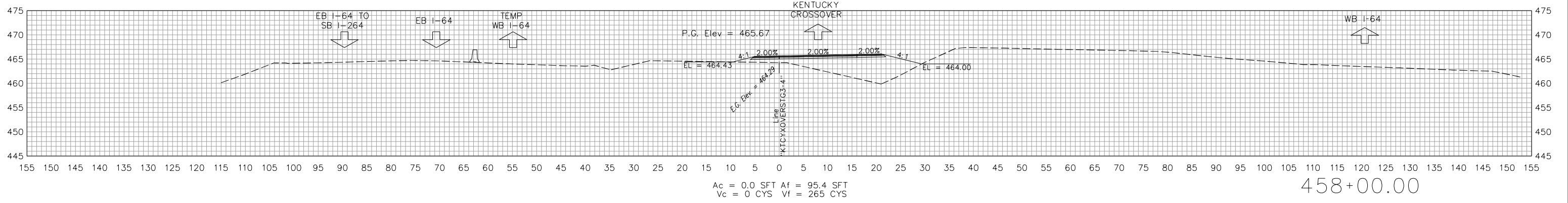
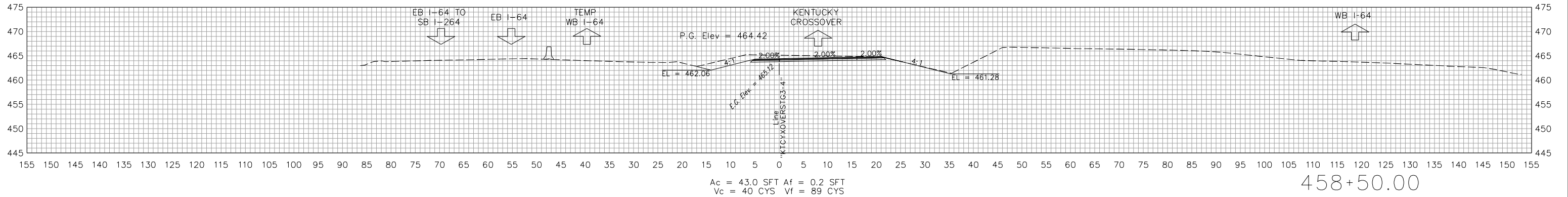
RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
DESIGNED: KLO	10/15/21	DRAWN: KLO 10/15/21
CHECKED: AWL	10/21/21	CHECKED: AWL 10/21/21

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
KENTUCKY CROSSOVER - I-264

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
14	33 of 42
CONTRACT	PROJECT
B-40719	1702255

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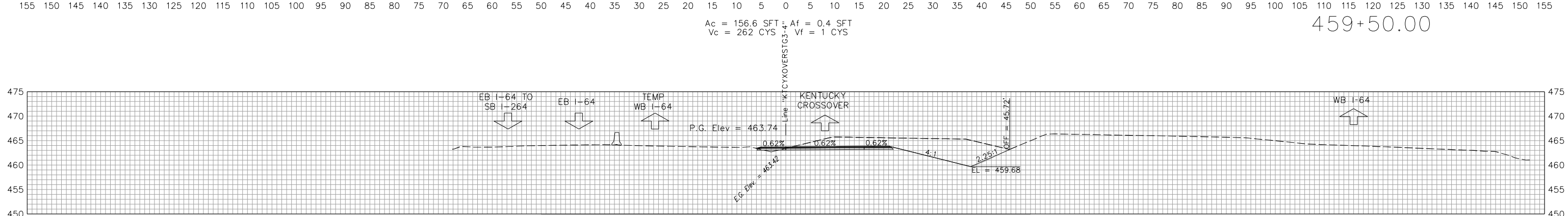
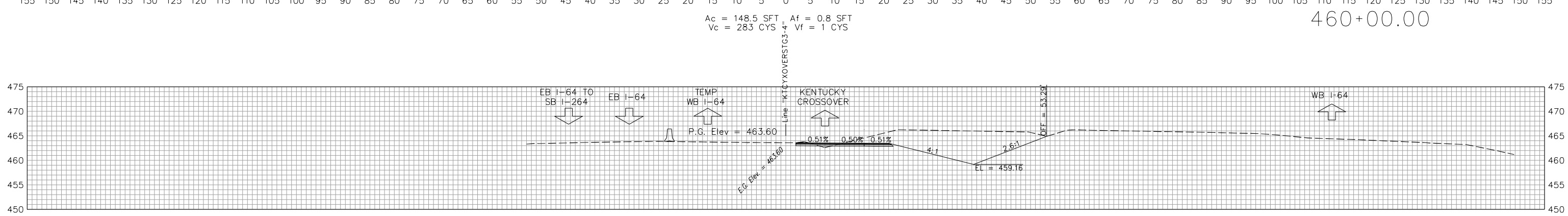
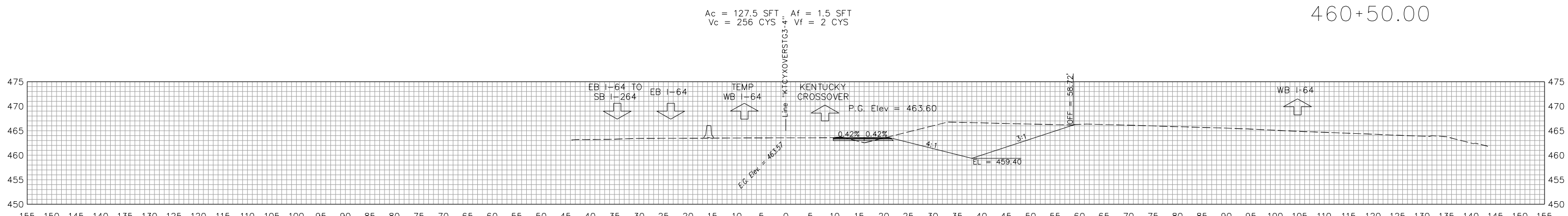
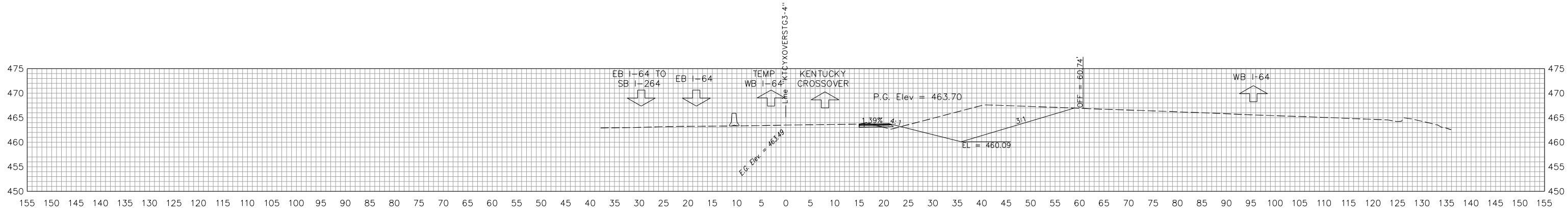


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
DESIGNED: KLO	10/15/21	DRAWN: KLO 10/15/21
CHECKED: AWL	10/21/21	CHECKED: AWL 10/21/21

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
KENTUCKY CROSSOVER - I-264

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
14	34 of 42
CONTRACT	PROJECT
B-40719	1702255

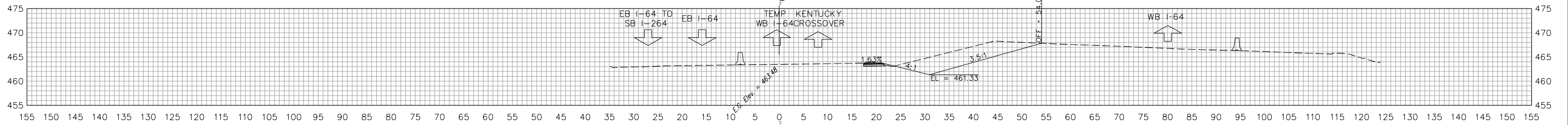


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
DESIGNED: KLO	10/15/21	DRAWN: KLO 10/15/21
CHECKED: AWL	10/21/21	CHECKED: AWL 10/21/21

INDIANA
DEPARTMENT OF TRANSPORTATION

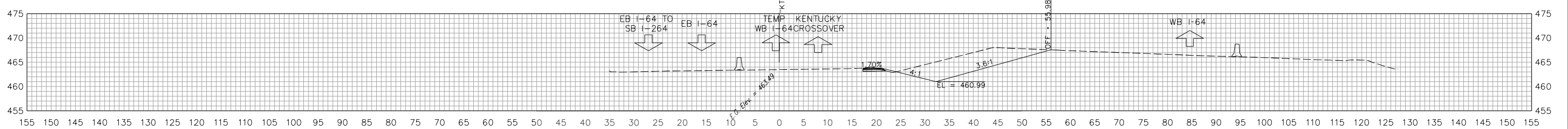
CROSS SECTIONS
KENTUCKY CROSSOVER - I-264

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
14	35 of 42
CONTRACT	PROJECT
B-40719	1702255



Ac = 80.0 SFT Af = 0.4 SFT
 Vc = 57 CYS Vf = 1 CYS

461+18.01



Ac = 91.5 SFT Af = 0.8 SFT
 Vc = 202.8 CYS Vf = 2 CYS

461+00.00



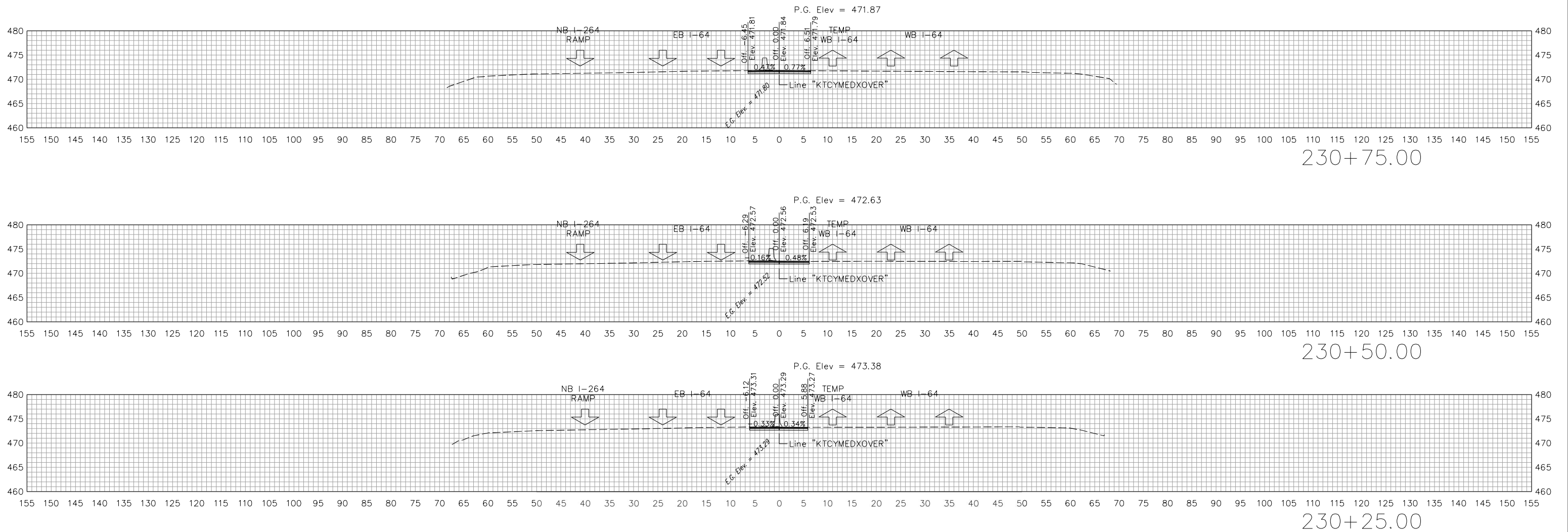
RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
DESIGNED: KLO	10/15/21	DRAWN: KLO 10/15/21
CHECKED: AWL	10/21/21	CHECKED: AWL 10/21/21

INDIANA DEPARTMENT OF TRANSPORTATION
 CROSS SECTIONS KENTUCKY CROSSOVER - I-264

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
14	36 of 42
CONTRACT	PROJECT
B-40719	1702255

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STA. 461+00.00 TO STA. 461+18.01

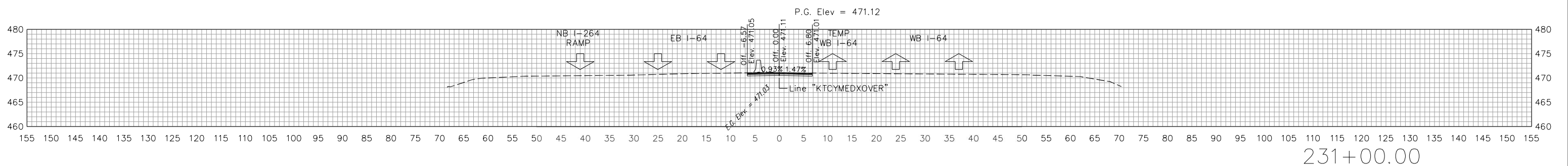
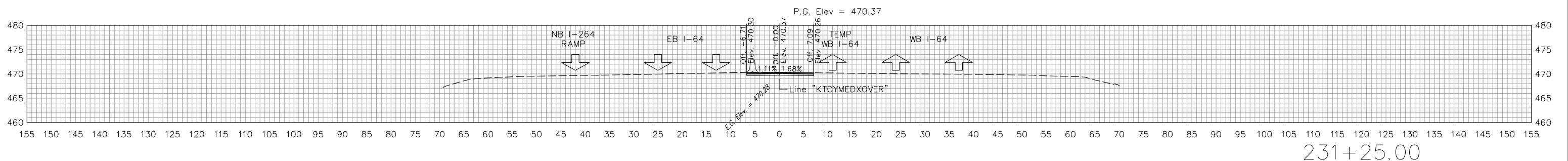
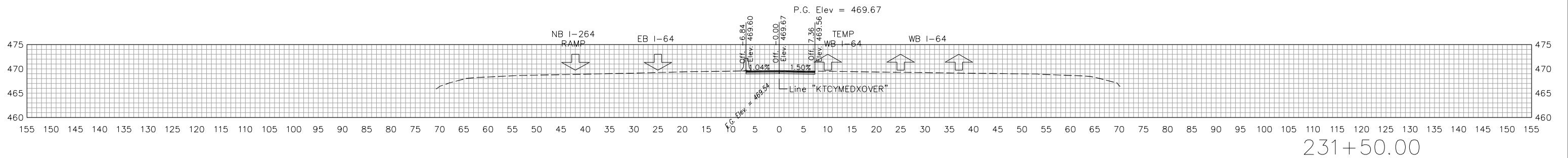


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
	DESIGN ENGINEER	DATE
DESIGNED: KLO	10/15/21	DRAWN: KLO 10/15/21
CHECKED: AWL	10/21/21	CHECKED: AWL 10/21/21

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
KENTUCKY CROSSOVER - I-64

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
14	37 of 42
CONTRACT	PROJECT
B-40719	1702255



RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
DESIGNED: KLO	10/15/21	DRAWN: KLO 10/15/21
CHECKED: AWL	10/21/21	CHECKED: AWL 10/21/21

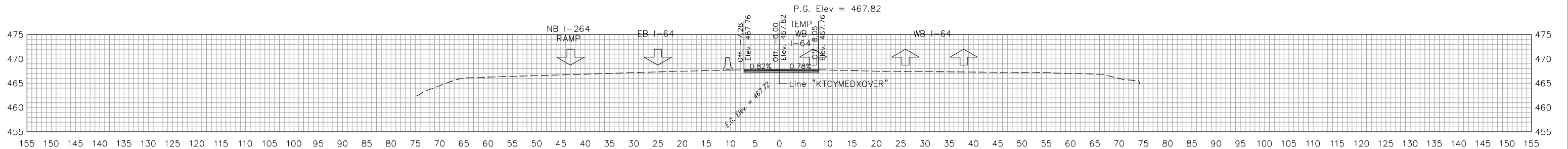
INDIANA DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
KENTUCKY CROSSOVER - I-64

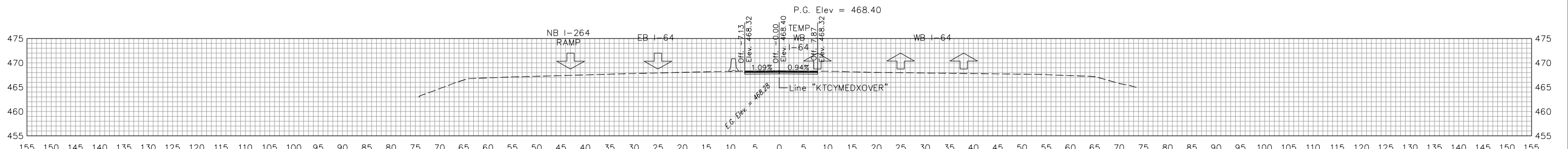
HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
14	38 of 42
CONTRACT	PROJECT
B-40719	1702255

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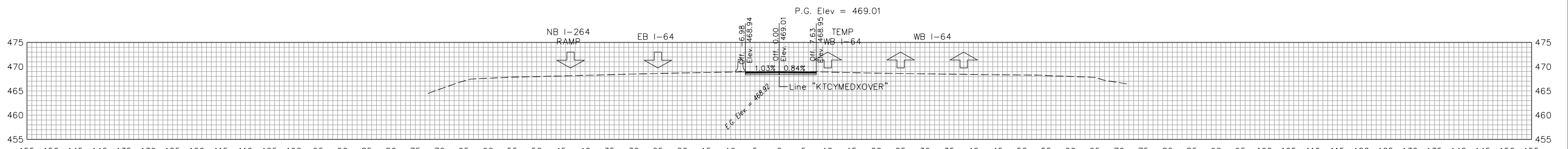
STA. 230+50.00 TO STA. 231+25.00



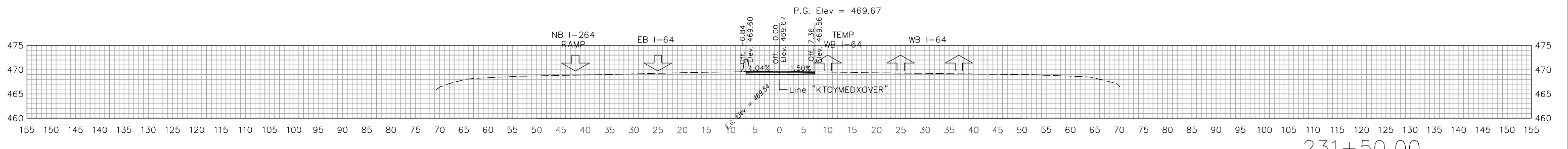
232+25.00



232+00.00



231+75.00



231+50.00

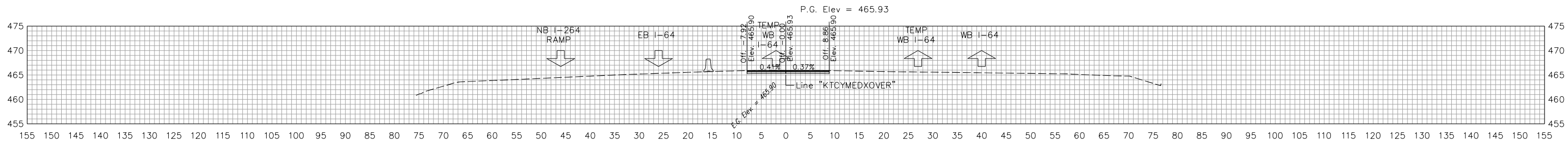


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
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CHECKED: AWL	10/21/21	CHECKED: AWL 10/21/21

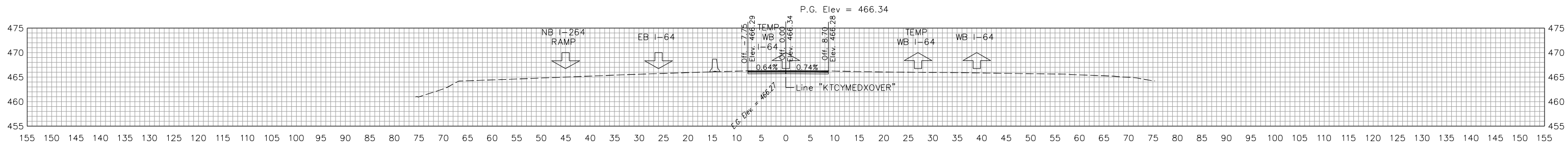
INDIANA DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS KENTUCKY CROSSOVER - I-64

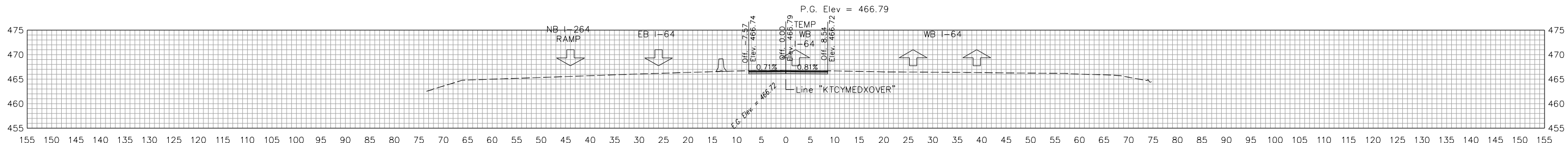
HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
14	39 of 42
CONTRACT	PROJECT
B-40719	1702255



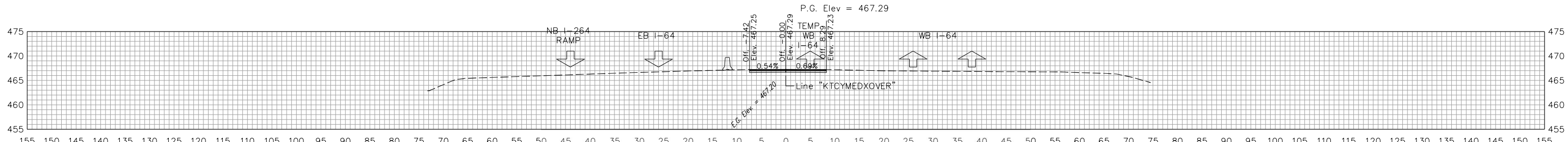
233+25.00



233+00.00



232+75.00



232+50.00

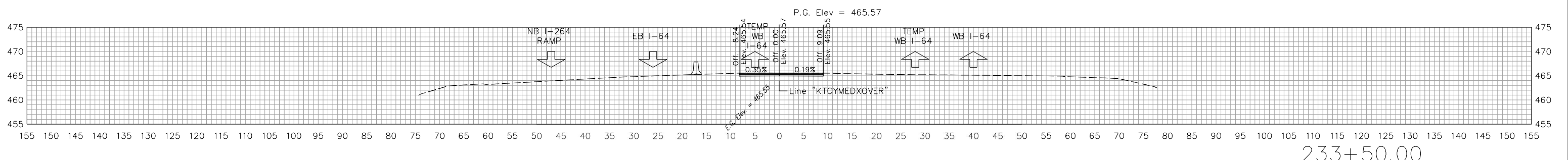
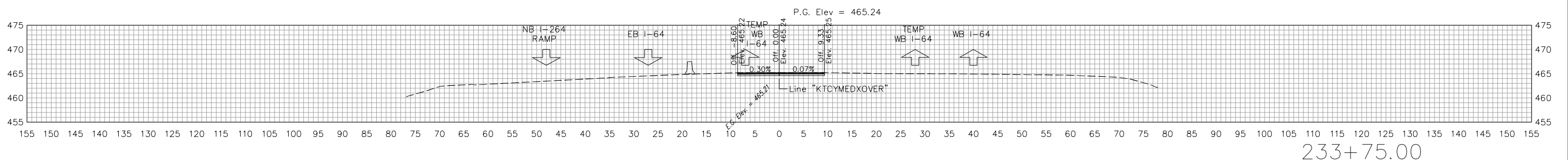
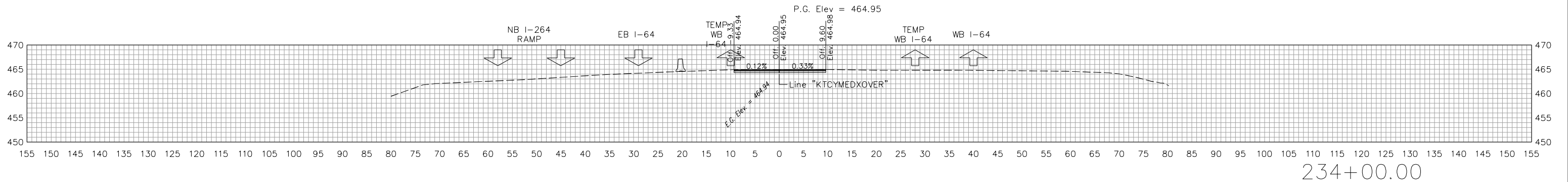
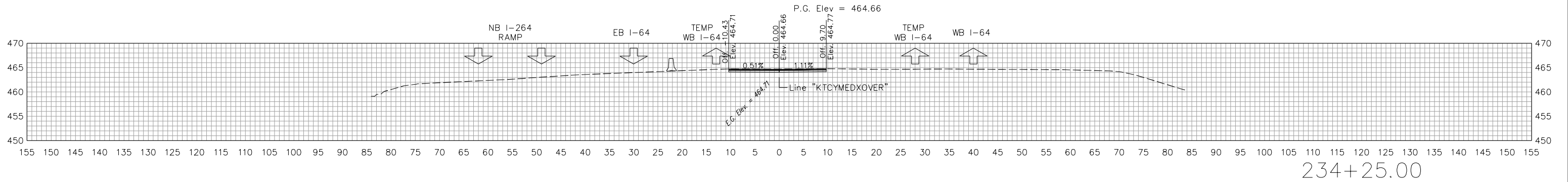
STA. 232+50.00 TO STA. 233+25.00



RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
DESIGNED:	KLO	10/15/21
CHECKED:	AWL	10/21/21
DRAWN:	KLO	10/15/21
CHECKED:	AWL	10/21/21

INDIANA DEPARTMENT OF TRANSPORTATION
CROSS SECTIONS
KENTUCKY CROSSOVER - I-64

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
14	40 of 42
CONTRACT	PROJECT
B-40719	1702255

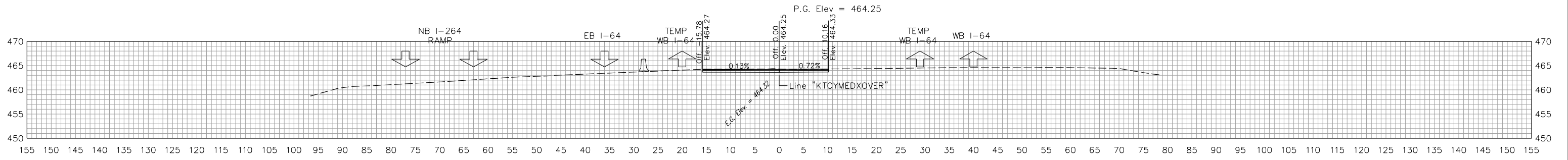


RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
DESIGNED: KLO	10/15/21	DRAWN: KLO 10/15/21
CHECKED: AWL	10/21/21	CHECKED: AWL 10/21/21

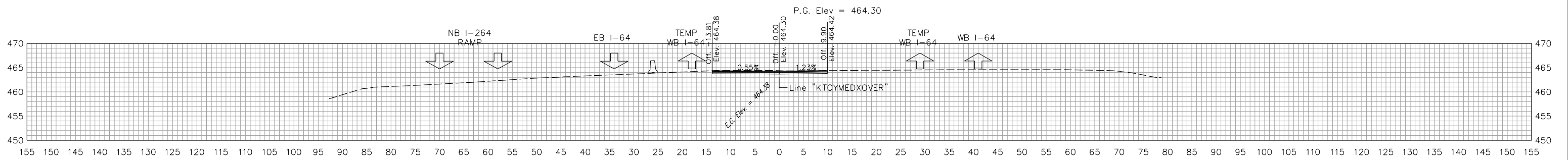
INDIANA DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS KENTUCKY CROSSOVER - I-64

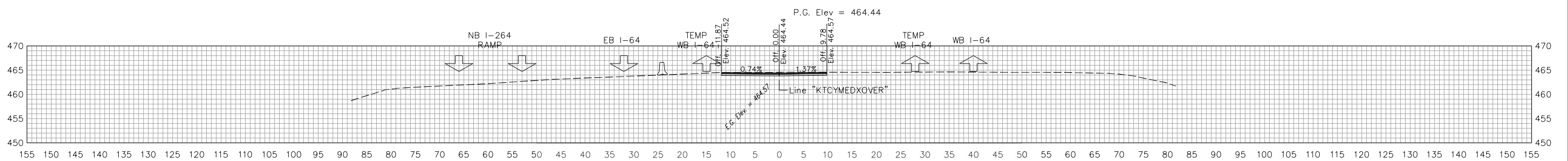
HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
14	41 of 42
CONTRACT	PROJECT
B-40719	1702255



234+97.40



234+75.00



234+50.00

STA. 234+50.00 TO STA. 234+97.40



RECOMMENDED FOR APPROVAL	<i>[Signature]</i>	4/15/2022
	DESIGN ENGINEER	DATE
DESIGNED: KLO	10/15/21	DRAWN: KLO 10/15/21
CHECKED: AWL	10/21/21	CHECKED: AWL 10/21/21

INDIANA
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
KENTUCKY CROSSOVER - I-64

HORIZONTAL SCALE	BRIDGE FILE
1" = 10'	NA
VERTICAL SCALE	DESIGNATION
1" = 10'	1702255
BUILDABLE UNIT	SHEETS
14	42 of 42
CONTRACT	PROJECT
B-40719	1702255